

UNDER the Resource Management Act
1991

IN THE MATTER A Notice of Requirement by
Wellington City Council to alter
Designation 58 (Moa Point
Drainage and Sewage
Treatment) to provide for the
construction, operation and
maintenance of the proposed
Sludge Minimisation Facility at
Moa Point, Wellington

JOINT WITNESS STATEMENT (PLANNING)

DATED 14 December 2022

Introduction

1. This joint witness statement relates to the Notice of Requirement ('**NOR**') by Wellington City Council (**WCC**), to alter Designation 58 (Moa Point Drainage and Sewage Treatment) to provide for the construction, operation and maintenance of the proposed Sludge Minimisation Facility (**SMF**) at Moa Point, Wellington.
2. At the hearing for the SMF on 5 December 2022, WCC (as the project 'applicant') proposed that Mr Paul McGimpsey and Ms Monique Zorn conference to attempt to resolve the outstanding conditions that had not been agreed.
3. Discussions between Mr McGimpsey and Ms Zorn have taken place since the hearing.

Expert Witness Code of Conduct

4. This joint statement is prepared in accordance with section 4.7 of the Environment Court Practice Note 2014.
5. Both Mr McGimpsey and Ms Zorn confirm they have read the Environment Court Practice Note 2014, and in particular Appendix 3 – Protocol for Expert Witness Conferencing, and agree to abide by it.

Purpose and scope of conferencing

6. The purpose of conferencing was to identify, discuss, and highlight points of agreement and disagreement in relation to the conditions for the SMF NOR.
7. Mr McGimpsey and Ms Zorn have reached agreement on all conditions and there are no outstanding issues to be resolved.
8. **Annexure A** records the agreed conditions.

