

Ngaio connection proposal

All submitters feedback


31 October – 21 November 2022

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817	Josh	Kelburn	An individual	435
818	Jan Pieter de Jong	Khandallah	An individual	436
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821	Kain	Mount Cook	An individual	439
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Feedback

562

114170181598

NAME: Dave Clingman	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Suggest fitting in one extra parallel P60 car park to the north of the other two proposed at the bottom of Cameron St.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral**Do you have any comments to make about the proposed design?**

Support the uphill bike line and sharrows downhill. Oppose raising the new pedestrian crossing, and strongly oppose adding in extra speed bumps before the crossing. Speed bumps are frustrating for cars, buses, cyclists and other active modes, trucks, and tradie vans. The traffic is already slow towards the new pedestrian crossing because of road steepness, sharp curves, narrower lanes and new signs. You don't need to add extra speed bumps which add annoyance and delays for little safety benefit in this location.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?**Neutral****Do you have any comments to make about the proposed design?**

Support the uphill bike line and sharrows downhill. Oppose raising the pedestrian crossing, and strongly oppose adding in extra speed bumps before the crossing. Speed bumps are frustrating for cars, buses, cyclists and other active modes, trucks, and tradie vans. The traffic is already slow towards the pedestrian crossing because of road steepness, sharp curves, narrower lanes and new signs. You don't need to add extra speed bumps which add annoyance and delays for little safety benefit in this location.

What do you think of the timing of the proposed approach?**The timing of the stages is about right****Why do you think that?**

Businesses and people need time to adjust and to find solutions to parking removal, whether that is moving business somewhere else, building parks on-site, or leasing parks elsewhere.

Feedback

563

114166783269

NAME: Adam Lewis	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

The present route is dangerous and needs to be changed. Cars travel too fast and do not give consideration to cyclists. The roads are too narrow to allow cyclists and cars to travel uphill when there are also parked cars to navigate. Too many very close calls when cars overtake on blind corners up Kenya St.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Don't travel this way but would still support in principle

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

A lot of pedestrians try to cross the road here after leaving the bus at the corner with Perth St. The only consideration will be traffic travelling to fast up Kenya St and around the corner on to Ngaio Gorge Rd, then having to slam on brakes if there was someone using the crossing and cars were stopped.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

With cars parked on the up hill side of the road, it makes it very dangerous for cyclists. Cyclists have to go around a car to get uphill. There is always the worry about being "doored" so a wider berth is required. Cars coming downhill at speed create a choke point around parked cars. Cars heading into town in the morning accelerate to get past without regard to whether there might be a car coming around the corner. I've had some narrow misses.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The removeable option mitigates inconvenience. There is more than enough parking around the area.

Feedback

564

114181245085

NAME: Alasdair Saunders	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

565

114180730184

NAME: Amie	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I want the diversity of businesses to continue to be available to me and I need a carpark to be able to access them.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I absolutely believe the car parks are necessary for these businesses. It appears that the council is trying to penalise small businesses and is not supporting them...as soon as car-parks are removed these businesses become less accessible. In the long term, I don't object to this proposal as strongly as I currently do, but I believe we need to start by significantly improving public transport access for all of the city instead of scraping public transport services. The WCC needs to work closer with Greater Wellington Regional Council to sort a reliable public transport system first.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

I actually don't believe the quantity of traffic in this area of the city justifies the expense of the alterations to the road and parking situation up Cameron Road

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

While I believe it is essential to provide pedestrians with a safe passage across the road at this point of the Ngaio Gorge Road, I don't believe this is a safe location for a pedestrian crossing such as this one. Pedestrians will be taking their lives into their own hands by stepping out onto a pedestrian crossing around a corner where drivers often speed up

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

Many of the parks that are proposed to be eliminated are there for the small businesses located along this street. They are businesses people often pop into and reducing parking will drive people away from stopping at the butchers and to take their cars elsewhere for their servicing. Let's not even mention the impracticality I will personally feel as someone with a disability who won't be able to visit my elderly aunt as there is no parking but the onstreet out the front of her home.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

I think I support this initiative in some areas but not all of the areas proposed. Leave Ngaio Village alone...please don't penalise the small businesses people often stop into on their way through the village, that they wouldn't stop into if they can't have a park.

Feedback

566

114181153438

NAME: Ann Hay	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

I support the cycleway, but think you need to consider parking options in that area. For example, people who use SWET and who are there for 60 + mins, where are their options to park? Are there some spaces on that area you could turn into parks?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Generally support, other than the change from the angle parks reducing to park numbers. Need to consider parking for local business

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The reduction of car parks 60 on Kenya Street is too much. Many properties do not have drive on access and need to park on the street, as well as visitors to the houses. What alternative arrangements for parking have you made? Is this warranted at all times? or could there be clearways for specific times? And not in weekends/evenings when cycle traffic is less. is there some space available where you can create alternative parking for residents? What is the impact on neighbouring streets when residents/visitors need to park there? I support creating a green alternative, but people will still have cars and need to park them somewhere. Part of creating a greener city should include parking solutions rather than just reducing parking and creating stress in other places. And clearly we need better, more reliable and more frequent public transport introduced at the same time as these changes. What support or projects do you have in place?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

yes stages, but also assess usage and whether the clearways are warranted or if the initial time meets the needs.

Feedback

567

114180482623

NAME: Andrew Bartlett	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This looks like a great proposal and should help improve safety for those on bikes. However to gain broad up-take the route shouldn't be cut off by a part-time clearway just shy of Hutt Rd. The clearway changes (to a full time bike lane) need to happen at the start, to see the benefit across the whole route.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

The clearway should be a permanent cycle lane from the outset, and if not then it certainly should be the longer times, and install safer crossings from the outset.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Bus stops should not be in the cycleway. Use raised tables and push the bus into the traffic lane. Cyclists are at risk around a bus as both travel at similar speeds and one is large, has poor sight lines and heavy (and I don't mean the cyclist!)

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Move the car parks to being next to the uphill cycle lane, to remove the downhill dooring risk from riding in the traffic lane.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The changes to remove parking should happen right at the start, no point compromising the whole scheme by not connecting it right at the key point where it links to the rest of the network.

Feedback

568

114174381574

NAME: Andrew Evans	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I do not support the phased three year approach to building this. Why should pedestrians have to wait three years for a crossing?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
Why is there no protection for the cycle lane? I would also request that the 30kph zone be extended past the tennis club towards Crofton Downs to protect young people using the crossing there, and conversion of this crossing to a raised crossing as there have been near misses

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback

569

114179735363

NAME: Ant Burt	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Need to provide separate spaces for cyclists and cars. This will encourage more cyclists

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

The separate lane for cyclist up hill needs to go all the way from Hutt rd to Trelissick crescent. A yellow line will do little. Most drivers are OK there are a minority, but enough to make this a problem, that will ignore any allocated space and continue to pass dangerously. They of course will not be the ones injured or killed. Also a separate pedestrian path all the way up. Cars park in front of garages on the way up which covers the pedestrian space and proposed bike space.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Looks good. Patches of bridle path need to be repaired and others widened to 2x handle bar width as can't safely pass pedestrians or a bike going the other way

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Great Driver education should be included. For example Cyclist are road users too so need to be given 1.5m when overtaking which is rare. Safe ways to open doors so you don't wipe out a cyclist.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Driver education should be included

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Because some drivers seem totally unaware of cyclists and can create dangerous situations. Driver education should be included. For example Cyclist are road users too so need to be given 1.5m when overtaking which is rare. Safe ways to open doors so you don't wipe out a cyclist when parallel parked

Feedback

570

114167914173

NAME: Amanda	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I support all measures to improve public transport and cycling ways in our city and reduce the need to drive

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I think we should not stall in making changes that will make our city more sustainable and provide a better standard of living

Feedback

571

114170547353

NAME: ian	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

how do you stop cars parking between 52-60 Cameron st on the grass verge as this has happened before. will there be some dedicated residents parks on cameron st

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback

572

114161610928

NAME: Brad	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

They are a more effective use of the space and do more than pay lip service to the idea of encouraging safe cycling

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I commute on this road daily on my bike. Putting safe lanes in place and removing car parks would make this commute safer. The type of vehicles that use the car parks are not customers and are employees/long term free car park users that I reckon can find other parking options.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

While i dont use Cameron Street on my bike commute, I see it is heavily used where it meets up with the main road in Kaiwharawhara and introduces a safer option for those commuting from Khandallah.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I have had countless near misses on this section of road on my daily commute, particularly going up hill. I regularly take the lane going downhill here so good to see it is encouraged with road markings. Pedestrian crossing is necessary due to bus commuters needing to cross.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I dont like driving or biking on this road, cars parked either side make this bit of road unsafe for every road user. Like the sharrow idea on downhill, that is a sensible use of the space.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Most of the business owners have off street parking so i would question the impact on businesses. I have seen a lot of long term parking after business hours in these areas and even people commuting into town using these parks.

Feedback

573

114170261525

NAME: Barrie Lucas	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Very much support the changes, although not convinced that there is much change apart from visual signage which hopefully will make a significant enough improvement in safety.

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Likely to get much opposition from drivers/businesses around parking removal. Is there sufficient need/benefit to extend clearway to 2-9pm when the road would be quieter anyway at those additional times (other than making businesses aware of the long term change a year later when clearway is permanent and which business operate after 7pm anyway)?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Never used this road, so can't comment.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Support the changes, but concerned that the biggest danger to cyclists going uphill are the buses that regularly creep into the existing cycle lane (I've physically had to stop cycling to let buses pass when you feel them that close with the back of the bus squeezing the gap to scary margins) and would potentially still do this. The greater visual awareness will be a significant improvement, but not necessarily physically safer. Is there an option to actively report buses that do ignore the signage and drive dangerously. Appreciate that there is limited space to keep bikes/pedestrians separate from cars/buses.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Positive changes. Not a significant difference in physical changes from what there is currently, but removing the parked cars will make it much safer for cars to pass bikes as bikes will not need to move into the middle of the road to go round parked cars. The roundabout at Ottawa/Crofton Road is very dangerous for both cars and bikes as cars coming from Waikowhai St often not even stop and drive straight through. Extending the 30 km/h slightly in preparation of approaching the roundabout (and more signage?) would be very welcome and make it much safer.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Cut out the 2-9pm change and bring full-time clearway timescale forward by a year. Alternatively, change it from 4-7pm to 2-7pm to full-time. What is the benefit beyond 7pm when businesses are finished for the day. Assume there will be massive opposition from businesses and will be the main area of contention on the overall proposal, but delaying the change would just provide more time to avoid preparing for changes.

Feedback

574

114160416382

NAME: Jenny Robertson	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Reducing speed limit is great but will be hard to police.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Ensure there are adequate sight lines from both sides of the road for pedestrians and in both directions for people driving cars.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The current set up is dangerous for people on bikes and micromobility, and pedestrians. Changes to improve the safety of vulnerable road users shouldn't be delayed.

Feedback

575

114160121137

NAME: Bevan Cresswell	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

576

114169973575

NAME: Karen Fisher	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

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Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

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What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

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Submission on proposed changes affecting Cameron Street and Kaiwharawhara Road as part of the Ngaio Connections project

Submitter: Karen & Brent Fisher, 18 Cameron Street, Kaiwharawhara, Wellington 6035. Email [REDACTED]

9 Nov. 22

General

Brent and I both work, Brent in Petone so he takes a car and I work in the CBD so use a combination of bus, electric scooter and walking. In summer we frequently cycle and walk the Hutt Road cycleway both as a cyclist and pedestrian and use to regularly walk the Bridle Track after work but find the number of cyclists and speed they go frightening so do that walk less frequently in commuter timeframes.

We also would like to remind the council that it took 7-10 years for them to remove parking from both sides of Cameron Street. When we moved in parking was allowed on both uphill and downhill sides and was dangerous to cyclists and residents. We contacted the council on many occasions to request add yellow lines to the uphill side. Also the trees that have now been planted on the verge /shoulder are now so tall they obscure the street lights which in winter especially is dangerous as cyclist lights are variable in effectiveness from behind.

Our home is located on the route of the proposed cycle shoulder in Cameron Street, and this has enabled us to directly observe the behaviours of cyclists and vehicles.

Accordingly, we consider we are in a very good position to contribute constructively to the conversation regarding the proposed cycling and pedestrian friendly changes, and parking changes, proposed by the Council for Cameron Street, Kaiwharawhara Road and the Bridle Track.

We are very supportive of cycle lanes and related changes to the transport infrastructure where they make sense and can be accommodated within the constraints of the Wellington environment (narrow streets, relatively hilly) without unnecessary disruption to businesses and the wider community affected by the cycling changes.

Within these constraints it is important to find the right balance between the interests of cyclists, public transport, businesses, commercial and private vehicle owners, and residents. Notwithstanding the trend towards more climate friendly transport solutions, it is inappropriate to categorise the future as a community that will be able to rely exclusively or even predominantly on public transport and cycling. The solutions chosen by the Council must consider the long-term (lifecycle) costs, benefits and be proportionate to the needs of all users and not just a small minority.

The following table sets out our comments on the key changes proposed for Cameron Street and Kaiwharawhara Road.

Proposed	Comments
Cameron Street uphill cycle shoulder: <ul style="list-style-type: none">“Multi Criteria Analysis” dated 23/6/22 describes preferred option as “buffered cycle lane”.	We note the “Project details” link has a picture which shows what was described by Jonathan but without any dimensions.

Proposed	Comments
<ul style="list-style-type: none"> In response to a request for clarification Jonathan Kennett confirmed that the Council is going to work with Waka Kotahi and trial a type of cycle lane that is often used in Europe, and plan to call it a “cycle shoulder” which would be about 1.0m wide with broken white line and green blocks. The intention is that cyclists know they should keep left going uphill, and that people driving know to look out for cyclists but can carefully pull over into the cycle shoulder if they need to allow a vehicle coming downhill to pass. 	<p>In response to a request for clarification we now understand that the cycle shoulder will likely be 0.75m wide.</p> <p>Although discussions with several cyclists suggests no cycle lane/shoulder is necessary, we are comfortable with a 0.75m wide cycle shoulder.</p>
<p>Cameron Street uphill cycle shoulder - Vegetation to be cut back up to 2.2m high to improve visibility:</p> <ul style="list-style-type: none"> “Cameron Street, General Arrangement Plan, Sheet 2” indicates that “Vegetation to be cut back up to 2.2m high to improve visibility”. In response to a request for clarification in respect of how far the vegetation will be trimmed (kerb line or behind kerb line) Jonathan Kennett confirmed on 4/11/22 that the detail will be decided following advice from horticulture team, but happy to hear what residents would like. He imagines WCC will want to trim 0.5-1.0m from kerb line but will need to ensure it does not damage the shrubs. 	<p>We note that the vegetation was trimmed on Friday 4/11/22 to the kerb line. This will need to be done more frequently than currently going forward.</p> <p>While we suggested that the vegetation should be cut back to improve visibility, we were thinking that it should be cut back behind the kerb line sufficiently to allow for re-growth before re-trimming once vegetation encroaches on the road.</p> <p>We suggest vegetation should be trimmed to around 0.2-0.3m behind the kerb, and at a frequency to ensure it does not encroach on the road below 2.2m.</p>
<p>Proposal for cycle shoulder and downhill sharrows painted on Cameron Street</p>	<p>We are comfortable with painted sharrows on the uphill cycle shoulder.</p> <p>However, we do not agree with painted sharrows on the downhill section as they are unnecessary and will make it unsafe for cyclists when it is wet. As an ex-motor-cyclist this is very important.</p>
<p>No Stopping Restriction (broken yellow lines) around the three corners between Cameron Street and the Bridle Track</p>	<p>We agree with the proposal to establish a No Stopping Restriction using broken yellow lines on the three corners between Cameron Street and the Bridle Track.</p>
<p>New Give Ways at bottom of Fore Street, Marsh Way, and intersection with Cameron Street</p>	<p>We are comfortable with the proposal for three new Give Ways to improve safety. The Fore Street intersection is ‘blind’ so we suggest some ability to see around the corner would be ideal.</p>
<p>Signs directing cyclists to cross private property to access the Bridle Track from Cameron Street</p>	<p>This needs to be resolved.</p> <p>When the Council has been approached on multiple occasions to maintain the section of road between Cameron Street and the Bridle Track, including when Cameron Street was resealed last summer, the Council has maintained it is private property and not the responsibility of the Council.</p>

Proposed	Comments
	<p>If the Council wants to direct cyclists to cross the private property, then it needs to find a solution acceptable to residents who have a direct interest in the private property and the associated easements for the purposes of right of way and services.</p>
<p>Bridle Track:</p>	<p>Anybody who walks up or down the Bridle Track and encounters cyclists will know of the dangers.</p> <p>With the Council promoting greater use of the Bridle Track by cyclists, it must consider and implement improvements in the surface (widening, additional fencing, surface cleaning and maintenance) and signage to make the track safer for pedestrians and cyclists.</p> <p>Often when walking the Bridle Track we have been 'spooked' by cyclists without any warning, often going too fast.</p> <p>It is only a matter of time before there is a serious incident involving a cyclist or cyclist hitting a pedestrian.</p>
<p>Cameron Street parking restrictions:</p> <ul style="list-style-type: none"> Changes to parking restrictions on the downhill side resulting in a combination of P60, P120 and P24hr carparks "Proposed roll out of changes" includes the words "39x P24-hr spaces except residents". In response to a request for clarification Jonathan confirmed that the "except residents" is an error and the website will be updated. If residents get annoyed with long-stay non-resident parking they can call WCC and get the cars ticketed and towed, but if no one calls WCC it is unlikely parking wardens will go to Cameron Street. Jonathan noted that residents did not want a residential parking scheme. "Changes in response to stakeholder feedback" includes a bullet point "Removing two carparks at the bottom of Cameron Street to make it safer for everyone" 	<p>In relation to the 39x P24hr carparks we agree residents do not want a residential parking scheme, although we did suggest a "P24hr except residents" restriction may be useful (consistent with streets near the airport). In any event, we are comfortable with a P24hr restriction without an exception for residents on the basis we can call the Council and get cars ticketed and towed if we get annoyed with long stay parking by non-residents.</p> <p>Removing two carparks at the bottom of Cameron Street is not identified on the "Cameron Street, General Arrangement Plan, Sheet 2". We are strongly of the view that the P60 carparks (15m of carparking space, 3x carparks?) at the bottom of Cameron Street on the downhill section between the entrance to Te Rau Ora (2 Cameron Street) and the existing broken yellow lines connecting with Kaiwharawhara Road could be removed and converted to a 'No Stopping Restriction' with an extension of the broken yellow lines. This section of road is a hazard for cyclists and vehicles alike as downhill vehicles are forced over the centre line to the opposite side of the road causing many near misses.</p> <p>Finally, we note the parking analysis for Cameron Street has included private rights of way (Curnow Street, Sargeson Way, Marsh Way) and suggested they provide 41x unrestricted parking spaces for non-residents. The rights of way are private property and only property owners (or guests with</p>

Proposed	Comments
	their approval) may park in these rights of way. Therefore they should not be included in the count.
Cameron Street change 50 km/hr to 30km/hr for safer speeds on Cameron Street	We agree this change makes sense.
Kaiwharawhara Road uphill cycle lane proposal: <ul style="list-style-type: none"> • Stage 1 (2023) - create a clearway 4pm-7pm in 2023 • Stage 2 (2024) - extend clearway times to 2pm-9pm • Stage 3 (2025) – replacing clearways with a separated cycle lane 	<p>We support the proposal for a shared cyclists/bus clearway from 4pm to 7pm.</p> <p>We do not support (at this point) the proposed Stages 2/3 changes outlined for 2024/2025.</p> <p>Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025.</p> <p>The staged approach is proposed “to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road”, relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks.</p> <p>We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use.</p> <p>Accordingly it would be inappropriate for Council to support Stages 2/3 until the business/safety case supported by evidence is prepared and consulted on.</p>

Feedback

577

114164498868

NAME: Karen	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

There are significant business in the area and workers travelling by public transport is not viable you are putting business at risk of having to close their doors. A number of people live on Kaiwharawhara Road and will have no where to park due to only 30 minute parking

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

This proposal does not support business and residents living in Kaiwharawhara Road. Making a clear way during these times on Saturday and Sunday makes no sense as bus and bike traffic minimal

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Where are workers in businesses in area able to park?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Strongly oppose
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly oppose
Do you have any comments to make about the proposed design?
Impact on residents and businesses significant

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
This is a loaded question I don't agree with any of these options

Feedback

578

114160330168

NAME: Brett Halkett	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

579

114166370037

NAME: Bronwen Wall	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

This will make it much easier for me to cross the road to connect with my bus into and from town.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I often hook onto the footpath in the uphill direction when I'm on my bicycle to avoid slowing traffic coming behind me (I've had a car overtake me on a dangerously narrow section). This change will really help improve safety for me and other road users.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

580

114169286155

NAME: Bruce Richards	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Extend the 30km/hr zone to include the pedestrian crossing on Waikowhai St just south of the Ngaio roundabout.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Install signage advising vehicles leaving the Kaiwharawhara business area of cyclists being present

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?
Install a solid barrier at ground level to protect the cycle lane. Plastic bollards by themselves will be quickly knocked over especially on the left handed corners with vehicles cutting the corner.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
Extend the project area and Raise the existing pedestrian crossing outside the tennis courts to the railway station and include that area in the 30km speed zone.

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Stage with stage two. Business in that area are already familiar with clearways along Kaiwharawhara rd.

Feedback

581

114161319825

NAME: Eric	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Protection for the bike lane would make it safer to use and prevent parking in it

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

582

114179554753

NAME: Cameron Jaine	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

It will really help the pedestrians using the crossings around Ngaio with car traveling slower it will be easier to cross the roads

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
I use the crossing everyday to get to the train station and this would really help

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback

583

114161009676

NAME: Carolyn Shivanandan	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I use the bridal path for walking/running - can you please install some signs to remind cyclists not to speed down the bridal path/to give way to pedestrians. Thanks

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?
I am SO pleased you are proposing a pedestrian crossing. This will make it so much safer there.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
Overall, strongly support. I am very pleased to see the crossing being raised - it is very dangerous and have personally witnessed a person being injured there. Also pleased with the 30km/h. I think the removal of the car parks will cause chaos, but I'm sure we will eventually learn to live with it.

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
I think that sounds reasonable

Feedback

584

114165652432

NAME: Cassandra Bahr	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

More bike lanes the better!

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

The bikes are going to get stuck behind busses in the bus stop, which is irritating when riding uphill.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

You've got it almost in one go already, just go for it!

Feedback

585

114181150591

NAME: Cate	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I strongly support these proposed changes. It will provide a safer space for cyclists on the road and supports the options for pedestrians and public transport users. Providing a safe route for cyclists where the lane is separate from cars is a great step in altering commuting habits for people. It will encourage those who don't bike to work to give it a go, and make people feel a lot safer on the road.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Love the plan to put in a safer cycle way separating cars from traffic flow on this route. I've cycled and driven this route pretty often and have felt unsafe often when cycling because I've had to pull out into traffic to ride around parked cars.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I haven't ridden this route as often but fully in support of lowering the speed limit and creating a safe space for bikes on the road. I know it is a frequently used path for people commuting on bikes so definitely a good idea to provide safer infrastructure for those people.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I am a big fan of the pedestrian crossing going in where it is. With the bus stop at the edge of Perth street have seen a lot of people struggle to cross safely at the top of the Gorge. I also think this is a good way to slow traffic where people often fly around the corner there. One thing to consider with the sharrows going down the gorge is to have them painted frequently enough along the route. Have had experiences where people driving cars try to overtake unsafely (too close to me or crossing the centreline) along the route when I'm riding at the speed limit anyway. I think this is an ideal solution when drivers respect the road being a shared space for cyclists and cars, at the moment this isn't always the case. A possible solution to this could be lowering the speed limit, though on this route I know cyclists often reach 45-50km/h anyway. Ideally an eventual separate lane for cyclists would be great, though on the gorge I know there isn't really any space for this.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Extending the lowered speed limit is a great idea, very happy with this. Also like the plan to raise the pedestrian crossing, it is one a lot of school students use walking to and from Ngaio Primary and this in combination with the speed limit will make it a lot safer for those crossing. Also a big fan of the idea to remove the roadside parking going up Kenya street. This is a good idea to provide room for cyclists heading towards the gorge. Have often felt unsafe with the speed and proximity of cars overtaking when I'm cycling up this road. I think if it's possible to separate this bike lane with more than just a paint line it will feel a lot safer for those biking the route, but even just taking out the parked cars on this side of the road is a good start, it'll provide more room for cyclists and mean those driving cars won't have to contend with overtaking cyclists on a narrow stretch.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think making these changes means people are going to need time to adjust to them. If people drive to work they'll have to find alternatives for parking at work, or change to riding a bike and making use of the new lanes. These changes can't be expected to happen overnight and people will need an adjustment period. This staged approach provides time to adjust.

Feedback

586

114164365478

NAME: C Burnett	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Doesn't matter how the timing is proposed, all roading projects undertaken by WCC run significantly over time due to inclement weather and general council incompetence

Feedback

587

114167983264

NAME: C. Anderson	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

I understand the need to provide safer cycling as I used to cycle myself. However, I think the aim for this section of road could be ably managed by the use of Clearways during peak hours. Downhill Clearway in the am and Uphill Clearway in the evening peak. Not sure I can see the point of extending clearway times to 9pm. Very very few cyclists travel after 7pm, especially in winter!

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

This is going to make the top of the gorge very narrow for vehicles to pass safely. Currently there are yellow lines in the centre of the road which should be retained. No problem with a pedestrian crossing provided it is adequately signposted and uphill traffic is warned about a potential queue prior to Perth St intersection.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

Providing just one P10 park outside the fish and chip shop is ridiculous! There are so many customers on a Friday night. Some walk, some cycle and some drive. There should be at least 4 x P15 car parks (ever got your order within 10 minutes when its busy??!!)

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Stage 2 Clearway operating hours extension?? Just don't, there should be absolutely no requirement for a 9pm extension.

Feedback

588

114181645435

NAME: Ian Turk	SUBURB: Crofton Downs	ON BEHALF OF: Ngaio Crofton Downs Residents Association	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

This will be a good trial for the area, which will enable practical feedback at completion, which can be included in final designs. The design appears to be a good compromise.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

We support the city wide introduction of cycleways. The Ngaio Connection trial will give our residents a good understanding of how they will work, and impacts before later planned cycle routes have a larger impact right through our suburbs.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

We support the uphill cycle way in Ngaio Gorge and agree these changes will improve road safety

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

The removal of car parking will impact residents in the area. We support the cycleway trial, and assume that some analysis of loss of parking will take place after its completion. In a more permanent solution, is there a possibility of utilising some of the berm on that section for increased road width, which may allow for some parking to be reinstated?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

We understand that the residents and businesses that will be impacted by this change, are by and large satisfied with the timetable.

Feedback

589

114165234937

NAME: Meg	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

With Wellington hills and weather, rising changes is only part of the picture for getting people cycling more. There only so many people who want our can cycle up Ngaio hill, so needs to be easier to take the bike on the bus and the train, or people won't be keen to do it.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Would need a very reliable tree trimming schedule or the cyclists will just get pushed into the road anyway

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

It's a good idea in theory, but bus drivers in Wellington can be very aggressive, and I don't think it's going to be safe for cyclists to be sharing space

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

Are you removing car parks on both sides of the road or just where the cycle path will be? This explanation is insufficient. Have the people living on Kenya Street been consulted?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Staged clearways seems confusing. If you're someone who only goes there every week or two, you're not likely to realize it's suddenly changed again.

Feedback

590

114181635286

NAME: Colin Matcham	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

This is a move in the correct direction, and I do 'support' these changes, as 'Perfect is the enemy of good'. However, in itself this is a very weak proposal. Did the designers and planners of this proposal take children as road users and pedestrians into consideration? This proposal as it stands will not make it safe for children to cycle to and from school on Kenya Street, and New Zealand laws prevent them from cycling on the pavement (but allows adults on scooters on the pavements). Private citizens do not have a 'right' to store their vehicles on public roads - roads should be prioritised for transport, not private vehicle storage. Removing all parking on Kenya would allow enough space for bikes, buses, cars and pedestrians to travel safely. Green paint on the road does not magically protect people or change behaviours. (Just look at the number of cars in Wellington that stop for traffic lights on the advanced cycle stop boxes!).

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I strongly support an uphill bike lane up Ngaio Gorge Road, however I feel that it needs to be a properly protected lane. In the initial discussion feedback, I reported instances of large articulated lorries and even buses cutting the corners of left hand turns up the gorge, and having to take evasive action to stop their back wheels going over me. This paint will not prevent that. Nor will it stop the SUV drivers and general traffic from driving far too close and cutting off cyclists. I strongly support the pedestrian crossing, as this will make it much easier and safer to get to/from bus stops. I am less supportive of the image of the bike lane between the two entrances to Trelissick Crescent. We take our 7 and 10 year olds for bike rides around the loop of Trelissick Crescent, and this area between the two entrances is extremely dangerous. The green paint will not magically keep them safe, and current NZ law prohibits cycling on the footpath (as the green helmeted kid in your image is doing), so we have the option of breaking the law or having them ride in traffic at danger to their lives. I would strongly making this a fully protected strip, or widening the footpath and making it a shared pedestrian/bike lane. Bike speeds uphill would not be a danger to pedestrians (in fact would be a lot slower than the currently legal electric scooters that ride up here).

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Strongly support extending the 30km/h stretch. Strongly support removing car parking. Roads should be designed and prioritised for TRANSPORT and not for private vehicle storage. If all car parking was removed from Kenya street, the road would be wide enough for a fully protected bike lane (or a wide shared pedestrian/cycle lane) in both directions, without affecting vehicle flows. The initial blurb in this consultation states that the Ngaio Conneciton project will make it safer...[to travel] into the city. Was consideration also given to travel in Ngaio itself? The current proposal would not make it safe for our 7 and 10 year-olds to cycle to and from school. I would strongly suggest that the planners and designers of this proposal cycle up and down Kenya Street with a seven year old, to understand the kind of protections we need to give. My strong suggestion would be that : - All parking is removed from Kenya Street - A protected cycle lane is built in both directions.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

591

114179503034

NAME: Jessica Cox	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Safety.

Feedback

592

114165357978

NAME: Ella	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I'm looking forward to feeling safer on my bike in this area

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

593

114173827942

NAME: Dan Byrne	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Please consult with Ngaio School as key community asset. Many school children will be keen to cycle to school if protected bike paths are built.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

The bike lane should be wider. Add a way for bikes to pass stopped buses without leaving bike lane.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Agree with reducing speed limit.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Bike lane should be protected in both directions.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Agree with removing on street parking here. There should be no street parking on the main road here. Bike lane should be protected in both directions.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Making all the changes at the same time less disruptive overall. Changing parking rules again and again seems more disruptive for community.

Feedback

594

114167822966

NAME: Dave Chowdhury	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Given the staged approach and sections where cyclists will have to negotiate around parking I would like to see speeds reduced to 30 km/h until the cycleways are properly established

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
I'd like the 30 km/h limit all the way along Kenya to the top of Ngaio Gorge. While I support a sharrow approach for the downhill, there are still cyclists who will find that unnerving - largely due to the speeds and dangerous overtaking by cars heading downhill.

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I'm happy with the Ngaio Gorge/Kenya section going ahead next year, I think it's better to get the entire proposal in place sooner, rather than having to negotiate still dangerous sections of the route along Kaiwharawhara Rd for another 2-3 years. Given the likelihood of opposition/court action by businesses let's just get it done instead of risking further delay beyond 2025.

Feedback

595

114181291920

NAME: David Bond	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I support the proposed changes. The existing arrangement presents particular hazards for pedestrians and cyclists: 1) Danger for pedestrians crossing the road at the top of the Ngaio Gorge and bottom of Kenya Street. 2) Danger imposed on cyclists by parked cars forcing them out into the traffic stream (particularly for uphill, non-E-cyclists, whose speed-differential from the traffic is greater). Parked cars on both sides of the road in Kenya Street also present a hazard to moving traffic, often forcing it to cross the centre-line in both directions.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Having a bike-lane free of parked vehicles in the uphill direction is necessary for cyclists' safety, because of the speed-differential between cyclists and traffic, compounded with the problem of parked vehicles forcing cyclists out into the traffic stream. But will the proposed uphill cycle lane be compromised outside of the Clearway times? Will the proposed car parks outside 53-57 K'whara Rd interrupt the cycle lane?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

An uphill cycle shoulder on Cameron Street as far as Sargeson Way will be a help for cyclists heading to the Bridle Path, but I feel this is not hugely important because there are already broken yellow lines over this stretch and traffic is generally light and fairly slow already. Formalising the speed limit to 30Km/h will help in this regard. Changing the car parks on the downhill side from unrestricted to P60/P120 or P24hr, I can't see will make much difference for cyclists. Is this proposed restriction for some purpose other than cyclists' safety? The four P60 angle-parks at the bottom of Cameron St are well off the road, and not really a problem for cyclists (unless over-length parked vehicles jut-out into the roadway, which I have never actually seen happen). I can't see why these need to be made into 2x parallel parks unless there is some other reason. Parallel parks create the ever-present hazard of vehicle-doors flying open and cyclists being hurled to the ground (or under the wheels of following traffic). I speak from experience.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

The proposed uphill bike lane will be an improvement on the current narrow shoulder which cyclists use (although I tend to ride up the footpath as this provides proper separation from uphill traffic - but I always defer to any pedestrians I may encounter (very few in reality)). I wonder if a wider, shared or divided path for both uphill cyclists and uphill/downhill pedestrians might be better for cyclists, but I accept that reducing the kerb-kerb road-width for this purpose may not be helpful for buses or trucks. I do have some doubts about the effect on cyclists of a raised pedestrian crossing at 5-7 Ngaio Gorge Road. Having to bump up-and-down over one of these, particularly for uphill cyclists, will be a bit of an imposition. I would prefer to see a 30Km/h speed limit applied from 35 Ngaio Gorge Road all the way down Kenya Street, thus lessening the need for raised pedestrian crossings (which are essentially speed-bumps). There is currently one of these on Cashmere Ave outside Cashmere Ave School and as a cyclist I find it a bit of a nuisance. Better in my view, to formally reduce the speed limit to 30Km/h, rather than keep it at 50Km/h and impose speed-bumps. However if a raised table is seen as important for such pedestrian crossings then can the ramps up-and-down be made longer and less-abrupt close to the kerbs where cyclists will be?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Having a bike-lane free of parked vehicles in the uphill direction is necessary for cyclists' safety, because of the speed-differential between cyclists and traffic, compounded with the problem of parked vehicles forcing cyclists out into the traffic stream. This is the road I live on, and cycle on daily. Parked vehicles are a significant hazard to cyclists along Kenya Street (and also act to push moving-traffic over the centre-line). I strongly support getting rid of it at least on the uphill side, and believe that the 'parking-public' needs to be re-educated as to the presumed 'right' to park private vehicles on a public roadway, in a way that compromises the function of the road. My

only concern is that the proposed measures do not go far enough! The current 50Km/h is too fast for this residential street. I would like to see the speed limit lowered to 30Km/h for the entire length of the street from 35 Ngaio Gorge Road, all the way into Ngaio village. If traffic is to be slowed to 30Km/h by a raised pedestrian crossing (and sharp bend) at the top of the Ngaio Gorge, and slowed to 30Km/h at the bottom of Kenya Street (from the Fish+Chip shop onwards), it seems counter-beneficial for both safety and for the neighbourhood, not to reduce it to 30Km/h between these points also. And doing this may be able to obviate the need for raised pedestrian-crossings at both the top and bottom of this stretch.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Given that this project is meant to be an easily-implementable transitional trial to see how things work out, and to be adjustable (or even reversible) if things do not work out, then I can't see why it shouldn't be done as quickly as possible. However, I accept that objectors may be mollified if incremental changes are given time to bed-in. And given that cyclists and pedestrians have found ways to cope with the present arrangement that has been in place for many decades, another couple of years will probably be endurable. I will continue cycling up the Ngaio Gorge footpath as I have done for the last 37 years! However I would definitely like to see the proposed changes to Kenya Street made as soon as possible. The sheer increase in the amount of traffic over the past few decades, and the growth in the size of many cars, has made Kenya Street significantly more dangerous than it used to be.

Feedback

596

114160279265

NAME: David	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I would like to see more options for carparking at the bottom of Kaiwhara Road, to help offset the loss of 43 parks: on a Saturday morning parking spaces to be able to attend local businesses are often at a premium

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The pedestrian crossing is likely to create undue congestion and risk of hidden queues causing crashes for those heading to town. The bike lanes make the road too narrow at the top of the Gorge. Please confirm that the safety posts currently in the middle of the road at the top of the Gorge will be removed as part of this, as they already pose a hazard

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Where do you propose residents and visitors park their cars? This is a suburban residential street with many homes having minimal off-street parking. The proposal will unreasonably impair residents' use and enjoyment of their homes. Also not supportive of raised pedestrian crossings.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

It is important to allow residents time to adjust to changes. The proposed staging strikes an appropriate balance

Feedback

597

114178455611

NAME: Davuth Thach	SUBURB: Broadmeadows	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

My opposition to the proposal relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the damage to our business. For this part of the route the uphill section needs to be treated in the same way as the downhill section. I am also disappointed with the consultation process. English is my second language and I have been helped with my submission by one of my customers. Until that person prompted Council officials, no-one had come to talk to me about the changes and implications for our business. That happened with just over one week left in the timeframe for submissions. Even after that visit the implications of the changes weren't clear to me, and I had no offer of assistance with a submission. Notes left under our door were hard to follow and were not clear on what was happening and the implications of that for our business. That we would be very badly affected is obvious – Council officials should have talked to us. As a result, we have had no input to current proposals. If I had more time, I know I could get many of our loyal customers (who are directly affected) to also submit to support us but that isn't possible to organize in a few days. The process adopted means that you will not get the depth and strength of feeling from those who are directly and adversely affected by the proposed changes.

