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Appendix D – Proposed Conditions

Appendix L: Moa Point Drainage and Sewage Treatment (Wellington City Council – 58): Details and Conditions

1. Introduction

Sewage Treatment

The discharge of untreated sewage effluent into the coastal waters off Moa Point is no longer acceptable for health or social considerations or for the safe operation of Wellington Airport.

In order to avoid contamination of the receiving waters, it is necessary to remove the solids from the sewage and to treat the effluent before it is discharged.

A wide range of treatment options and treatment plant site options have been explored. These include sites between Wry Valley in the east and the Karori Stream mouth to the west. The site finally chosen is adjacent to the airport and close to the Moa Point site which was previously abandoned in 1986 primarily because of the social concerns associated with discharge in this locality. In order to assuage those concerns, effluent is to be treated to a very high level.

Treated liquid effluent will be conveyed through an outfall pipe buried in the seabed, and will be discharged through diffusers sufficiently far off-shore to ensure that the established water classification is complied with.

At the time of designation 58 being confirmed, the Moa Point Wastewater Treatment Plant (WWTP) site was is neither sufficiently large nor conveniently located for the treatment and disposal of the solid waste to be removed from component of the sewage. This will Solid waste was therefore be pumped as a sludge through underground pipes to Carey's Gully dewatering facility, with. It will be treated, dewatered there and the inert waste then deposited in the adjacent sanitary landfill refuse disposal area, known as Southern Landfill.

The extension to the designation boundaries will now enable the site to accommodate a Sludge Minimisation Facility (SMF) which will treat solid waste to the extent that it is stabilised and substantially reduced in volume, and will be subject to a higher level of treatment than currently occurs at Carey's Gully. In turn, this will reduce the volume of solid waste which will need to be deposited to Southern Landfill. The SMF will deliver the resultant solid waste to landfill by vehicle and will therefore no longer require the pumping of sludge via underground pipes to Carey's Gully. The long-term aim is to find a beneficial re-use of the end sludge product to avoid disposal to landfill.

A wide range of treatment options and treatment plant site options have been explored for the SMF. These include sites at or near to the existing Moa Point WWTP or at or near to Carey's Gully sludge dewatering plant. Following a detailed multi-criteria assessment of a range of treatment process options across the two locations, a site was finally chosen adjacent to the existing Moa Point WWTP which will treat sludge through lysis digestion and thermal drying.

~~The following provisions are substantially the same as Plan Change 91/15 to the former District Plan, which has been considered and approved by the Planning Tribunal.~~

Details of the designation are shown on Plan 1 attached.

Drainage

~~The south Golf Course site is designated Drainage - Sewage Treatment and the following rules apply to the future development and management of the site~~ a separate set of conditions applies to the continued management and maintenance of the Moa Point Wastewater Treatment Plant (Moa Point WWTP) and the construction, operation, management and maintenance of the Sludge Minimisation Facility (SMF).

Application of conditions

For the avoidance of doubt:

- Conditions 4 to 18 apply only to the existing Moa Point WWTP.
- Conditions 19 to 32 apply only to the SMF.

2. Objectives, Conditions and Restrictions Relating to Drainage/Sewage Treatment Designation – Moa Point WWTP

The following objectives, conditions and restrictions are to apply to the construction and operation of the existing Moa Point WWTP ~~a sewage treatment plant~~ on the eastern area of the site designated for this purpose, which is located adjacent to the south eastern end of Wellington Airport.

2.1 Objectives

2.1.1 Primary Objective

To make planning provision for the establishment of a sewage treatment plant, through a designation to treat Wellington sewage to a level sufficient to ensure that it is no longer a source of pollution on the south coast.

2.1.2 Secondary Objectives

2.1.2.1 To recognise the importance of the site in Wellington especially its strategic location in relation to the existing sewerage system.

2.1.2.2 To avoid remedy or mitigate the social, psychological, ecological and physical impacts of locating a sewage treatment plant within an urban setting.

2.1.2.3 To ensure that in its location, form, character and operation, the plant is as inconspicuous as practicable and does not obtrude unnecessarily upon the community's consciousness.

2.1.2.4 To establish compatibility between the land uses on the site with minimum interference between the uses in the designated area and those in nearby residential areas, the Golf Courses and the Airport.

2.1.2.5 To ensure that there is no significant nuisance or inconvenience outside the site caused by the operation, particularly by its noise or odour.

2.1.2.6 To ensure that the land surrounding the plant is fully landscaped, to restore exposed ground and to reduce the visual impact of the development.

3. Moa Point WWTP Activities

3.1 Activities associated with the Moa Point WWTP ~~under the designation "Drainage – Sewage Treatment"~~ in the designated area shall be restricted to the following:

- sewage treatment plant and ancillary uses and amenities;
- laboratories ancillary to the treatment plant;
- administrative offices ancillary to the treatment plant;
- workshop and parts storage ancillary to the treatment plant;
- staff and visitors' amenities.

Moa Point WWTP Conditions

4. Height and Yards

- 4.1 Any building platform or platforms shall be constructed at a level between 20m and 35m above mean sea level. Structures shall in no case be located closer than 5m to the boundary of the designation.
- 4.2 Structures shall not exceed a height of 47m above mean sea level and shall be the same general height and location as shown on plans and elevations depicted in plans 2 and 3, except for the stack which shall not exceed a height of 51m above mean sea level. There may be limited variations to these plans and elevations to accommodate layout and design considerations, but only to the extent that the 'design and appearance' conditions are complied with and the buildings maintain the same degree of screening for the clarifiers for distant views from the suburban areas to the north and north-east.
- 4.3 In no case shall the airport height controls be exceeded. (Refer to District Plan Maps 36 and 37).
- 4.4 That a report from an air quality specialist be submitted to the Policy (District Plan) Section, Wellington City Council, within twelve months of the erection of the stack extension. That report shall review the effectiveness of the extension in reducing odour discernible beyond the boundaries of the plant site. Should the report indicate that the stack extension (as proposed or similar) has had no effect in reducing odour impact, the extension shall be removed and Condition 4.2 amended to reduce the height limit relating to the stack to 48m above mean sea level.

5. Noise

- 5.1 Before any development starts, Council will be required to obtain a certificate from an independent acoustical consultant approved by the Medical Officer of Health for the Wellington District, stating that in the consultant's opinion, the noise emission from the site after the establishment of the proposed use would not exceed the noise levels specified below.

Noise levels, with corrections applied, measured on any land zoned for residential purposes:

| | 7am-6pm | 6pm-10pm | 10pm-7am |
|------|----------------|-----------------|-----------------|
| L95 | 45dBA | 40dBA | 35dBA |
| L10 | 55dBA | 50dBA | 45dBA |
| Lmax | | | 65dBA |

When monitoring noise levels, the methods prescribed in NZ Standard 6801:1991 shall be used.

- 5.1.1 Provisions concerning the required noise level standards shall not apply to the following:

- (a) Noise of safety signals, warning devices and emergency pressure relief valves.
- (b) Between 7am and 8pm, for:
- noise from the engines of vehicles visiting the site for any purpose, but not including the repair, maintenance or servicing of those vehicles on the site; and
- the construction or alteration of buildings that have been approved by Council,

5.1.2 the following corrections shall be applied:

| Type of Operation or Character of Noise | Correction for dBA Limits |
|--|---------------------------|
| Where noise source operates for less than 20% of a one hour period | +5 |
| Where noise source operates for less than 5% of a one hour period | +10 |
| Where noise is impulsive in character (hammering, etc) | -5 |
| Where noise is of periodic character (hum, screech, etc) | -5 |

The treatment plant shall be required to operate within these levels.