Date: 14 December 2022



Ms Monique Zorn



Mr Paul McGimpsey

ANNEXURE A – AGREED CONDITIONS

DEFINITIONS, ABBREVIATIONS, ACROMYNS AND TERMS

| TERM | DEFINITION |
|--------------------------|---|
| AEE | Assessment of Environmental Effects for the Wellington Sludge Minimisation Facility Project |
| Airport Curfew | The period within which WIAL will not allow aircraft operations (take-off and landing) to occur on each day. |
| CMO | Wellington City Council's Compliance Monitoring Officer |
| Commissioning Phase | The final phase of construction involving a staged approach whereby individual systems are tested and then integrated with other systems prior to being made operational. |
| COMP | Commissioning Odour Management Plan |
| Council | Wellington City Council |
| CNVMP | Construction Noise and Vibration Management Plan |
| CTMP | Construction Traffic Management Plan |
| Dust | Means all non-combusted solid particulate matter that is suspended in the air, or has settled after being airborne. Dust may be derived from materials including rock, sand, cement, fertiliser, coal, soil, paint, animal products and wood. |
| Dust Nuisance | means the generation of dust resulting in visible evidence of suspended solid: <ul style="list-style-type: none"> a) in the air beyond the site the dust is generated from; or b) traceable from a dust source settling on the ground, building or structure on a neighbouring site, or water. |
| ESCP | Erosion and Sediment Control Plan |
| GWRC | Greater Wellington Regional Council, including any officer of Greater Wellington Regional Council |
| LRV | Light reflectance value which applies to colours applied to products (paint, powdercoating) |
| LVMP | Landscape and Vegetation Management Plan |
| Moa Point WWTP | Moa Point Wastewater Treatment Plant and includes the activities listed at 3.1 of this Designation. |
| MPTMP | Maintenance Period Traffic Management Plan |
| OOMP | Operational Odour Management Plan |
| OMP | Operation and Maintenance Plan |
| OTMP | Operational Traffic Management Plan |
| Outline Plan | An outline plan prepared in accordance with section 176A of the Resource Management Act 1991 |
| RCA | Road Controlling Authority |
| Requiring Authority | Wellington City Council |
| SMF | Sludge Minimisation Facility for the purpose of stabilising and reducing the volume of processed sludge produced by the Moa Point WWTP and Western (Karori) WWTP |
| SMF Early Enabling Works | These are works which can be authorised either through the existing designation without the requirement for an Outline Plan, or as permitted activities which do not require a resource consent. The early enabling works to be undertaken in association with the SMF works, include: <ul style="list-style-type: none"> • Below ground services relocation or upgrades • Demolition of on-site buildings • Slope stabilisation works |
| SMF Works | The design, construction, maintenance and operation of the SMF as referenced in the AEE and the related SMF designation conditions. SMF Works excludes early enabling works, as per the 'SMF Early Enabling Works' definition, which can be undertaken without the submission of an Outline Plan. |
| SMF Construction Works | All aspects of the SMF Works which relate to construction activities. |
| Summer Break | 20 December to 31 January of any calendar year |
| WCC | Wellington City Council |
| WIAL | Wellington International Airport Limited |
| Working Days | Has the same meaning under section 2 of the Resource Management Act 1991 |
| WWTP Access Road | Access road serving Moa Point WWTP, off Stewart Duff Drive |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey (Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough). | Additional comments |
|-------------------|--|---|---|
| CON No. | | | |
| 19.General | | | |
| 19.1 | | <p>While providing for further development of the design and layout, the SMF Works shall be undertaken in general accordance with:</p> <ul style="list-style-type: none"> • ‘Description of the proposal’ in the AEE Report, dated August 2022 • General Arrangement Plan – ref. 3258521-DA-000-K0121 Rev A • Street Elevation – ref. 3258521-DA-000-K1202 Rev A <p><u>However</u>, Changes to the SMF Works from that set out in these documents shall be allowed, provided that:</p> <ul style="list-style-type: none"> • The Requiring Authority demonstrates that any material changes from the plans referenced above results in no more than a minor change to the adverse effects on the environment; and • All other conditions are still able to be complied with. <p>Where there may be inconsistencies between information within the documents listed above and conditions of the designation, these conditions shall prevail.</p> | <p><i>Amendment to make it clear that the first test will be to establish if the SMF Works are in general accordance with the documents listed.</i></p> <p><i>If the design and layout is found not to be in general accordance, changes will be allowed, provided the second test is met.</i></p> <p><i>This amendment is supported by Mr McGimpsey and Ms Zorn.</i></p> |
| 19.2 | | <p>An outline plan/s shall be submitted to Council (as the Territorial Authority) in accordance with section 176A of the RMA, unless a waiver for this requirement is provided in writing by Council.</p> <p>As well as the matters set out in section 176A(3), the outline plan(s) shall include the following items in accordance with conditions on this designation:</p> <ul style="list-style-type: none"> • Design Statement • Landscape and Vegetation Management Plan • Construction Noise and Vibration Management Plan • Operational Traffic Management Plan | |
| 19.3 | | <p>As soon as reasonably practicable following the completion of construction of the SMF, the Requiring Authority shall give notice in accordance with section 182 of the RMA, seeking the revision of the designation boundary to that shown in Plan 3258521-DA-K0112 Rev A.</p> | |
| 19.4 | | <p>The Requiring Authority shall arrange and conduct a pre-construction site meeting with the contractor prior to any work authorised by this designation commencing on site and invite, with a minimum of 10 days’ notice, Council’s Compliance Monitoring Officer (CMO) and any other key Council representatives determined by the CMO.</p> <p><i>Note: In the case that any of the invited parties, other than the representative of the Requiring Authority and the contractor, do not attend this meeting, the Requiring Authority will have complied with this condition, provided the invitation requirement is met.</i></p> | |
| 19.5 | | <p>The Requiring Authority may request amendments to any of the management plans required by these conditions by submitting the amendments in writing to the CMO at least 10 Working Days prior to any changes taking effect. Any changes to management plans shall remain consistent with the overall intent of the management plan and relevant conditions and achieve the outcomes required by these conditions. The changes sought shall not be implemented until the Requiring Authority has received the CMO’s written approval for the amendments.</p> | |
| 19.6 | | <p>The designation does not authorise the removal of the landform to the west of Stewart Duff Drive and within the north-west area of the designation, known as the ‘hillock’, either in whole or in part.</p> | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments | | | | | | | | |
|-----------------------------------|---------------------------|---|--|---------------------------|-----------------------------------|-------|-----------------------------------|-------|-----------|-------|--|
| CON No. | | | | | | | | | | | |
| 20.Layout and Heights | | | | | | | | | | | |
| 20.1 | | <p>Each structure is depicted on the general arrangement plan attached to this designation (plan 3258521-DA-000-K0121 Rev A). Variations to the siting and footprints of these buildings/structures may occur to accommodate layout and design evolution, but only to the extent that all other SMF designation conditions are complied with.</p> <p>All key structures, identified in the table below, shall not exceed the following heights, based on NZ Vertical Datum 2016, as set out in the Table below:</p> <table border="1"> <thead> <tr> <th>Key Structure</th> <th>Maximum Height (NZVD2016)</th> </tr> </thead> <tbody> <tr> <td>Main Sludge Processing Building 1</td> <td>35.5m</td> </tr> <tr> <td>Main Sludge Processing Building 2</td> <td>32.0m</td> </tr> <tr> <td>Digesters</td> <td>30.0m</td> </tr> </tbody> </table> <p>These maximum heights are measured relative to New Zealand Vertical Datum and are not the absolute heights of each structure.</p> <p><i>Note: Maximum heights excludes roof mounted equipment and building facades.</i></p> | Key Structure | Maximum Height (NZVD2016) | Main Sludge Processing Building 1 | 35.5m | Main Sludge Processing Building 2 | 32.0m | Digesters | 30.0m | |
| Key Structure | Maximum Height (NZVD2016) | | | | | | | | | | |
| Main Sludge Processing Building 1 | 35.5m | | | | | | | | | | |
| Main Sludge Processing Building 2 | 32.0m | | | | | | | | | | |
| Digesters | 30.0m | | | | | | | | | | |
| 21.Design and Appearance | | | | | | | | | | | |
| 21.1 | | <p>The Requiring Authority shall prepare a Design Statement. The Design Statement shall set out how the following has been considered in the design:</p> <ul style="list-style-type: none"> • That site levels and building form, colour and texture reduce the apparent height and bulk of large buildings, and minimise reflectivity and glare when viewed from adjoining public, residential areas or the air. • That variations in building mass, height and architectural form have been considered to provide visual cohesiveness and interest, reduce any potentially adverse effects of building bulk and scale and promote visual permeability. • That landscape treatment has been incorporated where practicable to assist in the screening of parking, loading and storage areas. • That opportunities for low-level amenity planting within the Stewart Duff Drive frontage have been explored to benefit those passing by close to the site. • That the use of sustainable materials/techniques have been considered as part of the design. <p>The Design Statement shall also demonstrate how the development is consistent with the following requirements for the purpose of mitigating landscape and visual effects:</p> <ul style="list-style-type: none"> • All colours and materials applied to the SMF shall have a light reflectance value (LRV) or equivalent no greater than 42%, excluding the gas bag(s). • All colours and materials applied to the SMF shall be selected from the blue, blue/grey, grey, green and brown hues of the colour palette, excluding the gas bag(s). <u>a colour palette that relates to the natural environment, of either the hillside and its vegetation or the sky and seascape that surrounds it.</u> • The proposed gas bag(s) shall not be white <u>unless it can be demonstrated that the gas bag(s) colour selection has been considered as part of the visual cohesiveness of the overall SMF design and colour palette.</u> Where practicable, the gas bag(s) shall have a light reflectance value (LRV) no greater than 42%. • Details of the concrete compound and surface finish or mixture to be used externally on the buildings demonstrating that it achieves an equivalent light reflective value (LRV) of 42% maximum. • The SMF buildings shall not be clad in materials including stainless steel or unpainted galvanised steel, copper or polycarbonate translucent cladding. • Any glazed areas shall use low-reflectivity glass. • Any external lighting shall be shielded to prevent light spill. Any external lighting shall be for wayfinding and security reasons only. • The SMF buildings shall not include any sign-writing or advertising. Any signage shall be low level, for health and safety and way-finding purposes only, for the benefit of site users. • Details of fences, gates and walls will be provided. Such structures will be as inconspicuous as practicable. | <p><i>Amendments to reflect the approach discussed at the hearing, particularly between Guardians of the Bay and Council's Urban Designer.</i></p> <p><i>This change is supported by Mr McGimpsey and Ms Zorn.</i></p> | | | | | | | | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments |
|---------------------|--|---|----------------------------|
| CON No. | | | |
| | | <ul style="list-style-type: none"> • Details of the vegetated wire mattress system for stabilisation of cut slopes and the exact locations for any proposed shotcrete will be provided. <p>In preparing the Design Statement, the Requiring Authority shall consult with Council and a final draft shall be submitted to Council for comment. In finalising the Design Statement, the Requiring Authority shall take into account any feedback received from Council.</p> <p><i>Note – This would not preclude the use of pattern or artwork(s) (excluding text) on the exterior of the building, and the applicant is encouraged to consider this as an option.</i></p> <p><i>Note - To achieve an equivalent LRV of 42%, the concrete will need to be coated or include an oxide in the mix, or both. Any texture applied to the concrete will also assist in reducing the material's reflectivity. The concrete's reflectivity will need to be approximately midway between Gull Grey (50% LRV) and Sandstone Grey (27% LRV).</i></p> | |
| 21.2 | | <p>The Requiring Authority shall prepare a Landscape and Vegetation Management Plan (LVMP) which shall achieve the following:</p> <ul style="list-style-type: none"> • The creation of improved quality lizard habitat on the unaffected or modified slopes which surround the main construction site and identified Council reserves, in accordance with the Lizard Management Plan certified by the Department of Conservation (as required under Condition 27.1). • Where consistent with the aims and outcomes of the Lizard Management Plan (LMP), unaffected or modified slopes shall be revegetated in a manner that restores indigenous biodiversity values, whilst also being informed by Council's plant list for coastal headlands, cliffs, escarpments and gullies. • Where practicable, vegetation cover shall be established on disturbed surfaces as soon as possible for the purposes of reducing erosion, sedimentation and the visual impact of earthworks. • Planting areas shall be maintained for a minimum of five years including pest plant management. • Engagement with Predator Free Wellington to manage the potential for the SMF to adversely impact ongoing animal pest control measures in the surrounding area. • Any vertical cut faces are to be supported by retaining walls or similar engineering features. Retaining structures (excluding shotcrete) shall not be publicly visible (either forming part of building envelope or screened by proposed buildings). • Any gabion baskets shall be at the toe of the slope only and shall not be publicly visible. • All proposed planting shall be low-level only to ensure that the ongoing safety of airport operations is not adversely affected. • Where practicable, provide opportunities to work with the local community as part of vegetation restoration and/or ongoing management. <p><i>In preparing the LVMP, the Requiring Authority shall consult with the Council and a final draft shall be submitted to both parties for comment. In finalising the LVMP, the Requiring Authority shall take into account any feedback received from the Council.</i></p> <p><i>Note: Following the completion of the habitat enhancement programme (both on site and on identified Council reserves), an assessment of actual outcomes of relocation and habitat enhancement work shall be undertaken through a five-year monitoring programme, in accordance with the certified Lizard Management Plan and conditions of the Wildlife Act Authority.</i></p> <p><i>Note: Specifically for this condition, 'publicly visible' means visible from a member of the public at ground level on Stewart Duff Drive.</i></p> | |
| 22. Flooding | | | |
| 22.1 | | <p>The plant equipment, buildings and contouring of the site are to be designed in such a way as to ensure that any overflows, including the accidental or emergency breaches of any tanks, are directed away from the Airport and Cyclotek Pharmaceuticals Ltd. In particular, the stormwater (reticulated network) drainage system is to be designed for a 20 year return period and the layout of the plant is to incorporate such bunding and curbing as is necessary to direct stormwater / overland flows from a 100 year return period (with climate change) direct to Moa Point Road away from the Airport and avoiding the boundary of Cyclotek Pharmaceuticals Ltd.</p> | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments |
|--------------------------|--|---|--|
| CON No. | | | |
| 22A. Three Waters | | | |
| 22A.1 | | The Requiring Authority must comply with the requirements of the WCC Code of Practice for Land Development (either its current version or replacement document), unless otherwise modified by condition(s) of the designation or agreed in writing by the Wellington Water Land Development Team. These are the engineering standards for mitigating adverse effects on the environment from earthworks, traffic (roading and vehicle access), wastewater and stormwater drainage, water supply and utility structures. | |