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I am the owner of the Kaiwharawhara Café which operates from leasehold premises at level 1 of 15 Kaiwharawhara Rd (southern side). This café has been in operation for many years. I have owned the business since 2018. We operate six days per week (Monday to Saturday), with opening hours of 7am to 4pm weekdays and 9am to 3pm on Saturday. It is a family-run business which we depend on for our livelihood. Our customers include local residents and employees of local businesses. However, our largest customer segment, especially during the week, is trades staff who use the café for breakfast, morning tea and lunch (mixture of takeaway and dine-in). These customers drive to the café and rely on short-term parking for their visit. We have two off-street carparks, but this is insufficient for the number of customers that rely on parking to visit the café. We think we can operate with a clearway on the southern side of Kaiwharawhara Rd from 4pm to 7pm as proposed in Stage 1. Any extension of the clearway period (ie: from 2pm to 7pm) or loss of parking altogether (Stages 2 and 3) will result in the café not being viable. We see no way around this. Lower Kaiwharawhara Rd is a commercial area. All existing parking is heavily used. There is no practical, alternative parking which our customers can use. The northern side of Kaiwharawhara Rd is already a clearway until 9am and is heavily used with customers of other businesses. Cameron St parking is also already heavily used and, realistically, our café customers are not going to look for parking which is some distance from the café. The closure of our business will have dire consequences for our family. We have lease commitments which we need to pay even if the business is closed. This is very real for us – it is not some plan on paper. This is our livelihood. The proposed changes to the Kaiwharawhara Rd section are not based on any evidence (actual or likely bike volume) supporting the need for a separated cycle lane priority over a non-separated cycle lane with clearway times matched to the needs of commuter cyclists. The proposal to automatically transition to a cycle lane with excessive clearway times (2pm-9pm) after one year (in 2024), and/or a separated cycle lane after two years (in 2025), are a disproportionate response to the needs of cyclists versus businesses like ours who depend on on-street parking for staff and customers when there is little or no practical, alternative parking. The objectives of the changes can be achieved through having a 4pm to 7pm clearway for exclusive cycle use on this short stretch of road. While Council officers are unable to provide current cycle usage data, through discussions it appears that uphill use outside the hours of 4pm to 6pm is very low (fewer than an average of 20 cyclists per day). This small section of the overall cycleway can have a clearway from 4pm to 7pm to create space for cycle use and outside this time period be available for on-street parking, with a mixture of loading zone, short term and longer-term options. This is a sensible compromise which better balances the needs of existing users and the objectives of a connected bike network. It is also consistent with what is proposed on the downhill section of the road. Please recognize our position.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

For the reasons set out above, we oppose any changes beyond Stage 1.

Feedback

598

114179515224

NAME: David Stevens	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Most of the changes proposed are ideologically driven and do little to improve public transport. A 4-7pm clearway on Kaiwharawhara Road heading towards Ngaio on weekdays is fine, but that is the broad extent of the changes required. (a morning clearway towards the Hutt Road already exists). Commuter cyclists as well as buses will benefit from a weekday afternoon clearway on K Road, but the proposals for the uphill section bike lane are merely cosmetic - paint doesn't stop cyclists wobbling into traffic lanes nor stop vehicles getting too close to cyclists. Cyclists struggling uphill should be off road - this can be achieved by extending the existing footpath out to where the current white line is and making it a shared pedestrian/cycleway. The numbers of pedestrians walking up or down Ngaio Gorge Road at evening peak hours is minimal so there is plenty of space for a shared off-road path for uphill cyclists and pedestrians in both directions. Ngaio Gorge is not a busy recreational walking or cycle route (except for the occasional packs of lycra-clad riders at weekends), and traffic volumes are light when any recreational cyclists are around. A walking path already exists all the way down Ngaio Gorge Road, and as above it can easily be a shared pathway. Regarding next question re speed limits where there is no online box for comments. - Who is going to police a reduced speed limit on Cameron St? - It is not clear why the 30kmph zone needs to be extended along Crofton Road which is a fairly wide stretch of road. The pedestrian crossing is easily visible. A change to the speed limit will probably not make much difference overall.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Any traffic problems in this area are associated with commuting - vehicles and cyclists. The 4-7pm clearway on Kaiwawharawhara Road therefore only needs to operate on weekdays (excluding public holidays). It makes no sense to have a 4-7pm clearway at weekends and on public holidays as the area is practically deserted.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Painting pretty lines and green patches on the road make very little practical difference to cyclists. The road is already effectively a shared space. Residents presumably do not want loads of people parking long-term in this area, so parking restrictions negotiated with and acceptable to residents should be implemented.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Cyclists continually maintain that cycle lanes should be separated from vehicular traffic. Therefore the existing pedestrian footpath should be widened (out to where the current painted white line is), to provide a shared off-road path for cyclists and the occasional pedestrian. This is not a heavily used recreational walking route so the few cyclists and pedestrians who do use it can easily share the space - the important point is that it would be a raised off-road path separated from traffic. The current proposal only protects cyclists with paint, which is particularly ineffective when cyclists are wobbling a bit as they struggle up the hill. The downhill side is already a shared space between vehicle traffic and bikes. Painting sharrows does not make much difference. Important to note that sharrows should not be painted on bends as painted areas can be slippery for cyclists when wet.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Removing parking spaces will (a) make it more difficult for customers to visit local businesses, and (b) in the case of parking spaces near the top of Kenya St, will only divert car parking into other streets like Trellisick Crescent, to the annoyance of residents there. Removing 59 parking spaces on Kenya St seems excessive and unlikely to be popular with residents. How are raised pedestrian crossings safer? They provide more obstacles for cyclists.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The clearway only needs to operate on weekdays, not including public holidays. It does not make any sense to have a clearway operating from 4-7pm on weekends and public holidays given the lack of traffic at those times. There should be no commitment to extending the clearway operating times nor to a permanent bike lane until after at least a year of the 4-7pm weekday clearway, in order to assess the effectiveness and impact.

Feedback

599

114176019445

NAME: D Wright	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Hi, as a resident of the this section of Kenya Street I am supportive of the overall initiative to achieve the uphill bike lane section. Cars parking on both sides of Kenya Street makes this stretch of road "tight" so removing parks on the uphill side will make a positive contribution to safety. My specific concern is with the removal of the 4 carpark outside 54-58 kenya street given the overall reduction in parking across both sides of the road. If the primary rationale for losing these 4 carpark is better access to the bus stop outside number 52 then to my mind the negatives of this step outweigh the benefits. As many of these car parks need to be retained given the loss of 57 on the lower section of the street. The #26 bus is the only regular user of this bus stop and that is only 5 times a day for perhaps 30 seconds at a time. So the loss of 4 carpark for 24 hours a day to service under 3 minutes total bus access time in a day seems unwarranted. The fact the bus stop is already positioned over a shared driveway servicing 4 properties is evidence of the lack of actual bus stop use. I acknowledge the current layout does not work so my suggestion would instead be to extend the bus stopping area to make use of the single carpark outside number 52 and perhaps sacrifice one carpark at the southern entry to the bus stop, not all 4. These measures would provide the desired clearer entry and exit to the bus stop and preserve at least 2 more carpark in the street. The lack of cars parked on the other side of the street will also reduce the congestion on the curve.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

600

114171330439

NAME: Debbie Stowe-Hunt	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?**Strongly support****Do you have any comments to make about the proposed design?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner.

What do you think of the timing of the proposed approach?**Make the proposed changes more quickly****Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner.

Feedback

601

114171447608

NAME: Chris Owens	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I think this is great and it couldn't come soon enough.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I think it is vital to redesign streets to allow active road users (cyclists, pedestrians) an easier and safer way to navigate this part of the city. I support the changes to the on-street parking. If people are concerned about the loss of parking, alternative transportation modes need to be strengthened, and that includes reliable, convenient, and more frequent bus services. Please be mindful of people with sensory or physical disabilities in the design so that their safety is prioritised, as well as more able-bodied cyclists and pedestrians.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I support the changes to the on-street parking and the reduction of the speed limit to 30km/h.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

This is great. I used to commute along this route by bike and it was my least favourite part due to the narrow, bendy roads and little space for cyclists. This will help increase cyclist safety and pave the way for future road separation.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I support these changes. It is important to provide uphill cycle lanes for cyclists like this and the speed reduction is welcomed.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think the changes should progress more quickly, however I understand the resistance from some businesses. Still, there seems to be a fair bit of off-street parking that is available, and should it be an issue, they may want to consider expanding their capacity, privately. Public space should not be used to store private property (i.e. cars).

Feedback

602

114170392738

NAME: D. Pedlow	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I am wondering if anything could be done to assist those cyclists traveling from Hutt Road who turn right off Kaiwharawhara Rd into Cameron Street. Motorists get annoyed if you hold the lane or keep far right all the way from Hutt Road, especially if they also have cyclists on the left who are heading straight on up to Ngaio. It requires 20 seconds of motorist patience.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I have sent a separate email regarding the Cameron/Marsh/Sargeson intersection. I have seen several near misses at that intersection with cyclists failing to keep left as they come downhill off

the Bridle Track (shrub visibility issues cause them to move right) - plus cars coming up Cameron into Marsh do not treat it as an intersection.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

603

114181469797

NAME: Damian Jacobs	SUBURB: Other	ON BEHALF OF: SWET	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I think there should be other alternatives available. Shift cycle ways away from the road all together? Long term plan of making this work rather than a quick fix (is it though).

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

Have you thought about how this will affect local businesses and surrounding residents? What not make a look long term fixture for cyclists away from roads? Or wait and see what the numbers of cyclists are like, rather than a build it and they will use it mentality?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Nope! Making a narrow road even more narrow.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Nope. Taking away even more car parks.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Or don't make them?! Slow would be much better. See how these changes will actually work or if businesses suffer from it.

Feedback

604

114160131239

NAME: Anna	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

605

114181205214

NAME: David Murphy	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Far too much ratepayers' money being spent on providing a luxury to the minority of Wellingtonians who are cyclists. This is not Amsterdam or Christchurch and cycling in our difficult topography with frequently inclement weather (I have noted how much the total on the cycle counter outside Spotlight falls off when it rains) will never be a general form of transport here. The Council's priorities should be housing the homeless and fixing the shameful state of the city's roads and pipes.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

"Give businesses time to adapt to the changes" sounds like Orwellian Newspeak for "give them time to shut up shop and find something else to do". While I do not have a business there and live only walking distance away, the 2 cafes we frequent on Kaiwharawhara Road will suffer such a loss of business they will be forced to close. One of them is a small family-owned cafe. The Council, by deleting carparks all over the business area seems to be determined to drive retail customers out of town to Petone and the Hutt, which is already happening. Also, while environmentalists promote electric vehicles, this plan seems to ignore the fact that they also need somewhere to park.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

The proposed "cycle shoulder" and "sharrows" are largely a waste of paint and may be dangerously slippery in wet weather. A simple "watch for cyclists" sign at the bottom of Cameron Street should suffice as uphill cyclists are quite visible - a cycle shoulder will not afford them any increased protection. Downhill cyclists should also be speed-limited to say 20kph as some race down at high speeds. Cycles have much thinner tyres and weaker brakes than cars and in a narrow street may not be able to avoid a collision if they suddenly encounter an uphill vehicle on the narrow Cameron Street. The risk is heightened in rain. Cyclists must exercise caution themselves and cannot delegate all aspects of their safety to motorists. As previously discussed with your cycle lane planning team, the proposal to access the Bridle Path via Sargeson Way from Cameron Street is fatally flawed as the Council has no right of way over the private section of Sargeson Way from the inset brick panel to the intersection with the Bridle Path. The Council's right of way in gross crosses Sargeson Way and continues on the public Bridle Path behind nos. 4,6 and 8 Curnow Way. It then crosses the lower part of Curnow Way and continues down to Fore Street. This is the only legal route for pedestrians and cyclists and the Council should not, even now, be directing cyclists and pedestrians from Cameron Street to cross private land to access the Bridle Path, let alone give it pseudo-legal status by incorporating it into the cycle lane. This is an unwarranted arrogation by a government agency of rights over private property. I understand that an approach has been made to the owner of No.1 Curnow Way, which contributes land to this section of Sargeson Way, to discuss the possibility of the Council obtaining permission to use this land for public access to the Bridle Path. It is not possible for this to be done - one owner cannot grant additional rights over any part of their property which is subject to the mutual rights of way appurtenant to 24 properties in Curnow and Sargeson Ways. I suggest that you consult your legal division about this, if you have not already done so. The Bridle Path is narrow and steep with encroaching trees and foliage (including blackberry and gorse), lichen and fallen tree leaves on the surface and is only sporadically and poorly maintained by the Council. It has steep unfenced drops on the eastern side in several sections and is not wide enough for two walkers side-by-side and a cyclist to pass. Frequently walkers on the Path encounter cyclists (particularly moving fast downhill) who don't ring a bell or give any indication that they are there until they are right on top of us. A quick evasive movement by an alarmed walker, particularly if elderly as many are, could result in a fall on the path breaking bones or even many metres down one of the unfenced steep drops at the side of the Path. Your plan states that it is intended to make the route safer and easier for people walking in the area, but it fails dismally in this respect. This is a serious safety issue that must be addressed urgently, not wait until the other aspects of the cycleway project have been implemented. If the Councillors considering this have not walked the Bridle Path to understand the problem first-hand, I urge them to do so. I would be happy to accompany them

I strongly support the proposal to instal no-stopping restrictions around the Cameron/Marsh/Sargeson corners. This will become more important if carparks are deleted in Kaiwharawhara Road as motorists will be forced up Cameron Street looking for parks. We already have trouble with illegal parking in this area. I also support the installation of Give Way signs at the bottom of Marsh Way and on the Sargeson Way/Cameron Street intersection. I would also like to see some demarcation of the edge of Cameron Street across the intersection with Sargeson Way so motorists are given an indication that it is not just a continuation of Cameron Street and they are in fact turning into another street.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

I don't know what "adapt to the change and consider other ways of using off-street parking" means. As commented above, I fear that the businesses will find that their custom will collapse and simply close.

ROW%20Plan%20Sheet%204.pdf

MEMORANDUM OF EASEMENTS IN CROSS			
SHOWN	SERV. TEN.	PURPOSE	GRANTEE
K	Lot 34	GAS	NOVIAGAS LIMITED
Q	Lot 28		
V	Lot 29		
X	Lot 30		
Y	Lot 15		
Z	Lot 42		
AA	Lot 41		
AB	Lot 32		
M	Lot 36		
AC	Lot 37		
AD	Lot 38		
AE	Lot 40		



MEMORANDUM OF EASEMENTS IN CROSS			
SHOWN	SERV. TEN.	PURPOSE	GRANTEE
K	Lot 34	ELECTRICITY	UNITED NETWORKS LIMITED
Q	Lot 28		
V	Lot 29		
X	Lot 30		
Y	Lot 15		
Z	Lot 42		
AA	Lot 41		
AB	Lot 32		
M	Lot 36		
AC	Lot 37		
AD	Lot 38		
AE	Lot 40		
K	Lot 34	TELEPHONE	TELECOM NEW ZEALAND LIMITED
Q	Lot 28		
V	Lot 29		
X	Lot 30		
Y	Lot 15		
Z	Lot 42		
AA	Lot 41		
AB	Lot 32		
M	Lot 36		
AC	Lot 37		
AD	Lot 38		
AE	Lot 40		

MEMORANDUM OF EASEMENTS			
SHOWN	SERV. TEN.	SOM. TEN.	PURPOSE
I	Lot 35	Lots 15-21, 25-30, 32-34	R.O.W. ELECTRICITY WATER SUPPLY TELEPHONE SEWAGE DRAINAGE WATER DRAINAGE GAS
K	Lot 34	Lots 15-21, 25-30, 32, 33, 35	
Q	Lot 28	Lots 15-21, 25-27, 29, 30, 33-35	
V	Lot 29	Lots 15-21, 25-28, 30, 32-35	
X	Lot 30	Lots 15-21, 25-29, 32-35	
Y	Lot 15	Lots 15-21, 25-30, 32-42	
Z	Lot 42	Lots 32, 33, 36-41	
AA	Lot 41	Lots 32, 33, 36-40, 42	
AB	Lot 32	Lots 33, 36-42	
M	Lot 36	Lots 32, 33, 37-42	
AC	Lot 37	Lots 32, 33, 36, 38-42	
AD	Lot 38	Lots 32, 33, 36, 37, 39-42	
AE	Lot 40	Lot 39	SEWAGE DRAINAGE WATER DRAINAGE ELECTRICITY TELEPHONE GAS
L	Lot 33	Lot 34, 36	SEWAGE DRAINAGE WATER DRAINAGE
AF	Lot 42	Lot 40, 41	R.O.W.

NOTE: For Other EASEMENT MEMORANDA SEE SHEETS 2 & 3
CLASS I SURVEY
Total Area 6.8780 ha
Comprised in 36D/522 (ALL), 10C/752/1M (BALL), 48C/526(ALL), 68C/525(ALL), 61A/185(ALL), 61A/180 (ALL)

MEMORANDUM OF EASEMENTS IN CROSS			
SHOWN	SERV. TEN.	PURPOSE	GRANTEE
K	Lot 34	WATER SUPPLY SEWAGE DRAINAGE WATER DRAINAGE	Wellington City Council
Q	Lot 28		
V	Lot 29		
X	Lot 30		
Y	Lot 15		
Z	Lot 42		
AA	Lot 41		
AB	Lot 32		
M	Lot 36		
AC	Lot 37		
AD	Lot 38		
AE	Lot 40		
Z	Lot 42	R.O.W.	

LAND DISTRICT WELLINGTON
 SURVEY BLK. & DIST. VII PORT NICHOLSON
 NZMS 261 SHT R.27 RECORD MAP No 20.23 21.23
 20.24 21.23

LOTS 1-42, 106-111, 114-116, 119-121, 123-125 BEING
 SUBDIVISION OF LOTS 9-12 DP1149, Pts SEC 3 and
 SEC's 118 & 166 HARBOUR DIST and SEC 1 SO 36720

TERRITORIAL AUTHORITY WELLINGTON CITY
 Surveyed by Pritchard Group Limited
 Scale 1:400 Date August 2002 - February 2003

4 Robert Graham Longley
 being a person entitled to practise as a Licensed cadastral surveyor, certify that:
 (a) The surveys to which this document relates are accurate, and were undertaken by me or under my direction in accordance with the Cadastral Survey Act 2002 and the Surveyor-General's Rules for Cadastral Survey 2002;
 (b) This document is accurate, and has been created in accordance with that Act and those Rules.
 Dated at Christchurch this 24th day of March 2003
 of March 2003
 Robert Graham Longley
 Reference Plans DP's 1149, 1150, 2083, 2087A, 2087B, 2089, 2089A, 2089B, 2089C, 2089D, 2089E, 2089F, 2089G, 2089H, 2089I, 2089J, 2089K, 2089L, 2089M, 2089N, 2089O, 2089P, 2089Q, 2089R, 2089S, 2089T, 2089U, 2089V, 2089W, 2089X, 2089Y, 2089Z, 2089AA, 2089AB, 2089AC, 2089AD, 2089AE, 2089AF, 2089AG, 2089AH, 2089AI, 2089AJ, 2089AK, 2089AL, 2089AM, 2089AN, 2089AO, 2089AP, 2089AQ, 2089AR, 2089AS, 2089AT, 2089AU, 2089AV, 2089AW, 2089AX, 2089AY, 2089AZ, 2089BA, 2089BB, 2089BC, 2089BD, 2089BE, 2089BF, 2089BG, 2089BH, 2089BI, 2089BJ, 2089BK, 2089BL, 2089BM, 2089BN, 2089BO, 2089BP, 2089BQ, 2089BR, 2089BS, 2089BT, 2089BU, 2089BV, 2089BW, 2089BX, 2089BY, 2089BZ, 2089CA, 2089CB, 2089CC, 2089CD, 2089CE, 2089CF, 2089CG, 2089CH, 2089CI, 2089CJ, 2089CK, 2089CL, 2089CM, 2089CN, 2089CO, 2089CP, 2089CQ, 2089CR, 2089CS, 2089CT, 2089CU, 2089CV, 2089CW, 2089CX, 2089CY, 2089CZ, 2089DA, 2089DB, 2089DC, 2089DD, 2089DE, 2089DF, 2089DG, 2089DH, 2089DI, 2089DJ, 2089DK, 2089DL, 2089DM, 2089DN, 2089DO, 2089DP, 2089DQ, 2089DR, 2089DS, 2089DT, 2089DU, 2089DV, 2089DW, 2089DX, 2089DY, 2089DZ, 2089EA, 2089EB, 2089EC, 2089ED, 2089EE, 2089EF, 2089EG, 2089EH, 2089EI, 2089EJ, 2089EK, 2089EL, 2089EM, 2089EN, 2089EO, 2089EP, 2089EQ, 2089ER, 2089ES, 2089ET, 2089EU, 2089EV, 2089EW, 2089EX, 2089EY, 2089EZ, 2089FA, 2089FB, 2089FC, 2089FD, 2089FE, 2089FF, 2089FG, 2089FH, 2089FI, 2089FJ, 2089FK, 2089FL, 2089FM, 2089FN, 2089FO, 2089FP, 2089FQ, 2089FR, 2089FS, 2089FT, 2089FU, 2089FV, 2089FW, 2089FX, 2089FY, 2089FZ, 2089GA, 2089GB, 2089GC, 2089GD, 2089GE, 2089GF, 2089GG, 2089GH, 2089GI, 2089GJ, 2089GK, 2089GL, 2089GM, 2089GN, 2089GO, 2089GP, 2089GQ, 2089GR, 2089GS, 2089GT, 2089GU, 2089GV, 2089GW, 2089GX, 2089GY, 2089GZ, 2089HA, 2089HB, 2089HC, 2089HD, 2089HE, 2089HF, 2089HG, 2089HH, 2089HI, 2089HJ, 2089HK, 2089HL, 2089HM, 2089HN, 2089HO, 2089HP, 2089HQ, 2089HR, 2089HS, 2089HT, 2089HU, 2089HV, 2089HW, 2089HX, 2089HY, 2089HZ, 2089IA, 2089IB, 2089IC, 2089ID, 2089IE, 2089IF, 2089IG, 2089IH, 2089II, 2089IJ, 2089IK, 2089IL, 2089IM, 2089IN, 2089IO, 2089IP, 2089IQ, 2089IR, 2089IS, 2089IT, 2089IU, 2089IV, 2089IW, 2089IX, 2089IY, 2089IZ, 2089JA, 2089JB, 2089JC, 2089JD, 2089JE, 2089JF, 2089JG, 2089JH, 2089JI, 2089JJ, 2089JK, 2089JL, 2089JM, 2089JN, 2089JO, 2089JP, 2089JQ, 2089JR, 2089JS, 2089JT, 2089JU, 2089JV, 2089JW, 2089JX, 2089JY, 2089JZ, 2089KA, 2089KB, 2089KC, 2089KD, 2089KE, 2089KF, 2089KG, 2089KH, 2089KI, 2089KJ, 2089KK, 2089KL, 2089KM, 2089KN, 2089KO, 2089KP, 2089KQ, 2089KR, 2089KS, 2089KT, 2089KU, 2089KV, 2089KW, 2089KX, 2089KY, 2089KZ, 2089LA, 2089LB, 2089LC, 2089LD, 2089LE, 2089LF, 2089LG, 2089LH, 2089LI, 2089LJ, 2089LK, 2089LL, 2089LM, 2089LN, 2089LO, 2089LP, 2089LQ, 2089LR, 2089LS, 2089LT, 2089LU, 2089LV, 2089LW, 2089LX, 2089LY, 2089LZ, 2089MA, 2089MB, 2089MC, 2089MD, 2089ME, 2089MF, 2089MG, 2089MH, 2089MI, 2089MJ, 2089MK, 2089ML, 2089MM, 2089MN, 2089MO, 2089MP, 2089MQ, 2089MR, 2089MS, 2089MT, 2089MU, 2089MV, 2089MW, 2089MX, 2089MY, 2089MZ, 2089NA, 2089NB, 2089NC, 2089ND, 2089NE, 2089NF, 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Approved as to Survey
 Deposited this day of 2003
 for Registrar-General of Land
 File Harbourside Stage 1
 Received 17.04.03
 DP 321404

Feedback

606

114178564096

NAME: Donna Scammell	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

607

114161475297

NAME: Ed	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

608

114160152712

NAME: Eldon Tate	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The connection has a significant negative impact on local business and residents and very little positive impact if implemented. The multicriteria analysis model appears to have little quantifiable data basis and is unsuitable to evaluate the designs or impact.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

This design will have a significant negative impact on local residents and businesses. I believe that the multicriteria analysis is wholly unsuitable for determining this design. Baseline usage data is also unavailable to be able to measure any potential positive impact

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

This will have a significant negative impact on residents and pedestrians that utilize the Bridle Track for walking. There is significant parking pressure currently that will be increased by the proposed changes both on this section and the lower ngaio gorge road component.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

This is a low impact increase in cycle safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

I believe this design to be unsafe for both cyclists and motorists.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The multicriteria analysis for the project is flawed and the negative impact is overwhelming greater than any environmental or wellbeing impact of the connection. In regards to the change timings, what decision points are there to revert the changes should the outcome prove to be negative?

Feedback

609

114167671016

NAME: Ellen Smith	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The removal of parking and putting in time restrictions and clear ways has a detrimental affect on businesses and residents along Kaiwharawhara Road.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Give the public a chance to have their say

Feedback

610

114179425980

NAME: Eric Mattlin	SUBURB: Karori	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

If your four goals are below - this is not working! Making it easier for everyone to move around in our growing city. It's not getting easier - it's getting harder. I want to bus around the city but I can't. I go to the gym in Ngaio and live in Karori there are no servers that will get me there. I already find it difficult to get a car park and your changes will mean I'll need to quit the gym. Connecting the places where people want to go I want to go to Ngaio, and you are making no progress on making this easy in any way. Opening up our streets for people of all ages and abilities Unless you want to bike - tough luck! Reducing our emissions from road transport Fix the Buses!!!@!!!!!!!!!!!!!!!!!!!!^%\$##\$# I WANT to bus but they are a mess. I would love to know how many people do bus or could bus if we had a system that worked against how many people cycle. If you want to reduce emissions give me an option for public transport! Bikes will not get us there in Wellington - it's hilly and often has bad weather. We need public transport that works. All I see is constantly increased spending on bike lanes and NO improvements to public transport, this from the outside looking in seems based on ideology, not a system that will work or actually get

people out of cars. You need to prioritise and we need to invest in public transport. I try to bus from Karori and normally 2-3 buses will pass me because they are full. People are queueing up (literally) to take public transport and we are being ignored. FIX THIS. Once this is fixed then go nuts with bike lanes.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is a waste of money - this road does not need bike lanes. It's hardly ever used - why are you doing this?? Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks - what's to gain from this other than taking away 2 parks?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Buses already struggle with this road - why are making it harder? We need people to bus - this will have the largest impact!

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback

611

114162965802

NAME: Tommy Thomson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

There should be a bike lane all the way along Kaiwharawhara road from the start. Having only part of a bike lane is going to discourage people from cycling and my concern is that the clearway will remain past the planned 2025. Cycling between parked cars and high speed traffic is terrifying and dangerous, and that area at the bottom of Kaiwharawhara road has the most parked cars and is thus the area that most urgently needs a dedicated cycle lane. I'm concerned about the lack of protection in the picture above. There should be more barriers on either side of the driveway, to narrow the driveway and force drivers to make sure the cycle lane is clear before crossing it.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Unprotected cycle gutters are not infrastructure and neither are sharrows. Nobody was going faster than 30 on this street so the speed limit changes mean nothing. This changes essentially nothing about the viability of Cameron Street as a cycling route. I'm concerned about the poor visibility of cyclists around those blind corners. It would be great if this project was coupled with clear signage pointing cyclists towards the bridle path.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

None of the cycle lanes in the picture have protection, cycle lanes need protection. Ngaio gorge road is a busy cycling route and it urgently needs a protected cycle lane. The pedestrian crossing between the bus stops is a great addition.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

There should be barriers protecting the cycle lane. Ideally there would be a cycle lane on both sides of Kenya Street as a continuation of the Ngaio gorge cycle particularly at the top of the hill.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Cycling between parked cars and fast traffic at the bottom of Kaiwharawhara road is dangerous and terrifying. This area urgently needs to be converted to a cycle lane, not a clearway.

Feedback

612

114163989149

NAME: Felix Marwick	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

make sure there is no conflict in areas where business premise exits cross the cycle lane. Too often vehicles block these areas or cross in front of oncoming cyclists

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
Street parking should not be a priority on key arterial routes. However, the WCC should help property owners with off-street parking by waiving consent fees to allow them to build car pads/garages/carports on their properties

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The longer the construction period, the greater the chance of legal challenges, project delays, and increased construction costs. Get it right, do it fast

Feedback

613

114163038312

NAME: Gajan Shivanandan	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I'm concerned about the flow on effect this will have on the bottom of Old Porirua Rd. this already has cars parking (illegally??) on the grass verge and in narrow lanes, further constraining access to/from old porirua rd (as a resident who lives off old porirua rd directly).

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

is there any possibility of this being moved closer to Perth Street - this seems to be the most logical place for people to cross the road from the bus stop.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

my main concern is the overall roadwidth and whether it can safely accomodate the bike lane, 2x live vehicle lanes and parked cars. what would the impact look like to remove all parked cars at narrow points (both sides of road) and corners ?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

614

114174770061

NAME: Gloria Wheeler	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

The amount of money being spent on cycle lanes is excessive for the number of people currently and/or in the future using them

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

Support a 4pm-7pm Monday to Friday clearway but nothing further. There is little enough parking for business as it is.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Oppose

Do you have any comments to make about the proposed design?

While e-bikes may be able to stay within a cycle lane normal bikes cannot. They currently weave back and forth across the entire road going uphill. They are mainly commuting morning and night so to install restrictions on where they go will be impossible to police. To reduce the number of parks even further by taking out the more efficient angle parks and replacing them with parallel

parks is counterintuitive. P24 hr parking does nothing to reduce the number of cars parking 8 to 10 hours each day. 'Most' residents drive at 30-40 km along this road so reducing the speed limit to 30 km and then having to police it to stop the occasional idiot will not happen.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

I believe this road is too narrow to have a dedicated cycle lane and that sharrows would be a more practical solution

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

Removing car parks from outside houses who do not have room for parking on their own sections seems counter intuitive if you want people to take the bus and/or cycle to work

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Leave it at Stage 1

Feedback

615

114178557269

NAME: Grant Clarke	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

616

114160271842

NAME: G B	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

I support the concept of bike lanes on this road, but I do not see a need for a clearway.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Cameron street is a small spur supporting a small number of houses. The costs associated with the proposed changes are disproportionate to the benefit they provide to the average Wellingtonian. This is not a primary cycleway so should not be heavily invested in.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Kenya street is narrow in places. Insufficient detail has been provided for the narrow sections to understand how everything will fit.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

In general I support lowering the speed limit. but extending the 30km zone as proposed has minimal benefits. 30kmh on the road section between the roundabout and crossing at Crofton Road is pointless. Consideration should be given to speed limits near the scout hall on Waikowhai Street. I support parking on one side of Kenya street only.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Changing the rules frequently just creates confusion and adds taxpayer cost.

Feedback

617

114161592207

NAME: Phil McCracken	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The wanton destruction of wellingtons streets needs to stop. No more cycleways.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Changes are being made at a reckless pace to the detriment of businesses and residents

Feedback

618

114172886132

NAME: Hamish Brookie	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

You fail to understand that your community needs a transport system that works, where is the integration with bus services. You people are in Lala land if you believe these communities will not require car access. If the city wants to destroy peoples business's and investments and access to there homes. This is a disgrace!!!

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

You are proposing to destroy these businesses and devalue the investments have made in the commercial premises. What is the economic impact on these property's.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

What is the economic impact on the owners of these properties. What is the economic benifit of these changes.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

This is a main road link into Ngaio. Council should widen road to allow for two way traffic, parking a cycle track and a foot path.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Widen the roadway to provide for two way traffic, a cycle lane and pedestrian foot path. Organise smaller buses

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

This question is loaded it assumes your changes are a given which they are not. The scheme is wrong

Feedback

619

114167705812

NAME: Andrew Hemsley	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The proposed changes do not take into account the parking requirements of the people that live in the effected streets or adjacent streets. The council has already reduced the parking available in Kenya Street and Treslissick Cres, removing further parks from Kenya Street put even more pressure on parking. its a generalisation but the homes in Ngaio (especially around Kenya Street and Trelissick Cres, Crofton Road, Abbot etc are family homes and as such the utopian concept that every house has one car is fanciful is outdated.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Again this reduces carparks from the adjacent homes and properties

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The narrowing of the road any further is going to slow the flow of traffic even further, it will no doubt result in more damage to parked cars (broken wing mirrors and panel damage) and continue to foster animosity to cyclists to who the city's traffic management revolves. We partitioned the WCC to make tangible changes to the speed within Kenya Street especially at the top end with Trelissick. which saw the installation of an automatic flashing sign which at the admission of WCC made no difference to vehicle speed and secondly the installation of permanent plastic bollards which again had questionable effect on speeds. The council's response was to reduce parking further and painting white lines.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

With the removal of 57 parks in Kenya Street where do you honestly think the home owners will park. This is just a travesty of a small minority of road users, imposing their will and agenda on another suburb. Ngaio is a hill suburb, there is not the option of off street parking, Ngaio is also an older suburb and the garages that people have do not accommodate the newer size or weight of vehicles. I'd like to see a Tesla park in a 1970's garage perched over a bank. I would have thought the council would have learnt lessons from the fiasco of the Island Bay and the Newtown Cycle ways. To suggest that residents park in adjacent street from Kenya Street again is delusional - how far should people walk from their parked car to their place of residence? If the expectation is that people park in adjacent streets has any consideration been given to the insurance implications of not having your car parked adjacent to your property? Why is it that the council is prepared to permit the construction of the Paddington development 50+ units on the corner of Taranaki and Jessie Street and not require the developer to provide carparking - really

What do you think of the timing of the proposed approach?

Why do you think that?

The lack of option opposing this change is unacceptable - you either want feedback or you don't appreciate a negative answer here will not match the narrative but if you are genuinely wanting feedback you must collect all of it. The wording of the proposal document is fascial its a poor mix of "when we" implying yet again the decision has been made with the woefully masked weasel words of consultation

Feedback

620

114170024030

NAME: Khoi Phan	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Up hill cycle way help protecting those on the bike. We need to fast track these changes to keep up with the rest of the country like Auckland and Christchurch with the cycle network implementation.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Please install concrete (aka Timtam) instead of the plastic separators.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

This is a long stretch of road but at most this should be done within 2 years instead of 3. We need to ACCELERATE these !

Feedback

621

114181662515

NAME: Ian Hollins	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

The design needs to implement all necessary traffic safety measures before adding more cyclists to the network. Delaying safety measures is not a valid tactic of transitional plans

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly oppose
Do you have any comments to make about the proposed design?
Refer to attached document with issues and recommendations

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
The current plans are ignoring or delay safety measures.

Ian%20Hollins%20Ngaio%20Connection%20Submission.pdf

Kenya Street Traffic safety

This submission covers 6 safety issues and makes recommendations for the Ngaio Connections project to improve safety for the initial iteration in the transitional rollout.

Issues

1. Excessive speed in Kenya Street
2. Unsafe northbound cycle path at the Intersection at Kenya Street/Ngaio Gorge Road/Trelissick Crescent
3. Bus stop at 52 Kenya Street that does not allow space for buses to fully stop off the main traffic shared lane
4. Lack of Parking at upper Kenya Street
5. Safety measures for Trelissick Crescent
6. Roads not wide enough for the best cycle safety measure of separated cycle lanes

Issue 1. Excessive speed in Kenya Street

Recommendations

- 1.1 Make all of Kenya Street one official speed limit of 30km/hr.
- 1.2 Reduce speed up Kenya Street by installing a speed hump at about 39/41 Kenya Street, at least for the uphill side, heading towards the CBD.
- 1.3 Improve safety and reduce accidents at upper Kenya Street by installing a speed hump at about 45/64 Kenya Street.