6. Air Pollution

There shall be no discernible odour resulting from the operation of the waste water treatment plant, at or beyond the boundary of the plant site as designated in the District Plan.

There shall be no discharge into the air of any contaminant from the waste water treatment plant which has adverse effects at or beyond the site boundary.

Note - not forming part of the Plan: Notwithstanding the above standards, Council reserves the right to use its powers under the relevant statutory powers to protect the amenities of the district.

7. Clarifiers

The clarifiers are to be disguised by an open structure that will effectively conceal their function as water-retaining structures. The covers will be designed to suppress wave formation and the generation of aerosols.

8. Dust

- 8.1 All dust on the site shall be controlled to avoid remedy or mitigate any escape to neighbouring properties.

- 8.2 During the course of construction, all exposed or excavated surfaces shall be maintained in a condition to avoid remedy or mitigate dust nuisance. See under additional conditions and restrictions to apply during the construction phase of the treatment plant (Section 18).

9. Landscape design

- 9.1 A landscape plan shall be prepared with the following objectives:
- 9.1.1 To establish substantial vegetation cover as quickly as is practicable over the site surrounding the treatment plant for the purpose of:
- ameliorating excavations, cut faces, embankments, or bunds; and
 - reducing the visual impact of the treatment plant, particularly as viewed from residential properties to the east.
- 9.1.2 To provide vegetative cover to any disturbed surface as soon as possible.
- 9.1.3 Keeping to a minimum any excavation into the toes of the surrounding hill slopes to create the site platform and ensuring that the earthworks shall be implemented to conform as closely as possible to the existing "natural" slopes.
- 9.1.4 Where the bases of the cut faces need to be vertical, to support these by crib walls with decorative natural stone panels.
- 9.1.5 To implement the revegetation programme in stages to follow the stages of the earthworks programme. Planting work is to begin as soon as a surface is completed to final grade.
- 9.1.6 Where the slopes are to be 1:1 gradient, to carry out the revegetation work on 2m x 2m benches.
- 9.1.7 To choose the species for revegetation predominantly from plant material already occurring on the site and, preferably, grown from seed sourced from the site.
- 9.1.8 To spread back over the surfaces to be planted any topsoil stripped from the site during construction. This soil will provide a natural seed source.
- 9.1.9 To install a temporary irrigation scheme to ensure plant establishment, which must be phased out as soon as advised by the horticultural expert.
- 9.1.10 To source rock fill for gabion baskets from the site to ensure colour and texture conformity. Landscape development work on the site platform itself will be required to soften the structures and reduce their visual scale.
- 9.2 To ensure that the landscape plan includes provision for the planting of appropriate native species, the stabilisation of slopes and the installation of irrigation systems where necessary to facilitate plant growth.
- 9.3 To implement progressively the landscaping of the site until such time as the objectives of the plan are attained.

- 9.4 To incorporate within the site's landscaping and planting provisions the existing reserve or open space land between the site and Kekerenga Street. Planting in this area should cover an area sufficient to screen views of the treatment plant from properties in Kekerenga Street.

10. The Northern Bund

The bund to the north of the plant may be increased in height with the consent of the Miramar Golf Club and Wellington International Airport Limited, but should not be reduced below the level indicated in plan 3 attached.

11. Design and Appearance of Buildings

- 11.1 Without limiting the detailed architectural approach or design of a treatment plant on the site, the plant shall be developed to give the appearance of separate but closely integrated buildings or facilities generally, as depicted in plans 2 and 3 attached.
- 11.2 When viewed from the air or from a distance, the treatment plant shall present the appearance of being a largely industrial complex which has been architecturally designed to harmonise with and enhance the landscape. As discussed under Section 4 Height and Yards, the design shall not be modified so as to lessen its screening aspects.

12. Lighting

- 12.1 Lighting of the treatment plant shall be to a level which is sufficient for security and operations and designed to prevent the intrusion of direct light into residential properties.

13. Fencing

- 13.1 In addition to the security measures to safeguard unauthorised entry to the treatment plant, appropriate safety fencing shall be installed near the top of all steep slopes or cut faces to the east and south of the site to prevent access by children.

14. Flooding

The plant equipment, buildings and contouring of the site are to be designed in such a way as to ensure that any overflows, including the accidental or emergency breaches of any tanks, are directed away from the Airport. In particular, the stormwater drainage system is to be designed for a 20 year return period and the layout of the plant is to incorporate such bunding and curbing as is necessary to direct stormwater from a 50 year return period direct to Moa Point Road away from the Airport.

15. Plant Breakdown

- 15.1 Council is to ensure that in the event of any breakdown, steps are immediately taken to:
- 15.1.1 avoid any risk of bird strike;
 - 15.1.2 notify the Airport control tower of the breakdown and its expected duration;
 - 15.1.3 notify Airport management; and
 - 15.1.4 notify the Wellington Regional Council (or the relevant consent authority under the Resource Management Act).

16. Community Consultation and Mitigation

- 16.1 Council shall in consultation with Mr and Mrs Cornish, the Strathmore Park Progressive and Beautifying Association Inc by its Sewage Action Committee, and Mrs Hoang, establish a Community Liaison Committee which represents the interests of those persons affected by the Treatment Plant.
- 16.2 The role of the Committee will be to work with Council staff and consultants to establish ways to avoid remedy or mitigate any adverse effects of the plant (including its construction) on the adjacent communities. Matters upon which the Committee would be expected to provide input include such matters as the quality and location of fencing, lighting, landscaping, and the provision of facilities and beautification schemes to counteract any possible effects of the treatment plant on property values and the use and enjoyment of the area. The Committee will also be expected to assist Council in monitoring the effects of the construction period.
- 16.3 Council will employ a facilitator to service this Committee and to ensure good communication and coordination between the Committee and Council and between the Committee and other affected residents in adjacent communities.
- 16.4 A terms of reference, setting out the rights and responsibilities of the Committee and its relationship to Council and its officers, will be drawn up in consultation with the Committee.
- 16.5 Council will nominate one person within the Cityworks Division who will be responsible for ensuring that concerns raised by the Committee and others affected by the project are addressed promptly.

17. Land Acquisition

17.1 Sites with a View

If requested by an owner who was an owner at 19 June 1991, Council shall purchase any of the privately owned residences in Kekerenga Street with a view of the treatment plant or part of that plant either from the house or from the section surrounding the residence.

17.2 Adverse Environment Effects

If the plant malfunctions to the extent that it creates any adverse effects on the environmental conditions in the vicinity of Kekerenga Street within a 400m radius from the centre of the site, Council if requested by the owners of houses in that area must purchase those houses so affected.

17.3 Assessment of Purchase Price

The purchase price shall be based on market value and assessed as if the designation for the treatment plant upon this site was not in place. The existence, prior to this provision becoming operative, of the drainage designation closer to Lavender Bay shall be a relevant factor in assessing a value. Any disagreement or dispute as to market value shall be referred to arbitration under the Arbitration Act 1908 or any amendments or successors.

18. Additional Conditions and Restrictions to Apply During the Construction Phase of the Treatment Plant

18.1 Access

All access to the site shall be via Moa Point Road. There shall be no access to the site from the north using the airport perimeter road.