| 22A.2 | | No construction activities deemed to impact the existing water supply, stormwater or wastewater drainage assets shall start prior to sufficient documentation being submitted to the Wellington Water Land Development Team for their acceptance. Such documentation may include some or all of the following: <ul style="list-style-type: none"> i. engineering plans and design certificate, ii. specifications, iii. Design documentation (assessment, detailed drawings, and calculations) to support build over/near the Wellington Water. <ul style="list-style-type: none"> o wastewater pipes and pumpstation o Stormwater pipes o Water supply pipes ii. Construction management plans, including risk assessment, risk mitigations, contingency plans in response to potential damage to existing assets and consideration of ongoing operation and maintenance activities. | |
| 22A.3 | | Where existing buildings have been, or are to be, demolished or replaced, the end of the existing private water, stormwater and wastewater lateral(s) must be abandoned/decommissioned including disconnecting from the public main (water) and capping at the shared private drain (stormwater and wastewater). It is required that Council be advised of the final treatment by way of including the location of capping on the final as-built plan. <p>Advice Notes:</p> <ol style="list-style-type: none"> 1. Where drainage works are required, permits in addition to the works authorised by this designation are required: namely <ul style="list-style-type: none"> • Public Drainage Permit Some of the engineering plans and specifications in the designation condition above are to be submitted during the application stage for these permit(s). 2. Scheme and other indicative layout plans submitted as part of the application will be used by Council for information purposes only. These plans will not be used for granting approval under the condition above. Approvals will only be given on detailed engineering plans. 3. Prior to connection, an application for water supply and wastewater / stormwater (if required) is required to be made to Wellington City Council. All works must be inspected and tested by the Wellington Water, Water and Drainage Inspector. | |
| 22A.4 | | The Requiring Authority shall provide the buildings with an appropriately sized metered water service pipe connected to a water supply main for domestic supply. An engraved plastic tag reading “WATER SUPPLY MANIFOLD FOR (Street No)” is to be secured to the manifold clearly showing which property is served by the manifold. An RPZ-type backflow preventer is required if the connection is greater than 20mm DI. | |
| 22A.5 | | To accord with firefighting requirements, the Requiring Authority shall either: <ul style="list-style-type: none"> • install a sprinkler protection system in accordance with Automatic Fire Sprinkler Systems NZS4541 (excluding spaces where gas suppression systems are employed and electrical rooms which are fire rated in lieu of sprinklers), or, • provide a firefighting water supply complying with the NZ Fire Service Code of Practice for Firefighting Water Supplies SNZ PAS 4509:2008. <p><u>The Requiring Authority shall manage fire safety in the facility through either option 1 or 2:</u></p> <ol style="list-style-type: none"> 1. <u>Install a sprinkler protection system in accordance with Automatic Fire Sprinkler Systems NZS4541 (excluding spaces where gas suppression systems are employed and electrical rooms which are fire rated in lieu of sprinklers). The Requiring Authority shall</u> | <i>The wording of the new condition has been agreed to by Wellington Water Limited and is supported by Mr McGimpsey and Ms Zorn.</i> |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments |
|----------------|--|--|--|
| CON No. | | | |
| | | <i>demonstrate that flow rates are adequate to service the sprinkler system. If upgrade of public water supply infrastructure is required specifically to provide adequate flows, this shall be at the cost of the Requiring Authority.</i> | |
| | | <i>2. Provide a firefighting water supply complying with the NZ Fire Service Code of Practice for Firefighting Water Supplies SNZ PAS 4509:2008</i> | |
| 22A.6 | | The Requiring Authority may build over the existing public wastewater and stormwater network. No construction deemed to impact the existing water supply, stormwater or wastewater drainage assets shall start prior to sufficient documentation being submitted to the Wellington Water Land Development Team for their acceptance, including but not limited to; <ul style="list-style-type: none"> i. Engineering plans and design certificate, ii. Specifications, iii. Seismic and Structural Design Assessment and Calculations, including assessment of: <ul style="list-style-type: none"> o The potential for any interaction effects between the proposed building's foundations and piles near the wastewater assets (pipes and pumpstation) in a seismic event, and o The factors of safety and implications on the existing pipes and wastewater pumpstation as a result of any new buildings and structures, iv. Safety in Design Assessment, including <ul style="list-style-type: none"> o 24 / 7 access to the wastewater pumpstation for operation / maintenance purposes, o Methodology for maintenance / eventual replacement. | |
| 22A.7 | | The development of this site will require the public wastewater network to be amended to facilitate the build over. The public wastewater network modifications shall be inspected on site and certified by a suitably qualified Civil/Structural Engineer and at the conclusion of the engineering works the Requiring Authority is to provide a PS4 certificate or a design statement from a suitably qualified chartered engineer for the public wastewater assets (pipes and pumpstation) works. | |
| 22A.8 | | <i>Condition deleted as per Supplementary Evidence of Mr McGimpsey and is supported by Ms Zorn.</i> | |
| 22A.9 | | The development of this site may require the public stormwater and wastewater main to be altered to serve the development. All newly constructed stormwater and wastewater mains to be vested in Council shall be approved by Wellington Water Land Development Team based on a [video or] closed circuit television (CCTV) inspection carried out by the Requiring Authority in accordance with the New Zealand Pipe Inspection Manual. A pan tilt camera shall be used and lateral connections shall be inspected from inside the main. | |
| 22A.10 | | As the proposed construction may not comply with the Regional Standard for Water Services requirement for building/working near public wastewater and stormwater mains and pumpstation, the Requiring Authority must provide pre- and post- inspection footage and reports of the existing public infrastructure to the Wellington Water Land Development Team for approval. | |
| 22A.11 | | Any new defects identified post-development and deemed to be caused by the construction activities on the site must be repaired by the Requiring Authority prior to acceptance. Costs incurred for such repairs will be at the expense of the Requiring Authority. | |
| 22A.12 | | Piles and concrete foundations within the proposed public stormwater and wastewater assets must be kept a minimum of 1.0m from all public infrastructure, and pile foundations are required to be founded on solid ground below the main invert level within 1.0m of the main in accordance with Standards and Guidelines for buildings over or near Public Drains. | |
| 22A.13 | | The development must be provided with a suitably sized, separate and direct stormwater and wastewater lateral connections to a public stormwater and wastewater network at a location accepted in writing by the Wellington Water Land Development Team. | |
| 22A.14 | | To avoid impact on the receiving network and environment, stormwater management (treatment) is required. The stormwater management system(s) (treatment devices) shall be designed to treat stormwater runoff for all rainfall events up to and including the 1% AEP (1 in 100 year) | <i>Condition deleted. Rely on stormwater management /treatment required under the GWRC consent – permit 38113.</i> |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey (Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough). | Additional comments |
|------------------------------------|--|---|---|
| CON No. | | | |
| | | <p>event. The stormwater management system must be approved in writing by the Wellington Water Land Development Team and the following aspects must be met.</p> <p>(i) — The consent holder must construct an approved stormwater management system or systems in accordance with plans approved under the Building Consent and agreed with the Wellington Water Land Development Team.</p> <p>• — The stormwater management system must be designed to treat stormwater runoff from the site to for contaminants.</p> <p>(ii) — The consent holder must ensure that all connections to the system(s) are trapped to minimise debris entering the system.</p> | <i>This deletion is supported by Mr McGimpsey and Ms Zorn.</i> |
| 22A.15 | | <p>Prior to Engineering Approval, the consent holder will be required to prepare a draft Operation and Maintenance Manual for all stormwater device(s) setting out the principles of the general operation and maintenance for the stormwater system(s) and associated management devices. The draft Operations and Maintenance Manual shall be submitted to the Wellington Water Land Development Team for approval and is to include, but not be limited to:</p> <p>(i) — a detailed technical data sheet</p> <p>(ii) — details of who will hold responsibility for short-term and long-term maintenance of the stormwater devices</p> <p>(iii) — a programme for regular maintenance and inspection of the stormwater system</p> <p>(iv) — a programme for the collection and disposal of debris and sediment collected by the stormwater management device or practices</p> <p>(v) — a programme for post storm maintenance</p> <p>(vi) — general inspection checklists for all aspects of the stormwater system, including visual check of roadside sumps and outfalls</p> <p>(vii) — a programme for inspection and maintenance of vegetation associated with the stormwater devices.</p> | <p><i>Condition deleted. Rely on stormwater management for operation and maintenance required under the GWRC consent – permit 38113.</i></p> <p><i>This deletion is supported by Mr McGimpsey and Ms Zorn.</i></p> |
| 22A.16 | | Bare galvanised, zinc alum or unpainted metal (including copper) may result in contamination of stormwater runoff upon corrosion of surfaces and therefore shall not be used for exterior construction, including but not limited to roofing, cladding, gutters and downpipes. | <p><i>Condition deleted. Rely on stormwater management for operation and maintenance required under the GWRC consent – permit 38113.</i></p> <p><i>This deletion is supported by Mr McGimpsey and Ms Zorn.</i></p> |
| 22A.17 | | Any buildings constructed on the site must have a minimum floor level set under the building code requirements at the building consent stage. | <p><i>Condition deleted. Building consent is not required for this project. Moreover, this is a bespoke piece of infrastructure which will include basement levels.</i></p> <p><i>This deletion is supported by Mr McGimpsey and Ms Zorn.</i></p> |
| 22A.18 | | <i>Condition deleted as per Supplementary Evidence of Mr McGimpsey and is supported by Ms Zorn.</i> | |
| 22A.19 | | At the conclusion of the engineering works, the Requiring Authority is to submit as-built drawings that meet the requirements of Wellington Water Regional As-built Specification for Water Services for water supply, wastewater and stormwater drainage. | |
| 22A.20 | | Once an as-built plan has been submitted and within one month of completion of the drainage works and/or before vesting of assets or application, the Requiring Authority is required to arrange for a final inspection with the Wellington Water Drainage Inspector. | <i>This (part) deletion is supported by Mr McGimpsey and Ms Zorn.</i> |
| 23. Construction Work Hours | | | |
| 23.1 | | <p>Normal working hours for onsite construction activities will be as follows:</p> <ol style="list-style-type: none"> 7:00am to 6.00pm Monday to Saturday (excluding public holidays). All heavy construction vehicle access to the site shall be via Moa Point Road. There shall be no access to the site from the north using the airport perimeter road for such vehicles. Any construction work which cannot be reasonably undertaken during normal working hours shall be identified in the Construction Traffic Management Plan and/or the Construction Noise and Vibration Plan required by Condition 25. | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey (Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough). | Additional comments |
|--|--|--|---|
| CON No. | | | |
| 24. General Construction Restrictions | | | |
| 24.1 | | <i>Condition deleted as per Supplementary Evidence of Mr McGimpsey and is supported by Ms Zorn.</i> | |
| 24.2 | | <i>Condition deleted as per Supplementary Evidence of Mr McGimpsey and is supported by Ms Zorn.</i> | |
| 24.3 | | At all times, appropriate measures shall be taken to ensure that any arc-welding will not be hazardous to air traffic at Wellington International Airport. | |
| 24.4 | | At all times, diesel-powered machinery (including trucks) shall be maintained to avoid, remedy or mitigate minimise excessive smoke and/or diesel odour emissions. | |
| 24.5 | | The proposed construction shall be carried out in such a manner that the generation of dust is kept to a practicable minimum. In any case there must be no particulate matter beyond the boundary of the site that has an objectionable or offensive effect as a result of the activities authorised by this designation. For the purposes of this condition, the boundary of the site is as defined in Plan 7 (ref. 3258521-DA-000-K0113) attached to this designation. | |
| 24.6 | | Run-off must be controlled to prevent muddy water flowing, or earth slipping, onto neighbouring properties or the legal road. Sediment, earth or debris must not fall or collect on land beyond the site or enter the Council's stormwater system. Any material that falls on land beyond the site during work or transport must be cleaned up immediately (with the landowner's permission on land that isn't public road). The material must not be swept or washed into street channels or stormwater inlets, or dumped on the side of the road. Note: As a minimum, 100 mm clarity is required to allow water to be discharged offsite. If clarity is less than 100mm then the water is considered to be muddy and must be captured and treated on site. | <i>Condition deleted. Rely on conditions of regional earthworks permit [38469] and [38633]. This deletion is supported by Mr McGimpsey and Ms Zorn.</i> |
| 24.7 | | Where there is potential for a project or work to adversely affect the operation of Wellington Airport, including but not limited to visual navigation aids or air traffic facilities or activities, the Requiring Authority shall coordinate with Wellington international Airport Limited (WIAL) to ensure that the project or work will not affect WIAL's ability to meet its obligations under the Civil Aviation Authority New Zealand Rules. | |
| 25. Construction Management Plans | | | |
| 25.1 | | A Construction Traffic Management Plan (CTMP) shall be submitted to the CMO for certification at least 10 working days prior to the proposed date of commencement of the SMF works. The CTMP shall detail the procedures, requirements and standards necessary for managing the traffic effects during construction so that safe, adequate and convenient routes for local movements by all transport modes are maintained, including access for airport operations, throughout the construction of the Project. In particular, the CTMP shall include: <ul style="list-style-type: none"> a. Construction dates and hours of operation. b. The identification of the two heavy vehicle construction routes (as identified as Route 1 and Route 2 in the supporting Transport Assessment, prepared by Beca, dated August 2022). c. A requirement to co-ordinate and collaborate with WIAL regarding any overlapping WIAL construction activities, namely the freight / logistics hub, to manage and mitigate cumulative construction traffic effects as far as practicable. d. A requirement to co-ordinate with Cyclotek to agree measures to maintain vehicle and pedestrian access for staff and visitors associated with Cyclotek operations, including courier dispatch, waste management and delivery/service vehicles e. Details of over-dimension trucks and/or loads which could not use either Route 1 or Route 2 to access the site and would therefore need to cross the airport taxiway during airport curfew hours. Use of the taxiway requires WIAL approval on a case-by-case basis. f. A requirement to consult with WIAL and seek their agreement to impose a reduced speed limit of 30km/h along Stewart Duff Drive during construction, between the main construction yard and the junction with Moa Point Road. | <i>The Consent Authority raised concerns during the hearing on the impact of concrete pour truck movements on pedestrian crossings on Route 1 during school drop off and pick up times. Whilst the applicant believes these risks would be appropriately managed through Condition 25.1 (as presented at the hearing), an explicit provision is now identified which will require traffic management measures at key school pedestrian crossings at key times during concrete pour activities. The wording of the condition, as amended, is supported by Mr McGimpsey and Ms Zorn.</i> |