Rationale

The WCC has recognised the safety risk for the three corners at the top of Kenya Street and installed speed advisory signage of 35km/hr. They are safety traffic calming measures that must remain in place and be enhanced. Note there are 5 WCC 35km/hr speed guideline signs plus one electronic speed warning sign. The northern most sign is at 37 Kenya Street, and the southern most sign is on Ngaio Gorge Road.

The MCA states the “Recorded 85th percentile speeds (46 km/hr) are lower than the posted speed limit (50 km/hr)”. However, the top half of Kenya street is within the WCC 35km/hr speed advisory, so the 46 km/hr is 11km/hr above this speed or **131% of the speed guidance for this area**. The Ngaio Transitional reports do not include the 35km/hr advisory area, in the design maps or in any report.

Accidents. In the 90 a speeding vehicle heading south missed the corner at 60 Kenya Street and crashed into the house at #64. The WCC were so concerned about the excessive speed of traffic that they installed heavy duty road safety barriers behind the footpath from 62 to 64 Kenya Street. A **“real ambulance at the bottom of the cliff”** response. In 2016 after a resident petition, some traffic calming measures were installed including the 35km/hr signage. At the end of 2020 a speeding car coming from Ngaio end, left 35metre tyre skid tracks and destroyed part of the heavy steel barrier. To brake for 35ms and still have enough inertia to demolish the Armco barrier indicates they were going well over 100km/hr. The WCC had to replace the traffic safety barrier but this time

also added some two more 35km/hr signs. **The WCC should take this opportunity to “build a fence at the top”, by installing real traffic calming measures for this acknowledged safety hotspot.**

The Ngaio Connection project accident data underreports the accidents. It uses accident data from the NZTA CAS, which is collected by Police reports. However, the CAS have stated that *“under-reporting of crashes is a known issue. It is estimated that there is 40% under-reporting of serious crashes”*.

Between #56 and #68 Kenya Street, eight cars have been written-off due to excess speed related accidents, most were parked cars, and most were not reported to the Police so are not counted IN CAS data. They were reported to the WCC but do not appear to be included in the main reports or safety audit.

Even the recorded 46 km/hr is likely to be lower than normal speeds as over the past two years the Ngaio Gorge Road has been under construction and for most of the time, traffic is stopped by traffic lights, causing queues and has generally reduced the speed of traffic. This is due to the slower vehicles having a queue form behind them, so all queued vehicles are travelling at this lower speed.

U-turns on Kenya Street. The proposal does not seem to have considered that by having parking on only one side of the road, it will mean vehicles travelling from the Ngaio end, will have to cross the road to find parking spaces. Drivers would have to either perform a U-turn or a three-point turn on Kenya St, or drive to the Kenya St/Ngaio Gorge Road intersection and turn into Trelissick Crescent and then back onto Kenya Street, to head north. Also, everyone parking on Kenya Streets west side but who want to head south will either have to use the corner of Kenya Street/Crofton Downs, by the crossing or the roundabout at Waikowhai St/Ottawa Road, or carry out a U-turn.

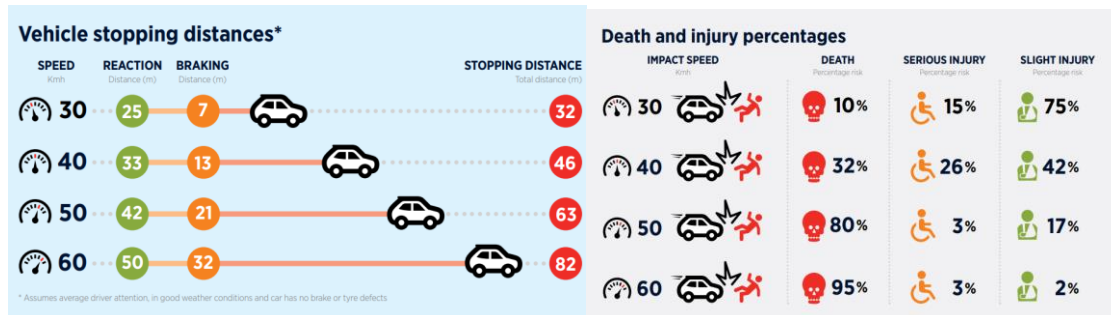
Performing a **U-turn in 50km./hr speed zone is dangerous**, and carrying out a U-turn or three-point turn will likely require vehicles using the cycle lane and then again in the opposite shared lane, potentially very dangerous and putting cyclists at risk.

The Ngaio Transitional proposal will have five speed zones for the 500m of Kenya Street. Traffic heading south, away from Ngaio, meet posted speed signage of :-

- | | |
|---|------------------------|
| • From Ngaio to 6 Kenya St | 30km/hr |
| • 6 Kenya Street to 37 Kenya Street | 50km/hr |
| • 37 Kenya Street to 10 Ngaio Gorge Road | 35km/hr speed advisory |
| • 10 Ngaio Gorge Road to just past Perth St | 25/km/hr speed humps |
| • After humps there is no speed signage so | 50km/hr |

Five speed changes in 500metres is excessive, wastes fuel, and is unlikely to be followed. Surely the whole of Kenya Street should be a 30/km/hr speed zone, as both safer and much less confusing.

The road design and visibility at the three bends at the top of Kenya street is unsafe for speeds greater than 35 km/hr. The Road Code states that drivers should drive at a speed that they can stop within the length of the clear visibility. So, with no more than 35 meters of visibility, vehicles should not be travelling at more than about 32km per hour. (from Auckland Transport Road Safety and Safe Road Programme).



Below is a graphic for the three corners. The driver visibility is shown by the green triangle, being what the driver of the blue vehicle will be able to see, out to about 30 metres ahead of them. The red graphic represents what could be a stationary vehicle, pedestrian, cyclist, truck, etc. Vehicles travelling at more than 30kphr and needing to urgently brake, will travel 25 metres in their reaction time and another 7 meters in braking. This clearly shows that to be safe, vehicles travelling around these corners **must not be going more than 30 or 35 kms per hours**.



So, the traffic must be slowed to less than 35kms per hour to make the road safe. If the WCC really cares for about road safety, then it must take urgent action to reduce the excessive and dangerous speed at the top of Kenya Street, signage alone is not working.

The proposal will introduce excellent traffic calming measures at both ends of Kenya Street, but nothing is proposed for the rest of the 35km/hr advisory speed zone, which is also a high accident Zone.

Issue 2. Unsafe northbound cycle path at the Intersection at Kenya Street/Ngaio Gorge Road/Trelissick Crescent

Recommendation

- 2.1 For cyclist safety at this intersection, the northbound cyclists be directed first down Trelissick Crescent, make a hook-turn and then take a left turn into Kenya St when clear.**

Alternate Recommendation

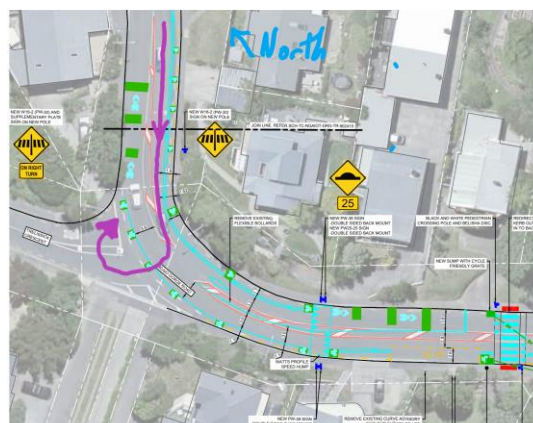
- 2.2 Install a roundabout at the Kenya Street/Ngaio Gorge Road/Trelissick Crescent intersection**

Rationale

The Intersection at Kenya Street/Ngaio Gorge Road/Trelissick Crescent is already a dangerous zone, as its on a tight bend, is very close to the crest of the Ngaio Gorge Road, so very poor visibility, and is a high accident zone. This is recognised by the WCC and it installed some traffic safety/calming measures, including

- Two 35km/hr advisory signs at the top of Ngaio Gorge Road,
- An electronic speed advisory sign
- Flexible bollards
- Yellow centre lines
- White traffic guidance lines

Currently traffic from the north, Ngaio end, will use this corner to swap direction, rather than make very dangerous U-turns or three-point turns. The raised concrete 7.5metre median strip for the Trelissick Crescent had to have centre section removed so that vehicles could make the turn immediately behind the give way sign.



[illegible]

Issue 3 Bus stop at 52 Kenya Street does not allow space for buses to fully park off the main traffic shared lane

Recommendation

2.1 As per the proposal, remove the four carparks at 48 – 54 Kenya Street and

- shorten the existing bus stop northern end to include the 52A driveway
- covert the northern half of the existing bus stop at 52 Kenya Street to an unrestricted car park.

This will provide a no-stopping zone for the bus stop that is 44 metres long.

Alternate recommendation

2.2 Do not remove the four unrestricted carparks at 48 – 54 Kenya Street.

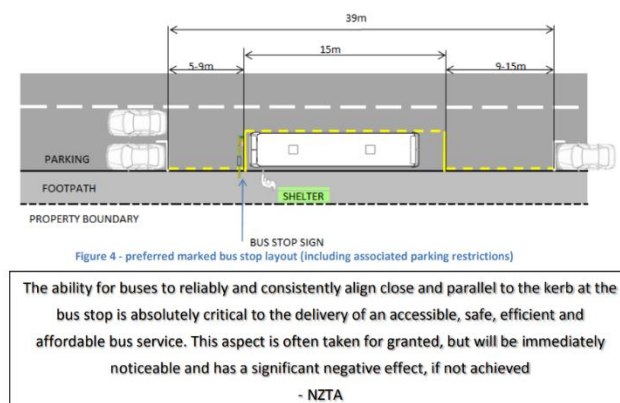
- Extend the no stopping zone by removing one more carpark outside 52 Kenya Street
- Extend the no stopping zone by including the driveway at 50 Kenya Street.

The no stopping zone would be 33m metres long, not quite the NZTA preferred length, but more than twice as long than as present and it saves three of the necessary on-street carparks.

Rationale

The TR180-22 Ngaio Transitional bike and pedestrian improvements document, page 2, proposes to install No Stopping Restriction (BYLs) for 36 metres on the bend outside Nos.54, 56, and 58 Kenya Street extending to the existing bus stop by removing 4 unrestricted carparks. The existing bus stop is 15 metres long, so will make the Bus top 51 metres long.

The WCC does not appear to have a standard ideal length of bus stops. The NZTA do have a preferred bus stop layout length to permit the bus to pull into, service and then pull out oi the stop to re-join traffic, of 39 metres, as below



At the northern end of the existing bus stop it includes what could be one car park space. Converting this carpark from no-stopping to unrestricted will still mean the proposed bus stop will become 44 metres long. More than enough, and provides for one more car park

Issue 4, Lack of Parking at upper Kenya Street

Recommendation

- 4.1 From 45 Kenya Street heading south towards the CBD, the separated cycle way is changed to a buffered/shared cycle lane
- 4.2 the on-street carparks remain from 43 to 53 Kenya Street.

Rationale

The parking occupancy for Kenya Street is 115%, which is a third higher than the threshold of 85%. The report states that there is ample alternate parking available “*within a five-minute walk from centroid of the parking removal*”, but this is not the case.

The report looks at a Kenya Street sector as a whole, however it does not consider the different characteristics with that sector, which is over 1.5 km long. The upper Kenya Street, from 43/56 Kenya Street to Ngaio Gorge Road, is

- i. About 140m relatively flat
- ii. Within the WWC speed advisory speed limit of 35km/hr
- iii. 25% of the residences do not have off-street parking
- iv. The lack of parking in Treliwick Crescent means the Treliwick Crescent frequently park in Kenya Street.

Lower Kenya Street is straight, a 50km/hr zone, and almost all residences have off-street parking.

The top section of Kenya Street has the most need due to lack of off-street parking. However, after removing on street carparks from the upper Kenya St section there will only be a ratio of two on-street carparks for every five residences.

The surveys for Kenya Street occupancy are reported as 41%, so 40 parked cars. (41% of the 98 current parks). After the 63 parks are removed, it leaves 35 available parks. With 40 cars attempting to park in 35 space it is **a parking threshold of 115%** (40 cars divided by the 35 proposed parks). However, the report states 87%. Surely having an occupancy ratio well above the occupancy threshold should ring some alarm bells.

The parking surveys results are not reliable due to:-

- Only measured at four times per day, for two days
- The parking surveys were undertaken from 9am to 5pm but Kenya Street is a residential area, so as residents return home, from work and outings, in the early evening. The peak parking occupancy is after 5pm. Maybe the WCC applied survey models for Business District or suburban centres, rather than residential areas?
- On both the survey days, there was road maintenance at the top of Kenya Street and all residents within 130 metres of the Kenya Street/Treliwick Cres corner were told not to park in the area or they would be towed away. 45 parks were not available for use, so not typical and significant number, relative to the plan to only have 35 carparks in Kenya Street. It is made worse as the residential characteristics are quite different for the top of Kenya Street and the bottom. 45 parks excluded is far too many to just do replace with some unexplained “adjustment” or extrapolating data from the rest of Kenya Street.
- The Parking report does not report the actual count but only refers to percentages of available parks. Reporting just one calculated data measure makes it impossible to get a reliable understanding of the facts.

Very limited alternate parking,

The parking Occupancy Threshold figure of 85% is when traffic circulation will be high as motorists search for an available car park and may not be able to find an available car park space. The reports also use a measure that alternate parking should be “within a five-minute walk from centroid of the parking removal”

From NZTA. “While the New Zealand Travel Survey does not currently record distances for walking trips (only times), based on a walking speed of 12 minutes per kilometre, it appears that 70% of our walking trips are for distances of under one kilometre, while 30% are likely to involve longer distance.” So, in **5 minutes people can walk 417 metres.**

The report list alternate parking for Kenya Street is available at Abbott Street and Trellissick Crescent

- a) The Crofton Road sector has an expected peak occupancy after installation that is even higher than at Kenya Street. It also lists Abbott Street as the alternate parking for the Crofton Downs sector. The Crofton Downs sector parking calculations do not appear to include the Kenya Street residents/visitors overflow parkers that are being directed to Abbott Street. Note Motor Doctors customers, at the corner of Kenya Street and Crofton Road, occupy 15 -20 car parks for most of the day, on business days.
- b) Trellissick Cresc as the main parking alternative for Kenya Street, but Trellissick Cres is
 - i) narrow, most of it is about 5-6 metres wide, so realistically parking is available only on one side. This was confirmed by CC but is not stated in the report.
 - ii) Tight corners so road visibility is poor
 - iii) The Trellissick Crescent residents already fully occupy the parks for the first 170m from Kenya Street to Jacobsen Lane. **So, from mid Kenya Street it's a 420m, a 5 minute walk to where the likely first available parking spaces.**
 - iv) The footpaths and curbing stop at Jacobsen lane, and further on there is less street lighting.
 - v) at the Ngaio Gorge Road end of Trellissick Crescent, the immediate residents use most of the available parks and only the first 100m have footpaths and curbing. Walking from Kenya Street to this end of Trellissick Crescent requires walking the 90 metres of Ngaio Gorge Road, a no stopping zone. **So, from mid Kenya Street it's a walk 440m, or over a 5 minute walk to where the likely first available parking spaces.**
 - vi) Its not safe to use , especially at night due to the poor lighting, remoteness and no footpaths or curbing

In conclusion Abbott St and Trellissick Cres do not meet the parking requirements as suitable alternate parking, and as no other parking is available, so more parks must be retained in Kenya St.

The Trellissick Community group is concerned that increased parking will mean damage to plants and compacting of the soil beside the road immediately adjacent to the reserve.

The buffered/shared cycle lane from 43 to 53 Kenya Street is viable as this section of the road is

- flat
- in a 35km/hr advisory zone (or 30km/hr speed zone)
- from 45 Kenya Street there is no parking on the opposite side of the road
- there are speed humps at in both directions

After 53 Kenya Street it is a no stopping zone so it is suitable for a separate cycle way for separation around the corner.

Issue 5 Safety on Trelissick Crescent

Recommendation

- 5.1 Trelissick Crescent is made to a 30km/hr zone.**
- 5.2 In the medium term the WCC install no stopping on one side, improve street lighting, add curbing and a footpath around all of one side of Trelissick Crescent**

Rationale

Trelissick Crescent is :-

- a. 700 metres long
- b. 100m of no parking at the Ngaio Gorge/Kenya St ends of the Crescent.
- c. about 450 metres is very narrow street, about 5-6 metres wide
- d. 70% is without curbing or footpaths or much street lighting
- e. Many very tight corners with short road visibility
- f. Only short sections with footpaths on both sides at the Ngaio Gorge Road and Kenya Street ends.
- g. No restriction parking on both sides for much of the road, but the road is not wide enough to actual park on both sides
- h. 50 km/hr speed limit
- i. A relatively low housing density but a high on-street parking ratio at the ends near Kenya St and Ngaio Gorge Road

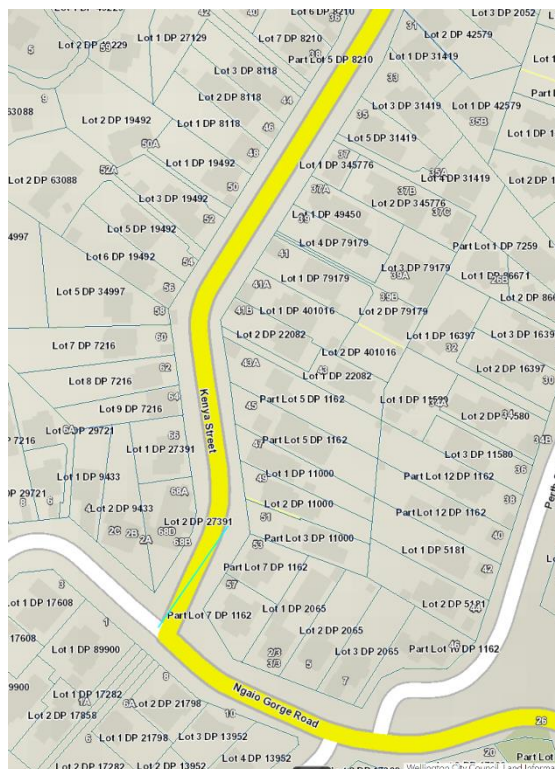
Issue 6 Roads not wide enough for best cycle safety measure of separated cycle lanes

Recommendation

6 Making better use of Road reserves

Rationale

Road reserves are the strip of land between a property's front boundary and an existing formed road or footpath. Often this land can be made available for lease or purchase from the Council, with a resource consent.



Making better use of this strip of land can remove on-street carparks and therefore better use the road for things such as cycle lanes. The cost to develop the road reserves is not in the WCC budget but by working close with the residents the majority of the development costs could be carried by the residents.

The council would also benefit by no longer having to maintain the road reserve and would gain revenue from the lease or rates.

The council could start this process by conducting a high level review of potential projects (if it is not needed for utilities, roads or footpaths) that may require the use of road reserve space and of those that are extremely unlikely to be used.

For the roads of extremely low likelihood of future WCC use, the residents could be informed.

For roads with a lot of interest the council could facilitate a group “Road stopping” review. These are usually \$15-\$20k each, but one review should be able to cover multiple nearby properties with little extra costs.

The council should consider discounted sales prices and encroachment lease fees for an initial period.

Feedback

622

114173734995

NAME: Ben Carter	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Cycle path should be fully separated (poles to define at a minimum) and not just painted

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

623

114181196405

NAME: James Sullivan	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

The proposal is an improvement over the status quo yet still has major issues that will not assist with the aim of increasing uptake of cycling. As this is an interim plan, council should feel confident in doing more than the bare minimum to best get an understanding of how changes will have a longer term impact. The interim design gives flexibility to pull back on some ideas if they do not work as expected.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Go straight to Phase 3 to avoid continual changes for parking which will repeatedly irritate people used to the status quo. If the parking must stay then the speed limit MUST be reduced to 30kph. This will have a massive impact on the safety of mixing unprotected road users with general motor vehicle traffic. It will also have a minor impact on large commercial vehicles as the stretch most commonly used by them stops at around the oil tanks. Lower speeds will also create a more pleasant environment for businesses as vehicles are not accelerating hard to get up to 50kph before slamming on the brakes.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

At a minimum the speed limit MUST be reduced to 30kph along sections of road where the physical cycle lane protections are removed. This will give drivers much more time to take a driving line that avoids crossing into the absurdly narrow cycle lane. It will also reduce the severity of any hit of a cyclist by a motor vehicle. Such action is highlighted as appropriate in both the Paneke Pōneke plan and Austroads guidance for cycle infrastructure.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

If micromobility users must share the lane with general motor vehicle traffic then the speed limit MUST be dropped to 30kph. This will encourage less confident cyclists to try it out, improve the safety of drivers exiting driveways, make cycling more pleasant, and reduce the roadway noise for nearby residential properties from vehicles accelerating hard.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The repeated changes will irritate all road users and gives little time for the full suggested design have data gathered on it. Phase 3 should be the only phase done.

James%20Sullivan%20Ngaio%20Cycleway%20Submission.pdf

James Sullivan Ngaio Cycleway Submission

The proposal is an improvement over the status quo yet still has major issues that will not assist with the aim of increasing uptake of cycling.

As this is an interim plan, council should feel confident in doing more than the bare minimum to best get an understanding of how changes will have a longer term impact. The interim design gives flexibility to pull back on some ideas if they do not work as expected.



*** This principle is really important for making interim street changes.**

I want to highlight this aim from Paneke Pōneke as to how the interim designs should be approached. We cannot fix everything in an interim design, but it doesn't mean that we should give up where the existing priority of road use comes into conflict with something else. Humans and roadways existed in Aotearoa long before thousands of large vehicles capable of doing 50kph uphill existed.

Proposed good improvements

The sometimes-protected cycle lane up the gorge

Any amount of physical protection along the uphill sections of this corridor is a vast improvement over the status quo. With vehicle traffic routinely close to 50kph the danger to unprotected road users is immense.

Sharpening some intersection corners to discourage high speed use

Several of the intersections along this corridor encourage high speed use by drivers into or out of roads that have little visibility due to parked vehicles. The effort to sharpen some of these turns will help discourage drivers from going through the intersections at high speed. Improving the safety and environment for unprotected road users.

Remaining Issues

While the proposal is an improvement over the status quo, it still leaves some major problems. Even within the context of this being an interim plan with a focus on changes that are fast and cheap to implement.

Following are some of the biggest impact issues that I think can be addressed to really make this interim change helpful in laying down a future design.

Why stage an interim plan?

It is repeatedly noted that this proposal is for an interim design. Why does it then need to be staged itself? It ends up creating interim interim plans.

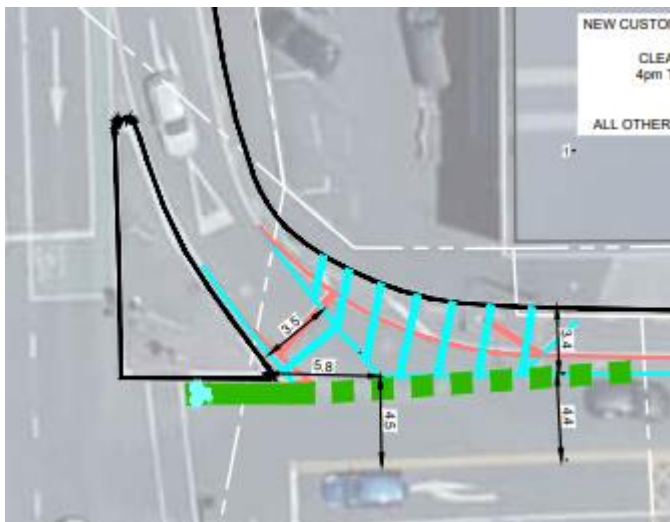
Capacity to do the work is limited so why is this plan requiring more expense for what is already going to be replaced? The current proposed plan will result in at least 4 rounds of construction before the final design is put in.

Suggestion

Go straight to phase 3. If sections are found to be needlessly disruptive then those individual items can be removed during the completion of the interim experiment. Otherwise they can be refined or removed for the longer term construction.

Some form of physical barrier is needed where paint is being used to sharpen intersections.

Paint is not protection. A flush median such as this will be cut by drivers who 'know' they don't see any cyclists. You can see this on many flush medians where vehicle marks have worn away or dirtied up the paint.



Suggestion

Some form of physical barrier needs to be installed to discourage this behaviour. Plastic or concrete lozenges as used elsewhere would work to do this. Such examples can be found on the uphill cycle lanes between Newtown and Kilbirnie or up to Brooklyn. These devices would not disrupt larger vehicles such as trucks or buses, but would disrupt and discourage smaller vehicles from taking the corner at speed.

Bollards or planters could also be installed in parts of the flush median to help discourage such behaviour as well.

Temporary cycle lanes forcing cyclists to routinely enter and exit 50kph flow of traffic

If you look through the Design and Safety Audit, the very first problem highlighted is that the 'temporary cycle lanes' will result in cyclists having to pop in and out of the main flow of traffic due to parked vehicles. This is even with the Audit making the assumption that cyclists are experienced and confident. Totally ignoring the aim of Paneke Pōneke to encourage less confident cyclists.

The flow of traffic along Ngaio Gorge has a speed limit of 50kph. Along Kaiwharawhara Road the 80th percentile speed is OVER this limit. The many light industrial and commercial properties along here also mean a high number of larger trucks and other vehicles with reduced mobility.

Phase 3 has the parking along here removed for a proper protected cycleway.



Suggestion

If the parking must stay then the speed limit MUST be reduced to 30kph. This will have a massive impact on the safety of mixing unprotected road users with general motor vehicle traffic. It will also have a minor impact on large commercial vehicles as the stretch most commonly used by them stops at around the oil tanks.

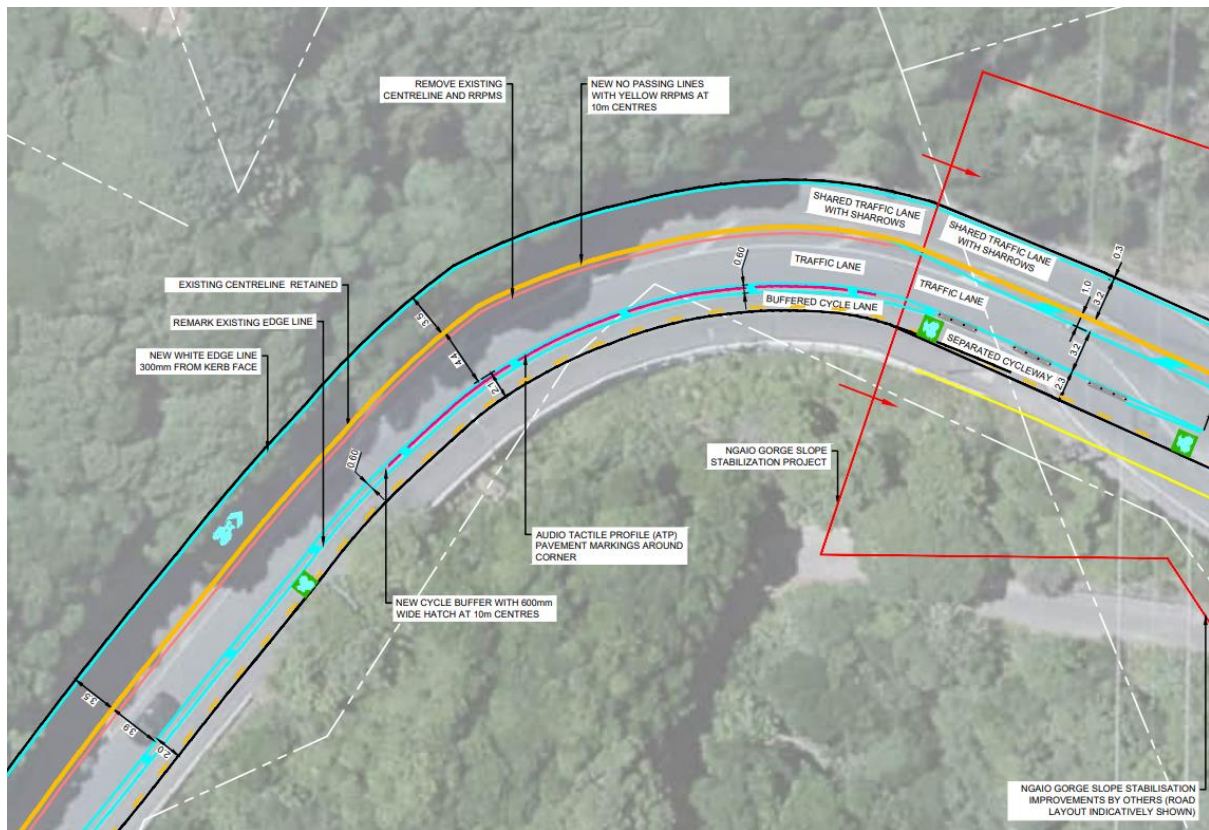
Lower speeds will also create a more pleasant environment for businesses as vehicles are not accelerating hard to get up to 50kph before slamming on the brakes.

Narrow cyclelane with no physical barrier on corner of 50kph traffic lane

This issue was also highlighted by the Design and Safety Audit in section 2.11. Which I will note again is assuming all cyclists using this are confident and experienced.

Removing any physical protection on these tight corners creates a massive danger for unprotected road users. Traffic will cut this corner without care. Drivers will get used to the noise and ignore it. The paint also does nothing to discourage long vehicles from using the corner at speed with a cyclist in the lane.

On top of this the cycle lane is absurdly narrow. For an interim plan that is supposed to encourage uptake of active transport modes, these sections alone will destroy much of that aim. Without a contiguous safe path this will fail to meet its aims.



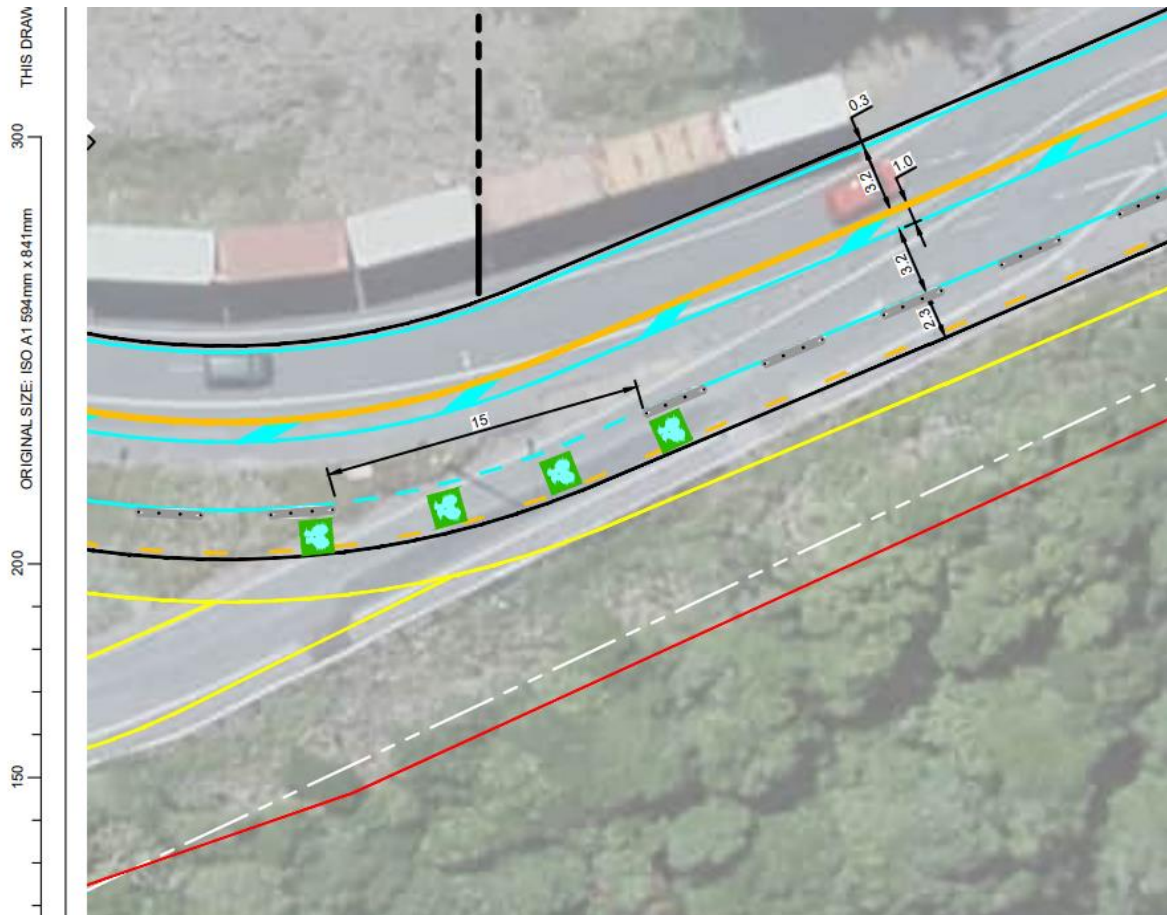
Suggestion

It's becoming a common theme in this submission but that's because it's fast and cheap to implement. At a minimum the speed limit **MUST** be reduced to 30kph along these sections of road. This will give drivers much more time to take a driving line that avoids crossing into the absurdly narrow cycle lane. It will also reduce the severity of any hit of a cyclist by a motor vehicle.

What will, and already does happen, is that drivers will tailgate cyclists and make dangerous overtaking attempts. On top of this the parking will obstruct the view of cars existing driveways into a narrow roadway.

If micromobility users must share the lane with general motor vehicle traffic then the speed limit **MUST** be dropped to 30kph. This will encourage less confident cyclists to try it out, improve the safety of drivers exiting driveways, make cycling more pleasant, and reduce the roadway noise for nearby residential properties from vehicles accelerating hard.

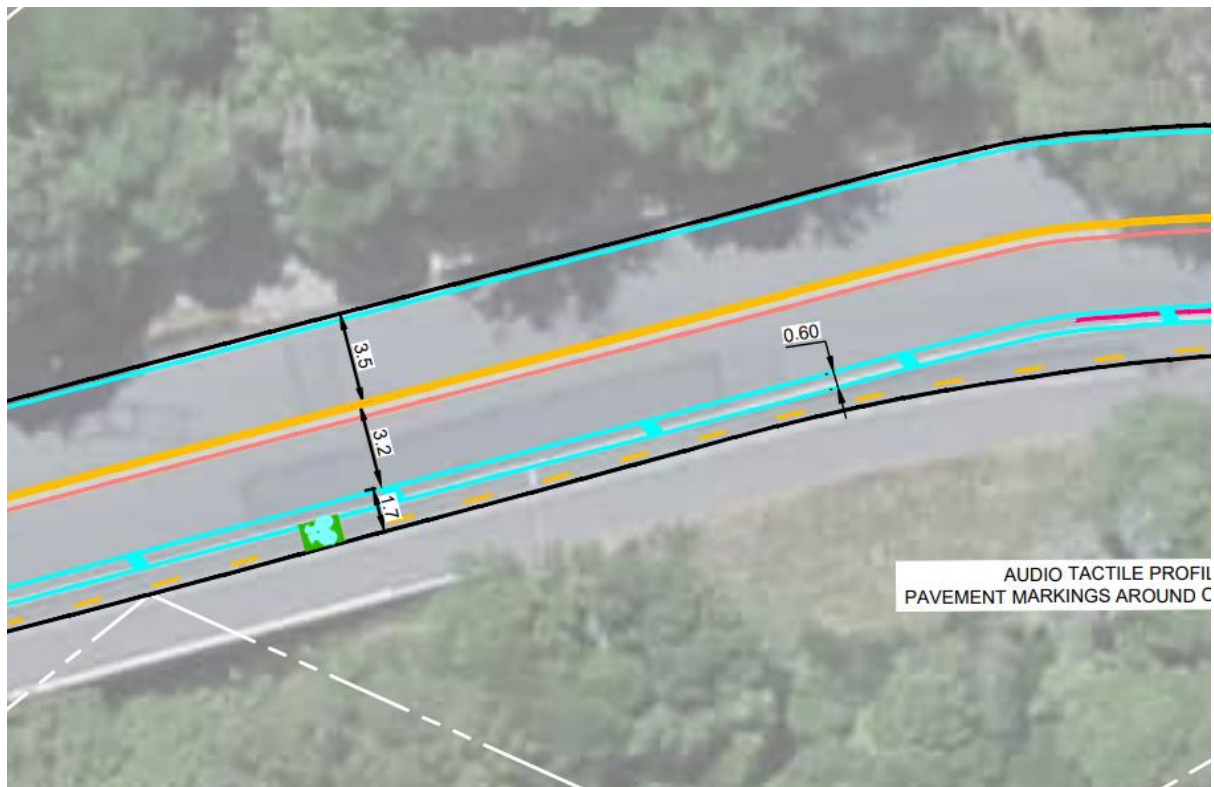
The intersection into the proposed parking for Trelissick park does nothing to discourage drivers from taking it at anything less than 50kph. Micromobility users will be travelling much more slowly due to the uphill at this point. This lower speed will encourage drivers to try and rush to get in ahead.