18.2 Flight Paths

No structure, building, machinery, or construction equipment, shall intrude into the approach fan for Runway 34.

18.3 Cover of Exposed Surfaces

Council will institute controls during the course of construction, specifically designed to minimise the attraction of the birds to the site. In particular, it will stage works to ensure a minimum exposure of material at any one time and will ensure that there is overnight cover of all exposed surfaces. In the event that any birds are observed using the site for roosting, Council will take immediate steps to remedy the situation. Specifically Council shall ensure:

- 18.3.1 all exposed and unsealed surfaces are to be kept moist to suppress the generation of dust;

- 18.3.2 at the completion of each working day, any exposed material or surface which is likely to attract birds is to be covered over until work recommences;
- 18.3.3 truckloads of excavated material are to be covered to prevent dust nuisance;
- 18.3.4 stockpiles are to be kept moist to suppress the generation of dust; and
the contractors are required to secure the site.

18.4 Blasting

- 18.4.1 Any blasting on the site necessary for construction purposes is to be carried out only between the hours of 9.00am and 4.00pm Monday to Friday.
- 18.4.2 In all cases, nearby residents in the immediate vicinity (in particular Kekerenga Street and Moa Point Road), and Airport management and Air Traffic Control are to be notified by letter drop at least 24 hours in advance of the blasting.
- 18.4.3 Blasting shall be immediately preceded by a siren or hooter (with a sound which distinguishes it from normal Police, Ambulance or Fire Service sirens).

18.5 Arc-Welding

At all times, appropriate measures shall be taken to ensure that any arc-welding will not be hazardous to air traffic at Wellington International Airport.

18.6 Machinery Maintenance

At all times, diesel-powered machinery (including trucks) shall be maintained to avoid remedy or mitigate minimise excessive smoke and/or diesel odour emissions.

18.7 Hours of Operation

Construction on the site shall be restricted to the following hours:

| | |
|------------------|------------------|
| Monday to Friday | 7.00am to 6.00pm |
| Saturday | 9.00am to 3.30pm |
| Sundays | no work |

The designation covers the area proposed for a sewage treatment plant for Wellington. In addition, conditions and restrictions are specified to ensure accountability in establishment, development and management of the facility.

The site is of particular strategic importance, being closely located to the existing sewerage reticulation system. It also offers available land of suitable size and reasonable contour.

The site is generally well isolated from nearby recreational, residential and airport activities, by distance, difference in elevation or landform. The plant will not be readily discernible by the general public. Although it is close to several residences in Kekerenga Street, the existing reserve provides a buffer zone. Controls on noise and odour will minimise potential nuisance and landscaping requirements will reduce visual impacts.

~~*The site is not sufficiently large to accommodate the sludge treatment process. Sludge in the form of a liquid slurry will be pumped through underground pipes to a special treatment plant in Carey's Gully near where Council's solid waste disposal landfill is located. There the sludge will be treated, dewatered and any inert waste will be disposed of in the sanitary landfill area refuse disposal area subject to any necessary consents or permissions.*~~

SMF Conditions**DEFINITIONS, ABBREVIATIONS, ACROMYNS AND TERMS**

| TERM | DEFINITION |
|--------------------------|---|
| AEE | Assessment of Environmental Effects for the Wellington Sludge Minimisation Facility Project |
| CMO | Wellington City Council's Compliance Monitoring Officer |
| Commissioning Phase | The final phase of construction involving a staged approach whereby individual systems are tested and then integrated with other systems prior to being made operational. |
| COMP | Commissioning Odour Management Plan |
| CNVMP | Construction Noise and Vibration Management Plan |
| CTMP | Construction Traffic Management Plan |
| ESCP | Erosion and Sediment Control Plan |
| GWRC | Greater Wellington Regional Council, including any officer of Greater Wellington Regional Council |
| LRV | Light reflectance value which applies to colours applied to products (paint, powdercoating) |
| Moa Point WWTP | Moa Point Wastewater Treatment Plant and includes the activities listed at 3.1 of this Designation. |
| MPTMP | Maintenance Period Traffic Management Plan |
| OOMP | Operational Odour Management Plan |
| OMP | Operation and Maintenance Plan |
| OTMP | Operational Traffic Management Plan |
| Outline Plan | An Outline Plan of Works prepared in accordance with section 176A of the Resource Management Act 1991 |
| Requiring Authority | Wellington City Council |
| Sensitive Activity | Activities considered sensitive for the purposes of this designation include any residential activity, any early childhood education centre, and any hotel or other accommodation activity. |
| SMF | Sludge Minimisation Facility for the purpose of stabilising and reducing the volume of processed sludge produced by the Moa Point WWTP and Western (Karori) WWTP |
| SMF Early Enabling Works | These are works which can be authorised either through the existing designation without the requirement for an Outline Plan, or as permitted activities which do not require a resource consent. The early enabling works to be undertaken in association with the SMF works, include: <ul style="list-style-type: none"> • Below ground services relocation or upgrades • Demolition of on-site buildings • Slope stabilisation works |
| SMF Works | The design, construction, maintenance and operation of the SMF as referenced in the AEE and the related SMF designation conditions. SMF Works excludes early enabling works, as per the 'SMF Early Enabling Works' definition, which can be undertaken without the submission of an Outline Plan. |
| SMF Construction Works | All aspects of the SMF Works which relate to construction activities. |
| Summer Break | 20 December to 31 January of any calendar year |
| WIAL | Wellington International Airport Limited |
| Working Days | Has the same meaning under section 2 of the Resource Management Act 1991 |
| WWTP Access Road | Access road serving Moa Point WWTP, off Stewart Duff Drive |

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|------------------------------|--|
| Reference | Conditions |
| 19.General | |
| 19.1 | <p>Without limiting the layout and design of the SMF, the SMF Works shall be undertaken in general accordance with:</p> <ul style="list-style-type: none"> • 'Description of the proposal' in the AEE Report, dated August 2022 • General Arrangement Plan – ref. 3258521-DA-000-K0121 Rev A • Street Elevation – ref. 3258521-DA-000-K1202 Rev A <p>Where there may be inconsistencies between information within the documents listed above and conditions of the designation, these conditions shall prevail.</p> |
| 19.2 | <p>An Outline Plan/s shall be submitted to the Territorial Authority in accordance with section 176A of the RMA, unless a waiver for this requirement is provided in writing by WCC.</p> <p>As well as the matters set out in section 176A(3), the outline plan(s) shall include the following items in accordance with conditions on this designation:</p> <ul style="list-style-type: none"> • Design Statement • Construction Noise and Vibration Management Plan • Construction Traffic Management Plan • Operational Traffic Management Plan |
| 19.3 | <p>As soon as reasonably practicable following the completion of construction of the Project, the Requiring Authority shall give notice in accordance with section 182 of the RMA, seeking the revision of the designation boundary to that shown in Plan 3258521-DA-K0112 Rev A.</p> |
| 19.4 | <p>The Requiring Authority shall arrange and conduct a pre-construction site meeting with the contractor prior to any work authorised by this designation commencing on site and invite, with a minimum of 10 days' notice, Wellington City Council's Compliance Monitoring Officer (CMO) and any other key WCC representatives determined by the CMO.</p> <p><i>Note: In the case that any of the invited parties, other than the representative of the Requiring Authority and the contractor, do not attend this meeting, the Requiring Authority will have complied with this condition, provided the invitation requirement is met.</i></p> |
| 19.5 | <p>The Requiring Authority may request amendments to any of the management plans required by these conditions by submitting the amendments in writing to the CMO at least 10 Working Days prior to any changes taking effect. Any changes to management plans shall remain consistent with the overall intent of the management plan and relevant conditions and achieve the outcomes required by these conditions. The changes sought shall not be implemented until the Requiring Authority has received the CMO's written approval for the amendments.</p> |
| 20.Layout and Heights | |
| 20.1 | <p>Each structure is depicted on the general arrangement plan attached to this designation (plan 3258521-DA-000-K0121 Rev A). Variations to the siting and footprints of these buildings/structures may occur to accommodate layout and design evolution, but only to the extent that all other SMF designation conditions are complied with.</p> <p>All key structures shall not exceed the following heights, based on NZ Vertical Datum 2016, as set out in the Table below:</p> |