| | Final conditions as agreed between Ms Zorn and Mr McGimpsey (Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough). | Additional comments |
|---------|---|----------------------------|
| CON No. | | |
| | <p>g. Notwithstanding Condition 25.1(f) above, a requirement that all construction vehicles are limited to speeds of 30km/h during construction along Stewart Duff Drive between the main construction yard and the junction with Moa Point Road.</p> <p>h. Safety control measures such as fencing, barriers, hoarding, signage and a temporary crossing point.</p> <p>i. A staff travel management plan including a car pool policy and consideration of off-site, off-road parking opportunities.</p> <p>j. A requirement that all construction vehicle drivers are trained and inducted to make them aware of: reduced speed limit along Stewart Duff Drive, as identified at Condition 25.1(f); and the potential for school children crossing the road in certain locations on the identified heavy vehicle construction routes.</p> <p>k. A temporary traffic management plan to avoid potential conflicts between pedestrians and construction traffic on Stewart Duff Drive, either in the form of protected pedestrian routes or via closure of footpaths with alternative routes/diversions provided.</p> <p>l. Measures to maintain vehicle and pedestrian access for staff and visitors to Moa Point WWTP.</p> <p>m. Contact details for appropriate person(s) implementing the CTMP, including after-hours contact details.</p> <p>n. The identification of any construction work which cannot be reasonably undertaken during normal working hours, as per Condition 23.1.</p> <p>o. A plan for the management of pedestrian and vehicular traffic along Stewart Duff Drive where the road is temporarily closed to the public, with alternative routes/diversions provided and associated signage, if appropriate. This plan shall be prepared in consultation with WIAL.</p> <p><i>The following further specific measures are required in relation to concrete truck activities exceeding 100m³ in any one day:</i></p> <p>p. A requirement that heavy vehicles associated with concrete pour activities to use Route 1 only.</p> <p>q. A requirement to co-ordinate with Council regarding any planned construction works on the road corridor along the proposed construction vehicle routes.</p> <p>r. Unless otherwise permitted through Condition 25.1(n), all concrete pour activities and associated heavy vehicle construction traffic to be undertaken within the following time restrictions:</p> <p>i. Outside of the summer break, all work to be undertaken on weekdays only, other than on a Saturday where work shall finish prior to 10:00am.</p> <p>ii. During the summer break, all work shall finish prior to 10:00am on any day, other than Sundays.</p> <p><i>Note: it is anticipated that concrete pour activities will take place outside of normal Construction Work Hours under Condition 23.1 and accordingly, such details shall be set out in the CTMP in accordance with Condition 25.1(n).</i></p> <p>s. Detailed management plan for the management of pedestrian and vehicular traffic along Stewart Duff Drive during this period, which would either require the presence of traffic controllers or a relevant portion of the road is closed to the public, with alternative routes/diversions provided and associated signage.</p> <p>t. <u>Traffic management measures at key school pedestrian crossings along Route 1 between the hours of 8:15am to 9:15am and 2:30pm to 3:30pm, Monday to Friday, during school term times only. These measures must be approved by the CMO, in consultation with the RCA, prior to the commencement of any works involving concrete pour truck movements.</u></p> <p>All construction works shall be undertaken in accordance with the certified CTMP.</p> <p>Once certified, amendments to the CTMP can be made by agreement in writing between the CMO and the Requiring Authority.</p> | |
| 25.2 | <p>The Requiring Authority shall submit a Construction Noise and Vibration Management Plan (CNVMP) which shall be prepared by an appropriately qualified acoustic and vibration specialist. The purpose of the CNVMP is to provide a framework for the development and implementation of measures to avoid, remedy or mitigate adverse construction noise and vibration effects, and to minimise any exceedance of the criteria set out in Conditions 25.3 and 25.4.</p> <p>As required by Condition 23.1, the CNVMP shall also identify any construction work which cannot be reasonably undertaken during normal working hours. The CNVMP shall include details of advance communication to be undertaken with residents prior to commencing activities that are predicted to exceed noise performance standards.</p> <p>The CNVMP shall also identify measures to avoid, remedy or mitigate adverse construction noise and vibration effects on Cyclotek.</p> | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|-----------------------------|-------------|---|----------------------------|-------------|-----------------|--------------|----------|-----------|----|----|-----------|----|----|-----------|----|----|-----------|----|----|-----------|-----------|----|----|-----------|----|----|-----------|----|----|-----------------------------|-----------|----|----|-----------|----|----|-----------|----|----|--|
| CON No. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.3 | | <p>Construction noise shall be measured and assessed in accordance with NZS 6803:1999 Acoustics – Construction Noise’. The construction noise shall, where practicable, comply with the following criteria, except as provided for in the CNVMP:</p> <table border="1"> <thead> <tr> <th>Time of week</th> <th>Time period</th> <th>dB LAeq (15min)</th> <th>dB LAF (max)</th> </tr> </thead> <tbody> <tr> <td rowspan="4">Weekdays</td> <td>0630-0730</td> <td>55</td> <td>75</td> </tr> <tr> <td>0730-1800</td> <td>70</td> <td>85</td> </tr> <tr> <td>1800-2000</td> <td>65</td> <td>80</td> </tr> <tr> <td>2000-0630</td> <td>45</td> <td>75</td> </tr> <tr> <td rowspan="3">Saturdays</td> <td>0630-0730</td> <td>45</td> <td>75</td> </tr> <tr> <td>0730-1800</td> <td>70</td> <td>85</td> </tr> <tr> <td>1800-0630</td> <td>45</td> <td>75</td> </tr> <tr> <td rowspan="3">Sundays and public holidays</td> <td>0630-0730</td> <td>45</td> <td>75</td> </tr> <tr> <td>0730-1800</td> <td>55</td> <td>85</td> </tr> <tr> <td>1800-0630</td> <td>45</td> <td>75</td> </tr> </tbody> </table> | Time of week | Time period | dB LAeq (15min) | dB LAF (max) | Weekdays | 0630-0730 | 55 | 75 | 0730-1800 | 70 | 85 | 1800-2000 | 65 | 80 | 2000-0630 | 45 | 75 | Saturdays | 0630-0730 | 45 | 75 | 0730-1800 | 70 | 85 | 1800-0630 | 45 | 75 | Sundays and public holidays | 0630-0730 | 45 | 75 | 0730-1800 | 55 | 85 | 1800-0630 | 45 | 75 | |
| Time of week | Time period | dB LAeq (15min) | dB LAF (max) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Weekdays | 0630-0730 | 55 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 0730-1800 | 70 | 85 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 1800-2000 | 65 | 80 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 2000-0630 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Saturdays | 0630-0730 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 0730-1800 | 70 | 85 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 1800-0630 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Sundays and public holidays | 0630-0730 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 0730-1800 | 55 | 85 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 1800-0630 | 45 | 75 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.4 | | A suitably qualified acoustic expert shall be engaged to assess and manage construction vibration. The vibration criteria set out in Table 4 of DIN 4150-3 2016 shall be met, where practicable. Where it is not practicable to achieve those criteria, the CNVMP shall set out how those activities will be undertaken to minimise, monitor and manage potential vibration effects. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25.5 | | All earthworks, including the extraction of the below ground fuel tank, to be undertaken in accordance with the Contaminated Soils Management Plan (July 2022), prepared by Beca Limited. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25A. Geotechnical | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25A.1 | | <p>Prior to the commencement of the northern section of the former quarry headwall, the Requiring Authority shall commission a Geotechnical Assessment Report (GAR) of any land that is to be disturbed. The GAR shall be provided to Council for certification at least 20 working days prior to any earthworks occurring. The GAR must be undertaken by an experienced “Geotechnical Professional” and as a minimum contain, but not be limited to, the following:</p> <ul style="list-style-type: none"> • A review of all available geotechnical reports for the site including the Geotechnical Interpretive Report by Connect Water (dated November 2020, reference 6511521/1916). • A summary of the ground conditions interpreted from geotechnical investigations. • A geotechnical analysis of the design concept and resulting recommendations that will mitigate any potential adverse effects. <p>The purpose of the GAR shall be to demonstrate geotechnical soundness and resilience of the earthworks and surrounding slopes.</p> <p>A ‘Geotechnical Professional’ is defined as a Chartered Professional Engineer (CPEng) with specialist geotechnical skills and experience in the design, construction and monitoring of excavations in similar ground conditions as the proposed project or work.</p> <p>A Geotechnical Professional shall be engaged for the detailed design and construction phases of the project or work.</p> <p>The name and the contact details of the Geotechnical Professional shall be provided to Council as part of the GAR required by this condition.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 25A.2 | | <p>Geotechnical Professional will monitor the earthworks. The Geotechnical Professional will advise on the best methods to ensure:</p> <ul style="list-style-type: none"> • The methods to ensure the stability of the site and surrounding land • The construction of cut faces, fill batters, staging, shoring, and benching as required for stability of the earthworks, • the design and construction of the temporary and permanent earthworks, retaining structures and drainage, are consistent with the recommendations from the geotechnical report as part of condition 1 above. | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey (Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough). | Additional comments |
|---|--|---|--|
| CON No. | | | |
| | | The Requiring Authority must follow all the advice of the Geotechnical Professional in a timely manner. | |
| 25A.3 | | Either a copy of the producer statement 'PS4 – Construction Review' or, an earthworks completion report for land development prepared under Schedule 1C of the NZS 4404:2010 shall be and its accompanying documents for structures/buildings required for the stabilisation of earthworks and, prepared for the associated building consent process, must be provided to the Council's Compliance Monitoring Officer within one month of the structures/buildings <u>earthworks</u> being completed. | <i>This amendment seeks to provide for an additional means of compliance with respect to the completion of earthworks.</i> <i>Mr McGimpsey and Ms Zorn support this change.</i> |
| 26.Erosion & Sediment Control Plan | | | |
| 26.1 | | The Requiring Authority shall provide at least 10 days prior to the commencement of works on the site a copy of the ESCP certified by Greater Wellington Regional Council to the CMO for their records. <i>Advice Note: The GWRC resource consent for earthworks requires the ESCP to be prepared in consultation with WIAL with regard to dust management controls.</i> If during the construction period any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification. | |
| 27.Ecology | | | |
| 27.1 | | At least 10 days prior to any works on land identified as lizard habitat, the Requiring Authority shall provide a copy of the Lizard Management Plan and a copy of the permit obtained under the Wildlife Act 1953, as certified by the Department of Conservation, to the CMO for their records. The Requiring Authority shall undertake the SMF works in accordance with the Lizard Management Plan and any conditions of the permit obtained under the Wildlife Act 1953. | |
| 27.2 | | Where practicable, the clearance of vegetation and loose rock along the toe of the embankment shall avoid avifauna breeding season (July to February). Where such works cannot be avoided within the avifauna breeding season, the following provisions apply: a. Within one prior to such works occurring, a visual inspection shall be carried out by a suitably qualified ecologist to confirm the presence or absence of active penguin nests or the nests of other native birds; b. If an active penguin or other native bird nest (other than black backed gull) is discovered, the area must be cordoned off with a 100m buffer. No works shall occur within this buffer until the nesting birds have fledged, or the nest has been naturally abandoned. | |
| 28. Community Liaison and Complaints Register & Mana Whenua Engagement | | | |
| 28.A1 | | The Requiring Authority shall engage with Taranaki Whānui and Ngāti Toa Rangatira and undertake the following: <ul style="list-style-type: none"> • invite Taranaki Whānui and Ngāti Toa Rangatira to confirm their desired level of involvement in the SMF and, if requested by either iwi: <ul style="list-style-type: none"> i. offer to meet regularly prior to and throughout the duration of the SMF construction and operation. ii. Inform iwi about progress with management plans and to provide an opportunity for feedback on any draft management plan or outline plan prior to submission to the CMO or WCC. iii. provide an opportunity for feedback on the draft Commissioning Odour Management Plan prior to submission to the GWRC. iv. consider any issues raised by iwi relating to compliance with designation conditions, including management plans and outline plans. v. be responsible for working with iwi to identify opportunities for the proposal to create education opportunities for Māori associated with the project. | |
| 28.1 | | The Requiring Authority shall work with Wellington Water Limited and the Community Liaison Committee (as originally established in association with the Moa Point WWTP) to review the frequency of CLC meetings, membership and current terms of reference to ensure it is able to adequately provide an effective forum for the construction phase and ongoing operations of the SMF. The Requiring Authority shall: <ul style="list-style-type: none"> • Invite parties interested in the SMF to become members of the Community Liaison Committee (CLC). • offer to attend regular CLC meetings prior to and throughout the duration of the SMF construction and operation. | |