Parts of the cycleway are so absurdly narrow as to not exist

Parts of the proposed cycleway are so narrow that it becomes a sick joke that someone thought they would be safe and appropriate. The section proposed below has a nominal value of just 1.1m which seems reasonable at first glance. However that width includes the edge of the road and the gutter. Neither of which are safe for use. The edge of the road where the asphalt meets the gutter has a rough and carrying drop from one surface to the other. And the gutter is steeply angled to the gutter wall compared to the main roadway.

On top of that you will have traffic less than 60cm away zooming by at 50kph.



Suggestion

In cases where the cycleway gets so narrow as to be unsafe I would suggest dropping the speed to 30kph and making it clear that the section of road is for mixed use of motor vehicles and micromobility.

Why does speed keep coming up?

Along the route where changes are proposed, all but one road has traffic flows over 8,000 vehicles per day. Kaiwharawhara Rd has traffic flows of over 12,000. According to the chart in Paneke Pōneke, traffic speeds of 50kph should only have unprotected cycle lanes up to around 3,000 vehicles per day. This is also reflected in the guidance given by Austroads.

The design as proposed does little to meet even the interim aim of Paneke Pōneke to maximise uptake of the network.

Reducing the speed limit to 30kph will have a dramatic impact on the perceived and actual safety for those using active transport modes. The impact to drivers would be minor as they have shown an ability to adapt to lower speeds and traffic signals due to the slope stabilisation work along Ngaio Gorge.

Slower speed limits would also improve the environment for residents through lower road/traffic noise as well as the industrial/commercial businesses through lower noise and improved worker safety.

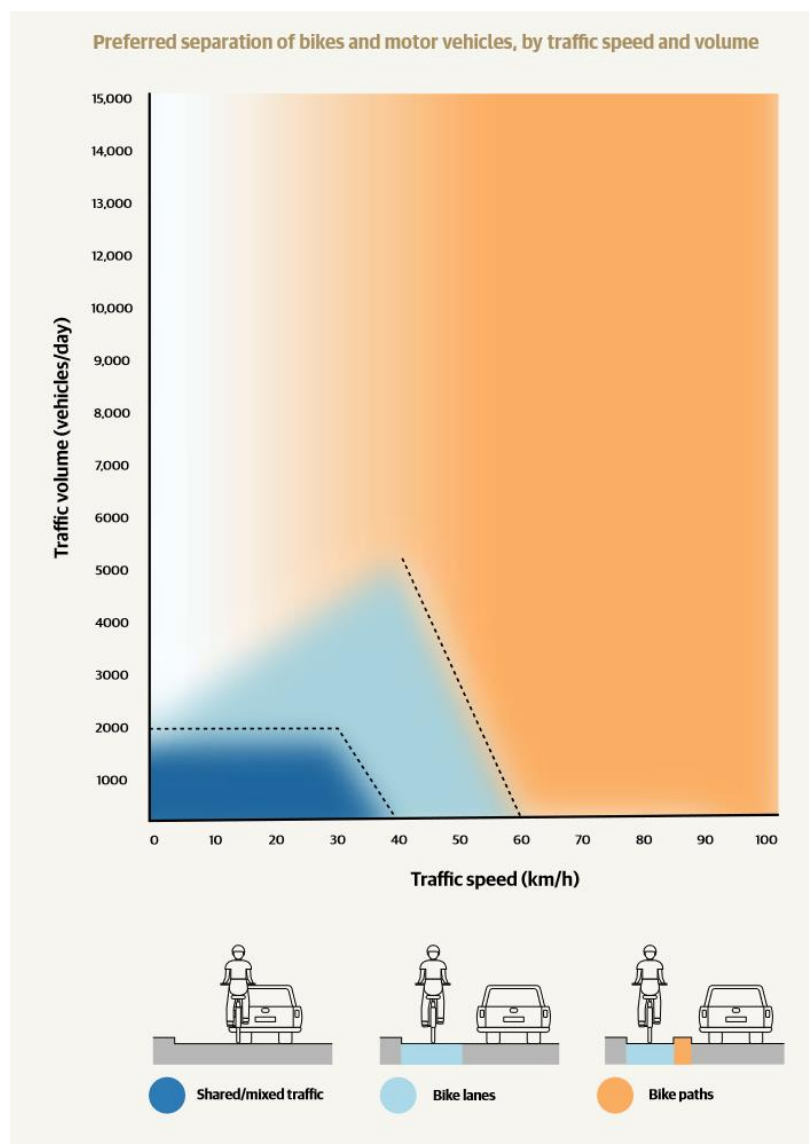
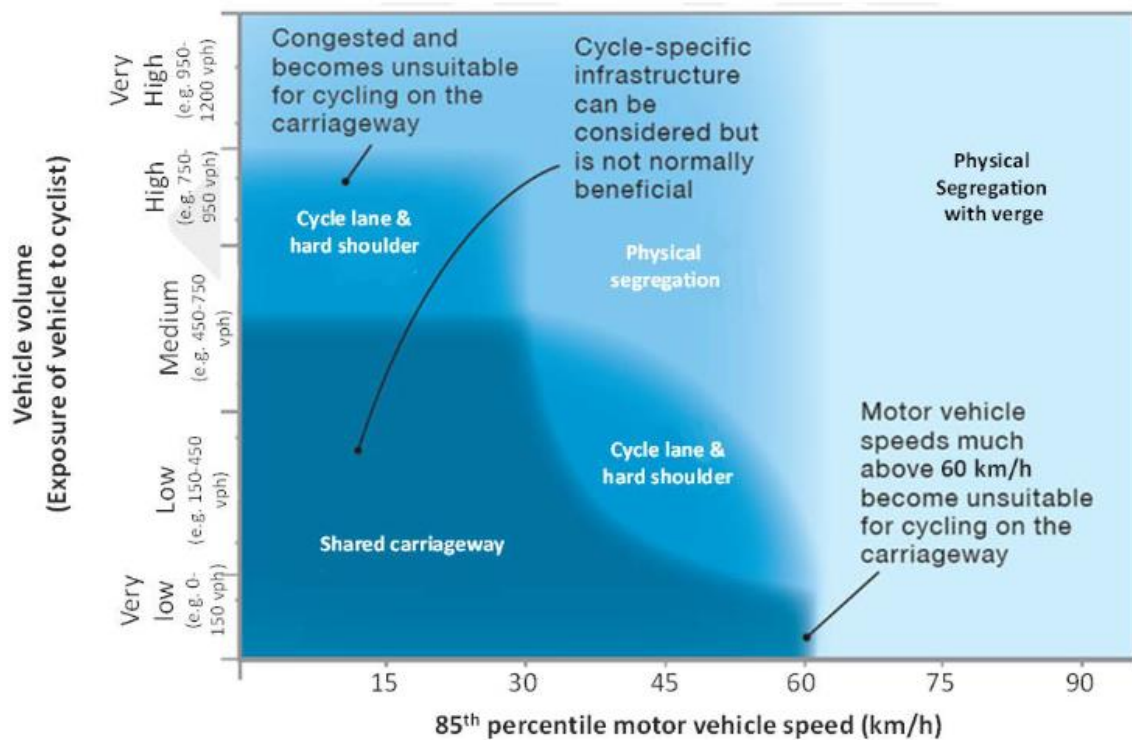


Figure from Paneke Pōneke on the preferred road designs for traffic volume and speeds

Figure 2.2: Guidance on the separation of cyclists and motor vehicles for the preferred bicycle route



Source: Sustrans (2014).

Figure from Austroads on the design of cycling infrastructure and suggested designs for given mixtures of road speeds and traffic volume

Feedback

624

114179380848

NAME: jan	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

NO. You are killing the businesses in the area and also giving residents no options for parking.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Are you trying to kill business in this area completely!!!!!!!!!!!!!! Seriously there are no carparks there now & you want to remove 43 carparks. How do you expect these businesses to survive? Where are cars going to park? Are you going to build a carpark? Where are the residents going to park? As a resident in the area this area I want to be able to park in this area without having to walk 500m carrying goods from these retailers or the local gym. Why do you need a clearway in the afternoon at all? Why do you need a pedestrian crossing? To my knowledge there has been no accidents here so why do you need a crossing when all it is going to do is create the loss of carparks? I'm all for progress but this is the most hair-brained scheme I've ever come across.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Really, are you trying to make it difficult for people to even live in this area? Why do the speeds need to be changed? 50km/hr is absolutely fine.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I see no benefit in this. Where are the residents meant to park/where are visitors to the residents meant to park?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Again where are people meant to park? As a resident in the area having a 30km/hr speed limit is completely unnecessary. It means that people will actually cause accidents when some drivers go at 20km/hr rather than the regulated 30km/hr which is what happens going through the current reduced speed zone.

What do you think of the timing of the proposed approach?

Why do you think that?

I completely disagree with the proposal & you have not asked that question. The timing above does not matter as I am completely opposed to the changes full stop!

Feedback

625

114180566759

NAME: Jeff Galt	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

While generally supportive, I'm not sure an uphill shoulder really adds much value. It's quite steep and with limited traffic doesn't cause much of a hazard for cyclists.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?
Question positioning of crosswalk - would this be better placed before Perth Street? Would benefit cyclists turning right (which is an issue) and pedestrians crossing to catch the bus?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
Not sure I really see the need for Stage 2, 2 pm seems early. Understand that businesses need time to adjust - but wonder if most of the parking isn't for commuters? Businesses will have a better understanding of who's parking there than I do...

Feedback

626

114181615844

NAME: Jessica Matcham	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I think it's a weak attempt at a cohesive cycle lane. It ruled out making the pavements wider and dual purpose because we can't have bikes/people mixing yet WCC is fine with e-scooters/people. It ignores children's right to safe cycling and it still prioritises on street private car storage over transport. It's one way on different sides and mostly only protected by coloured paint. Crossings are good.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I can't see how much of the bike lanes will be protected - essentially, would WCC think a 10 year old would be safe to bike along it? Because that's the safety of cyclists we should be aiming for. I also used to go to the gym down there, SWET and I'm really disappointed by their stance on this, I walked, took public transport to their gym and I feel they should be encouraging active transport. More pedestrian and cycle safety would benefit the businesses down there and surely some negotiation with the owners of the carpark across the road would be possibly to use it outside of their opening hours - the gym is most busy very early mornings.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

I support safer speeds but this appears to assume that green paint will protect cyclists from cars. I'm a cyclist and I can assure you that the paint isn't magic.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

I really strongly support the raised pedestrian crossing. As a cyclist I'm less thrilled by patches of paint being regarded as useful for separating cars and bikes. Especially as this isn't a cohesive bike lane - it's one side only and the side switches. If cars can drive and park on it then they will. Again, would my 10 year old be safe to bike on this to go up and down the gorge? Because his bike is too big to legally bike on the pavement, despite e-scooters being allowed to.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Don't know

Do you have any comments to make about the proposed design?

Putting aside the fact that this is a patch of cycle lane that doesn't connect to anything I have a 7 year old who would love to cycle to and from school. He'd need to cycle on Kenya street in both directions to do this. Will this keep him safe? Were children as users of roads and pavements considered in this design? Or will cars completely line the other side making cars and buses use the bike lane. Remove parking from both sides of the road - on street parking is taking public property for private use. Roads should be for transport, not storage. The picture also shows a bus passing a parked car while staying fully in their own lane. As someone who travels along Kenya street everyday by bike/car and bus I can assure you, that bus is not drawn to scale and would be well in the other lane. This might make the road slightly safer for current cyclists, it won't encourage new cyclists. We need cohesive, safe (barriers NOT paint) cycle lanes that everyone, adults and children can use safely if Wellington wants more people to use bikes.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think we proved during lockdowns that when people need to they can adapt to keep other people safe. There's no reason to delay safety measures so businesses can "have time to adapt"

Feedback

627

114181440165

NAME: Jeff Flavell	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

The gorge road is the major route to and from the Hutt and Wellington city for cyclists. I strongly support making this route more safe - particularly during morning and evening peak motor vehicle traffic on this route.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

The clearway will be really helpful and a separated lane will also make this stretch of road safer to travel through.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

With removal of parking off Kaiwharawhara Rd drivers will look to Cameron St to park - ensuring a safe uphill cycle route on the left is prudent. It is pretty heavily used by cyclists.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

A pedestrian crossing is long overdue and will be well used by bus passengers - many getting off a bus at the top of the gorge that continues a route that is not convenient if they live in Perth St or Trelissick/Kenya streets. The sharrows will help as taking the downhill lane from the Ped crossing is important for a cyclist not wanting to end up very left and off camber on corners lower down the Gorge Road. Please make the raised crossing gently to ride over on on a bike.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

This is a slow section uphill in a strong southerly - having a lane uphill where a cyclist can grind up into the wind, while being separated so the occasional wobble isn;t dangerously into passing traffic will make this safer. Sharrows down hill will help socialise and legitimise a cyclist taking the lane. In a strong northerly, this stretch has strong cross-winds and gusts - making taking the lane really important. I's support lowering the downhill speed to 40 km'h to reduce speed differential between different modes sharing the carriageway.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I agree that businesses should have time to adjust to the clearway changes - some businesses service vehicles and use the kerbside parking for customers vehicles, and others use it for their commercial vehicle/service vehicle parking at present.

Feedback

628

114181171945

NAME: Jonathan Markwick	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Not being killed for riding a bike is more important than convenient parking.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I support the removal of parking spaces for safe cycling infrastructure and all-day bus lanes. I disagree with safe cycling infrastructure being only part-time. People on bikes are not like buses. Bus lanes exist to make buses quicker and more reliable. Bike lanes are there for the safety of people on bikes, and should be permanent. Buses are not put in danger by being in mixed traffic, while bikes are. I support the Bus lanes being operational all day. If you are unwilling to remove parking spaces on Kaiwharawhara Road, please reconnect Kaiwharawhara Road with School Road so people can bypass most of the busy section using the quieter School Road. Signalise the intersection with Hutt Road if necessary, but there is already space in the median to wait.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I strongly support the removal of parking spaces so that people on bikes are not put at risk of death.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

If you insist on keeping the parking on Kaiwharawhara Road, please build an alternative route to bypass the business area (eg. reconnect School Road)

Feedback

629

114167357671

NAME: Joanne Bond	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This plan acknowledges: cyclists need a designated space on our byways to move safely, & efficient movement of all traffic can only occur if our byways are not used mainly as car parks! Congratulations on proposed changes.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I propose the 30km/h speed limit be extended to the top of Kenya Street, where it meets the Ngaio Gorge Road. Kenya Street is a busy narrow road. 50km/h is an unsafe speed for vehicles to travel down this suburban street.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

It is encouraging to hear expert transport planners acknowledging the safe & efficient movement of bicycles pedestrians & public transport (and private cars) can only occur when roads limit (or eliminate) car parking on roadways. Roads are byways not car parks.

Feedback

630

114167199512

NAME: George Sederis	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Yes it's great that cycleway improvements are being made in Ngaio. It will make it safer for cyclists, and introduce new cyclists. In the longer term it would be good to have more robust cycleways (more separated from the traffic) but this is a good start. Excellent!

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Great to positive cycling improvements, to make it safer and easier to cycle around Ngaio to/from the city. Will reduce the number of people driving, and encourage new cyclists.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Improves networks throughout the city for cyclists, by connecting areas together to cycle.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Positive to have more safety for cyclists on the Ngaio Gorge Road. Ideally would be great have a physically separated cycleway (eg concrete lozenges) and to have bus stops in line with the road (to enable a continuation of the cycleway), but this proposal is a good first step.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Good to make improvements for cyclist safety on Kenya St/Crofton Road. Ideally in the long term it would be good to have the cycleway physically separated (concrete lozenges etc), but this is a good first step to improvements.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Retailers in this area of Kaiwharawhara have had many years to prepare for the move from on street parking, and to have it replaced by cycleways. Retailers would have be witnessing the cycleways built just nearby in recent years. Retailers have already provided off street parking for their customers. This should be an easy change for them to adjust to. Clearways will need to be policed/ticketing to ensure they are clear during this interim period, if this is the approach that is taken. Even just a couple of cars parked in a clearway makes it hazardous for cyclists having to dip into the main traffic flow - most will just stay in the current car lane (if it is like any other 'clearway' in Wellington that is not enforced to ensure cyclist safety).

Feedback

631

114181482053

NAME: John Toogood	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

It is too disruptive the commercial aspect of Kaiwharawhara

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

We have a warehouse in the K-Road Complex for our online floor mats and rugs business. We provide a customer pick-up service but there is no public parking in the complex. Our customers often park out on Kaiwharawhara Road and walk around to us to confirm parking arrangements outside our premises. Our products are bulky and it is very import from a customer service point of view that the current facilities at Kaiwharawhara remain unchanged. There will be others with similar situations and it is totally irresponsible for this council to make town planning changes that favour one aspect of city use at the expense others. WCC's reputation with the commercial sector of our city is seriously challenged.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

I do not understand why there is plan for only an uphill trail. How do downhill cyclists ride in safety. The scheme should be abandoned

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback

632

114167914917

NAME: john andrews	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

In general I strongly support, but think more consideration of parking for the takeways business need to be given. This could be a clearway on the opposite side of the road, to provide evening parking for the shop's customers. The area outside the shop could have more parking at weekends.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

633

114160385040

NAME: Jonas	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

634

114181681064

NAME: John Beaglehole	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

These changes will make this important cycling connection far easier to use, and attract more cyclists.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?
I would like to see greater traffic separation than simply paint (which doesn't do much to stop drivers from running into vulnerable road users) -- and there is space here. How about at least some wands?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback

635

114160272504

NAME: Julia Rendell	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Good to have safer corridor for bikes and also to make it safer for pedestrians

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Would be good to have uphill bike lane and safer place to cross.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Support having a safer crossing as that one has poor visibility and cars speed. I think there should be more measures to reduce speed further along kenya as cars go very fast. This can make it difficult to cross further up the road. Would be good to have more clarity on where parks are being removed. Lots of cars park on footpath because of the difficulty of getting cars on both sides of the road. This makes it difficult for pedestrians. Would hope that changes would take that into account so that the remaining parks have enough room for people to park on the road.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

636

114180694875

NAME: Karl Tiefenbacher	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Narrowing roads will equal more accidents

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Designing a uphill bike lane that has to stop for buses is stupidity and is an accident waiting to happen as cyclists will try and overtake the bus

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Removing all these car parks is terrible

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Hopefully you will come to your senses and not do it at all

Feedback

637

114160113956

NAME: Kate Clarke	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Businesses consistently over estimate the value of parking

Feedback

638

114166384054

NAME: Kay	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

There a number of businesses who will become unviable with nowhere for workers to park or to allow customers to visit. Residents in apartments in area will have no where to park. The number of buses and cyclists are not that high to make such a disruptive change. Changes should wait until repairs completed in a Ngaio gorge road. Also need overbridge rebuilt at Kaiwharawhara to allow more commuter options. Footpaths in lower Kaiwharawhara Road reduced in width on both sides of road would allow more permanent parking while still accommodating cyclists

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

30 minute all day parking not viable for businesses and residents. Should be 60 minute 8-6 Mon to Friday. Having business meetings in this area usually longer than 25 minutes. Bus lane 4-7pm in weekends does not make sense, as not peak travel time on these days

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

What options do residents have to park particularly if they want to use public transport to travel to work

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Disagree totally no where to park for workers of businesses in this area

Feedback

639

114169261790

NAME: K Kelly	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

It is imperative that there are dedicated and separated cycle lanes for both uphill and downhill cycle traffic. Separation needs to be real barriers and not just paint on road. Cycle lanes shared with traffic is a hazard and will be of no benefit.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

There must be dedicated cycle ways both uphill and downhill. Shared downhill with cars are a hazard and of no use. Please ensure separation of cycle lanes both ways is real and not paint on the road.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Please ensure cycle lane has real barriers and not just paint on roads. Must be usable both ways.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Go faster. Get rid of used oil tank at bottom of gorge. Its an eyesore and land can be used for parking.

Feedback

640

114174280203

NAME: Kirsten Clayton	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

We live at [redacted] Ngaio Gorge Road. Our comments relate to the proposed crossing that will be right beside our house. We have two concerns: 1) Access to our premises - we have two cars, both of which we park on our property. For one of our cars, we access the carpark by driving over the footpath kerb (see white car in photos attached to show where we usually park our car). We are concerned that poles or other structures erected for the crossing may interfere with our car's access to its car park. Given the amount of on-street parking being removed in our area, it's important that access to our current off-street parking is preserved. 2) Heavy traffic noise from the raised crossing. The raised crossing will mean traffic has to slow down and then accelerate right outside our house. We think this is likely to be very noisy - particularly with heavy traffic in the middle of the night. We support a crossing, but not a raised crossing for this reason.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

There is not enough carparking available in this area to remove this number of car parks.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

IMG_3148.jpg



Feedback

641

114170479274

NAME: Kirsty	SUBURB: Ngaio	ON BEHALF OF: on behalf of an organisation	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The impact of these changes on the businesses and community is big. Some of these changes have a flow on effect that may mean businesses fold, and this is not a good look after a long, challenging few years. Ensuring that the needs of everyone are catered for and taken into consideration is key. More consultation, with longer lead in times (not in a week for the most recent meetings) means you can get wider input and ideas from people who represent the area. Collectively, people can navigate ideas together and feel empowered as they have had a voice, a say and felt included. I believe in change, but that this needs more work with us being listened to and heard in a broader representative way, not just small pockets of the community.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Removing car parks for businesses and residents in this area is not thinking of the wider picture. Swet gym's peak hours are 5-8pm and their business would suffer greatly with the parking changes. As would turning 5-6 parks that work in Cameron St, into two. Impacts on businesses is key. Staff also need to park and come to work. Life is expensive enough as it is, without extra costs. If the issue is long term cars on the south side, offer a solution for parking - eg: coupon parking after 2 hours. The safety of cyclists returning from the city is not impacted by parked cars.

How will the council offer income loss for businesses who lose out? Surely, we want to build out city, not damage businesses already functioning on a thin line. This needs more thought, options and discussion.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Why can't Old Porirua Road, a dangerous road for vehicles both ways, be used as a cycle corridor? Putting a cycle option puts people, young and old, at risk of being knocked off. The unrestricted carparks - are they used for gym members or cafe visitors? If so, P60 is a bit short, how about P75? Whilst we all acknowledge change with cars is occurring, some of these decisions are not practical. Make 4 cars into 2 seems irrelevant. Again impact visitors for shorter time frames and their access to these shops and businesses.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

This puts the crossing at risk, having cycle lanes on both sides. Can the downhill side be smaller? A crossing would be useful, but what tests have been done around the safety and backup of traffic in both directions, given that this crossing is so close to the corner of trellisick cres? Could different places for crossings be explored? The Kenya st traffic would essentially be a blind corner and it would be terrible for a car to hit another car and roll onto the crossing.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The parking close to the dairy, butcher and motor doctor, could be working hours instead. (Between the round about and the takeaway shop) so that these businesses are still frequented by customers. What offers of solutions are being offered to those who have visitors (elderly and the like) as well as those with no off street parking?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Again this hurts the businesses, where employees park and or short term customers. Hanging around needs places for the staff to park, perhaps with a ticket system. And the auto electrician works on vehicles so utilises street parking, to make a living. Again, thinking about the financial impact on businesses here is paramount. This needs more workshopping and discussing so we don't end up putting people out of business.

Feedback

642

114160125733

NAME: Kirill	SUBURB: Churton Park	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do it quicker. Too much consulting with the businesses.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do it quicker than indicated.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Safer place to cross should be installed much earlier than that. It should've been there 5 years ago.

Feedback

643

114174224312

NAME: Catharine	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

I am either a heavy user of this route depending on work or a once a month user as a cyclist so can't comment as if a commuter or resident or business. I am happy with the road the way it is using either mode. But would like to see counters along the road - top to bottom to see if the changes actually do bring on more cyclist. The council counters are not a good reflection of cycle lane usage as are often not where the changes are being made. The businesses and residents of Ngaio and Kaiwharawhara have got off lightly compared to those of Island Bay and Aro Street.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

I think cycle lane counters are needed on this section of the cycle lane. Only then will it be obvious if this cycle lane has lead to an increase in cyclists or it's just been a detriment to the local businesses. Not sure if it isn't better not to have a separated cycle lane. My experience is that it enables cyclists to take no responsibility for their own safety and they often are not aware of the road rules and courtesy to other users and other cyclists.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

I don't use this section so can't comment.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

The paint makes the road very 'busy' and hard to read as a cyclist and as a driver. There is paint everywhere and one is so busy trying to sort out what paint means what. Visually polluting. I'm surprised that the cyclists aren't complaining about the bus stop in the middle of the cycle lane and how they feel abandoned.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

As long as I don't have to use the cycle lane it's ok. And that there are no 'bumps' for residents. Again, cyclists need to take responsibility for their own safety as well and wear gear that can be seen, use lights and learn the road rules.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

The council will do what it wants to do anyway. This proposal isn't nearly as disruptive as Aro St or Thorndon Quay.

Feedback

644

114160167615

NAME: Lyn Garrett	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

A step in the right direction.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Strongly support these changes. I cycle this route most days and believe these changes will make a substantial difference to the safety of cyclist on this route. I'd like to see more work done around the Marsh Way / Sanderson Way / Cameron Street intersection to slow traffic speeding up Cameron Street and turning straight into Marsh Way at speed.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
I don't cycle this route into town, having done it a couple of times and feeling seriously unsafe pedalling up Kenya Street.

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback

645

114160225384

NAME: Tim	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This is a bold and progressive design, and as a regular cyclist on the route I am fully supportive. Thank you WCC.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I would like to see stronger signage/paint/enforcement in the current bus-lane and clearway on Kaiwharawhara Road. There are cars parked in it regularly before 9am or in the buslane.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

This is required. I would ask that the raised angle is not too steep for cyclists to ride over easily from a standing stop.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

This is bold, but the best option. As a cyclist on this route, it will make it much safer. Consider extending the 30kmph zone to top of the Gorge.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think 6 months between phases is sufficient. Support should be given by the Council for an off-street parking facility in the area. Potentially on the site of the disused tank farm.

Feedback

646

114160162642

NAME: Leah Russell	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

The proposed changes improve the safety of cyclists and pedestrians on the route that is well-serviced by public transport.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Because I currently ride to work from Johnsonville and would use this route home if it were safer

Feedback

647

114181587419

NAME: Libby Carson	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I am completely opposed to this entire initiative, not because I am opposed to cycling, walking or investing in the future by reducing carbon emissions, what I am opposed to is an initiative that does not appear to have any supporting evidence, a proposal that will have a catastrophic and detrimental effect on our business community, will have a major impact on people trying to earn a living and incur an unreasonable cost on our community across a number of areas.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I would like it noted that I strongly oppose the proposed changes to the designs currently being reviewed by the Wellington City Council. Our community works well and is highly considerate of pedestrians and cyclists alike and the cost, the impacts and the intrusions on our daily lives will be significant. Notwithstanding we don't believe the designers behind these changes have an understanding of how our community works and we also believe there to be a significant conflict of interest with the Project Team Leader. These suggestions seem flawed, at best, as despite interactions from numerous residents and businesses, there appears to have been no compromises made to address the concerns shared by our wider community.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Absolutely not - it seems preposterous for a Council to be making changes to a street that needs no changes made to it. We are strongly aware of all road users (pedestrians, cars, cyclists) and by limiting the usage of the roadways will create more issues. There doesn't seem to be any cohesion between Council departments regarding the care of the vegetation on the western side of Cameron Street and the flow into the Bridle Path - surely it would be prudent for all departments concerned to have a cohesive and considered approach to the whole project. What is the point of a cycle lane that will lead into a "no-man's land" of private roadways and onto the Bridle Path which needs significant work done on it to ensure we have a safe dual-purpose pathway. The painting of lines, changing of speed limits and adding parking signs - creates a cost that this city should neither be focussed on nor entertain given the other problems this city faces. We are also concerned that there does not appear to be any data to support this initiative nor an understanding of how our community uses Cameron Street - for example, a good proportion of cyclists and pedestrians don't use the lower part of Cameron Street, rather they use the pathway up from Picking Street and thus only use Cameron Street for some 150 metres.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Once again we find this to be an ill-considered attempt to remove vehicles from the road without a comprehensive solution that works for all road users. By narrowing down the uphill and downhill sections of Ngaio Gorge Road will create more congestion, more air pollution and more frustration with road users. Until such time as you have a sensible, reliable and functional public transport system in place, it seems an irresponsible decision to progress with this.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Once again this is a very narrow roadway at the best of times and by removing parking (and noting there are a number of residents in Kenya Street who rely on street parking) you are having a direct impact on a community. Whilst in an ideal world we would all be out of our cars and on public transport or on bikes, in Wellington currently it is simply not a feasible option.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The proposal to, ultimately, have a permanent cycle way on Kaiwharawhara Road will have an unreversible and catastrophic effect on the businesses in that area and ultimately on our community. There doesn't appear to be any data that supports this drive to put in a permanent 7-day cycle lane where there is no evidence of heavy cycling traffic. There are so many other options the Council can implement which will provide a compromise which will suit all parts of our

community. Additionally, why 4-7pm - why not 4-6pm Monday to Friday to align with the rest of the City?

Feedback

648

114164336876

NAME: Tim	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback

649

114162326119

NAME: Lorraine Joslin	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

As a cyclist a green line will not make me safer, slowing the traffic and encouraging pedestrians from drivers makes more sense. Slowing the traffic would in Kenya street would make everyone safer.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Business need the parking to trade. The road is wide enough to share.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

We should encourage cyclists to use the ngaio gorge not the bridel track. Electric bikes going up the track at speed are dangerous to pedestrians walking the track.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The road is too narrow for a designated bike lane. The speed should be reduced and the road can be shared

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The road is too narrow for designated bike lane, reduce the speed limit and share the road.
Business need the parking spaces to trade.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Dont agree with these changes.

Feedback

650

114172952935

NAME: Lucy Weston-Taylor	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

It's a good start, we badly need some proper cycle infrastructure along this route. The proposed timing is far too slow, we need to make cycling safer and more attractive now. There are a ton of people who would like to cycle, but.... we need to design so that these people who are hesitant feel that they can give it a try.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

My main issue is that the proposal here takes far too long, this should be done in the next 6 months. The parking on the south side of Kaiwharawhara is incredibly dangerous for cyclist, particularly outside Hanging Around. If you're serious about cycle safety, these parks need to go now, not wait around for another few years, just because some people are used to having free parking. Parking that hasn't been addressed is parking on the north side of Kaiwharawhara down towards the lights. This is dangerous because of dooring, and there is often barely any room to pass between parked cars and cars waiting at the lights. Why are these parks more important than cyclists' safety? Also the cycle lane/transition from Kaiwharawhara to Thorndon Quay needs to be addressed - at the moment if the lights are red you have the choice of waiting in traffic, or filtering through hoping 1) not to get doored by a parked car, 2) that there will be room between

the parked cars and cars waiting at the lights, and 3) that you will make it to the stop box before the lights turn green. If you are not at the stop box, then cars turning left will not expect cyclists turning right, and it can be tricky to get across that intersection. Some rethinking needs to be done here

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

I don't ride the Bridle Path, but anything to make it safer for cyclists is a good idea.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

An uphill bike lane is good, a separated bike lane would be better. There needs to be some markings (bike lane colour maybe) so that vehicles realise that cyclists will be taking the lane at the top of Ngaio Gorge to turn right into Perth St - currently this right turn can be hard to make as you're going uphill/checking behind you with not much visibility/trying to move into the lane. Sharrows are not going to do anything. I realise that there may not be space for a cycle lane here, but I think this needs to be prioritised. Some people are happy going down at 40/50 km/hr, however there are those of us who are not comfortable going so fast - I typically go down 25/30ish km/hr with a toddler on the back, and while cars at the moment are mostly ok because of the lights, it can get hairy when impatient drivers try to pass. This is one of the scariest places in terms of being a blocker for people to try cycling the route - it feels bad, and it feels pressured because of the narrow road and cars going fast downhill - if we want to encourage people who would like to cycle but are worried about traffic, this is one of those blockers. The pedestrian crossing is great, really needed

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I'm sure that people will be annoyed about the removal of the car parks, but we need to remember that individuals are not entitled to free use of council land. Safety for cyclists (and pedestrians), and supporting people moving towards a more climate friendly mode of transport

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Safety is paramount, dooring is a huge risk along that stretch of Kaiwharawhara, why are we prioritising individuals' free car storage over other's (also ratepayers!) safety? WCC agrees that we need to reduce carbon emissions urgently, making cycling and public transport use safer and more attractive aligns with that goal.

Feedback

651

114174894394

NAME: Lynn Cadenhead	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I would prefer if the time frames were shorter

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Paint is not adequate protection for cyclists

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Cycling needs to be safer now

Feedback

652

114173550990

NAME: Margaret Nixon	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I do not agree that cycles or scooters are sensible transport options in a city with Wellington's topography. I would prefer that the resources being put into cycling and scooting be put instead into reliable public transport.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Adding cycleways will add congestion and make the bus even slower.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Adding cycleways will add congestion and make the road even more dangerous.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Adding cycleways will make the road even narrower, cause congestion, and make the road even more difficult for pedestrians than it already is.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Adding cycleways will make the road even narrower than it already is, cause congestion, make it even more difficult for buses, and even more dangerous for pedestrians.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

"more slowly" must also encompass "never" as consultation, as defined in case law, must always include the option of a proposal not going ahead.

Feedback

653

114176798453

NAME: Martin Taylor	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

We want to encourage more cycling commuters

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I would like to see a dedicated cycle lane in each direction to make it safer and less daunting to encourage more people to cycle

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Sounds good but I don't know the route well

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?
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I would prefer to see dedicated bike lanes in both directions to encourage more people to cycle. It can be a bit daunting cycling down the gorge knowing that there are cars behind, causing you to perhaps go faster to reduce driver annoyance, or slower to stay safe
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Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
--

Support

Do you have any comments to make about the proposed design?
--

If there isnt room for cycle lanes in both directions, would you consider extending the 30km speed limit up Kenya St to Trellisick Cres

What do you think of the timing of the proposed approach?
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Make the proposed changes more quickly

Why do you think that?

Feedback

654

114181183139

NAME: Mary Munro	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

Oppose the position of the pedestrian crossing. I think it should be further east ie at the top of the Ngaio Gorge Road. Cars coming up are going reasonably slowly and deciding whether or not to turn into Perth Street. If the crossing were from the west side corner of Perth Street to Trelissick Cres. corner cars coming UP the Gorge could still turn freely into Perth Street and cars coming on into Kenya Street would have no need of a raised section as they are generally going reasonably slowly at that point and can see ahead. If the crossing were moved further east it would also mean that cars going DOWN the Gorge from Kenya Street would also have a better view of the upcoming crossing (which would mean no need for a raised section) and more time to slow down if there are pedestrians around.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?**Oppose****Do you have any comments to make about the proposed design?**

Oppose the removal of 57 car parks on Kenya Street. Where do people then park? The Council needs to seriously reconsider its current non-requirement for off-street parking for new developments as this puts many more cars on to the streets for parking. A lot of properties in Kenya Street do not have off-street parking either for the owners or visitors, and new developments eg at 128 Trelissick Crescent (potentially) are only going to compound the problem. Ngaio already has serious road congestion along major roads - Kenya, Ottawa, Cockayne. Removing 57 car parks in Kenya Street seems very short-sighted.

What do you think of the timing of the proposed approach?**Make the proposed changes more slowly****Why do you think that?**

I think you need EVIDENCE at each stage. I understand the need to reduce car use BUT with problematic public transport options (particularly the buses) AND the anticipated rise in electric car use AND an ageing population which has no interest in cycling AND the Wellington weather, it is unlikely that the planned changes are going to see much difference in road usage in the short-term. So, if you stick to the existing timeline without showing evidence of positive change in road use then the project becomes idealistic and not realistic.

Feedback

655

114161569018

NAME: Mat	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

656

114161172169

NAME: Matt	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Fantastic idea, making it much safer encourages more cyclists

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Makes it safer - too many near misses at present means less people cycling due to lack of safety

Feedback

657

114170572554

NAME: Matthew Lawrence	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

658

114173591206

NAME: Matthew	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Strongly support these proposed changes. They will help make cycling safer and more accessible for those living in Ngaio and Khandallah.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

The proposed design is good but see no need for a staged approach. It's in contrast with the trial based approach the council has adopted on rolling out cycle lanes across other parts of the city, which delivers faster results and allows for adaption if necessary.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Strongly support this. But can you consider putting in some form of separation for the cycle lane on the hard corner on ngaio directly below the perth st intersection. As a cyclist I'm conscious of going around that corner uphill and having traffic coming up behind you who won't see you until they're almost upon you. Perhaps some form of soft separate on like those flexible plastic bollards? Also for the downhill and in addition to the sharrows, plenty of signage reminding drivers to share the road.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Support all of this. The car parks on Kenya create pinch points for cyclists and cars. Agree on the extension of the 30km zone.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think you're overestimating the time people need to adapt to these changes. Also the approach adopted in other parts of the city is to trial an approach and see what works. If it creates problems then you can always revert to a staged approach.