| Proposed Conditions | | | | | | | | | |
|------------------------------------|---|------------------|----------------------------------|-----------------------------------|-------|-----------------------------------|-------|-----------|-------|
| Reference | Conditions | | | | | | | | |
| | <table border="1"> <thead> <tr> <th>Structure</th> <th>Maximum Height (NZVD2016)</th> </tr> </thead> <tbody> <tr> <td>Main Sludge Processing Building 1</td> <td>35.5m</td> </tr> <tr> <td>Main Sludge Processing Building 2</td> <td>32.0m</td> </tr> <tr> <td>Digesters</td> <td>30.0m</td> </tr> </tbody> </table> | Structure | Maximum Height (NZVD2016) | Main Sludge Processing Building 1 | 35.5m | Main Sludge Processing Building 2 | 32.0m | Digesters | 30.0m |
| Structure | Maximum Height (NZVD2016) | | | | | | | | |
| Main Sludge Processing Building 1 | 35.5m | | | | | | | | |
| Main Sludge Processing Building 2 | 32.0m | | | | | | | | |
| Digesters | 30.0m | | | | | | | | |
| 21. Design and Appearance | | | | | | | | | |
| 21.1 | <p>The Requiring Authority shall prepare a Design Statement to demonstrate how the development is consistent with the following requirements for the purpose of mitigating landscape and visual effects:</p> <ul style="list-style-type: none"> - All colours applied to publicly visible parts of the SMF shall have a light reflectance value (LRV) no greater than 42%. - All colours applied to publicly visible parts of the SMF shall be selected from the grey, green and brown hues of the colour palette. - Details of the concrete compound, surface finish or mixture to be used externally on the buildings demonstrating that it achieves an equivalent light reflective value (LRV) of 42% maximum. - The SMF buildings shall not be clad in materials including stainless steel or unpainted galvanised steel, copper or polycarbonate translucent cladding. - Any glazed areas shall use low-reflectivity glass. - Any external lighting shall be shielded to prevent light spill. Any external lighting shall be for wayfinding and security reasons only. - The SMF buildings shall not include any sign-writing or advertising. Any signage shall be low level, for way-finding purposes only, for the benefit of site users. - Details of fences, gates and walls. - Details of the wire mattress system for stabilisation of cut slopes and the exact locations for any proposed shotcrete. <p>In preparing the Design Statement, the Requiring Authority shall consult with the Council and a final draft shall be submitted to the Council for comment. In finalising the Design Statement, the Requiring Authority shall take into account any feedback received from the Council.</p> <p><i>Note – To achieve an equivalent LRV of 42%, the concrete will need to be coated or include an oxide in the mix, or both. Any texture applied to the concrete will also assist in reducing the material's reflectivity. The concrete's reflectivity will need to be approximately midway between Gull Grey (50% LRV) and Sandstone Grey (27% LRV).</i></p> | | | | | | | | |
| 22. Flooding | | | | | | | | | |
| 22.1 | <p>The plant equipment, buildings and contouring of the site are to be designed in such a way as to ensure that any overflows, including the accidental or emergency breaches of any tanks, are directed away from the Airport. In particular, the stormwater (reticulated network) drainage system is to be designed for a 20 year return period and the layout of the plant is to incorporate such bunding and curbing as is necessary to direct stormwater from a 100 year return period (with climate change) direct to Moa Point Road away from the Airport.</p> | | | | | | | | |
| 23. Construction Work Hours | | | | | | | | | |
| 23.1 | <p>Normal working hours for onsite construction activities will be as follows:</p> <ol style="list-style-type: none"> a) 7:00am to 6.00pm Monday to Saturday (excluding public holidays). b) All heavy construction vehicle access to the site shall be via Moa Point Road. There shall be no access to the site from the north using the airport perimeter road for such vehicles. | | | | | | | | |

| Proposed Conditions | |
|---|--|
| Reference | Conditions |
| | c) Any construction work which cannot be reasonably undertaken during normal working hours shall be identified in the Construction Traffic Management Plan and/or the Construction Noise and Vibration Plan required by Condition 25. |
| 24.General Construction Restrictions | |
| 24.1 | No structure, building, machinery, or construction equipment, shall intrude into the approach fan for Runway 34, unless undertaken in airport curfew hours of midnight to 6:00am and with appropriate permission from WIAL. |
| 24.2 | Controls must be instituted during the course of construction, specifically designed to minimise the attraction of the birds to the site. Works shall be staged to ensure a minimum exposure of material at any one time and will ensure that there is overnight cover of all exposed surfaces. In the event that any birds are observed using the site for roosting, immediate steps must be taken to remedy the situation. The measures adopted must ensure: <ul style="list-style-type: none"> • All exposed and unsealed surfaces to be kept moist to suppress the generation of dust. • At the completion of each working day, any exposed material or surface which is likely to attract birds is to be covered over until work recommences. • Truckloads of excavated material are to be covered to prevent dust nuisance. • Stockpiles are to be kept moist to suppress the generation of dust; and |
| 24.3 | At all times, appropriate measures shall be taken to ensure that any arc- welding will not be hazardous to air traffic at Wellington International Airport. |
| 24.4 | At all times, diesel-powered machinery (including trucks) shall be maintained to avoid, remedy or mitigate minimise excessive smoke and/or diesel odour emissions. |
| 25.Construction Management Plans | |
| 25.1 | The Requiring Authority shall submit a Construction Traffic Management Plan (CTMP) which shall detail the procedures, requirements and standards necessary for managing the traffic effects during construction so that safe, adequate, and convenient routes for local movements by all transport modes are maintained throughout the construction of the Project. In particular, the CTMP shall include: <ol style="list-style-type: none"> a) The identification of the two heavy vehicle construction routes (as identified as Route 1 and Route 2 in the supporting Transport Assessment, prepared by Beca, dated August 2022). b) A requirement to co-ordinate with WCC regarding any planned construction activities along the proposed construction routes. c) A requirement to co-ordinate and collaborate with WIAL regarding any overlapping WIAL construction activities, namely the freight / logistics hub, to manage and mitigate cumulative construction traffic effects as far as practicable. d) Details of over-dimension trucks and/or loads which could not use either Route 1 or Route 2 to access the site and would therefore need to cross the airport runway during airport curfew hours (midnight to 6am). e) A requirement to consult with WIAL and seek their agreement to impose a reduced speed limit of 30km/h along Stewart Duff Drive during construction, between the main construction yard and the junction with Moa Point Road. f) Notwithstanding Condition 25.1(e) above, a requirement that all construction vehicles are limited to speeds of 30km/h during construction along Stewart Duff Drive between the main construction yard and the junction with Moa Point Road. g) Safety control measures such as fencing, barriers, hoarding, signage and a temporary crossing point. h) A staff travel management plan including a car pool policy and consideration of off-site, off-road parking opportunities. i) A requirement that all construction vehicle drivers are trained and inducted to make them aware of: reduced speed limit along Stewart Duff Drive, as |