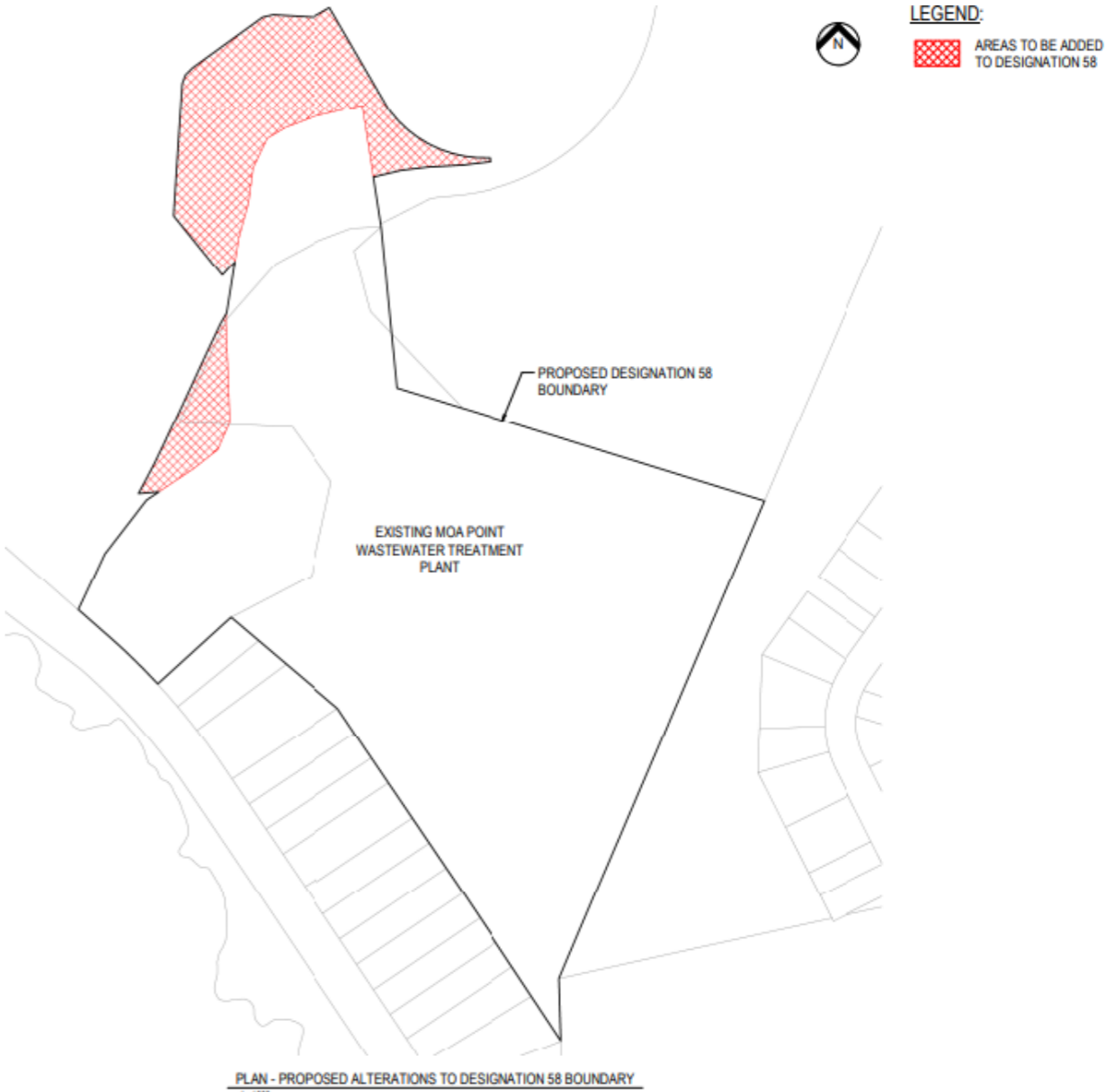
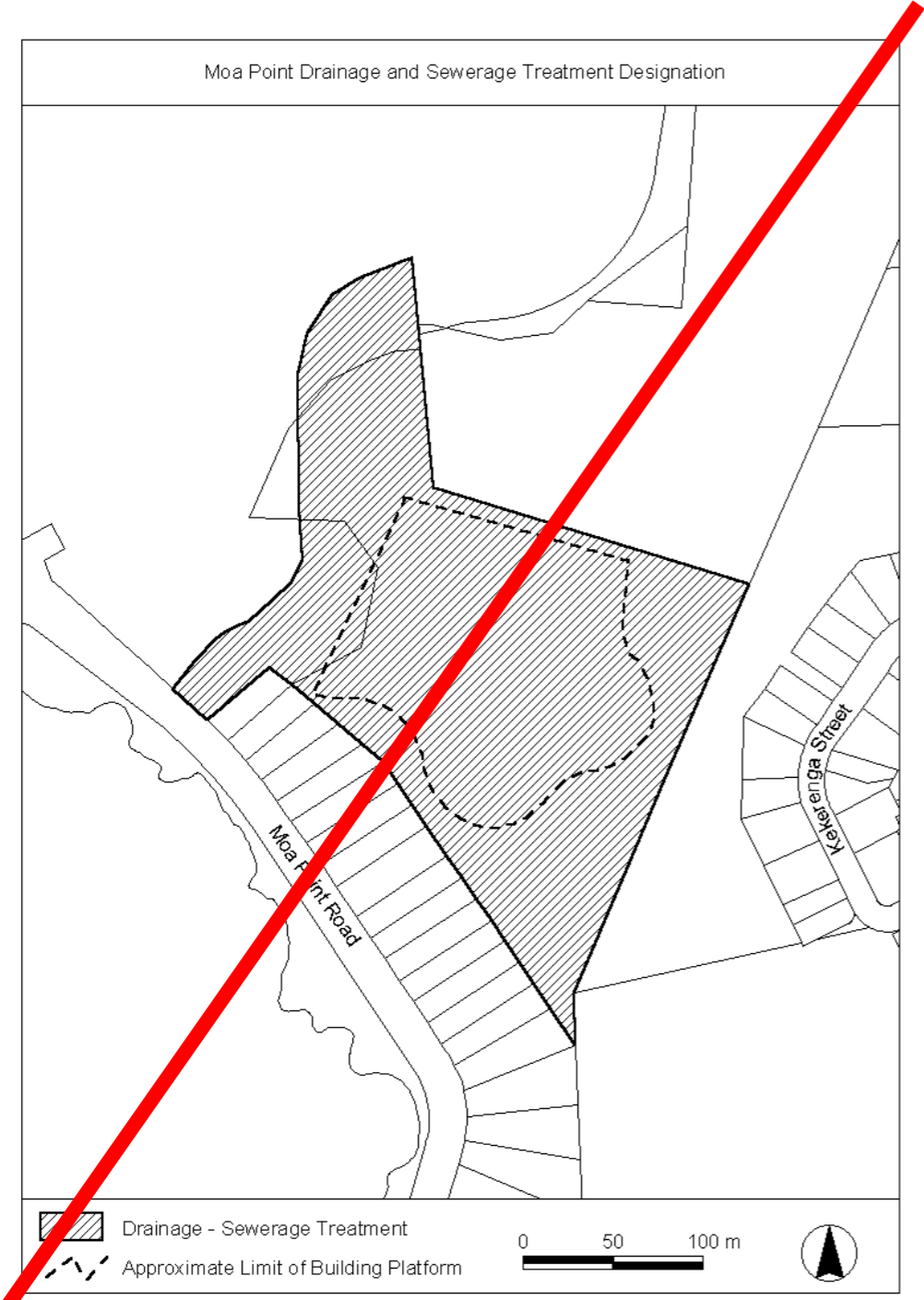
| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments | | | | | | | | |
|--|-------------------------|---|---------------------------------|-------------------------|--------------------------------------|-------------------------|------------------------------------|-------------------------|------------------------------------|--------------------|--|
| CON No. | | | | | | | | | | | |
| | | <ul style="list-style-type: none"> inform the CLC and its members about progress with management plans and to provide an opportunity for feedback on any draft management plan or outline plan prior to submission to the CMO or WCC. provide an opportunity for feedback on the draft Commissioning Odour Management Plan prior to submission to the GWRC. consider any issues raised by the Community Liaison Committee relating to compliance with designation conditions, including management plans and outline plans. be responsible for working with the Community Liaison Committee to identify opportunities for the proposal to create education opportunities associated with the project. | | | | | | | | | |
| 28.2 | | <p>At all times during the Works, the Requiring Authority shall maintain a permanent register of any complaints received alleging adverse effects from, or related to, the Works. The register shall include:</p> <p>a) The name and address (where this has been provided) of the complainant;</p> <p>b) The nature of the complaint;</p> <p>c) Location, date and time of the complaint and also of the alleged event;</p> <p>d) Weather conditions at the time of the event and including wind direction and approximate wind strength if the complaint relates to air quality or noise;</p> <p>e) The outcome of the Requiring Authority's investigation into the complaint;</p> <p>f) Measures taken to respond to the complaint; and</p> <p>g) Any other activities in the area, unrelated to the construction, which may have contributed to the complaint (such as non-Project construction, fires, traffic accidents or unusually dusty conditions generally).</p> | | | | | | | | | |
| 28.3 | | <p>In response to any complaints received, the Requiring Authority shall:</p> <p>a) Acknowledge the complaint within 2 Working Days.</p> <p>b) Promptly investigate, identify the urgency associated with the complaint and communicate that to the complainant.</p> <p>c) Take reasonable steps to remedy or mitigate the matters giving rise to the complaint if there are reasonable grounds for the complaint within 10 Working Days of receiving the complaint or such sooner time as may be reasonably necessary in the circumstances.</p> <p>d) Maintain a record of its responses and any remedial actions undertaken.</p> <p>e) This record shall be maintained on site and shall be made available upon request.</p> | | | | | | | | | |
| 29. Accidental discovery protocol | | | | | | | | | | | |
| 29.1 | | <p>If koiwi, taonga, or other archaeological material is discovered in any area during the works, work shall immediately cease and the requiring authority shall contact Taranaki Whānui, Ngāti Toa Rangitira, Heritage New Zealand and Wellington City Council within 24 Hours. If human remains are found, the New Zealand Police shall also be contacted. The requiring authority shall allow the above parties to inspect the site and in consultation with them, identify what needs to occur before work can resume.</p> <p><i>Note: Evidence of archaeological material may include burnt stones, charcoal, rubbish heaps, shell, bones, old building foundations, artefacts and human burials.</i></p> | | | | | | | | | |
| 30. Operational Noise | | | | | | | | | | | |
| 30.1 | | <p>Sludge Minimisation Facility operational noise emission levels when measured at or within the boundary of any residentially zoned site, shall not exceed the following noise limits:</p> <table border="1"> <tbody> <tr> <td>All days 7.00am to 7.00pm (day)</td> <td>55 dB $L_{Aeq(15 min)}$</td> </tr> <tr> <td>All days 7.00pm to 10.00pm (evening)</td> <td>50 dB $L_{Aeq(15 min)}$</td> </tr> <tr> <td>All days 10.00pm to 7.00am (night)</td> <td>45 dB $L_{Aeq(15 min)}$</td> </tr> <tr> <td>All days 10.00pm to 7.00am (night)</td> <td>75 dBA L_{AFmax}</td> </tr> </tbody> </table> <p>Noise measurements shall be measured in accordance with NZS 6801:2008 "Acoustics – Measurement of environmental sound". Noise measurements shall be assessed in accordance with NZS 6802:2008 "Acoustics - Environmental Noise".</p> | All days 7.00am to 7.00pm (day) | 55 dB $L_{Aeq(15 min)}$ | All days 7.00pm to 10.00pm (evening) | 50 dB $L_{Aeq(15 min)}$ | All days 10.00pm to 7.00am (night) | 45 dB $L_{Aeq(15 min)}$ | All days 10.00pm to 7.00am (night) | 75 dBA L_{AFmax} | |
| All days 7.00am to 7.00pm (day) | 55 dB $L_{Aeq(15 min)}$ | | | | | | | | | | |
| All days 7.00pm to 10.00pm (evening) | 50 dB $L_{Aeq(15 min)}$ | | | | | | | | | | |
| All days 10.00pm to 7.00am (night) | 45 dB $L_{Aeq(15 min)}$ | | | | | | | | | | |
| All days 10.00pm to 7.00am (night) | 75 dBA L_{AFmax} | | | | | | | | | | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments |
|--|--|---|----------------------------|
| CON No. | | | |
| 31. Air Quality | | | |
| 31.1 | | At least 10 days prior to the commencement of the Commissioning Phase of the SMF, the Requiring Authority shall provide a copy of the Commissioning Odour Management Plan (COMP) certified by Greater Wellington Regional Council to the CMO for their records. If, prior to the commissioning phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification. | |
| 31.2 | | At least 10 days prior to the commencement of the Operations Phase of the SMF, the Requiring Authority shall provide a copy of the Operational Odour Management Plan (OOMP) certified by Greater Wellington Regional Council to the CMO for their records. If, prior to the operation phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification. | |
| 31.3 | | At least 10 days prior to the commencement of the Operation Phase of the SMF, the Requiring Authority shall provide a copy of the Operation and Maintenance Plan (OMP) certified by Greater Wellington Regional Council to the CMO for their records. If, prior to the operation phase any changes are made to the certified plan that requires the recertification of GWRC, then a copy of the revised certified plan shall be provided to the CMO within 5 working days of receiving confirmation of the recertification. | |
| 31.4 | | There shall be no noxious, dangerous, offensive or objectionable odour or particulate matter discharged to air to the extent that it causes an adverse effect at or beyond the boundary of the site during commissioning or operation of the SMF. For the purposes of this condition, the boundary of the site is as defined in Plan 7 (ref. 3258521-DA-000-K0113) attached to this designation. | |
| 31.5 | | The Requiring Authority must operate the SMF in such a manner that the generation of dust is kept to a practicable minimum. In any case there must be no particulate matter beyond the boundary of the site that has an objectionable or offensive effect as a result of the activities authorised by this designation. For the purposes of this condition, the boundary of the site is as defined in Plan 7 (ref. 3258521-DA-000-K0113) attached to this designation. | |
| 32. Operational and Maintenance Traffic | | | |
| 32.1 | | The Requiring Authority shall submit an Operational Traffic Management Plan (OTMP). The purpose of the OTMP is to manage transport safety effects of operational traffic. Matters to be considered by the OTMP shall include: <ul style="list-style-type: none"> • alerting other road users and pedestrians along Stewart Duff Drive of truck movements occurring in association with the operation of the SMF • managing the potential for conflict between HGVs travelling in both directions along the Moa Point WWTP access road • managing the potential for conflict with other road users and pedestrians where HGVs need to reverse on to Stewart Duff Drive • managing the potential for conflict with other road users and pedestrians where site access/egress points along Stewart Duff Drive are extensively wide and/or have limited sightlines The OTMP must also include details of how SMF HGV drivers will be inducted/trained to assist in mitigating the transport safety effects of the operation of the SMF. | |
| 32.2 | | A Maintenance Period Traffic Management Plan (MPTMP) for the scheduled 1 week annual shutdown / maintenance period of the SMF shall be submitted to the CMO for certification at least 20 working days prior to the first planned maintenance activity during operations. The purpose of the MPTMP is to manage transport and parking effects of service and staff vehicles during maintenance periods alongside normal operational truck movements. | |