Feedback

659

114177995510

NAME: Matt Shipman	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Strongly support. We need people to feel safer accessing active modes of transport. Leave the driving to those who "need" to drive, and let others get the benefits of being able to get around in a sustainable and efficient way.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Currently a horrible place to ride uphill, but fairly good downhill. Too many parked cars and drivers not giving cyclists and scooter riders enough space or care when passing on the uphill. The parked cars are causing the issue.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

The Bridle path is a very nice, away from motor vehicles route, and is the most direct to Tawa (and also good for Johnsonville if you don't like breathing in the exhaust fumes on Ngauranga gorge). Currently hard to access and not known about by many people

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Excellent. Gets rid of the conflict of uphill cyclists and drivers sharing the same road space at very different speeds. Some education needed for drivers and riders about how to ride downhill, and where drivers should expect and respect riders to be (in the centre of the lane, using gravity). Newer or less confident riders may need some education and training on how to ride the downhill sections safely and efficiently. If this goes ahead more people will be able to take advantage of an active, safe, and lower congestion commute in to town and back. Also an excellent EBike destination with a much smaller carbon footprint than a single occupancy motor vehicle.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Current parking makes this area incredibly difficult to navigate for drivers and cyclists alike. No need to have this much on street storage for private property, given the houses and population along this stretch.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think this timeline works better than the Aro one - simply because the areas that are more intimidating for riders are mostly covered in stage one. I also acknowledge that the lower area is far more complicated

Feedback

660

114181255475

NAME: Usha and Roshan Patel	SUBURB: Ngaio	ON BEHALF OF: Crofton Road Dairy	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The proposed changes will impact our small business significantly in a negative way. Our business and livelihoods rely on the support of passing motor traffic. By removing the parking available outside our shop, the ability for these customers to park and enter our shop is removed. Hence, destroying the feasibility of our business and destroying our only source of income.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Neutral

Do you have any comments to make about the proposed design?

As long as the businesses have been consulted and detailed plans are in place to help and support businesses financially if the proposed changes negatively effect their businesses

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

We own the dairy at 2 Crofton Road, serving the local community for 25+ years. Our business and its financial feasibility rely on the passing motor traffic, parking up and entering their store. Your proposed design results in a significant loss of parking space directly outside our business. Parking space our customers utilise to enter our shop. Parking space that is utilised by delivery drivers to supply our shop. Parking space that my we rely on to park our business/private vehicles, which are a necessity to us. Overall, your design ultimately destroys our business and our only source of income. We understand the importance of the project and how it can make travelling through our community safer and more efficient. However, we feel it is not fair that our business and our lives should effectively be sacrificed as part of the project. We have had little communication from the council and don't believe the effect of the project on our business has been adequately analysed and considered.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The council should make the changes slowly so they are able to gather more feedback and data on the impact of the project. And use the feedback/data to shape the next stages

Feedback

661

114181657317

NAME: Michael Crowley	SUBURB: Other	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I grew up in Ngaio and have ridden this route many times, this is an important road for moving people safely

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Support
Do you have any comments to make about the proposed design?
Kenya St Uphill could do with some kind of physical protection, had several pinch type near misses from people driving up this road

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Dragging it out delayed the inevitable

Feedback

662

114160263404

NAME: Michelle White	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Mail sorters and courier van drivers need to park in the parks you propose to remove at afternoon/evening peak. Gym goes to Swet also attend classes at peak time.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Residents need on road parking near their properties.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly oppose
Do you have any comments to make about the proposed design?
This is crazy, those car parks are fully utilised by residents every day! Where are they supposed to park their cars?!

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
Removing car parks will kill businesses and encourage people to sell houses along Kenya Street.

Feedback

663

114179709236

NAME: Mike Russell	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

My opposition to the proposal primarily relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the likely damage to businesses and property owners. For this part of the route the uphill section needs to be treated in the same way as the downhill section.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The proposed changes to the Kaiwharawhara Rd section are not based on any evidence (actual or likely bike volume) supporting the need for a separated cycle lane priority over a non-separated cycle lane with clearway times matched to the needs of commuter cyclists. The proposal to automatically transition to a cycle lane with excessive clearway times (2pm-9pm) after one year (in 2024), and/or a separated cycle lane after two years (in 2025), are a disproportionate response to the needs of cyclists versus businesses who depend on on-street parking for staff and customers when there is little or no practical, alternative parking. Some of these businesses will need to plan for closure, the consultation documentation uses the word “adapt” but this is not commercial thinking. Those with leases (which are likely to have a personal guarantee from the business owner) will be faced with significant personal losses. Also, as a local resident, these

businesses are IMPORTANT to us. Commercial and residential properties will become vacant, with limited alternative use in the short to medium term.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

In the absence of usage data, a staged approach is sensible. However, Stage 1 should be longer to allow time to assess how those changes work and to collect actual usage data. There should be at least three years of usage experience and data collection which can then be assessed before any further changes are proposed. Any further substantive changes (such as extending the hours of the clearway or installing a permanent separated cycleway) need to be subject to further public consultation. While the Cameron Street cycle lane is sensible, there is no feasible link continuing from Cameron Street. i.e. the Bridle Path. Walking during the mornings and evenings is currently unsafe due to the speed that which the bikes travel down the path and the quietness of the e-bikes traveling up the path. This path is currently very unsafe for walkers and runners and more traffic will result in a serious accident and injury.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

My opposition to the proposal primarily relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the likely damage to businesses and property owners. For this part of the route the uphill section needs to be treated in the same way as the downhill section.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

My opposition to the proposal primarily relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the likely damage to businesses and property owners. For this part of the route the uphill section needs to be treated in the same way as the downhill section.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

There needs to be more data to make an informed decision.

Feedback

664

114181643194

NAME: Miriam Moore	SUBURB: Tawa	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I would prefer 24 hour cycle lane immediately, as that is the only way to provide equitable mode shift - not just for commuters. However, I understand the staged approach allows businesses the time to come on board.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Support but I am less likely to use this given through winter I often cycle outside of daylight hours and Bridle Path does not feel safe for women cycling alone. Would be great to have some long term thought into CPTED of improving Bridle Path.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Very glad to have protection up Ngaio Gorge. I would prefer more overtaking pockets. Previously I lived in Brooklyn and the width is fantastic for manual and e-bikes to ride in harmony. I respect Ngaio Gorge doesn't have the same width available, so if there were small overtaking pockets dispersed along the route it would help manage the two main speeds of cyclists. It would be better if paired with a speed change (lowered speed), or speed cameras. I cycle about 50kmph down the Gorge and buses and cars still feel the need to overtake. Sharrows are a good step for confident cyclists like me who can now feel validated to take the lane to avoid being over taken. However, they aren't great for children or learner cyclists who are more important targets of mode-shift. Love the speed hump as it is currently really hard to pull out into the lane to take the right hand turn into Perth Street, with the speed that cars come up the hill.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Really support this is a nasty place for overtaking as you reach the peak of the hill, I often take the lane to stop cars doing dangerous moves.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I understand the staged approach but 2025 is pushing it, we need mode shift fast, and timings only serves commuters. Families and more vulnerable users are more likely to need the cycleway outside of these times.

Feedback

665

114167151822

NAME: Mark Johnston	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Good changes with sufficient time for adaptation. Some more segregated bike lanes would be great.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?
A properly segregated bike lane would be preferable

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Support
Do you have any comments to make about the proposed design?
Again, properly segregated would be preferable, however this is a good change towards that and will encourage some uptake of cycling

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
I trust the timings of the clearway is backed up by evidence of when people mostly cycle. I'm glad that an evidence based approach is being taken and that you are giving people and businesses time to adapt. I think the timeframe is about right.

Feedback

666

114173722654

NAME: Matt	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Great start. Though believe it could be even more separated from traffic. Though understand cost, public acceptance etc are large hurdles.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?
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This is quite a busy area with blind corners and t intersections, that a lot of vehicles carry a lot of speed. If there is more that can be done to reduce speed I'd like that
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Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
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Strongly support

Do you have any comments to make about the proposed design?
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What do you think of the timing of the proposed approach?
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Make the proposed changes more quickly

Why do you think that?

Feedback

667

114181683893

NAME: Martin	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Driver behavior towards bikes are appalling and uphill bike lanes are a great initiative. I do not support extending slower speed zone. Traffic naturally slows during peak times.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Cameron Street isn't the most suitable street for this change. Could be worth investigating a cyclelane through Kaiwharawhara Park at a more suitable grade to connect or behind watertowers off Old Porirua Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Support
Do you have any comments to make about the proposed design?
Bike lanes are ignored by drivers without separation. Should use similar separators as with Brooklyn and Kilburnie cyclelane. Bus stops blocking both cycle lane and road lane are also dangerous as vehicles leave their lane to overtake and bikes become in the bus blindspot.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Support
Do you have any comments to make about the proposed design?
Support Uphill bike lane citybound on Kenya Street. Parking on downhill side should provide ample parking for residents and visitors while improving safety for two way traffic and bikes.

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback

668

114181663726

NAME: Anonymous	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

If the designers have decided that this new invention, the "cycle shoulder" is the only way to provide cyclists with safe road use here, then the only safe speed limit is an absolute 30KPH throughout. If it's too narrow for a proper cycle lane, it's too narrow for any speed over 30. Full stop.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Better signage to the Bridle Path, with the proviso that it is a steep climb; list the grade % so cyclists are aware.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

With this jury-rigged design, cyclists will not be safe unless speed limit is 30KPH throughout, and in both directions, at all times.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Extend the 30KPH speed limit all the way, on this entire project, from Kenya & Crofton right down to Hutt Road.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Aotearoa New Zealand have just promised at the COP27 climate summit to work with other countries to stop the worst effects of climate change as they continue to accelerate at a hazardous pace. We must likewise accelerate our local efforts to decarbonise our cities. There is no time to waste in continued dithering & deliberation. Make the changes right now to get more Wellingtonians moving without burning fossil fuels while they do.

Feedback

669

114181661416

NAME: Susan Coppersmith	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

My opposition to the proposal is due to the move beyond Stage 1 of the changes to Kaiwharawhara Rd and the lack of a plan for improvements to the Bridle Track. I am also disappointed with the consultation process. Insufficient effort was made to directly talk with clearly affected parties. The three-week feedback period is too short to affected parties to engage with, mobilise and get support of their customers.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

My strong opposition to the Kaiwharawhara Rd section is on the following factors. 1. A Shared Cyclist / Bus Clearway from 4pm to 7pm on Southern Side is Sufficient for the Short and Relatively Flat Section from The Living Room Down. Council officials have been reluctant to share information on current cyclist volume on this route, but in discussion I understand that uphill volume is around 150 per day, with a large majority (90%) of that volume being between 4pm and 7pm. I can support a clearway from 4pm to 7pm, but having extended clearway hours (2pm to 7pm) or moving to a permanent, separated cycleway on this short stretch of road is a disproportionate response to the small number of users (fewer than 20 per day) outside the 4pm to 7pm window. Even with significant growth in cycling numbers the volume will be low. A

separated cycleway can be put in place in sections which are already broken yellow line, leaving the remaining sections as a mixture of loading zone and parking of various time lengths. This change would make the treatment of the southern side of this short stretch of road consistent with approach being taken on the northern side of the road. It strikes a balance between the need for improved and safer cycling corridor, recognising the volume (and time of use) of cyclists with the legitimate needs of business that operate in this commercial area.

2. The Impact on Businesses and Residents Has Been Downplayed and Underestimated I know that businesses on the lower part of Kaiwharawhara Rd have a strong reliance on short-term customer parking in what is a commercial area. The change proposal talks about giving businesses time to “adapt”. The reality is that there is little or no practical alternative parking for these businesses (all existing parking is already heavily used). In this context “adapt” means closure, with the loss of livelihoods. These businesses also have lease commitments which continue even if businesses are closed. If these leases have personal guarantees from the business owners, the impact will be particularly severe. From talking to these businesses, it is clear that the consultation to date has been poor. While some businesses have engaged in the informal consultation with Council officials, many I spoke with did not appreciate the severity of the proposed changes. Flyers put in letterboxes or under doors use very indirect language about the impact of changes. One business I know of (Kaiwharawhara Café) has English as a second language and did not understand the documentation provided or the implications of that. They had no direct contact from Council officials until my husband explicitly asked the officials to talk to the owners. Even then they were unsure of what would be happening. There was no offer by Council officials of assistance with a submission. There are also residents who live above the commercial buildings in Kaiwharawhara Rd.

3. The Time Allowed for Submission is Too Short Detail of the proposal was only open to consultation for 3 weeks. That period is too short for businesses (even if they have been engaged in the process) to talk to customers about the specific implications of the proposal and get their support via submission. Customers will not engage until specifics are known. The consultation process will not reflect the opposition of hundreds of directly affected Wellingtonians.

4. There Should be No Presumption for Changes Beyond Stage 1 Any change beyond Stage 1 should only occur once better information is available and the concerns noted above are factored into decision making.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

My opposition to the proposed changes to Cameron St is based on the following factors.

1. Safety Issues with the Bridle Track need to be Addressed Before there is an Increase in Cyclist Use (and even with the current use) One of the overall objectives of the proposals is to significantly increase cycling volume. That includes cycling volume on the Bridle Track. This is a walking track which is also shared with cyclists. The Bridle Track is unsuitable for this shared use as it is:

- Steep – very steep in parts
- Windy – resulting in many blind corners
- Narrow – less than 3m wide in many parts
- In poor repair
- Damp – not only from rainfall run off but also from unrepaired water leaks
- Slippery from moss growth
- Subject to extensive leaf drop from the tree canopy and surrounding vegetation

If there is action to reduce speed, a significant increase in maintenance (weekly clearance of vegetation drop) and widening of the path, shared use can continue. Improvements to the track must have clear focus to reduce cyclist speed. I have witnessed numerous near misses with speeding cyclists. I have also spoken with Council contractors working on the track who are concerned for their safety. Increased signage is inadequate – there needs to be changes to physically reduce speed. I understand that there is a separate project looking at this matter but getting any details of that

from Council officials has not been possible. I have requested but have had no response as to how users of the track can have input to any improvement plan. 2. Painted sharrows on downhill section are unnecessary and reduce safety as slippery when wet. 3. Signage needs to be kept to a minimum. 4. Kerb vegetation needs to be trimming right back all the way from the bottom of Cameron St to the Bridle Track – if this isn't done a 0.75m cycle shoulder will be ineffective.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

For the reasons set out earlier, there should be no changes beyond Stage 1. Any further changes should not be made without assessment of the impact of Stage 1 and the balance between all users of the space.

Feedback

670

114181650894

NAME: Murray Coppersmith	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

My opposition to the proposal is due to the move beyond Stage 1 of the changes to Kaiwharawhara Rd and the lack of a plan for improvements to the Bridle Track. I am also disappointed with the consultation process. Insufficient effort was made to directly talk with clearly affected parties. The three-week feedback period is too short to affected parties to engage with, mobilise and get support of their customers.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

My strong opposition to the Kaiwharawhara Rd section is on the following factors. 1. A Shared Cyclist / Bus Clearway from 4pm to 7pm on Southern Side is Sufficient for the Short and Relatively Flat Section from The Living Room Down. Council officials have been reluctant to share information on current cyclist volume on this route, but in discussion I understand that uphill volume is around 150 per day, with a large majority (90%) of that volume being between 4pm and 7pm. I can support a clearway from 4pm to 7pm, but having extended clearway hours (2pm to 7pm) or moving to a permanent, separated cycleway on this short stretch of road is a disproportionate response to the small number of users (fewer than 20 per day) outside the 4pm to 7pm window. Even with significant growth in cycling numbers the volume will be low. A

separated cycleway can be put in place in sections which are already broken yellow line, leaving the remaining sections as a mixture of loading zone and parking of various time lengths. This change would make the treatment of the southern side of this short stretch of road consistent with approach being taken on the northern side of the road. It strikes a balance between the need for improved and safer cycling corridor, recognising the volume (and time of use) of cyclists with the legitimate needs of business that operate in this commercial area.

2. The Impact on Businesses and Residents Has Been Downplayed and Underestimated. I spent an hour visiting businesses on the lower part of Kaiwharawhara Rd. For many there is strong reliance on short-term customer parking in what is a commercial area. The change proposal talks about giving businesses time to “adapt”. The reality is that there is little or no practical alternative parking for these businesses (all existing parking is already heavily used). In this context “adapt” means closure, with the loss of livelihoods. These businesses also have lease commitments which continue even if businesses are closed. If these leases have personal guarantees from the business owners, the impact will be particularly severe. From talking to these businesses, it is clear that the consultation to date has been poor. While some businesses have engaged in the informal consultation with Council officials, many I spoke with did not appreciate the severity of the proposed changes. Flyers put in letterboxes or under doors use very indirect language about the impact of changes. One business owner I spoke with (Kaiwharawhara Café) has English as a second language and did not understand the documentation provided or the implications of that. They had no direct contact from Council officials until I explicitly asked the officials to talk to the owners. Even then they were unsure of what would be happening. There was no offer by Council officials of assistance with a submission. There are also residents who live above the commercial buildings in Kaiwharawhara Rd.

3. The Time Allowed for Submission is Too Short Detail of the proposal was only open to consultation for 3 weeks. That period is too short for businesses (even if they have been engaged in the process) to talk to customers about the specific implications of the proposal and get their support via submission. Customers will not engage until specifics are known. The consultation process will not reflect the opposition of hundreds of directly affected Wellingtonians.

4. There Should be No Presumption for Changes Beyond Stage 1 Any change beyond Stage 1 should only occur once better information is available and the concerns noted above are factored into decision making.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

My opposition to the proposed changes to Cameron St is based on the following factors.

1. Safety Issues with the Bridle Track need to be Addressed Before there is an Increase in Cyclist Use (and even with the current use) One of the overall objectives of the proposals is to significantly increase cycling volume. That includes cycling volume on the Bridle Track. This is a walking track which is also shared with cyclists. The Bridle Track is unsuitable for this shared use as it is:

- Steep – very steep in parts
- Windy – resulting in many blind corners
- Narrow – less than 3m wide in many parts
- In poor repair
- Damp – not only from rainfall run off but also from unrepaired water leaks
- Slippery from moss growth
- Subject to extensive leaf drop from the tree canopy and surrounding vegetation

If there is action to reduce speed, a significant increase in maintenance (weekly clearance of vegetation drop) and widening of the path, shared use can continue. Improvements to the track must have clear focus to reduce cyclist speed. I have witnessed numerous near misses with speeding cyclists. I have also spoken with Council contractors working on the track who are concerned for their safety. Increased signage is inadequate – there needs to be changes to physically reduce speed. I understand that there is a separate project looking at this matter but getting any details of that

from Council officials has not been possible. I have requested but have had no response as to how users of the track can have input to any improvement plan. 2. Painted sharrows on downhill section are unnecessary and reduce safety as slippery when wet. 3. Signage needs to be kept to a minimum. 4. Kerb vegetation needs to be trimming right back all the way from the bottom of Cameron St to the Bridle Track – if this isn't done a 0.75m cycle shoulder will be ineffective.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

For the reasons set out earlier, there should be no changes beyond Stage 1. Any further changes should not be made without assessment of the impact of Stage 1 and the balance between all users of the space.

Feedback

671

114174899554

NAME: Neil Deans	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

I cycle this in both directions daily. It is not too bad at present.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

672

114181231692

NAME: Ngaire and Andrew Best	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

See detailed submission

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

See detailed submission

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

See detailed submission

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Don't know

Do you have any comments to make about the proposed design?
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We rarely travel this far up Kaiwharawhara Road and would take advice from local residents in that location.
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Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
--

Don't know

Do you have any comments to make about the proposed design?
--

As per earlier question

What do you think of the timing of the proposed approach?
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Make the proposed changes more slowly
--

Why do you think that?

See detailed submission - we are disappointed that there is not a survey option to support only stage 1.
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Submission on proposed changes affecting Cameron Street and Kaiwharawhara Road as part of the Ngaio Connections project

Submitter: Ngaire & Andrew Best, [REDACTED] Hervey Way, Kaiwharawhara,
Wellington 6035. [REDACTED]

20 November 2022

General

Ngaire and I are regular commuter and recreational cyclists and have been regularly traveling the route to and from the city and our home in Kaiwharawhara which includes the proposed cycleway from the Hutt Road to the intersection of Marsh Way and Cameron Street, then continuing up the hill. We are also regular walkers on both Cameron and surrounding streets and the Bridle Path with our two dogs.

Our observation over the past 7 years is that there is presently no difficulty or safety concern cycling up or down Cameron Street and question the problem definition that is sought to be resolved through the proposals on Cameron Street. We do hold pedestrian safety concerns having encountered cyclists riding both up and down the Bridle path some e-bike and scooters traveling upwards 15-20km/hr and cyclists travelling downwards with what we guess is up to 50km/hr this cycling behaviour creates a significant hazard for both us and our dogs while walking in this area. The desire to increase cycling will exacerbate the potential for serious injury to both the cyclist and walkers.

As a local resident and user, we consider we are in a very good position to contribute constructively to the conversation regarding the proposed cycling and pedestrian friendly changes, and parking changes, proposed by the Council for Cameron Street, Kaiwharawhara Road and the Bridle Track.

We are very supportive of cycle lanes and related changes to the transport infrastructure where they make sense and can be accommodated within the constraints of the Wellington environment (narrow streets, relatively hilly) without unnecessary disruption to businesses and the community affected by the cycling changes.

We do not believe that sufficient analysis or data collection of cyclist, pedestrian or vehicle movements has been undertaken to make a good decision for all users and have yet to see any evidence of what benefit that these proposed interventions will have on our community and consider this proposal to be a solution to a problem that does not exist.

Within these constraints it is important to find the right balance between the interests of cyclists, public transport, businesses, commercial and private vehicle owners, and residents. Notwithstanding the trend towards more climate friendly transport solutions, it is inappropriate to categorise the future as a community that will be able to rely exclusively or even

predominantly on public transport and cycling. The solutions chosen by the Council **must** consider the **long-term (lifecycle) costs and benefits and be proportionate to the needs of all users and not just a small minority.**

The following table sets out our comments on the key changes proposed for Cameron Street and Kaiwharawhara Road.

Proposed	Comments
<p>Cameron Street uphill cycle shoulder:</p> <ul style="list-style-type: none"> • “Multi Criteria Analysis” dated 23/6/22 describes preferred option as “buffered cycle lane”. • In response to a request for clarification we understand Jonathan Kennett confirmed that the Council is going to work with Waka Kotahi and trial a type of cycle lane that is often used in Europe, and plan to call it a “cycle shoulder” which would be about 1.0m wide with broken white line and green blocks. The intention is that cyclists know they should keep left going uphill, and that people driving know to look out for cyclists but can carefully pull over into the cycle shoulder if they need to allow a vehicle coming downhill to pass. 	<p>We note the “Project details” link has a picture which shows what was described by Jonathan but without any dimensions.</p> <p>In response to a request for clarification we now understand that the cycle shoulder will likely be 0.75m wide.</p> <p>The area proposed for this includes a blind corner that will result in both down hill cyclists and vehicles including large rubbish trucks etc. on a collision course with uphill vehicles. While safety features in our vehicles and what is a fairly slow speed area are unlikely to cause death, if a downhill cyclist where to encounter a vehicle traveling uphill, crossing what is currently the centre line, there is a real danger. In our opinion that serious injury or death could occur. We believe that the Council should <u>NOT</u> be creating a more dangerous environment to what is existing.</p>
<p>Cameron Street uphill cycle shoulder - Vegetation to be cut back up to 2.2m high to improve visibility:</p> <ul style="list-style-type: none"> • “Cameron Street, General Arrangement Plan, Sheet 2” indicates that “Vegetation to be cut back up to 2.2m high to improve visibility”. • In response to a request for clarification in respect of how far the vegetation will be trimmed (kerb line or behind kerb line) Jonathan Kennett confirmed on 4/11/22 that the detail will be decided following advice from horticulture team, but happy to hear what residents would like. He imagines WCC will want to trim 0.5-1.0m from kerb line but will need to ensure it does not damage the shrubs. 	<p>We note that the vegetation was trimmed on Friday 4/11/22 to the kerb line.</p> <p>We support the ongoing maintenance of the curb side vegetation should and agree that it could be cut back further to improve visibility, however this subdivision was designed to nestle into the existing escarpment which includes conservation strips and protected trees. Complete removal of the curb side vegetation would damage the amenity and environment agreed by the commissioners at the time that consent was given to this subdivision. This should be considered and respected when further considering this proposal.</p> <p>We suggest vegetation should be trimmed to no greater than 0.2-0.3m behind the kerb, and at a frequency to ensure it does not encroach on the road below 2.2m.</p>
<p>Proposal for cycle shoulder and downhill sharrows painted on Cameron Street</p>	<p>We are un-comfortable with painted sharrows on the uphill and downhill cycle shoulder. As an all-weather commuter cyclist painted road surfaces are treacherous when wet, particularly downhill.</p>
<p>No Stopping Restriction (broken yellow lines) around the three corners between Cameron Street and the Bridle Track</p>	<p>We agree with the proposal to establish a No Stopping Restriction using broken yellow lines on the three corners between Cameron Street and the Bridle Track.</p>

Proposed	Comments
New Give Ways at bottom of Fore Street, Marsh Way, and intersection with Cameron Street	We are comfortable with the proposal for three new Give Ways to improve safety.
Signs directing cyclists to cross private property to access the Bridle Track from Cameron Street	<p>This needs to be resolved.</p> <p>We understand the Council has been approached on multiple occasions to maintain the section of road between Cameron Street and the Bridle Track, including when Cameron Street was resealed last summer, the Council has maintained it is private property and not the responsibility of the Council.</p> <p>If the Council wants to direct cyclists to cross the private property, then it needs to find a solution acceptable to residents who have a direct interest in the private property and the associated easements for the purposes of right of way and services.</p> <p>Although we are not directly affected, we have suggested that the Council should consider taking ownership and maintenance responsibility, or maintenance responsibility, to gain the support of residents directly affected.</p>
Bridle Track:	<p>Anybody who walks up or down the Bridle Track and encounters cyclists will know of the dangers.</p> <p>With the Council promoting greater use of the Bridle Track by cyclists, it must consider and implement improvements in the surface (widening, additional fencing, surface cleaning and maintenance) and signage to make the track safer for pedestrians and cyclists.</p> <p>Often when walking the Bridle Track we have been 'spooked' by cyclists without any warning, as mentioned earlier, often going too fast.</p> <p>It is only a matter of time before there is a serious incident involving a cyclist or cyclist hitting a pedestrian.</p>
All cycle lane interventions in lower Cameron Street	<p>We question the value of this part of the proposal in its entirety. Our observation is that many cyclists do not use lower Cameron Street in an uphill capacity, but choose to use Pickering Street, into lower Fore Street and up the path onto Cameron Street. Has this route been explored to resolve the perceived issues? Use of this land would negate many of the interventions proposed and the economic and social impacts that the proposal will have on our community.</p>

Proposed	Comments
<p>Cameron Street - Parking and parking restrictions:</p> <ul style="list-style-type: none"> Changes to parking restrictions on the downhill side resulting in a combination of P60, P120 and P24hr carparks “Proposed roll out of changes” includes the words “39x P24-hr spaces except residents”. In response to a request for clarification Jonathan confirmed that the “except residents” is an error and the website will be updated. If residents get annoyed with long-stay non-resident parking they can call WCC and get the cars ticketed and towed, but if no one calls WCC it is unlikely parking wardens will go to Cameron Street. Jonathan noted that residents did not want a residential parking scheme. “Changes in response to stakeholder feedback” includes a bullet point “Removing two carparks at the bottom of Cameron Street to make it safer for everyone” Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks 	<p>In relation to the 39x P24hr carparks we agree residents do not want a residential parking scheme, although we did suggest a “P24hr except residents” restriction may be useful (consistent with streets near the airport). In any event, we are comfortable with a P24hr restriction without an exception for residents on the basis we can call the Council and get cars ticketed and towed if we get annoyed with long stay parking by non-residents.</p> <p>Further, we understand that there is a plan to change to remove two carparks at the bottom of Cameron Street is not identified on the “Cameron Street, General Arrangement Plan, Sheet 2”. We are strongly of the view that if this proposal were to be implemented then the P60 carparks (15m of carparking space, 3-4 carparks) at the bottom of Cameron Street on the downhill section between the entrance to Te Rau Ora (2 Cameron Street) and the existing broken yellow lines connecting with Kaiwharawhara Road must be removed and converted to a ‘No Stopping Restriction’ with an extension of the broken yellow lines. This proposal and section of road would become a significant hazard for cyclists and vehicles alike as downhill vehicles would be forced over the centre line to the opposite side of the road adjacent to the intersection with Kaiwharawhara Road. This will create further pressure on Kaiwharawhara businesses, potentially causing them to close or relocate.</p> <p>In relation to the angle parking at the bottom of Cameron Street we suggest that this remain as parking for 5 vehicles, but that they be reconfigured so that vehicles are required to back in angled uphill to allow them to exit safely to maintain their visibility of cyclists.</p> <p>Finally, we note the parking analysis for Cameron Street has included private rights of way (Curnow Street, Sargeson Way, Marsh Way) and suggested they provide 41x unrestricted parking spaces for non-residents. The rights of way are private property and only property owners (or guests with their approval) may park in these rights of way.</p>
<p>Cameron Street change 50 km/hr to 30km/hr for safer speeds on Cameron Street</p>	<p>We agree this change makes sense.</p>
<p>Kaiwharawhara Road – Clearway</p> <ul style="list-style-type: none"> Stage 1 (2023) - create a clearway 4pm-7pm in 2023 Stage 2 (2024) - extend clearway times to 2pm-9pm 	<p>We support the proposal for a shared cyclist/bus clearway from 4pm to 7pm.</p> <p>Our observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road</p>

Proposed	Comments
<ul style="list-style-type: none"> Stage 3 (2025) – replacing clearways with a separated cycle lane 	<p>are relatively few and rarely affected by peak time traffic.</p> <p>As a regular cyclist I observe cyclists exiting the Hutt Road cycleway at the controlled cycle crossing and holding the lane or cycling toward the centre line to allow them to turn up Pickering Street or continue on to the turning lane into Cameron Street, therefore a cycle lane on the left hand side will not assist this group of cyclists and may in fact create greater risk with the resulting reduced lane width.</p> <p>Accordingly, if you choose to pursue this proposal at this point only a shared cyclist/bus clearway (southern side of the Road) from 4pm-7pm makes sense.</p> <p>While the Multi Criteria Analysis gave a separated cycle lane the highest rating there was not a lot in it between Option 1 (morning peak shared bike/bus lane/clearway) and Option 3 (a separated cycle lane). Until the Council, users and others affected by the changes can observe the success (or otherwise) of the 4pm-7pm clearway and obtain real information on actual usage patterns, we are strongly opposed to any decision at this point to default to implementing the proposed changes outlined for Stages 2 (2024) and/or 3 (2025).</p> <p>Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence (other than the rating analysis based on assumptions) has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025. It will also be important to undertake analysis of the economic and social impact on businesses, residents and visitors to the area before any decisions are taken. This proposal has the potential to damage the Kaiwharawhara community that has built over the past 10 years.</p> <p>The staged approach is proposed “to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road”, relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks, there are no public off-road parking areas.</p> <p>We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use.</p> <p>Accordingly, it would be inappropriate for Council to support a default to Stages 2 and/or 3 until the</p>

Proposed	Comments
	business/safety case supported by evidence is prepared and consulted on.
<p>Kaiwharawhara Road – Pedestrian Crossing</p> <p>Install a new pedestrian crossing outside SWET and Immigrants Son which will result in 3 carparks directly in front of our building being removed</p>	<p>We are also a little perplexed as to the proposed location of the new pedestrian crossing. Firstly, as it is very close to the intersection with Cameron Street (and its turning lane), and secondly it is some way from the bus stop. As a bus user and resident of the area I predominantly get off a bus on the Hutt Road (5028) however when I get off the bus at the Kaiwharawhara stop (5401), I do not walk up to Cameron Street, I walk up Pickering Street, into lower Fore Street then onto Cameron Street. This is the pattern that I observe from others who get off at this stop to access our subdivision. Therefore, I question why this location is proposed and what analysis has been undertaken in relation to pedestrian journey patterns to support this location?</p>

Feedback

673

114160283137

NAME: Janice Gibb	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

These changes will assist people wanting to bike or use public transport which is beneficial for the planet.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Making Kenya Street safer with raised pedestrian crossings and more room for cars is a very welcome change.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

The addition of a pedestrian crossing is a very welcome. As it is a blind corner for traffic coming from Kenya Street a 30kph speed limit would be beneficial.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Extend the 30kph to the southern end of Perth Street.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

674

114181237880

NAME: Neil Rees	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

We support this in part. We have no issue with the proposed clearway. We do not support the extension of this to include the removal of car parks and the formation of a bike lane without significant review of the clearway implementation. This staged approach should include feedback and review before each stage is commenced.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

We support this in part. Please see the attachment for our response to each component.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Don't know

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

We agree with Stage 1. This should be followed by a review and justification before additional changes are made. The plan as written commits to all stages regardless of whether Stage 1 is a success.

2022-11-20%20Ngaio%20Connections%20project%20-%20Rees%20submission.docx

Submission on proposed changes affecting Cameron Street and Kaiwharawhara Road as part of the Ngaio Connections project

Submitter: Neil and Jillian Rees [REDACTED] Marsh Way, Kaiwharawhara, Wellington 6035. Email [REDACTED]

20 November 2022

General

We retired to Wellington in 2018 to be near family. We use buses regularly and often walk the Hutt Road to the city. We regularly walk the Bridle Track. Neil is a recreational cyclist and often uses the Hutt Road cycleway and cycles up Kaiwharawhara Road.

Our home is located in Marsh Way, so our only vehicle access is via Cameron Street. We also use Cameron Street as pedestrians and cyclists, this has enabled us to directly observe the behaviours of cyclists and vehicles.

Accordingly, we consider we are in a very good position to contribute constructively to the conversation regarding the proposed cycling and pedestrian friendly changes, and parking changes, proposed by the Council for Cameron Street, Kaiwharawhara Road and the Bridle Track.

We are very supportive of cycle lanes and related changes to the transport infrastructure where they make sense and can be accommodated within the constraints of the Wellington environment (narrow streets, relatively hilly) without unnecessary disruption to businesses and the community affected by the cycling changes.

Within these constraints it is important to find the right balance between the interests of cyclists, public transport, businesses, commercial and private vehicle owners, and residents. Notwithstanding the trend towards more climate friendly transport solutions, it is inappropriate to categorise the future as a community that will be able to rely exclusively or even predominantly on public transport and cycling. The solutions chosen by the Council must consider the long-term (lifecycle) costs and benefits and be proportionate to the needs of all users and not just a small minority.

The following table sets out our comments on the key changes proposed for Cameron Street and Kaiwharawhara Road.