| Proposed Conditions | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---------------------|---|----------------------------|-------------------------|--------------|-------------|----------------------------|-------------------------|----------|-----------|----|----|-----------|----|----|-----------|----|----|-----------|----|----|-----------|-----------|----|----|-----------|----|----|-----------|----|----|
| Reference | Conditions | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | <p>identified at Condition 25.1(f); and the potential for school children crossing the road in certain locations on the identified heavy vehicle construction routes.</p> <p>j) A temporary traffic management plan to avoid potential conflicts between pedestrians and construction traffic on Stewart Duff Drive, either in the form of protected pedestrian routes or via closure of footpaths with alternative routes/diversions provided.</p> <p>k) Measures to maintain vehicle and pedestrian access for staff and visitors to Cyclotek and Moa Point WWTP.</p> <p>l) Contact details for appropriate person(s) implementing the CTMP, including after-hours contact details.</p> <p>m) The identification of any construction work which cannot be reasonably undertaken during normal working hours, as per Condition 23.1.</p> <p><i>The following further specific measures are required in relation to concrete truck activities:</i></p> <p>n) A requirement that heavy vehicles associated with concrete pour activities to use Route 1 only.</p> <p>o) Unless otherwise permitted through Condition 25.1(n), all concrete pour activities and associated heavy vehicle construction traffic to be undertaken within the following time restrictions:</p> <ol style="list-style-type: none"> i. Outside of the summer break, all work to be undertaken on weekdays only, other than on a Saturday where work shall finish prior to 10:00am. ii. During the summer break, all work shall finish prior to 10:00am on any day, other than Sundays. <p><i>Note: it is anticipated that concrete pour activities will take place outside of normal Construction Work Hours under Condition 23.1 and accordingly, such details shall be set out in the CTMP in accordance with Condition 25.1(m).</i></p> <p>p) Detailed management plan for the management of pedestrian traffic along Stewart Duff Drive during this period, which would either require the presence of traffic controllers or a relevant portion of the road is closed to the public, with alternative routes/diversions provided and associated signage.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.2 | <p>The Requiring Authority shall submit a Construction Noise and Vibration Management Plan (CNVMP) which shall be prepared by an appropriately qualified acoustic and vibration specialist. The purpose of the CNVMP is to provide a framework for the development and implementation of measures to avoid, remedy or mitigate adverse construction noise and vibration effects, and to minimise any exceedance of the criteria set out in Conditions 25.3 and 25.4.</p> <p>As required by Condition 23.1, the CNVMP shall also identify any construction work which cannot be reasonably undertaken during normal working hours.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.3 | <p>Construction noise shall be measured and assessed in accordance with NZS 6803:1999 Acoustics – Construction Noise’. The construction noise shall, where practicable, comply with the following criteria, except as provided for in the CNVMP:</p> <table border="1"> <thead> <tr> <th>Time of week</th> <th>Time period</th> <th>dB_{LAeq} (15min)</th> <th>dB_{LAF} (max)</th> </tr> </thead> <tbody> <tr> <td rowspan="4">Weekdays</td> <td>0630-0730</td> <td>55</td> <td>75</td> </tr> <tr> <td>0730-1800</td> <td>70</td> <td>85</td> </tr> <tr> <td>1800-2000</td> <td>65</td> <td>80</td> </tr> <tr> <td>2000-0630</td> <td>45</td> <td>75</td> </tr> <tr> <td rowspan="3">Saturdays</td> <td>0630-0730</td> <td>45</td> <td>75</td> </tr> <tr> <td>0730-1800</td> <td>70</td> <td>85</td> </tr> <tr> <td>1800-0630</td> <td>45</td> <td>75</td> </tr> </tbody> </table> | | | Time of week | Time period | dB _{LAeq} (15min) | dB _{LAF} (max) | Weekdays | 0630-0730 | 55 | 75 | 0730-1800 | 70 | 85 | 1800-2000 | 65 | 80 | 2000-0630 | 45 | 75 | Saturdays | 0630-0730 | 45 | 75 | 0730-1800 | 70 | 85 | 1800-0630 | 45 | 75 |
| Time of week | Time period | dB _{LAeq} (15min) | dB _{LAF} (max) | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Weekdays | 0630-0730 | 55 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 0730-1800 | 70 | 85 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 1800-2000 | 65 | 80 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 2000-0630 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Saturdays | 0630-0730 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 0730-1800 | 70 | 85 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 1800-0630 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