| | | Final conditions as agreed between Ms Zorn and Mr McGimpsey <i>(Changes from wording proposed in supplementary evidence of Mr McGimpsey, and now agreed by Ms Zorn marked up in red/underline or strikethrough).</i> | Additional comments |
|----------------|--|---|----------------------------|
| CON No. | | | |
| | | <p>The MPTMP shall include the schedule of the maintenance activity and the normal truck operation, anticipated movements of the maintenance trucks and the management of temporary on-site parking.</p> <p>The MPTMP shall be implemented and maintained throughout each annual maintenance period for the SMF to manage potential adverse transport and traffic effects arising from maintenance activities to the greatest practicable extent.</p> <p>Once certified, amendments to the MPTMP can be made by agreement in writing between the CMO and the Requiring Authority.</p> | |

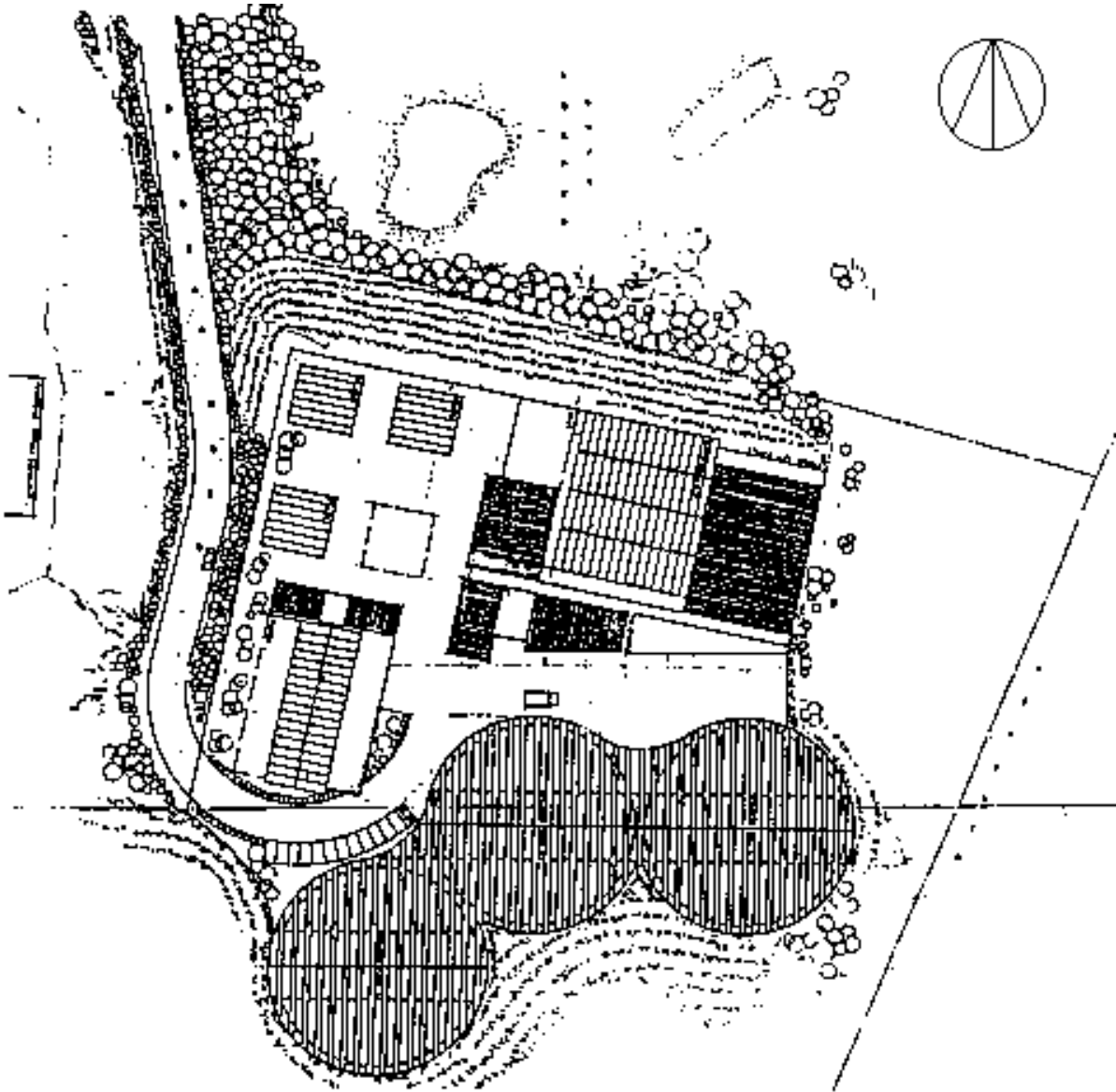
Moa Point Drainage and Sewage Treatment Designation