Proposed	Comments
<p>Cameron Street uphill cycle shoulder:</p> <ul style="list-style-type: none"> • “Multi Criteria Analysis” dated 23/6/22 describes preferred option as “buffered cycle lane”. • In response to a request for clarification Jonathan Kennett confirmed that the Council is going to work with Waka Kotahi and trial a type of cycle lane that is often used in Europe, and plan to call it a “cycle shoulder” which would be about 1.0m wide with broken white line and green blocks. The intention is that cyclists know they should keep left going uphill, and that people driving know to look out for cyclists but can carefully pull over into the cycle shoulder if they need to allow a vehicle coming downhill to pass. 	<p>We note the “Project details” link has a picture which shows what was described by Jonathan but without any dimensions.</p> <p>In response to a request for clarification we now understand that the cycle shoulder will likely be 0.75m wide.</p> <p>We are comfortable that this change will not cause any issues, and may encourage cyclist to keep left, but we do not believe that it is necessary.</p> <p>We have not encountered, or observed, a problem between cyclists and vehicles in Cameron Street.</p>
<p>Cameron Street uphill cycle shoulder - Vegetation to be cut back up to 2.2m high to improve visibility:</p> <ul style="list-style-type: none"> • “Cameron Street, General Arrangement Plan, Sheet 2” indicates that “Vegetation to be cut back up to 2.2m high to improve visibility”. • In response to a request for clarification in respect of how far the vegetation will be trimmed (kerb line or behind kerb line) Jonathan Kennett confirmed on 4/11/22 that the detail will be decided following advice from horticulture team, but happy to hear what residents would like. He imagines WCC will want to trim 0.5-1.0m from kerb line but will need to ensure it does not damage the shrubs. 	<p>We note that the vegetation was trimmed on Friday 4/11/22 to the kerb line.</p> <p>While we earlier suggested that the vegetation should be cut back to improve visibility, we were thinking that it should be cut back behind the kerb line sufficiently to allow for re-growth before re-trimming once vegetation encroaches on the road.</p> <p>We suggest vegetation should be trimmed to around 0.2-0.3m behind the kerb, and at a frequency to ensure it does not encroach on the road below 2.2m.</p>
<p>Proposal for cycle shoulder and downhill sharrows painted on Cameron Street</p>	<p>We are comfortable with painted sharrows on the uphill cycle shoulder.</p> <p>However, we do not agree with painted sharrows on the downhill section as they are unnecessary and potentially dangerous. When wet they will become a slip hazard for cyclists, particularly on this gradient.</p>
<p>No Stopping Restriction (broken yellow lines) around the three corners between Cameron Street and the Bridle Track</p>	<p>We agree with the proposal to establish a No Stopping Restriction using broken yellow lines on the three corners between Cameron Street and the Bridle Track.</p>

Proposed	Comments
<p>New Give Ways at bottom of Fore Street, Marsh Way, and intersection with Cameron Street</p>	<p><i>Fore Street</i></p> <p>This does not directly impact us, but we do not object to the proposal for a Give Way at the bottom of Fore Street to improve safety.</p> <p><i>Marsh Way</i></p> <p>We live in Marsh Way. This is a private road with a small number of residents. Our observation is that drivers already treat this as a Give Way.</p> <p>As Marsh Way is only one car wide the proposed markings will cover most of the width of the road. Vehicles will be required to drive over the Give Way markings the wrong way to gain access. We are concerned that a Give Way at the bottom of Marsh Way will produce more confusion than clarity.</p> <p>We feel it is superfluous given the proposed Give Way at the intersection with Cameron Street a few metres way, and may be confusing to traffic entering Marsh Way.</p> <p><i>Cameron Street</i></p> <p>We already treat the intersection with Cameron Street as a Stop. The vegetation on the Reserve at 2 Marsh Way is overgrown which makes it difficult to see Cameron Street traffic coming from the left.</p> <p>We do not have any issue with the proposed Give Way, but feel it is unlikely to make any change to current driver behaviour.</p> <p>It would be better to trim the vegetation to provide improved visibility.</p>
<p>Signs directing cyclists to cross private property to access the Bridle Track from Cameron Street</p>	<p>This needs to be resolved.</p> <p>When the Council has been approached on multiple occasions to maintain the section of road between Cameron Street and the Bridle Track, including when Cameron Street was resealed last summer, the Council has maintained it is private property and not the responsibility of the Council.</p> <p>If the Council wants to direct cyclists to cross the private property, then it needs to find a solution acceptable to residents who have a direct interest in the private property and the associated easements for the purposes of right of way and services.</p> <p>Although we are not directly affected, we have suggested that the Council should consider taking ownership and maintenance responsibility, or maintenance responsibility, to gain the support of residents directly affected.</p>

Proposed	Comments
<p>Bridle Track:</p>	<p>Anybody who walks up or down the Bridle Track and encounters cyclists will know of the dangers.</p> <p>With the Council promoting greater use of the Bridle Track by cyclists, it must consider and implement improvements in the surface (widening, additional fencing, surface cleaning and maintenance) and signage to make the track safer for pedestrians and cyclists.</p> <p>Often when walking the Bridle Track we have been 'spooked' by cyclists without any warning, often going too fast. The frequency and severity of these encounters has increased as the number of e-Bikes has increased.</p> <p>It is only a matter of time before there is a serious incident involving a cyclist or cyclist hitting a pedestrian.</p>
<p>Cameron Street parking restrictions:</p> <ul style="list-style-type: none"> • Changes to parking restrictions on the downhill side resulting in a combination of P60, P120 and P24hr carparks • "Proposed roll out of changes" includes the words "39x P24-hr spaces except residents". In response to a request for clarification Jonathan confirmed that the "except residents" is an error and the website will be updated. If residents get annoyed with long-stay non-resident parking they can call WCC and get the cars ticketed and towed, but if no one calls WCC it is unlikely parking wardens will go to Cameron Street. Jonathan noted that residents did not want a residential parking scheme. • "Changes in response to stakeholder feedback" includes a bullet point "Removing two carparks at the bottom of Cameron Street to make it safer for everyone" 	<p>In relation to the 39x P24hr carparks we agree residents do not want a residential parking scheme, although we did suggest a "P24hr except residents" restriction may be useful (consistent with streets near the airport). In any event, we are comfortable with a P24hr restriction without an exception for residents on the basis we can call the Council and get cars ticketed and towed if we get annoyed with long stay parking by non-residents.</p> <p>Removing two carparks at the bottom of Cameron Street is not identified on the "Cameron Street, General Arrangement Plan, Sheet 2". We are strongly of the view that the P60 carparks (15m of carparking space, 3x carparks?) at the bottom of Cameron Street on the downhill section between the entrance to Te Rau Ora (2 Cameron Street) and the existing broken yellow lines connecting with Kaiwharawhara Road must be removed and converted to a 'No Stopping Restriction' with an extension of the broken yellow lines. This section of road is a hazard for cyclists and vehicles alike as downhill vehicles are forced over the centre line to the opposite side of the road causing many near misses.</p> <p>Finally, we note the parking analysis for Cameron Street has included private rights of way (Curnow Street, Sargeson Way, Marsh Way) and suggested they provide 41x unrestricted parking spaces for non-residents. The rights of way are private property and only property owners (or guests with their approval) may park in these rights of way.</p>
<p>Cameron Street change 50 km/hr to 30km/hr for safer speeds on Cameron Street</p>	<p>We agree this change makes sense.</p>

Proposed	Comments
<p>Kaiwharawhara Road uphill cycle lane proposal:</p> <ul style="list-style-type: none"> • Stage 1 (2023) - create a clearway 4pm-7pm in 2023 • Stage 2 (2024) - extend clearway times to 2pm-9pm • Stage 3 (2025) – replacing clearways with a separated cycle lane 	<p>We support the proposal for a shared cyclist/bus clearway from 4pm to 7pm.</p> <p>Our observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road are relatively few and rarely affected by peak time traffic. Accordingly, at this point only a shared cyclist/bus clearway from 4pm-7pm makes sense.</p> <p>While the Multi Criteria Analysis gave a separated cycle lane the highest rating there was not a lot in it between Option 1 (morning peak shared bike/bus lane/clearway) and Option 3 (a separated cycle lane). Until the Council, users and others affected by the changes can observe the success (or otherwise) of the 4pm-7pm clearway and obtain real information on actual usage patterns, we are strongly opposed to any decision at this point to default to implementing the proposed changes outlined for Stages 2 (2024) and/or 3 (2025).</p> <p>Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence (other than the rating analysis based on assumptions) has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025.</p> <p>The staged approach is proposed “to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road”, relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks, there are no public off-road parking areas.</p> <p>We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use.</p> <p>Accordingly, it would be inappropriate for Council to support a default to Stages 2 and/or 3 until the business/safety case supported by evidence is prepared and consulted on.</p>

Feedback

675

114174843525

NAME: Pamela Jane Stainton	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

I object as not every resident can park on their own property, people will not be able to park and then catch a bus into town. Businesses will lose custom if people cannot park. Some shopping cannot be carried on a bus or a bike. The Wellington public transport system needs to be hugely improved before any of these concepts are put in place. For example - I am currently taking the car into town for daytime appointments as I cannot rely on the train getting me there on time. I live in Crofton Downs. The buses and trains are not frequent enough, the system assumes that everyone works in the CBD, they don't.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

Does not take into account local residents, people wanting to park to go to businesses in Kaiwharawhara or making improvements to the bus service and making it more reliable and frequent.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Oppose**Do you have any comments to make about the proposed design?**

Road pictured is too narrow, people who live there may lose their carpark and not have parking space on their properties.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?**Oppose****Do you have any comments to make about the proposed design?**

Will cars still be able to drive both ways safely, the pedestrian crossing is not in the safest of places.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?**Oppose****Do you have any comments to make about the proposed design?**

Removal of too many carpark, not everyone has room on their property to park, people are also likely to park and catch the bus from here they will no longer be able to do so.

What do you think of the timing of the proposed approach?**Make the proposed changes more slowly****Why do you think that?**

IS there room for cars and buses etc with a bike lane? The clearway times are for too long, what if people want to go to the businesses in Kaiwharawhara and cannot park, also people who work there may have to take their cars if the public transport options do not work for them.

Feedback

676

114174308295

NAME: Patrick Wilkes	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This will be a huge improvement for existing cyclists and should start to encourage more people to consider cycling in the area. Compromises have been made due to the width of the road and the desire to retain some of the on-street parking, and that will mean that some people will still not consider the route safe enough. Hopefully in time these can be addressed as well. Overall I'm excited to see these changes, they have been a long time coming but still most welcome. Hopefully those affected by the removal of parking spaces can appreciate the difference that this will back to people travelling the route.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

This will make a huge difference to cyclists, as they are currently squeezed between traffic and parked cars. However retaining parking on the Hutt Road end of the Ngaio-bound lane except for the evening peak for another two years exposes cyclists to continued risk (as noted in the safety audit) and means that the route will be less attractive to off peak users. On the other hand the road here is at its widest, so this is the best place to retain parking.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I am not familiar with Cameron Street, so not in a position to comment.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

It's not clear from the description if the uphill bike lane is intended to be separated or just painted. If it is separated then it needs to be wide enough to allow cyclists to safely pass each other. The slowest cyclist is probably managing about 10 km/h up the gorge road, faster non-electric cyclists might manage 15-20km/h and people on electric bikes will manage 30km/h plus. Putting in a narrow separated uphill lane will force faster cyclists in to the main driving lane, or result in dangerous passing in the cycle lane. It can be hard to ride an electric bike uphill at 10km/h behind a slower cyclist. Expecting electric bikes to use the uphill driving lane would be OK if you reduced the speed limit to 30km/h perhaps. Painted sharrows to encourage downhill cyclists to take the lane is welcome. Any other signage or 'no overtaking' lines to prevent people in other vehicles from trying to overtake cyclists going downhill would be most welcome. The pedestrian crossing will be great, many people have to dart across the road here. Bear in mind that cyclists travelling uphill and having to stop for people on the crossing will be unstable when they start off again. Important to ensure that there is enough space for them at this point. The slope of the approach to the raised crossing should be shallow enough that cyclist stability is not affected if they travel over it at speed as they start to accelerate down the hill.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

This will be a huge improvement for all users of this road. Cycling past parked cars travelling uphill on Kenya Street is really scary. I can't tell if the uphill cycle lane is separated or painted. There is a big difference in speeds between electric and non-electric bikes on this uphill section, and electric bikes need to be able to overtake slower cyclists. On the other hand 'paint is not protection' and while the removal of car parks on the uphill section is a huge improvement the painted lane does not feel 'safe' - even in your illustration the bus is crossing the centre line, which will force uphill vehicles in to the cycle lane. This stretch will not feel 'safe' until parking is gone from both sides and there is an uphill protected cycle land with enough space to cycles to overtake. The reduced speed limit to include the roundabout and the crossing on Crofton Road is great. It is hard to see people waiting to cross here, particularly at night, the reduced speed and raised crossing is great. However the incline on the raised crossing should not be so steep that it would affect stability for cyclists heading towards Ngaio.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

There is so much off street parking in this area, much of it appears to be unused most of the time. Surely it doesn't need to take two years for the few businesses in the area to sort themselves out.

Feedback

677

114171684370

NAME: Patrick Morgan	SUBURB: Te Aro	ON BEHALF OF: CAN	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I support these plans to achieve Wellington's mode shift, safety, and climate goals. The election demonstrated strong public support. Please get on with it without the staged approach.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I recommend WCC avoid the staged approach, as it delays the benefits, and undermines the transitional approach. I support protected uphill bike lanes. I would prefer protected lanes on both sides of Kaiwharawhara Rd, to maximise the benefits and achieve mode shift and safety goals. Retaining parking contradicts WCC's parking policy.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

I prefer protected bike lanes to maximise the benefits. Paint is not protection.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I prefer protected bike lanes, to maximise the benefits and achieve the project goals.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

I prefer protected bike lanes, to maximise the benefits. Painted bike lanes do not achieve the project goals of mode shift or safety.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

A staged approach is not necessary. It delays the benefits and contradicts the transitional approach. Do more, sooner, to make best use of the electoral mandate from the recent election. Delays just make the job harder and harder.

Feedback

678

114177241070

NAME: Paula Beret	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

They types of businesses that run there are the sort that require vehicle access, some of these parking restrictions are going to adversely affect their business

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly oppose
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
Don't make these changes! It's effectively a high business area, make cycling safer but not the rest!

Feedback

679

114161570158

NAME: Peter Shackleton	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This will make the route safer for everyone. It will encourage more people to cycle this route - currently it is only for the very confident.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I have cycled this route 3-4 times daily for the last 4 years, and prior to that have driven it thousands of times since the early 1990s. Strongly support the approach of removing cars on the south side of K road and Ngaio Gorge road. This is the section of my commute from Lambton Quay that feels the most dangerous. I would argue you should not install the loading zones and P30 that you are proposing to phase out by 2025. The ones outside Hangin' around are at a real pinch point. In fact, this is the narrowest part of the road until you get into the gorge proper. I will attach photos from Google Street View which show this. If a cyclist is going past some parked cars, here then any overtaking cars need to cross the double yellow lines to do so safely. I see this happen every day when they overtake me. I've attached one photo from street view in September 2000 where you can see the cyclist is only a metre from the parked cars and there would not be room for a car to be the Road Code required 1.5m from him and not be across the median double yellows. I've attached another from 2017 that shows a red car narrowly on the

right side of the double yellows and you can easily imagine that a cyclist in here would not have safe clearance on both sides. This could potentially be resolved by changing the alignment of the double yellow centre line and for instance making the other note side of the road non-parking.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

Strongly support the bike lane uphill here. I'd like to see the bike lane separated using the flexible plastic bollards please. Too many drivers cut into the shoulder on left hand bends at the moment. I've also had close misses on my bike when they have understeered into a right hand bend and drifted into the left shoulder. The other thing - and this is my most significant feedback - is that the Perth St/Ngaio Gorge Road intersection needs to be made *much safer* for pedestrians. You are proposing a pedestrian crossing on the north side of the Perth intersection, which is great. However, by far the most dangerous crossing there, is the one from the bus stop on the corner of Perth St/Ngaio Gorge Road, to the other side of Perth St. This is a totally blind corner due to the bank there. Pedestrians crossing have to hope there isn't a car coming quickly south on Ngaio Gorge Road turning into Perth, where they wouldn't see any pedestrians until it was too late. In addition, buses stopping here block traffic in both directions on Perth St as they cut across the middle of the road. I don't think it's possible to put a safe crossing across the bottom of Perth St. Instead I would propose the following: 1. Remove the bus stop at this location and relocate it down Ngaio Gorge, perhaps at No.34. This would serve anyone at the top of Ngaio Gorge and Trellissick Cres. 2. Relocate the bus stop at 20 Perth St (which is itself only 150m from the next bus stop at the corner of Crofton Road) down to 34 Perth St. This is where the western footpath begins on Perth St, and provides a safer place to cross for anyone looking to get to the other side of Perth St. 3. Add a second pedestrian crossing across Ngaio Gorge Road south of Perth St. This allows anyone coming down Perth St to cross NGR safely without having to make a dangerous crossing of Perth St to get to the pedestrian crossing you are proposing. This would allow people who wanted to safely cross Perth St by using the pedestrian crossings. It would also mean people coming down Perth St, or from the north Side of NGR, who wanted to get to Trellissick Park, can cross NGR directly, without having to undertake the dangerous Perth St crossing. If this isn't doable, then put a pedestrian traffic island here to enable people to cross more safely.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Strongly support the creation of a bike lane uphill. Please use the plastic bollards or similar to separate the bike lane from the road. And use signage/dashed yellow paint to show that parking is

not permitted. I think it is very likely that people will park in the bike lane otherwise. The onus should not be on cyclists to have to constantly report misbehaviour.

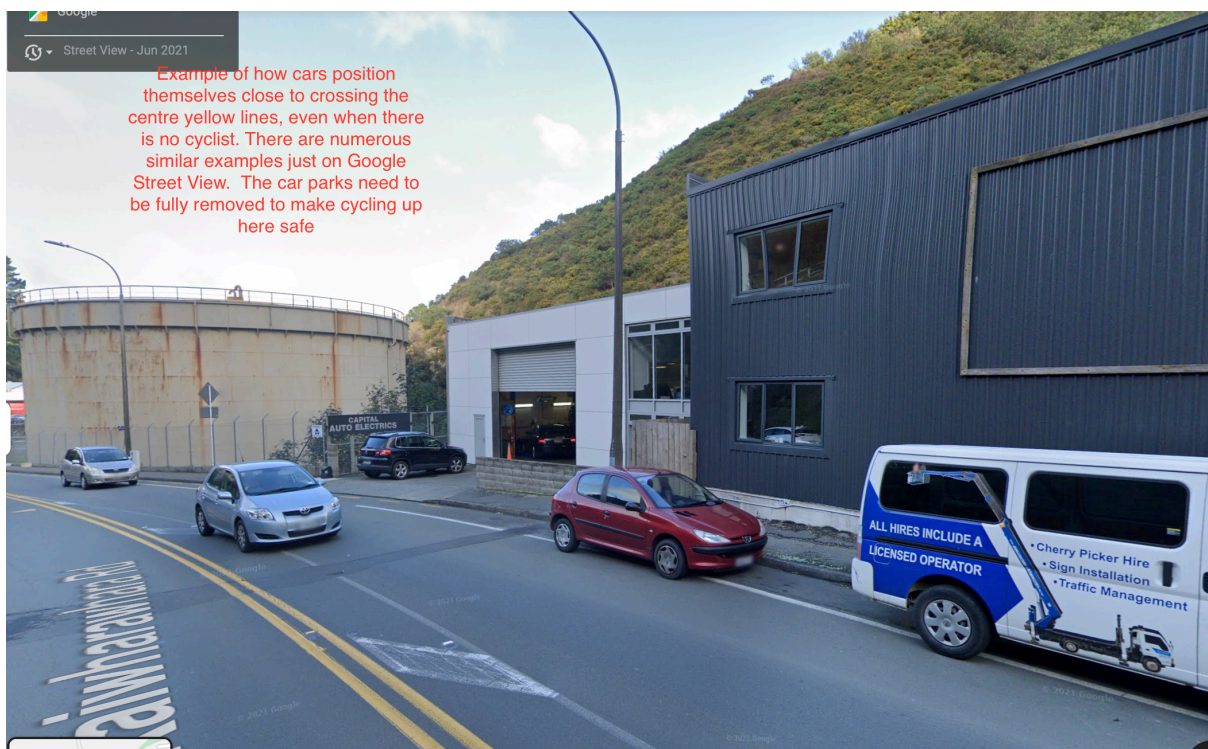
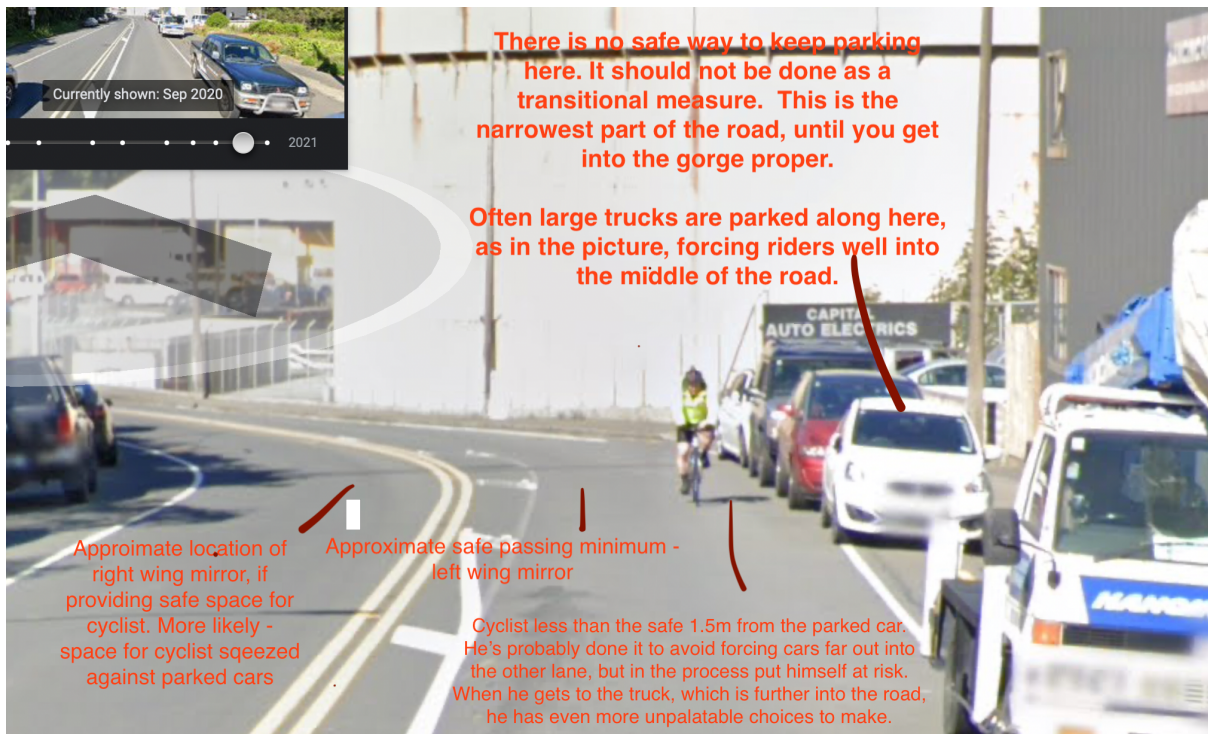
What do you think of the timing of the proposed approach?

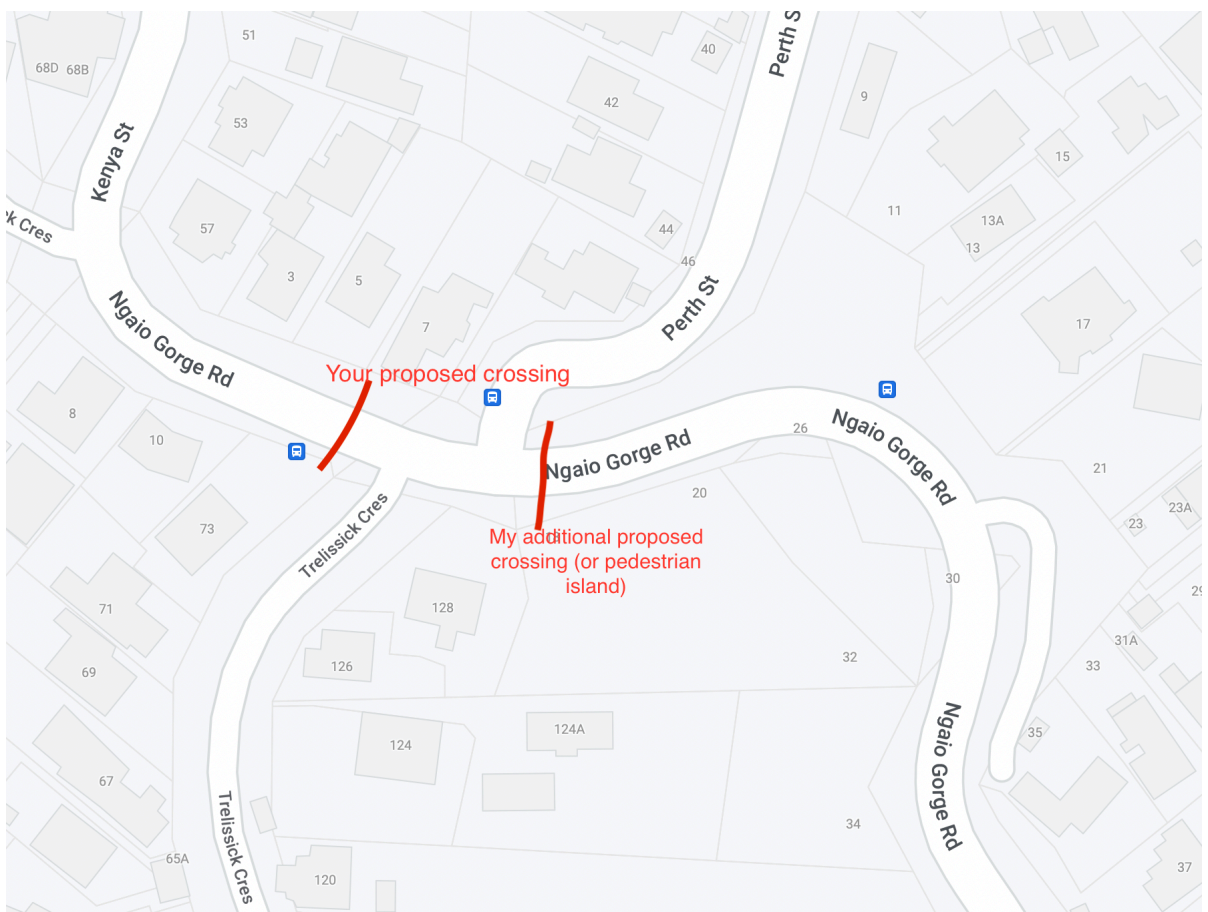
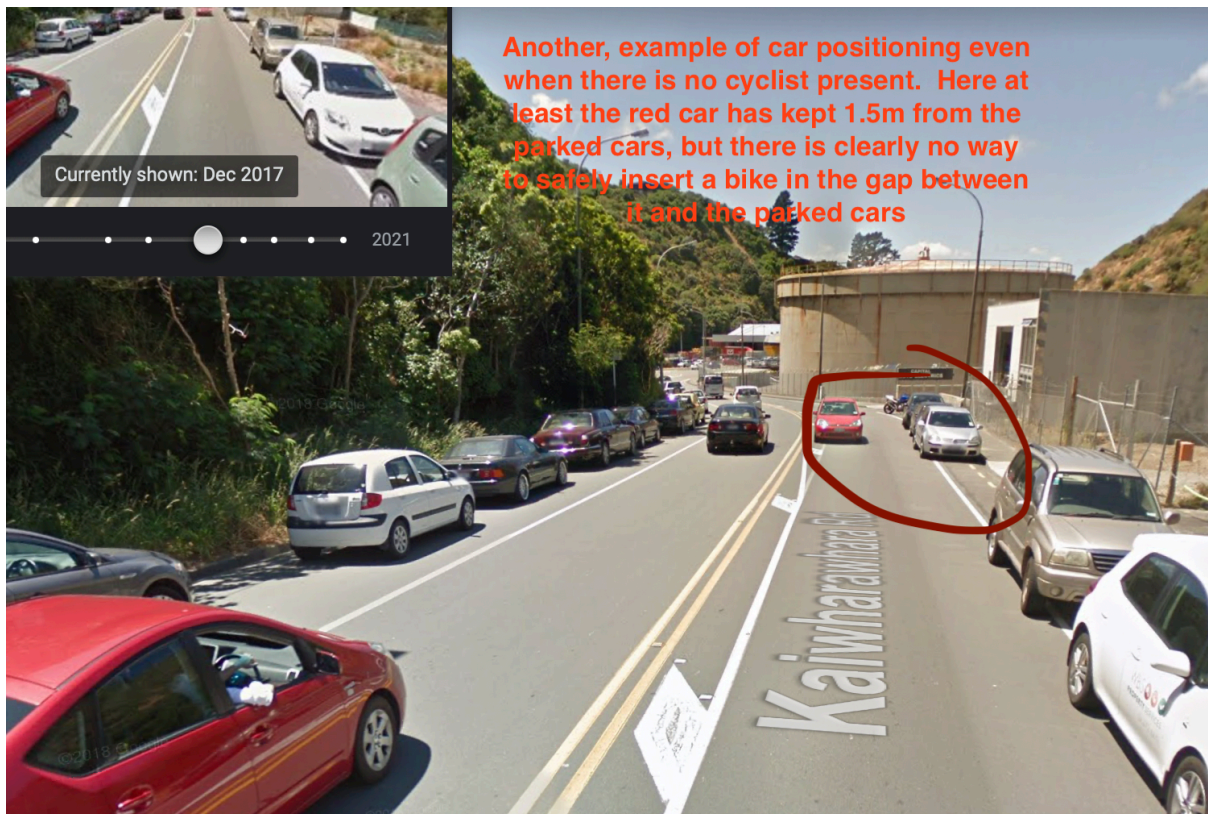
Make the proposed changes more quickly

Why do you think that?

As I have said in my other comments, the proposed temporary parking outside Hanging around are in the *least safe* location in this whole stretch of road, where it narrows and a right hand turn encourages cars to cross the double yellow centre line.

Ngaio%20cycleway%20feedback.pdf





Feedback

680

114170611324

NAME: Peter shaw	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I regularly drive and occasionally cycle through the area, I would move to cycling more if design enabled safer (especially uphill) cycling connections.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Compared to aro scheme I see less impact for residents round parking changes (area not as tight on parking), think doing faster will reduce accidents and enable more locals to cycle safer sooner

Feedback

681

114178516697

NAME: Phil Haslam	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

WCC + cycleways = shambles

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The WCC has displayed incredible ineptness with other cycle lanes. I don't want the same shambles in my neighbourhood.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

The residents of Cameron St etc should be in charge of making decisions on this one.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral
Do you have any comments to make about the proposed design?
Pedestrian crossing makes sense. A roundabout at the Perth street junction would also make sense - it is currently a crap intersection.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly oppose
Do you have any comments to make about the proposed design?
Residents need somewhere to park. Until there is decent public transport less cars is unrealistic.

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback

682

114160163180

NAME: Patrick McKenna	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

safety can't wait

Feedback

683

114161721539

NAME: John Wilson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

Can the for path be a shared path?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Oppose

Do you have any comments to make about the proposed design?

Te parking for trade vehicles

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

What about business parking ie motor doctors as they work on many cars and need the room

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback

684

114181509888

NAME: Richard Chung	SUBURB: Seatoun	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

My comments are in relation to the lower parts of Kaiwharawhara Road which is an established business area of Wellington City. As such, it accommodates a range of businesses and activities that rely on street profile, access, loading and parking. Council indicated in briefing sessions that it was considering alternative off-site parking, but no solutions appear to have been found. This is important and should be pursued and put in place ahead of material changes to this part of Kaiwharawhara Road. Additionally, Council should consider School Road as an alternative for cyclists. Council at a public briefing session indicated that the issue was that School Road did not link up to existing pedestrian and cycleways along Hutt Road. Surely this can be addressed? If School Road cannot work after it is given due and proper consideration, then there should be a full assessment of how the proposed changes along Kaiwharawhara Road are performing after each stage. If the performance of the lower parts of Kaiwharawhara Road are meeting requirements after Stage 1 or 2, the following stage/s needn't be implemented.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

I have attended numerous briefing sessions made by Council. My comments are as follows: 1. Council was proposing some new replacement carparking along Kaiwharawhara Road on a dis-

used site (where slope stabilisation and footpath widening works were already taking place). I can't seem to see reference to these but believe they should be provided as it will be important to replace some of the lost carparking spaces along the lower parts of Kaiwharawhara Road given the mixed use nature of activities in the area. 2. School Road should be properly considered for cyclists as an alternative for the lower part of Kaiwharawhara Road. Council advised at a consultation street that the issue was that School Road didn't link up with existing pedestrian and cycle crossings along Hutt Road. Making a link would not be that difficult and could mean retention of important loading, access and carparking along this part of Kaiwharawhara Road which is important for the properties and businesses in this area. 3. If there are valid reasons for School Road not to be used in lieu of proposed changes to the south/up-hill side of Kaiwharawhara Road, then if the clearway changes in the initial stages proves effective, these should remain as the long term solution rather than a dedicated full time cycleway. That way multiple city users get the benefits rather than only one group.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Need to make sure the needs of residents of Cameron Street are still met.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

This is a material change for what is an established business area of Wellington city that accommodates a mix of activities that rely on street profile, access, loading and parking. 3 comments reiterating earlier points: 1. Replacement off-street carparking should be fully investigated by Council and installed in advance of any material changes to Kaiwharawhara Road. 2. Properly consider School Road for cyclists as an alternative to changes to the lower part of Kaiwharawhara Road. 3. If School Road after proper analysis cannot be made to work, allow some of the earlier stages to Kaiwharawhara Road become the long term solution, if they prove successful in their own right i.e. don't install a dedicated uphill bike lane if it is not required and allow a continuation of a clearway type arrangement so multiple people benefit. That could be alongside hefty fines/prompt tow-aways for vehicles that illegally park on clearways to ensure they are effective during the important parts of the day for cycle commuting.

Feedback

685

114178529136

NAME: Richard Taylor	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

I think a downward cycleway should be added. While a shared lane is generally safe at present, it is a major factor (in fact the only factor) that stops me from cycling in the wet. Any sudden breaking from cars can be extremely dangerous and i have seen multiple near misses going downwards.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

This is a great idea. My concerns re: breaking cars travelling downhill on Ngaio Gorge are less of an issue here where the width of the road provides sufficient space for all.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Support
Do you have any comments to make about the proposed design?
Downhill cycleway should be added for increased safety in wet conditions.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Let's get it done! It's already a well-travelled route by bike, and strongly justified.

Feedback

686

114181622387

NAME: Richard	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The roads that are being effected are just to narrow as it is. The Gorge has some good ideas but once you get to Kenya Street there are just to many houses on either side to who are going to be effected.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The roads on this path are already really narrow for the current traffic. Adding in a specific bike lane will just make it worse.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

This part I actually do support. Especially the pedestrian crossing

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Kenya Street is quite a narrow street with a lot of houses on either side. Most of the houses don't have garages so removing 57 car parks is going to force some households to have to park quite far away from their house. It is also going to ruin places like the takeaway shop as all you are doing is adding in 1 car park but removing 57. On a busy Friday or Saturday afternoon there isn't going to be anywhere for these people to park. There is just too many houses on either side to justify removing that many car parks

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback

687

114179541060

NAME: Richard Jaine	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I fully support the changes and would like to see them implemented earlier if possible.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

This will add greatly to the safety for both cyclists and pedestrians. As a cyclist, I have noticed that cars frequently try to pass cyclists on the downhill direction, even around the intersection with Perth St. Sharrows will allow cyclists to use the road more safely.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

The uphill bike lane will improve safety greatly. Currently, with cars parked on both sides of the road, there is often narrow parts of the road where two cars comin from opposite directions can not pass, let alone cyclists. A bike lane will provide further space and a safe space for cyclists who will not further slow traffic.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

688

114174294907

NAME: Rob Cameron	SUBURB: Other	ON BEHALF OF: Cameron Harrison Butchery & Delicatessen	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I agree with the need for safer cycleways for commuters but DO NOT AGREE with the REMOVAL of any of the existing car parks outside my business (4 Crofton Road). Without these car parks, we will have NO BUSINESS and after investing in this community for over 14 years, I would be FORCED TO CLOSE MY BUSINESS if I don't have any car parks for my customers to access, outside my store.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

I agree with the need for safer cycleways for commuters but DO NOT AGREE with the REMOVAL of any of the existing car parks outside my business (4 Crofton Road) and the others along Crofton Road heading towards the roundabout. Without these car parks, we will have NO BUSINESS and we after investing in this community for over 14 years, I would be FORCED TO CLOSE MY BUSINESS if I don't have any car parks for my customers to access, outside my store.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

689

114181510130

NAME: Robert Quigley	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

If you want parents to let children cycle, or encourage people who are new to cycling, then it needs to be made way safer than the current layout.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

As a sometimes cyclist along this route, the cars parked on the southern side always worry me due to the risk of door openings. There's no way I'd let my kids ride this as it is. A 4-7pm clearway will be insufficient for people who are cycling home outside these hours, especially any school kids. Suggest it is at least 3 to 7 in the first instance.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

I don't cycle this route so have no informed comments I could make.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

I strongly support treatment of this area, just not the treatment suggested. When I cycle/drive around this corner in the morning, this is an area of significant risk for drivers and cyclists. Cars coming down sometimes try to overtake cyclists, on a blind corner. To prevent this people on bikes need to pull into the centre far earlier than the sharrows indicate and car drivers need to slow down when there is a cyclist and give them priority. The current green strip on the left hand side will encourage some cars to try and overtake before the new pedestrian crossing i.e. they'll be speeding up heading into a crossing, and there are two side streets joining here too. I'd suggest deleting the green downhill cycling lane just prior to the top of the hill, before the bend, and installing the sharrows much earlier. People on a bike need to be in the centre of the road or else cars try to overtake in a zone with two other streets joining the roadway. The designers need to get the cars and cycles in sync before this highly hazardous area.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

I'm sometimes on a non-e-bike on this downhill section. With a strong northerly blowing (most common wind in Ngaio), I don't actually go very fast on this section, despite it being downhill. Cars therefore are trying to pass me and with the parked cars, they have to accelerate to over 50km hour to make it. There have been instances where I've had to break as the car has misjudged the time it takes to get around me prior to oncoming traffic. While I can imagine getting rid of the parked cars would be tough for the residents, it'll make this route safe for children to cycle. At the moment there's no way I'd let my kids cycle this section of road heading down into Ngaio.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Any change is hard for people. Providing some time might help.

Feedback

690

114177107407

NAME: Robin Simenauer	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Bicycle use to and from the city should be encouraged, but currently the lack of dedicated lanes for cyclists makes potential users reluctant to use this mode of transport.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?
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May I suggest - as a regular rider up and down Ngaio Gorge Road - that the existing footpath could be slightly widened as a shared cycle/pedestrian path? Very few pedestrians ever use it, and this option would retain most of the existing road for cars. Than you.
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Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
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Strongly support

Do you have any comments to make about the proposed design?
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What do you think of the timing of the proposed approach?
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The timing of the stages is about right
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Why do you think that?