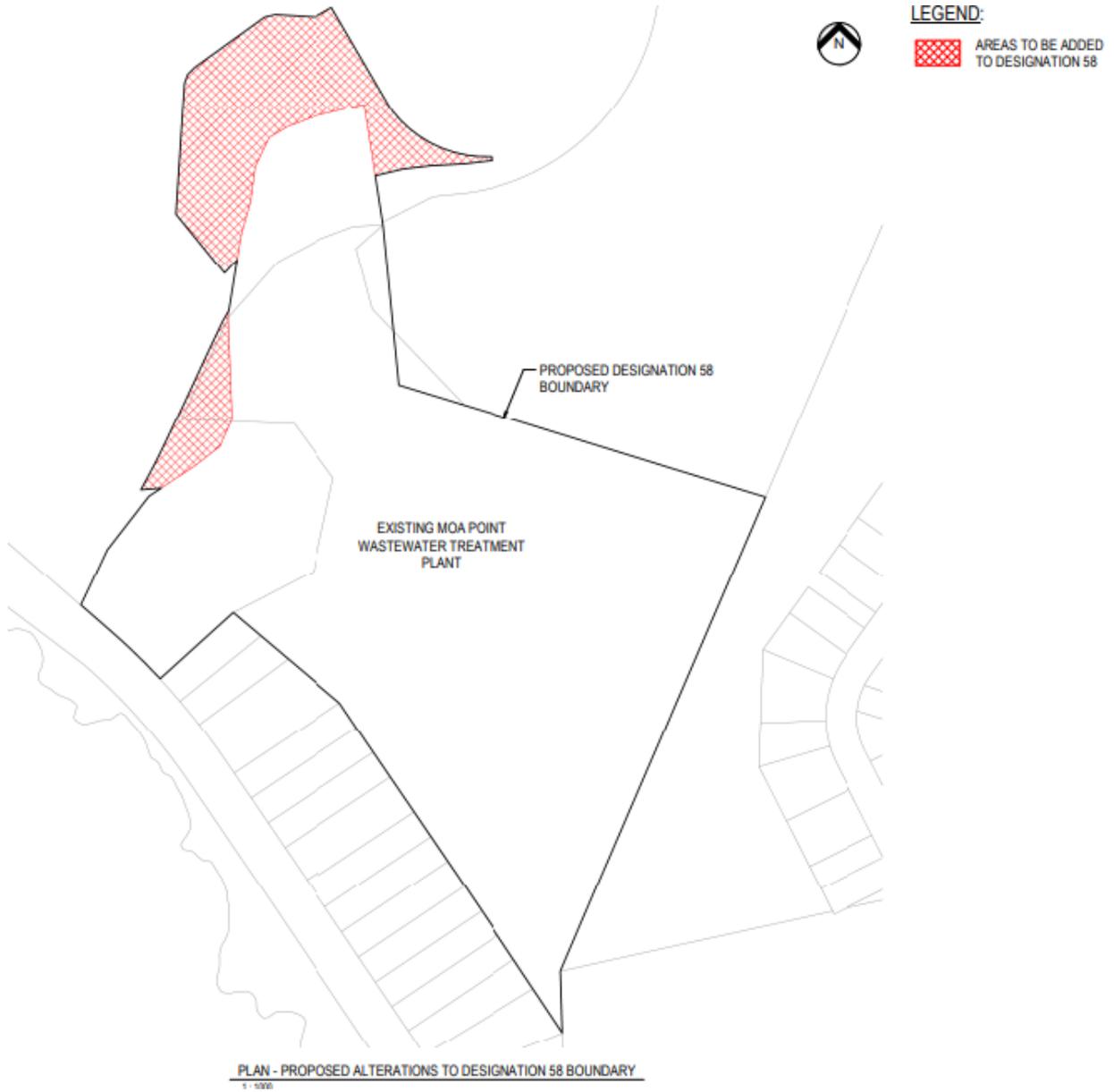
| Proposed Conditions | | | | | |
|--|---|-----------|----|----|--|
| Reference | Conditions | | | | |
| | Sundays and public holidays | 0630-0730 | 45 | 75 | |
| | | 0730-1800 | 55 | 85 | |
| | | 1800-0630 | 45 | 75 | |
| 25.4 | A suitably qualified acoustic expert shall be engaged to assess and manage construction vibration. The vibration criteria set out in Table 4 of DIN 4150-3 2016 shall be met, where practicable. Where it is not practicable to achieve those criteria, the CNVMP shall set out how those activities will be undertaken to minimise, monitor and manage potential vibration effects. | | | | |
| 26.Erosion & Sediment Control Plan | | | | | |
| 26.1 | <p>The Requiring Authority shall provide at least 10 days prior to the commencement of works on the site a copy of the ESCP certified by Greater Wellington Regional Council to the CMO for their records.</p> <p>If during the construction period any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification.</p> | | | | |
| 27.Ecology | | | | | |
| 27.1 | The Requiring Authority shall provide at least 10 days prior to the commencement of works on the site a copy of the Lizard Management Plan and a copy of the permit obtained under the Wildlife Act 1953, as certified by the Department of Conservation, to the CMO for their records. | | | | |
| 27.2 | <p>Where practicable, the clearance of vegetation and loose rock along the toe of the embankment shall avoid avifauna breeding season (July to Feb). Where such works cannot be avoided within the avifauna breeding season, the following provisions apply:</p> <ol style="list-style-type: none"> Within one prior to such works occurring, a penguin detector dog shall confirm the presence of absence of active penguin nests; If an active penguin nest is discovered, the area must be cordoned off with a 100m buffer. No works shall occur within this buffer until the nesting birds have fledged, or the nest has been naturally abandoned. | | | | |
| 28. Community Liaison and Complaints Register | | | | | |
| 28.1 | <p>The Requiring Authority shall work with the Community Liaison Committee (as originally established in association with the Moa Point WWTP) and shall:</p> <ul style="list-style-type: none"> offer to attend regular Community Liaison Committee (CLC) meetings prior to and throughout the duration of the SMF construction and operation inform the CLC and its members about progress with management plans and to provide an opportunity for feedback on any draft management plan or outline plan prior to submission to the CMO or WCC. provide an opportunity for feedback on the draft Commissioning Odour Management Plan prior to submission to the GWRC. consider any issues raised by the Community Liaison Committee relating to compliance with designation conditions, including management plans and outline plans. be responsible for working with the Community Liaison Committee to identify opportunities for the proposal to create education opportunities associated with the project. | | | | |
| 28.2 | <p>At all times during the Works, the Requiring Authority shall maintain a permanent register of any complaints received alleging adverse effects from, or related to, the Works. The register shall include:</p> <ol style="list-style-type: none"> The name and address (where this has been provided) of the complainant; The nature of the complaint; Location, date and time of the complaint and also of the alleged event; | | | | |

| Proposed Conditions | | | | | | | | | |
|---|---|---------------------------------|--------------------------|--------------------------------------|--------------------------|------------------------------------|--------------------------|------------------------------------|--------------------|
| Reference | Conditions | | | | | | | | |
| | <p>d) Weather conditions at the time of the event and including wind direction and approximate wind strength if the complaint relates to air quality or noise;</p> <p>e) The outcome of the Requiring Authority's investigation into the complaint;</p> <p>f) Measures taken to respond to the complaint; and</p> <p>g) Any other activities in the area, unrelated to the construction, which may have contributed to the complaint (such as non-Project construction, fires, traffic accidents or unusually dusty conditions generally).</p> | | | | | | | | |
| 28.3 | <p>In response to any complaints received, the Requiring Authority shall:</p> <p>a) Acknowledge the complaint within 2 Working Days.</p> <p>b) Promptly investigate, identify the urgency associated with the complaint and communicate that to the complainant.</p> <p>c) Take reasonable steps to remedy or mitigate the matters giving rise to the complaint if there are reasonable grounds for the complaint within 10 Working Days of receiving the complaint or such sooner time as may be reasonably necessary in the circumstances.</p> <p>d) Maintain a record of its responses and any remedial actions undertaken.</p> <p>e) This record shall be maintained on site and shall be made available upon request.</p> | | | | | | | | |
| 29.Accidental discovery protocol | | | | | | | | | |
| 29.1 | <p>If koiwi, taonga, or other archaeological material is discovered in any area during the works, work shall immediately cease and the requiring authority shall contact Taranaki Whanui, Ngati Toa Rangitira, Heritage New Zealand and Wellington City Council within 24 Hours. If human remains are found, the New Zealand Police shall also be contacted. The requiring authority shall allow the above parties to inspect the site and in consultation with them, identify what needs to occur before work can resume.</p> <p><i>Note: Evidence of archaeological material may include burnt stones, charcoal, rubbish heaps, shell, bones, old building foundations, artefacts and human burials.</i></p> | | | | | | | | |
| 30.Operational Noise | | | | | | | | | |
| 30.1 | <p>Sludge Minimisation Facility operational noise emission levels when measured at or within the boundary of any residentially zoned site, shall not exceed the following noise limits:</p> <table border="1"> <tbody> <tr> <td>All days 7.00am to 7.00pm (day)</td> <td>55 dB $L_{Aeq(15\ min)}$</td> </tr> <tr> <td>All days 7.00pm to 10.00pm (evening)</td> <td>50 dB $L_{Aeq(15\ min)}$</td> </tr> <tr> <td>All days 10.00pm to 7.00am (night)</td> <td>45 dB $L_{Aeq(15\ min)}$</td> </tr> <tr> <td>All days 10.00pm to 7.00am (night)</td> <td>75 dBA L_{AFmax}</td> </tr> </tbody> </table> <p>Noise measurements shall be measured in accordance with NZS 6801:2008 "Acoustics – Measurement of environmental sound". Noise measurements shall be assessed in accordance with NZS 6802:2008 "Acoustics - Environmental Noise".</p> | All days 7.00am to 7.00pm (day) | 55 dB $L_{Aeq(15\ min)}$ | All days 7.00pm to 10.00pm (evening) | 50 dB $L_{Aeq(15\ min)}$ | All days 10.00pm to 7.00am (night) | 45 dB $L_{Aeq(15\ min)}$ | All days 10.00pm to 7.00am (night) | 75 dBA L_{AFmax} |
| All days 7.00am to 7.00pm (day) | 55 dB $L_{Aeq(15\ min)}$ | | | | | | | | |
| All days 7.00pm to 10.00pm (evening) | 50 dB $L_{Aeq(15\ min)}$ | | | | | | | | |
| All days 10.00pm to 7.00am (night) | 45 dB $L_{Aeq(15\ min)}$ | | | | | | | | |
| All days 10.00pm to 7.00am (night) | 75 dBA L_{AFmax} | | | | | | | | |
| 31.Odour | | | | | | | | | |
| 31.1 | <p>At least 10 days prior to the commencement of the Commissioning Phase of the SMF, the Requiring Authority shall provide a copy of the Commissioning Odour Management Plan (COMP) certified by Greater Wellington Regional Council to the CMO for their records.</p> <p>If, prior to the commissioning phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification.</p> | | | | | | | | |
| 31.2 | <p>At least 10 days prior to the commencement of the Operation Phase of the SMF, the Requiring Authority shall provide a copy of the Operational Odour Management Plan (OOMP) certified by Greater Wellington Regional Council to the CMO for their records.</p> <p>If, prior to the commissioning phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be</p> | | | | | | | | |