Plan 1 - Detail of Designation



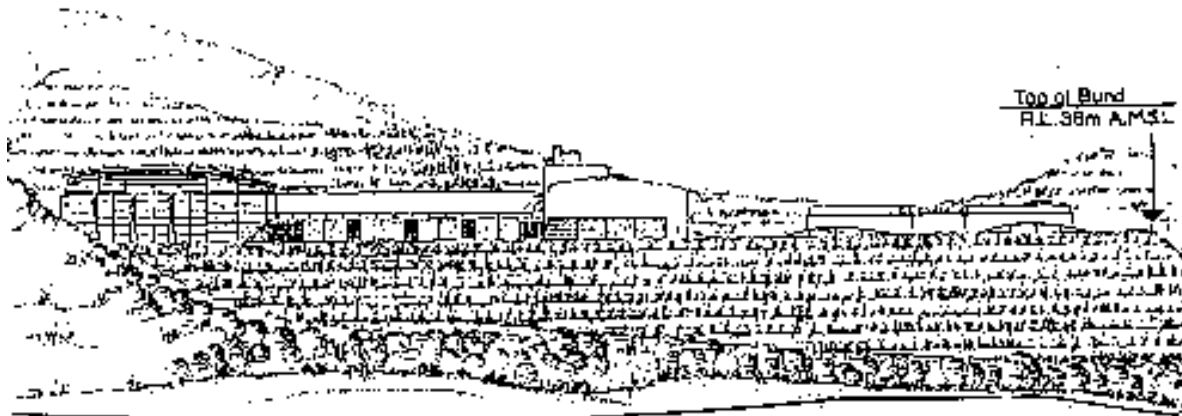
Moa Point Drainage and Sewage Treatment Designation

Plan 2 - Plan of Proposed Plant

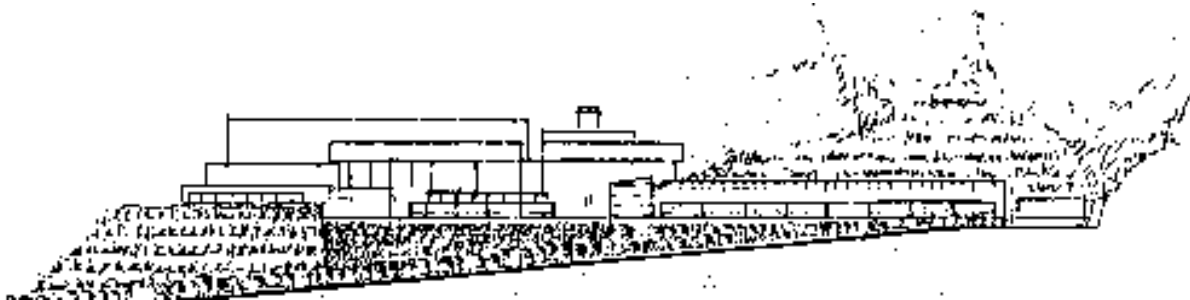


Moa Point Drainage and Sewage Treatment Designation

Plan 3 - Elevations of Proposed Plant



North Elevation



West Elevation

Insert new plans:

Plan 4 – SMF General Arrangement Plan - ref. 3258521-DA-000-K0121 Rev A

Plan 5 – SMF Elevation - ref. 3258521-DA-000-K1202 Rev A

Plan 6 – Post-Construction Designation Boundary Plan - 3258521-DA-K0112 Rev A (as referenced at condition 19.3).

Plan 7 – Site Plan of ‘The Site’ boundary – 3258521-DA-000-K0113



MOA POINT WASTE WATER TREATMENT PLANT

GAS BAG

ODOUR CONTROL AREA

ODOUR STACK

MAIN SLUDGE PROCESSING BUILDING 1

DIGESTER

DIGESTER

MAIN SLUDGE PROCESSING BUILDING 2

CYCLOTEK

STEWART DUFF DRIVE

DHL LOGISTICS CENTRE

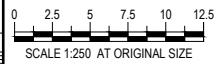
GENERAL ARRANGEMENT PLAN

SCALE: 1:250-A1

FOR CONSENT
NOT FOR CONSTRUCTION

A1 REPRODUCTION SCALE

A3 REPRODUCTION SCALE



| No. | Revision | By | Chk. | Appd. | Date |
|-----|-------------------|-----|------|-------|------|
| A | ISSUE FOR CONSENT | SMB | | | |



| | | | | |
|---------------------|--------------|-------------|----------|----------------------------|
| Original Scale (A1) | Design | P.McGIMPSEY | 07.07.22 | Approved For Construction* |
| NTS | Drawn | S.BINDON | 07.07.22 | Date |
| Reduced Scale (A3) | Dwg Verifier | | | |
| | Dwg Check | | | |