Feedback

691

114168615128

NAME: Ross Parry	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

692

114181150582

NAME: Michelle Rush	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I cycle this route frequently, and sometimes several times a day. Having a marked route for the uphill to Ngaio, and up Kenya to the top of the gorge, along with the protection planned for tight corners will improve both the reality of safety, and the perception, for cyclists - important when we need to encourage more people to take up this transport option for health, climate, environmental and congestion reduction reasons. I strongly support the traffic calming with a raised crossing, and extension of the 30km hour area: these will help make the route more attractive and safe for cyclists and pedestrians. I also hope the initiatives will encourage more Ngaio residents (including young people) to start using bikes for the local trips and not just the commute - appreciating that more work will be needed to make this a reality, e.g. Waikowhai St needs attention, to help less confident cyclists want to ride to the supermarket for instance. I support the staging, so that businesses in Kaiwharawhara can sort out alternatives to on-street parking for their staff.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I strongly support the proposals. Parts of the road are narrow, and having to be on the outside of parked cars makes me uneasy when I am cycling. As a long time resident of Ngaio, and both a

driver and a cyclist, I have noticed increasing use of this part of the gorge road for parking: a lot appears to be for people working in the businesses here. I strongly recommend WCC lobby GWRC to get the Kaiwharawhara train stop re-instituted: this would provide a good alternative to vehicle use for the staff working in the businesses located in Kaiwharawhara (both those coming from the Hutt and those coming from Porirua direction) and help reverse this trend of more and more on-street parking.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

The Bridle Track has the potential to be a significant cycle route for Khandallah residents - esp given the real space constraints on Onslow Road - I support this as part of enhancing this route as a safe cycle way and also helping 'traffic calm' Cameron St for the many residents in this area.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

This is a really great idea as the pedestrian crossing here will improve bus commuter safety, as well as 'walk to school' safety and having a raised crossing at the beginning of the downhill will significantly slow traffic through this busy area. This will make is significantly safer for cyclists: I have had cars try to overtake me here and it is highly dangerous - I've seen a car try to overtake my husband when he was cycling - ending up across the double yellow lines only having to pull back. The raised crossing here will stop that nonsense to the benefit of cyclists, pedestrians and other drivers.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

This will be a huge safety improvement for cyclists: I have had several scares cycling up Kenya St with drivers overtaking too close, or forcing me to the side passing me when there is a parked car. This also increases the likelihood of more children feeling safer to cycle to and from school. I strongly support the extension of the 30km/h speed limit, as this combined with the cycle way and sharrows, signals that this is a 'multi-use street.' This also reduces the real risk of the Crofton Rd - Abbott St - Kenya intersection, which is a very busy area with pedestrians, drivers and cyclists plus people accessing the businesses here.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I would hope we could use this time to lobby, and get reinstated, the Kaiwharawhara train stop, which will provide an additional and very useful transport option for staff accessing the businesses: it also allows time for those businesses to invest in some sustainable transport

planning, e.g. car pooling, encouraging staff who live closer in to use cycles and if necessary, eg. for their trade vehicles, building additional onsite parking and so on.

Feedback

693

114162946485

NAME: Ruth Pirie	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

Car drivers often don't see people who are already on the existing pedestrian crossing (something to do with the angle of the crossing or the crossing being on the bend of the road). I've narrowly missed being hit at least twice.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

People will need time to adjust and there are limited alternatives to parking on street in some areas.

Feedback

694

114170047978

NAME: Rod & Victoria Crone	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

While we are retired we own hybrid e-bikes and regularly cycle on cycle lanes and purpose built trails, but avoid cycling on busy roads because of the risks (having also had a work colleague lose his life when hit by a car on SH3 near Horokiwi). Accordingly we support cycle lanes and associated changes provided they are proportionate to the needs and use patterns of cyclists, public transport and other users within the constraints of Wellington's very narrow and relatively hilly streets.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Please refer to attached submission. We support the proposal for a shared cyclist/bus clearway from 4pm to 7pm. Our observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road are relatively few and rarely affected by peak time traffic. Accordingly, at this point only a shared cyclist/bus clearway from 4pm-7pm makes sense. While the Multi Criteria Analysis gave a separated cycle lane the highest rating there was not a lot in it between Option 1 (morning peak shared bike/bus lane/clearway) and Option 3 (a separated cycle lane). Until the Council, users and others affected by the changes can observe the success (or

otherwise) of the 4pm-7pm clearway and obtain real information on actual usage patterns, we are strongly opposed to any decision at this point to default to implementing the proposed changes outlined for Stages 2 (2024) and/or 3 (2025). Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence (other than the rating analysis based on assumptions) has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025. The staged approach is proposed “to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road”, relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks, there are no public off-road parking areas. We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use. Accordingly, it would be inappropriate for Council to support a default to Stages 2 and/or 3 until the business/safety case supported by evidence is prepared and consulted on.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Please refer to attached submission. Although discussions with several cyclists suggests no cycle lane/shoulder is necessary, we are comfortable with a 0.75m wide cycle shoulder. There is a 15m section (around 3 carparks) between the entrance to 2 Cameron Street and the existing broken yellow lines downhill that should be changed to no stopping and the broken yellow lines extended. The current parking forces downhill vehicles to cross the centre line and is a hazard for cyclists and motorists. We agree with no stopping around the three corners between Cameron Street and the Bridle Track. The proposed painted sharrows on the downhill section are unnecessary and will be a hazard for cyclists in wet weather, accordingly we disagree with this part of the proposal. There is a section of private road/property between Cameron Street and the Bridle Track. Council has been unwilling to maintain this section of road, yet the Council is now encouraging cyclists to use this route as part of the Ngaio Connections project. This needs to be resolved, we suggest it will likely get residents support if Council agrees to maintain this section of road (and any services under the road).

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

We support the proposal apart from painted sharrows on the downhill section which we consider are unnecessary and will be a hazard for cyclists in wet weather.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

We support the proposal apart from painted sharrows on any non-cycle lane sections which are unnecessary and a hazard for cyclists in wet weather.

What do you think of the timing of the proposed approach?**Make the proposed changes more slowly****Why do you think that?**

Please refer to our submission. We support the proposal for a shared cyclist/bus clearway from 4pm to 7pm, but not the proposed stages 2 and/or 3 without a more robust business case based on usage patterns after stage 1 is completed and in place for at least 12 months. Our observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road are relatively few and rarely affected by peak time traffic. Accordingly, at this point only a shared cyclist/bus clearway from 4pm-7pm makes sense. While the Multi Criteria Analysis gave a separated cycle lane the highest rating there was not a lot in it between Option 1 (morning peak shared bike/bus lane/clearway) and Option 3 (a separated cycle lane). Until the Council, users and others affected by the changes can observe the success (or otherwise) of the 4pm-7pm clearway and obtain real information on actual usage patterns, we are strongly opposed to any decision at this point to default to implementing the proposed changes outlined for Stages 2 (2024) and/or 3 (2025). Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence (other than the rating analysis based on assumptions) has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025. The staged approach is proposed "to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road", relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks, there are no public off-road parking areas. We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use. Accordingly, it would be inappropriate for Council to support a default to Stages 2 and/or 3 until the business/safety case supported by evidence is prepared and consulted on.

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Submission on proposed changes affecting Cameron Street and Kaiwharawhara Road as part of the Ngaio Connections project

Submitter: Rod & Victoria Crone, ■ Cameron Street, Kaiwharawhara, Wellington 6035. Email

9 November 2022

General

Before retirement Rod used public transport every day to travel to and from work, and since retirement we both use buses whenever practical. We frequently cycle and walk the Hutt Road cycleway both as a cyclist and pedestrian, and regularly walk the Bridle Track.

Our home is located on the route of the proposed cycle shoulder in Cameron Street, and this has enabled us to directly observe the behaviours of cyclists and vehicles.

Accordingly, we consider we are in a very good position to contribute constructively to the conversation regarding the proposed cycling and pedestrian friendly changes, and parking changes, proposed by the Council for Cameron Street, Kaiwharawhara Road and the Bridle Track.

We are very supportive of cycle lanes and related changes to the transport infrastructure where they make sense and can be accommodated within the constraints of the Wellington environment (narrow streets, relatively hilly) without unnecessary disruption to businesses and the community affected by the cycling changes.

Within these constraints it is important to find the right balance between the interests of cyclists, public transport, businesses, commercial and private vehicle owners, and residents. Notwithstanding the trend towards more climate friendly transport solutions, it is inappropriate to categorise the future as a community that will be able to rely exclusively or even predominantly on public transport and cycling. The solutions chosen by the Council must consider the long-term (lifecycle) costs and benefits and be proportionate to the needs of all users and not just a small minority.

The following table sets out our comments on the key changes proposed for Cameron Street and Kaiwharawhara Road.

Proposed	Comments
<p>Cameron Street uphill cycle shoulder:</p> <ul style="list-style-type: none"> “Multi Criteria Analysis” dated 23/6/22 describes preferred option as “buffered cycle lane”. In response to a request for clarification Jonathan Kennett confirmed that the Council is going to work with Waka Kotahi and trial a type of cycle lane that is often used in Europe, and plan to call it a “cycle shoulder” which would be about 1.0m wide with broken white line and green blocks. The intention is that cyclists know they should keep left going uphill, and that people driving know to look out for cyclists but can carefully pull over into the cycle shoulder if 	<p>We note the “Project details” link has a picture which shows what was described by Jonathan but without any dimensions.</p> <p>In response to a request for clarification we now understand that the cycle shoulder will likely be 0.75m wide.</p> <p>Although discussions with several cyclists suggests no cycle lane/shoulder is necessary, we are comfortable with a 0.75m wide cycle shoulder.</p>

Proposed	Comments
they need to allow a vehicle coming downhill to pass.	
<p>Cameron Street uphill cycle shoulder - Vegetation to be cut back up to 2.2m high to improve visibility:</p> <ul style="list-style-type: none"> • “Cameron Street, General Arrangement Plan, Sheet 2” indicates that “Vegetation to be cut back up to 2.2m high to improve visibility”. • In response to a request for clarification in respect of how far the vegetation will be trimmed (kerb line or behind kerb line) Jonathan Kennett confirmed on 4/11/22 that the detail will be decided following advice from horticulture team, but happy to hear what residents would like. He imagines WCC will want to trim 0.5-1.0m from kerb line but will need to ensure it does not damage the shrubs. 	<p>We note that the vegetation was trimmed on Friday 4/11/22 to the kerb line.</p> <p>While we earlier suggested that the vegetation should be cut back to improve visibility, we were thinking that it should be cut back behind the kerb line sufficiently to allow for re-growth before re-trimming once vegetation encroaches on the road.</p> <p>We suggest vegetation should be trimmed to around 0.2-0.3m behind the kerb, and at a frequency to ensure it does not encroach on the road below 2.2m.</p>
<p>Proposal for cycle shoulder and downhill sharrows painted on Cameron Street</p>	<p>We are comfortable with painted sharrows on the uphill cycle shoulder.</p> <p>However, we do not agree with painted sharrows on the downhill section as they are unnecessary and will make it unsafe for cyclists when it is wet.</p>
<p>No Stopping Restriction (broken yellow lines) around the three corners between Cameron Street and the Bridle Track</p>	<p>We agree with the proposal to establish a No Stopping Restriction using broken yellow lines on the three corners between Cameron Street and the Bridle Track.</p>
<p>New Give Ways at bottom of Fore Street, Marsh Way, and intersection with Cameron Street</p>	<p>We are comfortable with the proposal for three new Give Ways to improve safety.</p>
<p>Signs directing cyclists to cross private property to access the Bridle Track from Cameron Street</p>	<p>This needs to be resolved.</p> <p>When the Council has been approached on multiple occasions to maintain the section of road between Cameron Street and the Bridle Track, including when Cameron Street was resealed last summer, the Council has maintained it is private property and not the responsibility of the Council.</p> <p>If the Council wants to direct cyclists to cross the private property, then it needs to find a solution acceptable to residents who have a direct interest in the private property and the associated easements for the purposes of right of way and services.</p> <p>Although we are not directly affected, we have suggested that the Council should consider taking ownership and maintenance responsibility, or maintenance responsibility, to gain the support of residents directly affected.</p>

Proposed	Comments
<p>Bridle Track:</p>	<p>Anybody who walks up or down the Bridle Track and encounters cyclists will know of the dangers.</p> <p>With the Council promoting greater use of the Bridle Track by cyclists, it must consider and implement improvements in the surface (widening, additional fencing, surface cleaning and maintenance) and signage to make the track safer for pedestrians and cyclists.</p> <p>Often when walking the Bridle Track we have been 'spooked' by cyclists without any warning, often going too fast.</p> <p>It is only a matter of time before there is a serious incident involving a cyclist or cyclist hitting a pedestrian.</p>
<p>Cameron Street parking restrictions:</p> <ul style="list-style-type: none"> • Changes to parking restrictions on the downhill side resulting in a combination of P60, P120 and P24hr carparks • "Proposed roll out of changes" includes the words "39x P24-hr spaces except residents". In response to a request for clarification Jonathan confirmed that the "except residents" is an error and the website will be updated. If residents get annoyed with long-stay non-resident parking they can call WCC and get the cars ticketed and towed, but if no one calls WCC it is unlikely parking wardens will go to Cameron Street. Jonathan noted that residents did not want a residential parking scheme. • "Changes in response to stakeholder feedback" includes a bullet point "Removing two carparks at the bottom of Cameron Street to make it safer for everyone" 	<p>In relation to the 39x P24hr carparks we agree residents do not want a residential parking scheme, although we did suggest a "P24hr except residents" restriction may be useful (consistent with streets near the airport). In any event, we are comfortable with a P24hr restriction without an exception for residents on the basis we can call the Council and get cars ticketed and towed if we get annoyed with long stay parking by non-residents.</p> <p>Removing two carparks at the bottom of Cameron Street is not identified on the "Cameron Street, General Arrangement Plan, Sheet 2". We are strongly of the view that the P60 carparks (15m of carparking space, 3x carparks?) at the bottom of Cameron Street on the downhill section between the entrance to Te Rau Ora (2 Cameron Street) and the existing broken yellow lines connecting with Kaiwharawhara Road must be removed and converted to a 'No Stopping Restriction' with an extension of the broken yellow lines. This section of road is a hazard for cyclists and vehicles alike as downhill vehicles are forced over the centre line to the opposite side of the road causing many near misses.</p> <p>Finally, we note the parking analysis for Cameron Street has included private rights of way (Curnow Street, Sargeson Way, Marsh Way) and suggested they provide 41x unrestricted parking spaces for non-residents. The rights of way are private property and only property owners (or guests with their approval) may park in these rights of way.</p>
<p>Cameron Street change 50 km/hr to 30km/hr for safer speeds on Cameron Street</p>	<p>We agree this change makes sense.</p>

Proposed	Comments
<p>Kaiwharawhara Road uphill cycle lane proposal:</p> <ul style="list-style-type: none"> • Stage 1 (2023) - create a clearway 4pm-7pm in 2023 • Stage 2 (2024) - extend clearway times to 2pm-9pm • Stage 3 (2025) – replacing clearways with a separated cycle lane 	<p>We support the proposal for a shared cyclist/bus clearway from 4pm to 7pm.</p> <p>Our observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road are relatively few and rarely affected by peak time traffic. Accordingly, at this point only a shared cyclist/bus clearway from 4pm-7pm makes sense.</p> <p>While the Multi Criteria Analysis gave a separated cycle lane the highest rating there was not a lot in it between Option 1 (morning peak shared bike/bus lane/clearway) and Option 3 (a separated cycle lane). Until the Council, users and others affected by the changes can observe the success (or otherwise) of the 4pm-7pm clearway and obtain real information on actual usage patterns, we are strongly opposed to any decision at this point to default to implementing the proposed changes outlined for Stages 2 (2024) and/or 3 (2025).</p> <p>Removing carparks in Kaiwharawhara Road will only incentivise more people to park in Cameron Street, yet no useful analysis or evidence (other than the rating analysis based on assumptions) has been provided to justify the case for extending the clearway to 2pm-9pm in 2024, or to a separated cycle lane in 2025.</p> <p>The staged approach is proposed “to give residents and businesses time to adjust to the relocation and removal of carparks on one side of the road”, relocated to where (Cameron Street??). All other off-road parking in the area is associated with businesses who presumably own or lease the parks, there are no public off-road parking areas.</p> <p>We are strongly of the view that additional restrictions beyond a 4pm-7pm clearway must be justified by a sound business/safety case which at this point has not been provided and cannot be provided until more information is available after the initial clearway is established and in use.</p> <p>Accordingly, it would be inappropriate for Council to support a default to Stages 2 and/or 3 until the business/safety case supported by evidence is prepared and consulted on.</p>

Feedback

695

114169290111

NAME: Richard	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Please see attached supporting document. In brief:

- I fully support a new pedestrian crossing at Kenya Road.
- I support a cycleway on Ngaio Gorge Road, Kenya Road and Stage 1 of the cycleway on Kaiwharawhara Road, but with changes. I will give my full support if the following amendments are made. These include:
 - Amend the staging of Kaiwharawhara Road by removing Stage 2 and making changes to Stage 1.
 - Council to work with land owners and/or purchase land in the Kaiwharawhara Road area to build more off street parking and loading zones if/before Stage 3 is done.
 - No dividers separating the cycleway and the rest of the road as it reduces the cycleway to a "single track" and creates additional hazards for cyclists.
 - No speed bumps on Ngaio Gorge Road and Crofton Road. The pedestrian crossings to be/remain a standard pedestrian crossing.
 - Changes to the end of the cycleway at Ngaio Gorge Road where they merge with vehicles.
 - Cycleway not required on Crofton Road as it will be a 30 kph zone.

Cycleway not required on Cameron Street. I currently use or have used public transport, car, bicycle and run in the area. I park in the area to then cycle into the CBD. Taking away parking in the area means I will be unable to do that and would rather park in town instead. The changes must not have a detrimental impact on businesses in the area and must support people who do the right thing and use public transport or cycling for a portion of their trip rather than the whole trip e.g. those who drive for a portion then take public transport, bike or walk for the other portion of their trip.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Please see attached supporting document. In brief:

- I fully support a new pedestrian crossing at Kenya Road.
- I support a cycleway on Ngaio Gorge Road, Kenya Road and Stage 1 of the cycleway on Kaiwharawhara Road, but not as per proposed. I will give my full support if the following amendments are made. These include:
- Amend the staging of Kaiwharawhara Road by removing Stage 2 and making changes to Stage 1.
- Council to work with land owners and/or purchase land in the Kaiwharawhara Road area to build more off street parking and loading zones if/before Stage 3 is done.
- No dividers separating the cycleway and the rest of the road as it reduces the cycleway to a "single track" and creates additional hazards for cyclists.
- No speed bumps on Ngaio Gorge Road and Crofton Road. The pedestrian crossings to be/remain a standard pedestrian crossing.
- Changes to the end of the cycleways at Ngaio Gorge Road where they merge with vehicles.
- Cycleway not required on Crofton Road as it will be a 30 kph zone.
- Cycleway not required on Cameron Street.

I currently use or have used public transport, car, bicycle and run in the area. I park in the area to then cycle into the CBD. Taking away parking in the area means I will be unable to do that and would rather park in town instead. The changes must not have a detrimental impact on businesses in the area and must support people who do the right thing and use public transport or cycling for a portion of their trip rather than the whole trip e.g. those who drive for a portion then take public transport, bike or walk for the other portion of their trip.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Please see comments made in previous section and in supporting document

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

I support the new pedestrian crossing. I oppose raising it and the two additional speed bumps before and after it. Please see comments made in earlier pages and supporting document

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

Please see comments made in supporting document

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Support stage 1 but with clearway starting at 3:30pm. Oppose stage 2 as it is petty and won't benefit the majority of cyclists. Support stage 3 however timing should be conditional on WCC working with businesses and land owners to provide more off street car parking and loading zones. This may include WCC buying sites to use for car parking. Please see supporting document for more details.

TR180-22%20submission.docx

Submission for Traffic Resolution TR180-22.

I oppose the changes as currently proposed in Traffic Resolution TR180-22.

- I fully support a new pedestrian crossing at Kenya Road.
- I support a cycleway on Ngaio Gorge Road, Kenya Road and Stage 1 of the cycleway on Kaiwharawhara Road, but not as proposed.

I will give my full support if the following amendments are made. These include:

- Amend the staging of Kaiwharawhara Road by removing Stage 2 and making changes to Stage 1.
- Council to work with land owners and/or purchase land in the Kaiwharawhara Road area to build more off street parking and loading zones if/before Stage 3 is done.
- No dividers separating the cycleway and the rest of the road as it reduces the cycleway to a “single track” and creates additional hazards for cyclists.
- No speed bumps on Ngaio Gorge Road and Crofton Road. The pedestrian crossings to be/remain a standard pedestrian crossing.
- Changes to the end of the cycleway at Ngaio Gorge Road where it merge with vehicles.
- Cycleway not required on Crofton Road as it will be a 30 kph zone.
- Cycleway not required on Cameron Street.

I also suggest the building a right turn lane from Ngaio Gorge Road to Perth Street through minor road widening.

An explanation of each of the points is below.

Amend the staging of Kaiwharawhara Road by removing Stage 2 and making changes to Stage 1.

Council to work with land owners and/or purchase land in the Kaiwharawhara Road area to build more off street parking and loading zones before Stage 3 is done.

- As shown in the parking demand document, the demand for car parking will exceed the supply of car parking in the area if the cycleway is to go ahead. Given it was done while New Zealand was in the Orange traffic light setting, parking demand is understated compared to what it currently is now. For instance, Cameron Street parking is at over 90% occupancy at around 7am-7:30am on the days I used this street.
- One thing I plan to do far more often because of a return to the office is to park in the Kaiwharawhara Road/Cameron Road area and ride into work as the distance and topography from here is short enough ride (and not too hilly) so that I do not require the use of end of trip facilities. If I cannot find a space to park here, I will likely drive all the way into the CBD instead rather than spend time using end of trip facilities. It is increasingly likely more people will do what I do given the cycleway improvements on Thorndon Quay, the extreme lack of reliability in our public transport system, the rapidly increasing cost of living, and the price of car parking in the CBD.
- As demand for car parking exceeds what will be available once the cycleway goes in, council must work with land and site owners in the area to provide more off street parking and/or

buy land to build more off street parking. This is the only equitable way to compensate residents and business operators in the area for any loss of trade arising from the cycleway.

- In my view Stage 1 of Kaiwharawhara Road should be a clearway from 3:30pm to 7pm. This is 30 minutes longer than what is proposed and would benefit a number of cyclists who ride between 3:30pm and 4pm.
- In my view, stage 2 should be canned. This is a petty stage in my view as there is a fairly reasonable number of people who would need to use the on street parking and proposed loading zone from 2pm to 3:30pm. The number of cyclists who may benefit in the 2pm to 3:30pm and 7pm to 9pm period is likely to be minimal.
- In my view, stage 3 should not proceed until council has worked with land owners and/or purchase land in the Kaiwharawhara Road area to provide more off street parking.
- As stage 3 removes all parking and loading zones in the Ngaio bound direction, I'd imagine it would be extremely challenging to deliver goods to businesses there that do not have off street parking. That would really suck for them and council must work constructively to deliver an equitable outcome for businesses there who do not have off street parking.
- I cannot in good faith support a proposal that would have a deleterious impact on business and those who live in the area even if it would benefit myself. I would hate to see businesses close down, leaving behind vacant storefronts because of a cycleway, as unfortunately proven on Riddiford Street in Newtown.

No dividers separating the cycleway and the rest of the road as it reduces the cycleway to a “single track” and creates additional hazards for cyclists.

- Rather than using dividers, the cycleway should be painted with thermo-plastic audio feedback lines given relatively low speed (50kph) environment. The thermo-plastic audio feedback lines aren't used in Wellington, but are used effectively in the Kapiti Coast on both the Kapiti Expressway (whole length) and on the old SH1 between Lindale and Otaihanga roundabout. These provide a noise for drivers to move back into the correct lane without being as loud as a rumble strip.
- While I acknowledge some people on bikes “feel” safer with dividers, I don't believe dividers are a good idea because for the reasons below.
 - Dividers means faster cyclists (especially those on e-bikes) cannot overtake slower cyclists. They will get frustrated leading to unsafe overtaking manoeuvres. I'd hate to be clipped by someone on an e-bike doing a dodgy passing manoeuvres going faster than myself.
 - Dividers create an additional safety issue. If I make a mistake (or if someone is very wobbly) and veer into the divider, I will likely fall off my bike, and land on the road. Whereas if it was a painted line between the road and cycleway, I'd still be on my bike (and maybe get beeped at by a vehicle).
 - Should a pedestrian walk in front of a bicycle, I would have no option but to hit them or brake very suddenly, increasing the risk I lose control of my bike. A painted line would mean I have more options to manoeuvre.
 - Dividers means if a vehicle blocks the cycleway at a driveway or intersection, I would have to stop and wait for them as they have blocked the cycleway. If it was just paint, I can manoeuvre around them.
 - Should there be debris, rocks or glass in a cycleway separated by dividers, I would have no option but to dismount and go on the footpath or the road (where there is

no footpath) whereas if it was a painted line, I would be able to get around the debris, rocks or glass quite easily.

- Having dividers means the cycleway is more likely to fill up with debris, glass and rocks because if it was a painted line, the few vehicles or trucks that veer into the cycleway grinds down the debris so it is less of a hazard or less likely to cause a puncture.
- Having dividers means I am unable to ride 2 abreast. If it was a painted line, I can do that and move into the cycleway when I see a vehicle behind me.
- Dividers may benefit on higher speed roads or roads with a large volume of heavy trucks –e.g. SH2. But on a standard 50kph urban road, I feel any perceived safety benefits are less than the issues caused by them. A standard 1.5m wide painted cycle way would be quite comfortable here. A thermo-plastic audio line separating the cycleway and the road would make it better than that.
- A cycleway with dividers makes no sense in stage 1 or 2 on Kaiwharawhara Road because it goes for 50m or so and then stops. A 50m cycleway, lets be honest, is pretty useless. Painted lines here is sufficient and allows for cyclists to ride on the line if they feel comfortable not having to duck in and out of the line of parked cars when the clearway is not operational.

No speed bumps on Ngaio Gorge Road and Crofton Road. The pedestrian crossings to be/remain a standard pedestrian crossing.

- I oppose the addition of new speed bumps and raising the crossings on the route. This will have a negative impact on bus passengers (especially standing passengers).
- It would also make it more difficult to turn in and out of side roads. Currently traffic bunches up and there are gaps between every bunch of cars. With the new speed bumps and raised crossing (essentially another speed bump) this will cause cars to unbunch, reducing the number of suitable gaps in the traffic to turn and in and out of side streets. Suppose there are 700 cars in one hour travelling in one direction. Without the speed bumps they may follow 2-3 seconds behind one another, which means there are gaps between each bunch of vehicles. However with 25 kph speed bumps and the braking at every speed bump, each bunch will unbunch and travel with a 5 second gap for example. This means there are far fewer suitable gaps for turning traffic.
- I also oppose it because this should not be a precedent to install speed bumps before and after every pedestrian crossing and having the pedestrian crossing raised too. For example, the route between Johnsonville (Alex Moore Park) to Crofton Downs (Chartwell Drive) has 10 pedestrian crossings. If speed bumps before and after were constructed and the pedestrian crossing raised, that would mean 10 raised crossings and 20 speed bumps. Given the braking and acceleration required for every speed bump and raised crossing, this would massively increase carbon emissions given the braking, acceleration, braking, acceleration, braking and acceleration at those crossings.

Changes to the end of the cycleway at Ngaio Gorge Road where it merge with vehicles.

- The merge between the cycleway and the roads needs to be better than what is shown. In Ngaio Gorge Road going citybound, ending the cycleway on a speed bump seem to make no sense. Surely merging in the area between where the speed bump is and the pedestrian crossing makes more sense.

No cycleway on Crofton Road as it will be a 30 kph zone.

- A city bound cycleway is not required for the 30kph zone on Crofton Road. A 30kph zone is plenty safe for all to share and contributes towards compliance of the 30kph limit as people on bikes should take the lane. A cycleway here takes away carparks that are used by Ngaio and Crofton Downs residents as an “overflow” park and ride for the bus and train. There is insufficient park and ride parking for both stations. Crofton Downs Station park and ride is full by 7:30am and Ngaio not long after that pre 2022. Therefore bus/train commuters who need or prefer to use cars to get to the station/stop after say 8am need to park on street. Taking away the parking on Crofton Road will mean those people will probably drive all the way to their destinations instead, leading to higher carbon emissions, not lower. What I suggest for Crofton Road is either:
 1. Leave the road layout as per status quo or
 2. Have citybound parking here but with a 7-8:30am Monday to Friday clearway so it can be used as a wider lane during clearway hours and parking being allowed from 8:30am onwards.

Cycleway not required on Cameron Street.

- Cameron Street is a low volume quiet street. Therefore a cycleway here is not needed.

Feedback

696

114161611746

NAME: Ryan abrey	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

It's ok

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I have been running up or down kaiwharawhara road nearly every other day since we moved to ngaio and this will make my commute much more pleasant and safer.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

697

114161580755

NAME: S watson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

no thank you

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I want a safer and more accessible ride into the city but not sure if the changes fully address all the issues. But happy that this is being looked at. My issues are the lack of safety I feel as I travel towards the crofton round about, from kenya and crofton street, there isn't much space and the cars drive very fast and close to me...Going into the lane to turn right at the round about is super scary. I would never let my teenagers travel by bike into thorndon to attend school. Also the track down the gorge is very scary as bikes need to travel in the car lane due to the condition of the road and debris and lack of space on the side.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

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Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Strongly support
Do you have any comments to make about the proposed design?
can we have a bike lane going down as well that is in a good condition

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
this part of the road is very dangerous. excited for these changes

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback

698

114181627607

NAME: Sally	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Considering my personal day to day activities in this area, the changes will severely restrict my ability to undertake my daily exercise which I need to drive to and be able to easily park at as I go at lunchtime and only have a short window away from work. I also need have good friends who I support on Kenya street and need to be able to park by their house.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I feel like the design has not taken into account real world scenarios of the people who live, work and operate in this area. It has focus on a small number of the population and is basing it on what they think people will do if they is implemented, rather than understanding the current requirements and taking an considered approach.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Given the limited number of car parks there currently is removing reducing the number of angle parks would not help. Also cyclists come down Cameron Rd very fast.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I disagree with having a bike land up the gorge, I don't feel the number of cyclists warrant the cost of this. Also as I said at the being, I think it is incorrect to assume that if you build cyclists will come.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Where will people who live there park if they don't have off street parking as a number of them don't. How will friends and family visit them if there is no were to park?

What do you think of the timing of the proposed approach?

Why do you think that?

I don't agree that the proposed change should be made at all.

NAME: Sandra and Mark Tilsley	SUBURB: Karori	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The design has removed car parks required for businesses to operate. Businesses have chosen this location outside of the CBD for reasons and one of which is that it provides access and parking for customers, staff and suits business operations such as truck access, courier drop offs, large product item pick ups etc. It does not have suitable public transport provisions for staff. Planners and designers could have utilised the entire width better and come up with a design that maintained car parking while adding in a cycleway. This is a disappointing design that will impact a valued business district in Wellington. It fails to address the impact on businesses, jobs, staff, and the business community.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Clearway should remain at 4pm permanently and not go to 2pm then fully clearway. The loading zone should remain permanently outside Capital auto electrics and Hanging around. Our business is Capital auto electrics and the loading zone until 4pm is workable. However, the loading zone from 2pm will mean delivery trucks and car tow trucks that drop cars off will just clog up the road as they will just park on the road regardless and tow trucks dropping off vehicles will cause

traffic backlog and safety issues for cyclists. If there is no where for delivery vehicles to park ie couriers, parts trucks, tow trucks, then they will just stop on the road if they have nowhere else to go. The roading design should continue to provide loading zones for business operations to be maintained. The design and plan of this route has overlooked businesses and the impact it will have. It is greatly disappointing that designers and planners have failed to utilise the overall width of the the road where they could easily have maintained car parks while adding in a cycleway.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Clearway from 4pm should not be altered to an earlier time let alone so quickly. Clearway from 4pm is workable and manageable for business continuity. However, a 2pm clearway is potentially damaging to business operations. The speed of the changes do not allow enough time for businesses to adapt or relocate.

Feedback

700

114160292705

NAME: Sarah	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Oppose

Do you have any comments to make about the proposed design?

The council has consulted with those who live on Cameron Street about still having parks for residents - what about those who reside between 5 - 25 Kaiwharawhara Road? Where do you expect these residents to park? If the clear way operates from 4pm - 7pm and then 2pm - 9pm it doesn't give residents the option to move their cars if they are at work. Other than the clear way there will be nowhere to park.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback

701

114178461298

NAME: Senghong Dim	SUBURB: Broadmeadows	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

My opposition to the proposal relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the damage to our business. For this part of the route the uphill section needs to be treated in the same way as the downhill section. I am also disappointed with the consultation process. English is my second language and I have been helped with my submission by one of my customers. Until that person prompted Council officials, no-one had come to talk to me about the changes and implications for our business. That happened with just over one week left in the timeframe for submissions. Even after that visit the implications of the changes weren't clear to me, and I had no offer of assistance with a submission. Notes left under our door were hard to follow and were not clear on what was happening and the implications of that for our business. That we would be very badly affected is obvious – Council officials should have talked to us. As a result, we have had no input to current proposals. If I had more time, I know I could get many of our loyal customers (who are directly affected) to also submit to support us but that isn't possible to organize in a few days. The process adopted means that you will not get the depth and strength of feeling from those who are directly and adversely affected by the proposed changes.

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

I am the owner of the Kaiwharawhara Café which operates from leasehold premises at level 1 of 15 Kaiwharawhara Rd (southern side). This café has been in operation for many years. I have owned the business since 2018. We operate six days per week (Monday to Saturday), with opening hours of 7am to 4pm weekdays and 9am to 3pm on Saturday. It is a family-run business which we depend on for our livelihood. Our customers include local residents and employees of local businesses. However, our largest customer segment, especially during the week, is trades staff who use the café for breakfast, morning tea and lunch (mixture of takeaway and dine-in). These customers drive to the café and rely on short-term parking for their visit. We have two off-street carparks, but this is insufficient for the number of customers that rely on parking to visit the café. We think we can operate with a clearway on the southern side of Kaiwharawhara Rd from 4pm to 7pm as proposed in Stage 1. Any extension of the clearway period (ie: from 2pm to 7pm) or loss of parking altogether (Stages 2 and 3) will result in the café not being viable. We see no way around this. Lower Kaiwharawhara Rd is a commercial area. All existing parking is heavily used. There is no practical, alternative parking which our customers can use. The northern side of Kaiwharawhara Rd is already a clearway until 9am and is heavily used with customers of other businesses. Cameron St parking is also already heavily used and, realistically, our café customers are not going to look for parking which is some distance from the café. The closure of our business will have dire consequences for our family. We have lease commitments which we need to pay even if the business is closed. This is very real for us – it is not some plan on paper. This is our livelihood. The proposed changes to the Kaiwharawhara Rd section are not based on any evidence (actual or likely bike volume) supporting the need for a separated cycle lane priority over a non-separated cycle lane with clearway times matched to the needs of commuter cyclists. The proposal to automatically transition to a cycle lane with excessive clearway times (2pm-9pm) after one year (in 2024), and/or a separated cycle lane after two years (in 2025), are a disproportionate response to the needs of cyclists versus businesses like ours who depend on on-street parking for staff and customers when there is little or no practical, alternative parking. The objectives of the changes can be achieved through having a 4pm to 7pm clearway for exclusive cycle use on this short stretch of road. While Council officers are unable to provide current cycle usage data, through discussions it appears that uphill use outside the hours of 4pm to 6pm is very low (fewer than an average of 20 cyclists per day). This small section of the overall cycleway can have a clearway from 4pm to 7pm to create space for cycle use and outside this time period be available for on-street parking, with a mixture of loading zone, short term and longer-term options. This is a sensible compromise which better balances the needs of existing users and the objectives of a connected bike network. It is also consistent with what is proposed on the downhill section of the road. Please recognize our position.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

For the reasons set out above, we oppose any changes beyond Stage 1.

Feedback

702

114163098768

NAME: Sinny West	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

703

114163080290

NAME: Steve	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I've been riding these roads for 10 years. These changes will make it safer and encourage many more to cycle. E-bikes have flattened Wellington's hill - this helps with safety concerns, removing another barrier to participation.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

staged intro looks good

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

excellent to make the bridle way more accessible

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

I wonder whether this bus stop at the top on the south side is necessary - there is another stop just 150-200m around the corner

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

like the extension of the low speed zone

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

i'm hearing that businesses are comfortable, so just crack on

Feedback

704

114167711860

NAME: Mark Patchett	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I have never understood why we don't have a two way cycle lane down Ngaio Gorge. It would be on the uphill side, so that people could safely bike up and down hill. The reality is that it will be most busy down hill in the morning and uphill in the evening due to commuter traffic, which seems like a lot of the cycle traffic. Also is there any proposal to widen and encourage more use of the bridal path?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Earlier comment which is to widen bridal path and encourage more use as a cycle way.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?
Oppose
Do you have any comments to make about the proposed design?
There should be seperate shared biking and walking way all the way to Trelissick

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Make changes more quickly and share with businesses the international research that shows how they can thrive when people cycle more.