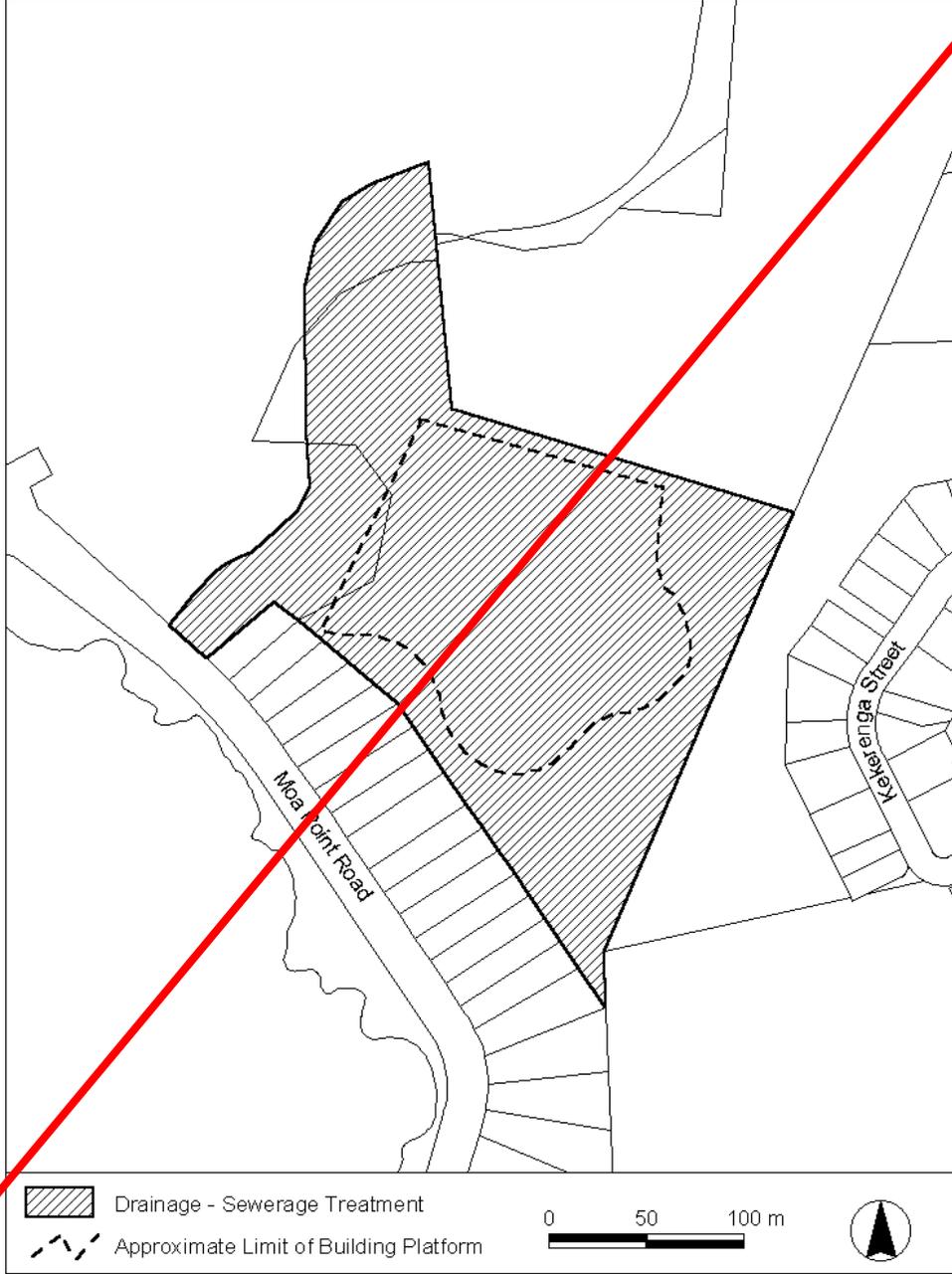
| Proposed Conditions | |
|---|---|
| Reference | Conditions |
| | provided to the CMO within 5 working days of receiving confirmation of the recertification. |
| 31.3 | <p>At least 10 days prior to the commencement of the Operation Phase of the SMF, the Requiring Authority shall provide a copy of the Operation and Maintenance Plan (OMP) certified by Greater Wellington Regional Council to the CMO for their records.</p> <p>If, prior to the commissioning phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification.</p> |
| 31.4 | During operation the discharge of contaminants into air shall not cause offensive or objectionable odour at the boundary of a sensitive activity. |
| 32.Operational and Maintenance Traffic | |
| 32.1 | <p>The Requiring Authority shall submit an Operational Traffic Management Plan (OTMP). The purpose of the OTMP is to manage transport safety effects of operational traffic. Matters to be considered by the OTMP shall include:</p> <ul style="list-style-type: none"> - alerting other road users and pedestrians along Stewart Duff Drive of truck movements occurring in association with the operation of the SMF - managing the potential for conflict between HGVs travelling in both directions along the Moa Point WWTP access road - managing the potential for conflict with other road users and pedestrians where HGVs need to reverse on to Stewart Duff Drive - managing the potential for conflict with other road users and pedestrians where site access/egress points along Stewart Duff Drive are extensively wide and/or have limited sightlines <p>The OTMP must also include details of how SMF HGV drivers will be inducted/trained to assist in mitigating the transport safety effects of the operation of the SMF.</p> |
| 32.2 | <p>A Maintenance Period Traffic Management Plan (MPTMP) for the scheduled 1 week annual shutdown / maintenance period of the SMF shall be submitted to the CMO for certification at least 20 working days prior to the first planned maintenance activity during operations. The purpose of the MPTMP is to manage transport and parking effects of service and staff vehicles during maintenance periods alongside normal operational truck movements.</p> <p>The MPTMP shall include the schedule of the maintenance activity and the normal truck operation, anticipated movements of the maintenance trucks and the management of temporary on-site parking.</p> <p>The MPTMP shall be implemented and maintained throughout each annual maintenance period for the SMF to manage potential adverse transport and traffic effects arising from maintenance activities to the greatest practicable extent.</p> |