* Refer to Revision 1 for Original Signature

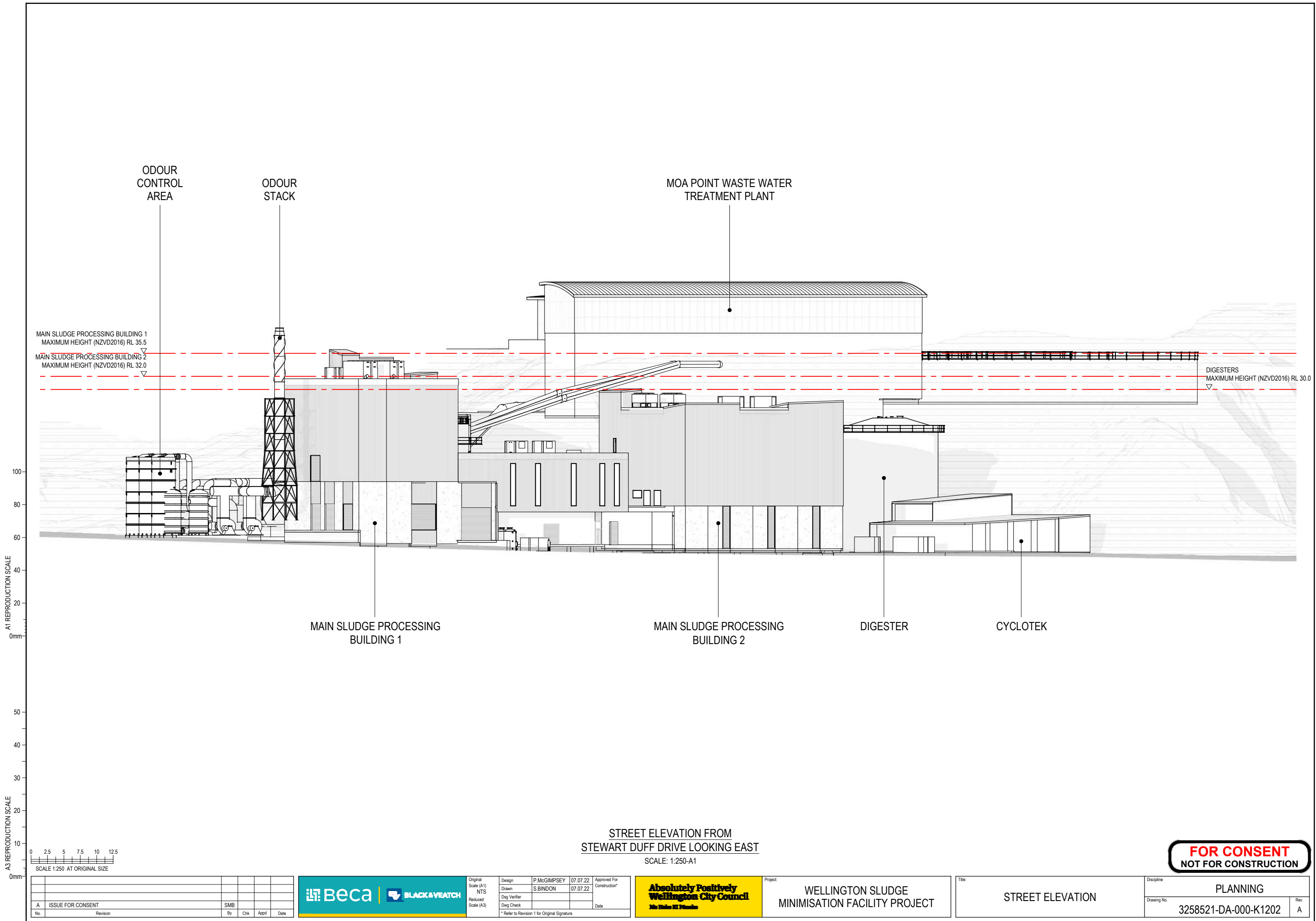


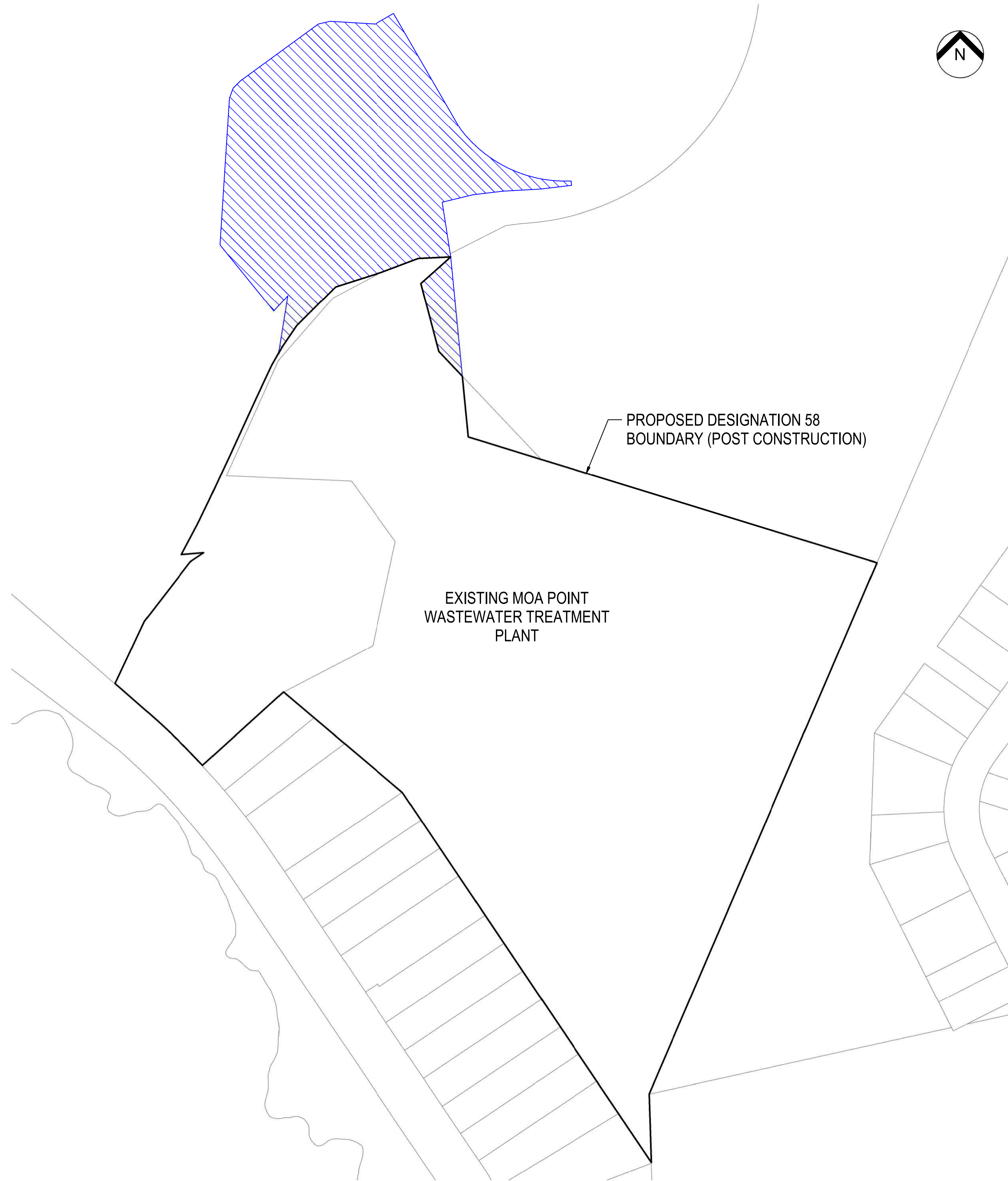
Project: WELLINGTON SLUDGE MINIMISATION FACILITY PROJECT

Title: GENERAL ARRANGEMENT PLAN

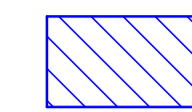
| | |
|-------------|----------------------|
| Discipline | PLANNING |
| Drawing No. | 3258521-DA-000-K1201 |
| Rev. | A |

DO NOT SCALE - IF IN DOUBT ASK





LEGEND:



AREAS TO BE REMOVED FROM DESIGNATION 58 POST CONSTRUCTION

PROPOSED DESIGNATION 58 BOUNDARY (POST CONSTRUCTION)

EXISTING MOA POINT WASTEWATER TREATMENT PLANT

PLAN - PROPOSED ALTERATIONS TO DESIGNATION 58 BOUNDARY (POST CONSTRUCTION)
1 : 1000



| No. | Revision | By | Chk | Appd | Date |
|-----|-------------------|----|-----|------|----------|
| A | ISSUE FOR CONSENT | AW | JB | | 15.07.22 |



| | | | |
|---------------------|----------------|--------------|----------|
| Original Scale (A1) | Design | J. Beardsall | 15.07.22 |
| As indicated | Drawn | A. Warmerdam | 15.07.22 |
| Reduced Scale (A3) | Desig Verifier | | |
| Half A1 | Dwg Check | | |



Project: WELLINGTON SLUDGE MINIMISATION FACILITY PROJECT

Title: PROPOSED ALTERATIONS TO DESIGNATION 58 BOUNDARY (POST CONSTRUCTION)

| | |
|-------------|----------------------|
| Discipline | PLANNING |
| Drawing No. | 3258521-DA-000-K0112 |
| Rev. | A |



LEGEND:

— SITE BOUNDARY

PLAN - SITE BOUNDARY
1 : 1000

**ORIGINAL DRAWING
IN COLOUR**

**FOR CONSENT
NOT FOR CONSTRUCTION**

| No. | Revision | By | Chk | Appd | Date |
|-----|----------|----|-----|------|------|
| | | | | | |



| | | | |
|---------------------|--------------|--------------|----------|
| Original Scale (A1) | Design | R. McAlister | 17.11.22 |
| As indicated | Drawn | A. Warmerdam | 17.11.22 |
| Reduced Scale (A3) | Dwg Verifier | | |
| Half A1 | Dwg Check | | |

**Absolutely Positively
Wellington City Council**
Me Heke Ki Pōneke

Project: **WELLINGTON SLUDGE
MINIMISATION FACILITY PROJECT**

Title: **SITE BOUNDARY**

| | |
|-------------|-----------------------------|
| Discipline | PLANNING |
| Drawing No. | 3258521-DA-000-K0113 |
| Rev. | |