Moa Point Drainage and Sewage Treatment Designation

Plan 1 - Detail Of Designation

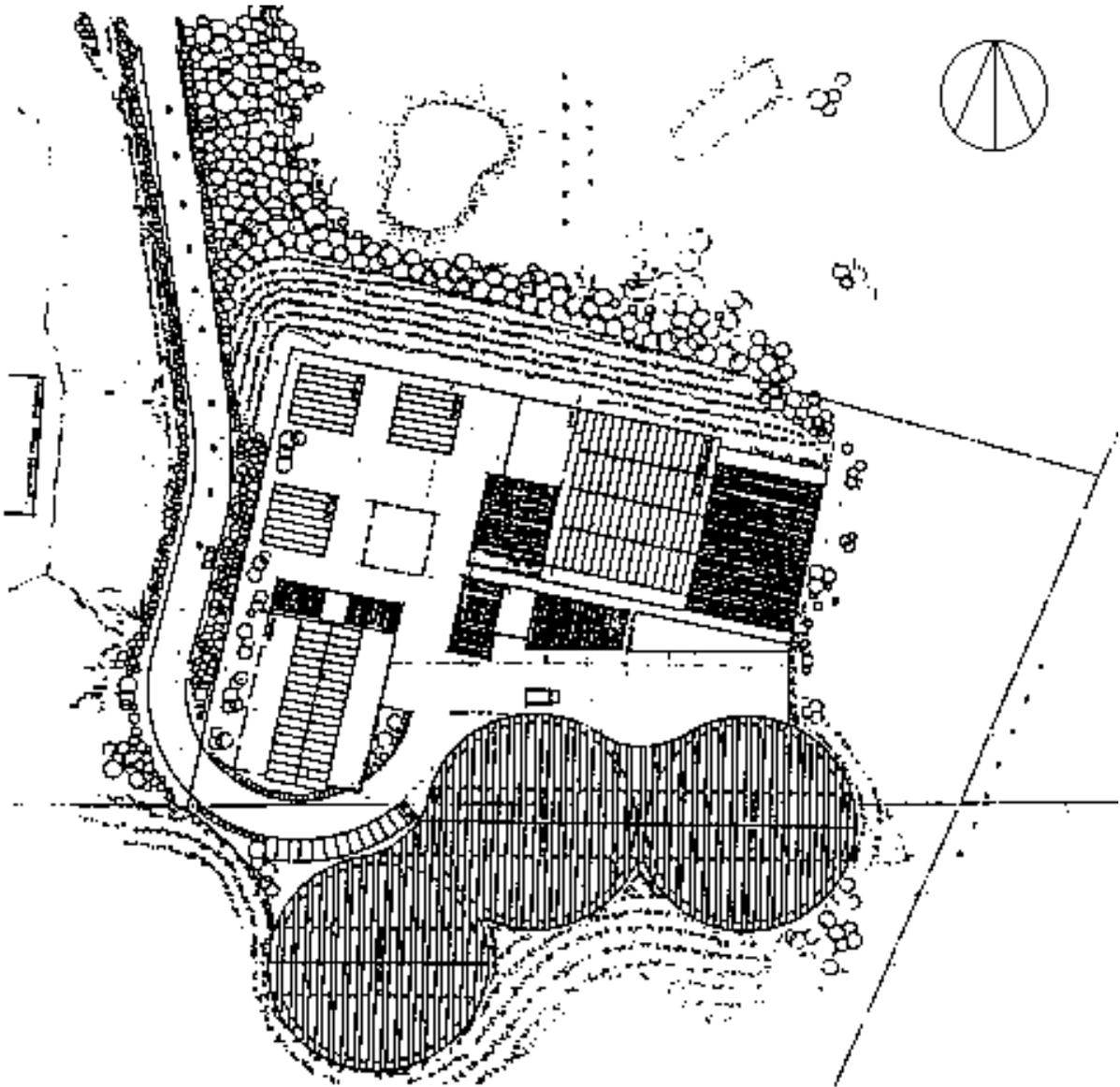


Moa Point Drainage and Sewerage Treatment Designation



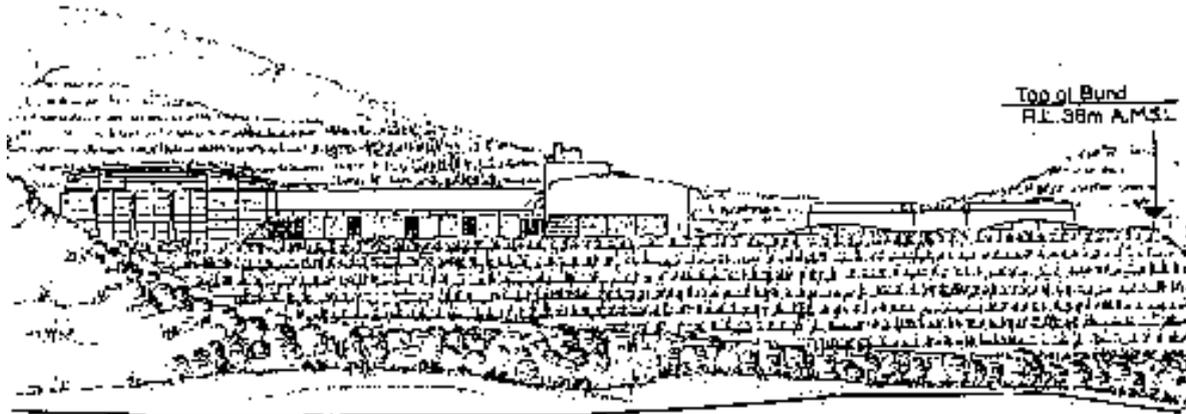
Moa Point Drainage and Sewage Treatment Designation

Plan 2 - Plan of Proposed Plant

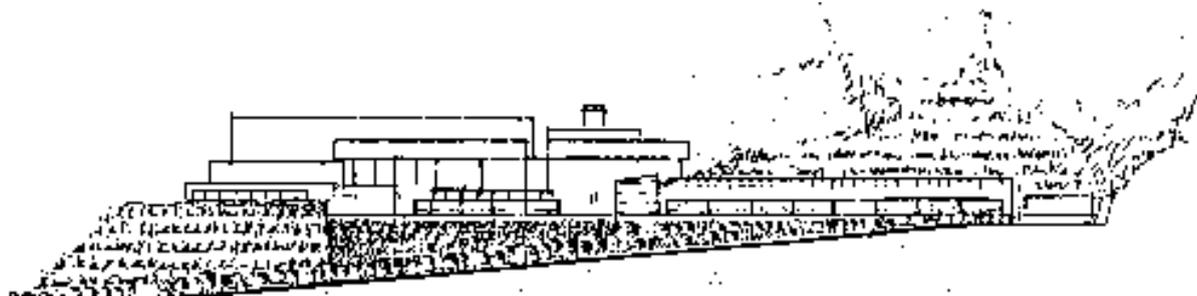


Moa Point Drainage and Sewage Treatment Designation

Plan 3 - Elevations of Proposed Plant



North Elevation



West Elevation

Insert new plans:

- Plan 4 – SMF General Arrangement Plan - ref. 3258521-DA-000-K0121 Rev A
- Plan 5 – SMF Elevation - ref. 3258521-DA-000-K1202 Rev A