Feedback

705

114160175845

NAME: Wayne	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The changes are impractical, poorly designed, don't consider needs of residents businesses or employees. Also fix the pipes first. I cycled everyday for 30 plus years thru this route until I broke my leg (own fault and wasn't in the area). Don't see the need for these changes

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Most of those parking on kaiwharawhara road are bus drivers or people working at the nz post depot. These guys go to work very early to start their shifts. Where are they supposed to park now? We are going to have less bus drivers and more bus cancellations as a result of these changes

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

Again many of those parking here are bus drivers going to the bus depot for their shifts. If you ever go past these areas at 7am all the parks are gone because people are going to work. These changes are nuts

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Support

Do you have any comments to make about the proposed design?

These seem ok as it won't impact on businesses or residents too much

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

57 parks gone. Geeze the side roads like Trelissick Creek and Crofton Road already have over flow from Kenya Street. These changes are impractical

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Do make the changes at all

Feedback

706

114181696129

NAME: Sue Todd	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The removal of 43 car parks is considerable and will negatively affect businesses as well as consumers. Many of the businesses have no ability to provide off street parking and sell products that will be difficult to transport via bus and bicycles e.g. tiles, furniture. It will also negatively affect disabled consumers as they will not be able to park nearby. Businesses are important for Wellington and Wellington is becoming increasingly busuiness unfriendly. Many of these businesses may close as a result of these changes resulting in people losing their jobs. WCC should be doing more to support businesses rather than hinder them.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

This will be extremely negative for residents of Cameron street. Where are they supposed to park their vehicles and guests park?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly oppose

Do you have any comments to make about the proposed design?

This is already a very narrow piece of road and the road will become even more narrow with the inclusion of a cycle lane

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

This will negatively impact residents. Where will they park their cars and visitors park? Reducing speed limits on this main route will also impact travel times for commuters. It will also force vehicles to run in low gears increasing emissions - as we do not have a totally electric car fleet yet.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

This is a leading question implying this consultation is just a tick box process and the proposal is going ahead despite feedback - so much for democracy and also WCC being true to its values.

Feedback

707

114178433322

NAME: Susie Toogood	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

There is too much consideration given to the few who bike. A city needs to flow with all manner of transportation. But by limiting the main users (cars) in lieu of a small road user group, it doesn't seem a fair way to treat those who live and work in the city. I agree that safe biking is good but I never see people loading their bikes with tiles or wood or parcels of whatever. They tend to be commuting to and from work, so for those few that do bike why do so many others have to be inconvenienced with less access to our city. It will be easier to go to the Hutt and I suspect many of the businesses in this area will be thinking this way too.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The area being considered in this proposal is where industrial and semi industrial business have located. It is not high pedestrian/cycling at present and until it shows more use in this regard I would suggest any severe changes to the parking be reconsidered or the area will become an empty zone. Not great for any city. Outskirts of cities have always been for small industries. I all here regularly and never have a problem with the space of crossing roads etc.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Don't know

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Oppose

Do you have any comments to make about the proposed design?

Don't understand why a cycle lane is being created on one side and road arrows on the other. Can't the arrows be done on both sides. Cheaper and cars do have to get used to bikes actually being on the road too.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I think removing any parking from Kaiwharawhara Rd is unnecessary and raising the footpaths also. Not enough people walk this area and especially not visitors. To access most of this area (businesses) one needs a car. Therefore one needs somewhere to put it, Whether it's an electric car or not, most of us need and would use one to access this area.

Feedback

708

114162851150

NAME: Taison	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I support it overall but there are problems with rider safety that need to be addressed if the council wants people to actually ride their bikes more and reduce local emissions. The council should be more bold with their decision-making and design bike paths that are protected and safe. Prioritising rider safety over an entire route is important.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

It's not overly clear if the bike lane is protected for the entirety of Kaiwharawhara Rd or not, but if so then I support it. I'm giving the benefit of the doubt in my rating here. There is not enough detail about how the crossing from Hutt rd will be done, because I would like to see that crossing feed straight into a protected bike path. There is also not a lot of detail about how the crossing into Cameron st is to be made by cyclists. At the moment, it's awful. You sit in the middle of the road completely exposed and it sucks. There needs to be a better way to cross into Cameron st. There is also no detail as to whether there will be any traffic calming installed on Cameron st to slow traffic down. Just adding a sign doesn't help, as anyone will notice when they drive on Victoria st - which was reduced to 30 by LGWM a few years ago and made absolutely no difference because no calming infrastructure was built. If you are actually going to slow people

down on Cameron st using calming infrastructure then great, I'd love for downhill traffic to be slowed a bit there. If not, then it's a waste of money putting up the signs so don't bother.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Oppose

Do you have any comments to make about the proposed design?

Cycle lanes and cycle shoulders are no good. I have explained my disdain for them in my feedback for the Aro Valley connections. Be bold. It might be weird, but why not make the footpath a shared path for pedestrians and uphill only cycling? Make the path slightly wider and then add connections at the top and bottom. So at the bottom, you can connect it to the crossing you're making at 24-28 Kaiwharawhara rd, so uphill cyclists can cross safely and then be fed straight into the shared uphill path. Then at the top, make some kind of colourful road marking in the T-intersection and up Curnow Way to the bottom of the bridle path to indicate the way to go, whilst also signalling to drivers that it's a shared space? Add some signs on Curnow Way and Cameron st as well to indicate to cyclists and drivers that it's shared road space so proceed with caution.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Oppose

Do you have any comments to make about the proposed design?

If the uphill lane ain't protected, I'm opposed to it. As is becoming a familiar theme with my feedback, be bold. Find a way to make it protected or don't waste the money. You need to find a way to make it protected or it will not make more people cycle, period. Raised crossing is a goodie though, love that.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

I'm giving the benefit of the doubt in that it looks like the proposed bike lane is going to be protected. If not, then I'm once again opposed. Lowering the speed limit is a great idea in this area. However, there is once again no proposed traffic calming to enforce the speed limit. Ngaio shops area is another great example where people drive 50 through it all the time because you can. Add traffic calming to the shops area and the proposed extension and then I'm supporting this proposal. Raising crossings is always a good idea so I support that.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Similar to my Aro feedback, quicker is always better but this proposed timeline is good so I support it.

Feedback

709

114176226582

NAME: Tim	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Support

Do you have any comments to make about the proposed design?

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

710

114160125653

NAME: Patrick	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Clearways are only effective if there is consistent monitoring and enforcement.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

Will the raised pedestrian crossing also include improved street lighting, as that area can get quite dark at night time.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback

711

114181483910

NAME: Marcail Parkinson	SUBURB: Thorndon	ON BEHALF OF: VUWSA	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

Additional bike lanes and safer pedestrian options are very important to city safety and will make our city a more attractive option for students.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support
Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?
Strongly support
Do you have any comments to make about the proposed design?
These plans sound great as long as some parking is still available near the places where parks are recommended to be removed so that people with accessibility issues or who need emergency parking are still able to access the area.

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback

712

114166771977

NAME: Jane	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

This "consultation" process is not representative of the views of Wellingtonians. Please conduct a robust and independent survey of the Wellington public. Cyclists are an extremely small proportion of road users but are obviously over-represented in the survey comments to the point that the results are quite frankly unreliable. The questions are so leading and biased that it's as if they were designed to confirm what the Council has already decided to do. It's impossible to say whether one supports these changes when there are so many of them, and some may be liked but others may not. How did LGWM come to the conclusion that removing parking spaces and adding cycle lanes would be better for all? Electric-vehicles are starting to become more mainstream so this doesn't seem like a wise move. Finally, and most importantly please ensure the views of the elderly, the disabled, parents of young children, business owners and workers, and homeowners (ratepayers) are adequately considered. Make sure the voices of all types of Wellingtonians come through - this is a democracy.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly oppose

Do you have any comments to make about the proposed design?

The majority of Wellingtonians don't want or need these changes. They make it more difficult for 99% of us to get around.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly oppose

Do you have any comments to make about the proposed design?

In the near future we'll move to electric vehicles - where will people park? Don't remove the car parking that most households need to use to appease the very few fit and able people who cycle.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Neutral

Do you have any comments to make about the proposed design?

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly oppose

Do you have any comments to make about the proposed design?

Don't remove the car parks - future electric vehicles will need to park somewhere. Most Wellingtonians don't want these changes. Just a very vocal, very small and pushy push bike minority.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

What a leading question! I don't want the changes made at all.

Feedback

713

114160168860

NAME: Nicholas Gibb	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I've witnessed numerous incidents where cyclists have been hit by drivers along this route. These changes are long overdue. It's not acceptable that people have to risk their life because they choose not to drive.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

Strongly support

Do you have any comments to make about the proposed design?

I often walk, run and drive through that area and it's currently a terrible experience for all those activities. The road is however, very wide and offers plenty of opportunity for conversion into a cycle lane. Having somewhere to safely cross would also be a great improvement.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

Strongly support

Do you have any comments to make about the proposed design?

That's a blind corner that's no good for safely parking on anyway. It makes sense to turn it into a cycleway.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

Strongly support

Do you have any comments to make about the proposed design?

That short section of road is a nightmare at the moment. It would be great to make it safer for all users. I wonder if the speed limit should be lowered and speed bumps put in to slow drivers coming around the corner towards that pedestrian crossing.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

Strongly support

Do you have any comments to make about the proposed design?

Great improvements. Safer cycling along Kenya St is long overdue. That road is not wide enough to have parking on both sides, so it's a win for driving too. Can you consider making the speed limit 30km/h all the way along Kenya Street too? A lot of drivers tend to speed along that road at the moment.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

The only change I would suggest is installing the pedestrian crossing sooner.

Feedback

714

114165218630

NAME: t peters	SUBURB:	ON BEHALF OF: delivery drivers	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

destroying the fabric of these villages idealistic nonsense

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

715

114173539633

NAME: Nathan Hall	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

716

114181454624

NAME: Andrew Sussman	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Cycle lanes save lives and the environment

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

717

114179435975

NAME: Margaret Gabbitas	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

ngaio%20sub%203.PNG

I totally support the safer cycling route proposed via Kenya Street/Ngaio gorge, but have the following suggestions and one query:

1. The speed limit along Kenya Street should be lowered to 30kms or 40kms as presently once cars are past the corners they will speed up and this will make it unsafe for cyclists/walkers.
2. All parking spots should be clearly marked and yellow lines put near driveway entrances
3. The parking bays in Ngaio Gorge should have a limited time of 2 hours to allow visitors to Trellisick Park to do a walk, rather than workers parking their cars there all day.
4. I support the suggestion of a small roundabout at the top of Kenya Street/entrance to Trellisick Crescent and the moving of the crossing to further down Ngaio Gorge for safety reasons

Query

Given the high rate of bike burglaries, what consideration has been given to safe secure places to leave your bike when you cycle to the city? Given our terrain is hilly there are alot of e-bikes and these are heavy and difficult to store in 2-tier storage facilities as per Grey Street.

Thank you for the opportunity to submit.

Feedback

718

114179292319

NAME: Alejandra	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Whilst I support adding bike lanes, I don't want local shops to have to close because there isn't enough parking spots to accompany customers. Some really amazing shops have had to close because of a lack of business. Also, I support the raised crossings next to the motor doctor. Sometimes, I walk to the dairy, and cars are so fast and forget to stop.

Do you support the proposed speed changes on the Ngaio route?


Strongly support

Decision made by Waka Kotahi

Feedback

719

114179295831

NAME: Aiden 	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?


Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?
These include traffic resolution TR180-22

Support

Why do you think that?

it will be good and allow more cycling 

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

720

114175704629

NAME: Adam Tulloch	SUBURB:	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Ngaio-Connection-paper-consultation-form%20(1).pdf

Have your say on pedestrian and bike improvements in Ngaio

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
- Install a bike lane on the southern side in the uphill direction
- Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
- Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.

We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.

- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
- In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

I generally agree with changes to improved public transportation and cycle / pedestrian access for Wellington CBD and the inner City, including Aro Valley and Hutt Road, but I oppose this particular cycleway.

Leave Kaiwaharawhara and Ngaio alone, this cycleway is not needed, the project should not proceed.

90% of the time this cycleway will not be used. It is a complete waste of ratepayers money.

It will close businesses, cost jobs and hurt our local community, much more than it will provide benefit for the few people who are wish and are able to walk or ride a push bike up Ngaio Gorge Road.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trellisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

I oppose the cycleway changes, the cycleway is not required.

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

I oppose the cycleway changes, the cycleway is not required.

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☒ Don't know

Do you have any comments to make about the proposed design?

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☐ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

I generally agree with changes to improved public transportation and cycle / pedestrian access for Wellington CBD and the inner City, including Aro Valley and Hutt Road, but I oppose this particular cycleway.

Leave Kaiwaharawhara and Ngaio alone, this cycleway is not needed, the project should not proceed.

90% of the time this cycleway will not be used. It is a complete waste of ratepayers money.

It will close businesses, cost jobs and hurt our local community, much more than it will provide benefit for the few people who are wish and are able to walk or ride a push bike up Ngaio Gorge Road.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

I have read through the full project details for Ngaio Connection and do not support it based on the following information.

Your survey is biased, it only provides data on how many carparks are being used, you don't state how many people walk and or cycle from Ngaio to Wellington City and return each day.

Providing a safer route for cyclists and pedestrians will not greatly increase the amount of cyclists and walkers. People who wish to cycle or walk to work each day are already doing this - and largely they do so without incident.

The small benefit of building this cycleway is far outweighed by the cost to local business, employment, and Ngaio and Kaiwharawhara's prosperity.

Without sufficient carparks for staff and customers, local businesses (of which there are many) will be forced to close down, jobs will be lost, and buildings will sit empty. With empty buildings this vibrant area will deteriorate, driving commercial property values down.

Eventually these properties will be bought up by developers, at bargain prices. These developers will build multi-storey residential Apartments that will be bought by investors and turned into rental accommodation.

More rental accommodation, brings more people into the area, leading to more congestion on our roads.

By removing all of these carparks, local businesses will be forced to relocate or close, you will force local people from Ngaio, Khandallah, Newlands, Johnsonville and Tawa to shop outside of Wellington. Instead they will shop in Porirua, Lower Hutt or the Kapiti Coast where it is easier to find a carpark.

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Slowing traffic through this area is a good idea, it provides all road users more time to make decisions.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☒ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☒ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☒ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☐ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

721

114181666607

NAME: Ken Carson	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I think the idea of impacting business and our community with an idealism that doesn't make sense on level. We are completely aware and respectful of our cycling community as we are of pedestrians, scooters and all other users of the street. I cannot understand why we would be doing this when the city has so many other things that need investment.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

722

114181658055

NAME: Julia A	SUBURB: Other	ON BEHALF OF: SWET	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The consultation process was very short considering the huge negative impact these proposed changes will have on the businesses operating out of this area. Most of the businesses in this area require car parks for their customers. Public transport and bikes will not be suitable for these types of businesses and if the proposed changes go ahead, it will likely mark the end of a lot of businesses in this area. Concerning and scary after an already difficult past 2+ years.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

723

114181045218

NAME: Aimee Williams	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

As a local resident who frequently uses this road and businesses on Kaiwharawhara Road, the proposed changes in particular removal of car parking to have a dedicated cycle lane on the uphill side is a drastic measure that affects the livelihood of many. The changes do not seem consummate with the number of cyclists this affects - nor seems to be an improvement to safety, and being an already narrow strip of road, I believe both cyclists and car users share this road with each other respectfully at present.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

724

114160159014

NAME: Alex Kinzett	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

725

114162352800

NAME: Pat	SUBURB: Churton Park	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Local retail businesses will lose the ability to trade without parking

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

726

114170642294

NAME: Alistair	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

727

114161066713

NAME: Alyson Howell	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

728

114163020384

NAME: Amanda van Gorp	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The affected areas already struggle for parking. It's a busy family zone, where families need more than bikes to get around. The public transport is horrendously unreliable. This means even healthypeople can't base plans on public transport, let alone anyone with health issues for whom getting stranded is even more of a problem- especially in Wellington weather.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

729

114167977005

NAME: Amelia	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Specifically the changes to Cameron street are a waste of money and not needed. As someone who bikes to work everyday, Cameron's street is the safest street in my commute. The street is already quiet so a cycle lane would not provide any additional benefit and disrupt residents parking/ force them to pay for parking to fund something that isn't needed in the first place.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

730

114176180619

NAME: Ana	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I live in Ngaio and would like to safely cycle to the town with my kids

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

731

114180727840

NAME: Andrea	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

This does not take into account the devastating affect your WCC proposals will have on the local businesses. Particularly concerning for the fitness centre SWET.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

732

114171605816

NAME: Andre Geldenhuis	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

733

114160941731

NAME: Angela Houston	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The streets are too narrow to accomodate cycle lanes, 2 way traffic and parking

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

734

114181651368

NAME: Anke	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Supporting active travel from the suburbs into Wellington city is crucial for the health and safety of everyone. There will be more and more people coming into Wellington in the future, and car travel not only leads to more and more congestion, but also negatively impacts air quality, climate change and the safety of people choosing other travel modes. Active travel is beneficial for mental and physical health and makes the city more livable. Every solution is a compromise - as a cyclist, pedestrian, driver and user of public transport myself, I experience Wellington from all those angles. Cyclists have long been underinvested in in this city and deserve to be kept safe. Anyone cycling in Wellington's wind, weather and hills (Go ebikes!) to the benefit of everyone should be celebrated and supported, not forced to survive amongst trucks and cars on roads designed for the latter. Thank you for looking after all people traveling in Wellington and future-proofing Wellington's transport infrastructure for everyone.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

735

114168712483

NAME: Anne Jacobsen	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I do not have a personal connection to the area but I do believe that everyone deserved to have options in how they travel, Ive heard that Ngaio Gorge is not cycle friendly and believe making it cycle friendly could mean achieving mode shift away from primarily personal vehicles

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

736

114169153351

NAME: Anne	SUBURB: Broadmeadows	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Cyclists can already use the Bridle Path and Old Porirua Road

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

737

114181680475

NAME: Arran Whiteford, Transport Portfolio Lead	SUBURB:	ON BEHALF OF: WCC Environmental Reference Group	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

See written response attached

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Ngaio-Connection-paper-consultation-form.pdf.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council

Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
 - Install a bike lane on the southern side in the uphill direction
 - Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
 - Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.
- We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.
- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
 - In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☒ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☐ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

We strongly support the proposed changes, and particularly the longer term plan for full time cycleways on both sides of the road in the areas for which clearways are initially planned from Hutt Road to 25 Kaiwharawhara Road. We strongly support installation of the bike lane and broken yellow lines to link up with the new bike lane being built as part of the slip stabilisation work.Improvements to this route. This is an important addition to the Hutt Road cycleway and addresses an area in which cycling can currently feel unsafe.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trellisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

We strongly support the proposed changes and particularly the installation of an uphill cycle lane and removal of parking as part of this. We believe installation of the raised pedestrian crossing will significantly improve pedestrian and cyclist safety at a busy intersection area by slowing traffic (having a raised table crossing) and the crossing itself (signalling to traffic to have to slow).

This work helps to connect Ngaio properly to the Hutt Road cycleway, and will also help improve safety for in-suburb cycle trips. An addition we would like to see is the inclusion of a downhill cycle route for the gorge section: whilst sharrows will work for experienced cyclists, a number of less experienced cyclists have voiced their concern about going down the gorge: the addition of a downhill cycleway will better provide for these cyclists. They will also provide safe passage for cyclists filtering to the front when traffic is stopped in the gorge (frequent at peak hour, as well as at the moment with the roadwork lights). At the moment many drivers hug the left hand edge making this impossible.

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

We strongly support the removal of car parks and installation of an uphill cycle lane: we have heard evidence from local people that this part of the road currently feels very unsafe for cyclists, with traffic tending to go up the hill at speed, and sometimes cutting in on cyclists. Removal of the parked cars will see the road space freed up for cyclists with room for vehicles to pass more safely. We seek that WCC consider extending the 30km limit to the proposed new pedestrian crossing at the top of the gorge: this is a busy residential area with children walking to school, as well as residents accessing services by foot, cycle and vehicle. Extending the speed limit better signals this as a 'go slow' area in which drivers do not have the main priority.

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The Bridle Path provides an important off-road route for both pedestrians and cyclists to and from Khandallah: it has the potential to be even more used (acknowledging that improvements will be needed to enable this). Ensuring a safer approach to the Bridle Path entrance, and marking the cycleway makes the route more visible, and having the reduced speed limit makes the area safer for pedestrians, residents and cyclists.

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one – 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two – 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three – 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

<input type="checkbox"/> Make the proposed changes more quickly	<input checked="" type="checkbox"/> The timing of the stages is about right	<input type="checkbox"/> Make the proposed changes more slowly	<input type="checkbox"/> Don't know
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Why do you think that?

Whilst we would prefer the changes happen more quickly, we believe there is merit in the staged approach, including the 'trial' period, and then the gradual introduction of planned changes as it does give time for residents and businesses to make the necessary changes. We would like to emphasise, however, that this not be an excuse to see the interim continue indefinitely: having a clear deadline that WCC sticks to is important for certainty and signalling of the need for the long term change to a roading network that caters for multiple modes including active modes and not just private and commercial vehicles.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

The Ngaio Connection route is a crucial part of Paneke Poneke. Once implemented the cycleway will improve modal shift to cycling, as well as public transport and walking (the safer crossings) . These changes are consistent with the transport principles that ERG holds (see attached Appendix for our transport portfolio principles).

To meet the emissions reductions goals set out in the Te Atakura: Climate Action Plan (especially the goal to reduce city emissions by 57% by 2030, and to net zero by 2050) there will need to be a considerable mode shift in transport use across the city and cycleways will play a big role.

The modal shift benefits of the cycleway will only ever be fully realised until a well-connected network is established. Implementing the changes through taking an experiential approach is supported: this does allow time to 'check out'

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

We strongly support the reduction in speed, and seek that WCC consider extending this to the planned new raised pedestrian crossing at the top of the gorge: this signals to drivers that the area is a busy residential area, and that the street is a multi use street, and not an arterial route to be driven through at speed.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☒ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number:

Are you providing feedback as ☐ An individual ☒ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation WCC Environmental Reference Group

Name of school

Feedback

738

114181700328

NAME: A. G.	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Yes, but I don't understand the small portions labeled stage 2 & 3, right in the middle of stage 1. That seems weird.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

739

114179292845

NAME: B.W	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

It will make biking safer

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

740

114179296022

NAME: Ben	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I think it is a good idea cose people who ride can ride quiker and more FUN! :)

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

741

114160221272

NAME: Ben Sandle	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Changes should make much safer for people riding bikes and help more people chose bike or ebike over car for more trips

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

742

114160169345

NAME: Hadyn Nicholls	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

The houses from 31 Kenya uphill all have road reserve in front of them, instead of yellow lines, could the reserve be cut into, the footpath pushed back and in doing so widen the road? Simply removing carparks is not really a good solution when you have council reserve.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

743

114181493009

NAME: Benjamin Swale	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

744

114175689301

NAME: Bill Hester	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Ngaio-Connection-paper-consultation-form.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
 - Install a bike lane on the southern side in the uphill direction
 - Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
 - Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.
- We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.
- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
 - In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support☐ Support☒ Neutral☐ Oppose☐ Strongly oppose☐ Don't know

Do you have any comments to make about the proposed design?

I am supportive of the attempts to make it easier and safer for people to cycle to and from Wellington, but see little in the proposed plan to make it easier and safer for pedestrians and nothing in the plan which will improve the currently abysmal and unreliable bus service. (In many ways this plan is a continuation of the existing WCC approach which makes coming into the CBD harder and less feasible for most people, and encourages increased carbon usage by diverting traffic to other suburbs for shopping.)

It does make life easier and safer for potentially 1% of those commuters who are able and fit when they can commute to town via bikes or scooters during rush weekday hour on fine days.

It would be good to see a cost/benefit study on this project.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trelissick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Crossing Ngaio Gorge Road in that proposed pedestrian crossing area is currently unsafe - so trial of a pedestrian crossing is good. Please take care to ensure that traffic heading to Wellington is kept to 30 km/hr and adequately warned of the pedestrian crossing. That Ngaio Gorge Road, Kenya Street, Trelissick Crescent junction has limited visibility for all - with potential further distractions being introduced with the restricted road space after the bicycle changes.

Crossing Perth Street at the Ngaio Gorge Road junction is currently even more dangerous for pedestrians than crossing Ngaio Gorge Road, and I see nothing in the plan to improve this. The addition of bicycle lane uphill and sharrows downhill may introduce more distractions for motorists - and make it more dangerous at that Perth Street intersection for pedestrians.

The Ngaio Gorge Road proposed plan ignores most of Ngaio Gorge Road other than having a bike lane and sharrows. The uphill bike lane is parallel to the Trelissick Park boundary. There are currently three entrances to Trelissick Park on that road - with a small mention in the plan that 15 car parks will be re-instated when the road works are completed (which is now delayed until sometime in 2023) - after two years of entrance closures. Trelissick Park is a major park/reserve area - and adequate parking for access is required. The parking near the lower entrance to the Park will be tempting for current Kaiwharawhara parkers once you reduce parking in Kaiwharawhara. Likewise some bicycle commuters who currently drive to Kaiwharawhara and park their car all day could be tempted to leave their cars further up the hill outside of Trelissick Park.

Please have a look at the three current park entrances to the Park on Ngaio Gorge Road - see <https://trelissickpark.org.nz/maps.htm> especially going uphill, entrance 6, entrance 5 and the minor entrance nearer the top of the Ngaio Gorge Road, to ensure the bicycle lane going uphill will not interfere with access to Trelissick Park. I see no provision for any parking for the minor entrance.

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The removal of car parks from Kenya Street, with the plan that Kenya Street overflow can use places on Trelissick Crescent ignores the reality of the limited parking options currently on Trelissick Crescent especially at the Kenya Street end of the Crescent. There is rarely any free parking now, especially in the evening.

At a minimum, Trelissick Crescent should be made into a 30 km/hr street (now!) There is high current pedestrian traffic around the Crescent and it is dangerous day and night for pedestrians. Extra cars to be parked on Crescent will make things less safe.

It makes good sense to do this change now for safety. The winding street does not support 50 km/hr traffic.

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

no comments

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

<input type="checkbox"/> Make the proposed changes more quickly	<input type="checkbox"/> The timing of the stages is about right	<input type="checkbox"/> Make the proposed changes more slowly	<input checked="" type="checkbox"/> Don't know
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Why do you think that?

Even though I don't often stop in Kaiwharawhara for business, it will be even less likely that I would be able to find a place to stop there in future with these planned changes. It's a congested mess now, and likely to be just as much of a mess after the changes, though with less parking available.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☒ Don't know

Why do you think that?

Please fix up the public transport options. This is about dozens of times more important and beneficial to Wellington than scooters and bicycles.

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☒ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☒ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☒ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☒ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☒ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

745

114172626995

NAME: Elliot Blyth	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

It looks broadly good, but the sharrows for the downhill section are still scary to bike down – it would be great if there was an alternate, safer route

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

746

114160138215

NAME: Bryan Park	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Feedback

747

114161500734

NAME: Luca	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

The current cycle lanes don't feel that safe travelling between Hutt/Kaiwharawhara Road. Having it be easier to cross and increased protection uphill will be welcome. Based on Google Street View, it looks like a major slip has altered the road layout where Kaiwharawhara/Ngaio Gorge Road meet. I was wondering how road space would be reallocated through this area?

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

748

114161563421

NAME: Brian Johnston	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

This route needs to be properly set up to make it safer for cyclists, so great to see the consultation underway. But I worry that there is massive parking removal with no off-road options being developed, which will fuel opposition.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

749

114169040015

NAME: Paul Bruce	SUBURB: Brooklyn	ON BEHALF OF: Our Climate Declaration	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Vitally important to improve safety to cyclists and pedestrians so as to provide attractive alternative to using private vehicle

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

750

114171725983

NAME: Bruce Herron	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

751

114179292477

NAME: C.S	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Because I don't ride my bike on the road Charlie, ngaio

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

752

114161527976

NAME: Carl Blackmun	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

753

114181551810

NAME: Laurence Diack	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I cycle this route occasionally

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

754

114181647456

NAME: Catherine Watson	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

755

114165734131

NAME: Cathy Blakely	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

756

114160367049

NAME: Chelsea Grant	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

757

114181706427

NAME: Charles Horrell	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

758

114179766466

NAME: Charlotte Daly	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

This changes the road for businesses and customers significantly in the area, there are less disruptive ways to do this

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

759

114160318044

NAME: Chris	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I'm an experienced cyclist (10,000kms+ per year) who regularly uses this route. I'm in favour of uphill bike lanes and downhill sharrows. I prefer this type of design - minimal separation, green lanes and plastic bollards. The uphill section should provide little disruption as there is already a reasonable shoulder.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

760

114181687815

NAME: Chris Ford	SUBURB:	ON BEHALF OF: Disabled Persons Assembly	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?
These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

DPANgaioConnectionProjectSubmissionNovember2022.docx

Disabled Persons Assembly NZ



November 2022

To Wellington City Council

Please find attached DPA's submission on the Ngaio Connection Project

Disabled Persons Assembly NZ

Contact:

Prudence Walker

Chief Executive



Introducing Disabled Persons Assembly

We work on systemic change for the equity of disabled people

Disabled Persons Assembly NZ (DPA) is a not-for-profit pan-impairment Disabled People's Organisation run by and for disabled people.

Since our formation in 1983, DPA has brought disabled people together and shaped our collective input in a way that drives system level change.

We recognise:

- Māori as Tangata Whenua and [Te Tiriti o Waitangi](#) as the founding document of Aotearoa New Zealand;
- disabled people as experts on their own lives;
- the [Social Model of Disability](#) as the guiding principle for interpreting disability and impairment;
- the [United Nations Convention on the Rights of Persons with Disabilities](#) as the basis for disabled people's relationship with the State;
- the [New Zealand Disability Strategy](#) as Government agencies' guide on disability issues; and
- the [Enabling Good Lives Principles](#) and [Whāia Te Ao Mārama: Māori Disability Action Plan](#) as avenues to disabled people gaining greater choice and control over their lives and supports.

We drive systemic change through:

Leadership: reflecting the collective voice of disabled people, locally, nationally and internationally.

Information and advice: informing and advising on policies impacting on the lives of disabled people.

Advocacy: supporting disabled people to have a voice, including a collective voice, in society.

Monitoring: monitoring and giving feedback on existing laws, policies and practices about and relevant to disabled people.

The submission

DPA is providing this submission for the benefit of the Wellington City Council in its consideration of the Ngaio Connection Project.

Firstly, DPA is pleased to see the following proposals:

- Better access to bus stops.
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio village and Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business area on the lower part of Kaiwharawhara Road.

Secondly, DPA recognises the lengths that Council have gone to in order that the project is well consulted upon. We were pleased to be approached regarding this and would welcome the opportunity to participate in offering feedback on future projects. Our brief submission makes some recommendations around changes that could be made to make the Aro Connection more accessible, inclusive and safer for everyone, including disabled people.

Thirdly, DPA acknowledges the Council's contracting of consultants ViaStrada who conducted an extensive safety and accessibility audit which has been useful in assisting us with developing our recommendation.

Our brief submission makes some recommendations around changes that could be made to make the Ngaio Connection more accessible, inclusive and safer for everyone, including disabled people.

Wayfinding signage

DPA welcomes the proposal to erect wayfinding signage here in order to avoid unnecessary collisions between pedestrians, cyclists and motorists at the Kaiwharawhara Road and Cameron Street intersection and also at the intersections

of Cameron and Pickering Street. However, we wish to stress that our preference is that cyclists, motorists and pedestrians would all be best served by having separated cycling and pedestrian lanes in order to enable everyone, especially pedestrians, to be able to mobilise safely.

However, DPA recommends that any wayfinding signage be developed in a way where print, height and colour contrast are fully considered, especially for blind and low vision users. We would like to encourage the development of signage in accessible formats as well (i.e., in New Zealand Sign Language, Easy Read, Te Reo and ethnic languages) which can be done via the use of electronic apps where people can access this information via a QR code. This would help make the signage clearer, accessible and more visible for everyone.

DPA's recommendations

The Disabled Person's Assembly recommends:

Recommendation 1: That any wayfinding signage be developed in a way where print, height and colour contrast are fully considered, especially for blind and low vision users. We would like to encourage the development of signage in accessible formats as well (i.e., in New Zealand Sign Language, Easy Read, Te Reo and ethnic languages) which can be done via the use of electronic apps where people can access this information via a QR code. This would help make the signage clearer, accessible and more visible for everyone.

Feedback

761

114171231042

NAME: Nat	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Strongly support safer and separated cycle ways

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Feedback

762

114163969309

NAME: Christian Hoerning	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

The current lack of cycling infrastructure makes it unsafe for people to use this healthy, fun and environmentally sound transport mode.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

763

114181242022

NAME: Mark Coburn	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

764

114164529096

NAME: Jarrod Crossland	SUBURB: Maupuia	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

765

114180825052

NAME: Dan Perkins	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Feedback

766

114164065192

NAME: Dan	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

767

114174290312

NAME: Darren Young	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

I think that the safety audit team are right you should do all the changes in one stage otherwise the uptake wont be maximised. Also you need cycle counters at the relevant points along this route to see what affect the changes have made on uptake in cycling.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

768

114164335173

NAME: Digby	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

769

114179509343

NAME: Tim Watson	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

770

114179284930

NAME: Danny Redhill	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Danny%20submission.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council
Me Haka Ki Pōwhiri

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session. If you need help filling out a submission.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
- Install a bike lane on the southern side in the uphill direction
- Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
- Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.

We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.

- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
- In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

See attached.

IT'S AN ABSOLUTE DISASTER TO ALL BUSINESSES ALONG THIS ROUTE & TOO ALL THEIR STAFF & CLIENTS. ALL FOR NEXT TO NO CYCLIST USING THE BIKE LANE BETWEEN 7AM - 4.30PM MON-FRI.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trelisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

WHY DON'T YOU JUST USE THE EXISTING BIKE LANE ON THE OPPOSITE SIDE OF THE ROAD WHICH IS A CLEARWAY 7-9 AM TO ALSO BE A CLEARWAY 4-7 PM. THEN IT WON'T COST US RATE PAYERS A CENT?

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

Do you support the proposed changes on this section on Cameron Street
(between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Staged approach for Ngalo Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

See attached. BECAUSE NO ONE WILL USE IT OVER THOSE TIMES. THIS AREA IS ZONED COMMERCIAL NOT RESIDENTIAL. WE HAVE MOVED HERE TO GET AWAY FROM CBD & RESIDENTIAL TRAFFIC ONLY FOR COUNCIL TO MESS IT ALL UP.

I WANT IT NOTED THAT THERE ARE 11 COMMERCIAL DRIVEWAYS BETWEEN NO 1 & NO 59 KAIWHARAWHARA RD BEING USED EVERY DAY ALL DAY BY CLIENTS, STAFF & DELIVERY OF GOODS. GOODS DELIVERIES ARE IN BIG TRUCKS. THIS CYCLE LANE IS A HEALTH & SAFETY DISASTER & IT'S NOT A MATTER OF IF BUT WHEN THERE WILL BE A MAJOR FATALITY. ONCE CYCLISTS GET INTO THESE CYCLE LANES THEY THINK THEY ARE BULLET PROOF & ALL TRAFFIC ARE GOING TO STOP FOR THEM. THEY SWITCH OFF HEAD DOWN & ASIDE. IF THERE IS A FATALITY THEN I HOLD THE COUNCIL FULLY RESPONSIBLE FOR IT.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

BECAUSE YOU ARE NOW RUINING PEOPLE'S BUSINESSES
& LIVELY HOODS WITH ZERO CARE, THE COUNCIL
HAVEN'T EVEN DONE A R&D ON HOW MANY BIKES
GO UP TO NGAIO BETWEEN 7AM & 4PM. I CAN
TELL YOU THAT YOU ARE LUCKY TO SEE 4 BIKES
A DAY!

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

See attached. NO ONE WILL TAKE A BLIND BIT OF
DIFFERENCE TO THIS. ANOTHER WASTE OF MONEY.

Your relationship with the area

What is your main relationship to the area? Please tick one:

- ☒ I live in the area
- ☒ I work in the area
- ☒ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? Please tick one:

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very Important ☐ Important ☐ Moderate importance ☐ Low importance ☒ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

771

114171791273

NAME: David Whitaker	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

221110%20Ngaio%20Connections%20project%20-%20David%20Whitaker%20submission%20.docx

Submission on proposed changes affecting Cameron Street and Kaiwharawhara Road as part of the Ngaio Connections project

Submitter: David Whitaker, 1 Sargeson Way, Kaiwharawhara, Wellington 6035.

Email [REDACTED]

10 November 2022

General

I walk my neighborhood several times a day, usually with my dog, along Cameron Road, often up and down the Bridle Track, and sometimes down to Kaiwharawhara Road, over to/from the bus stops on Hutt Road, and occasionally I take long walks along Hutt Road, in either direction. I occasionally walk to the Immigrant's Son coffee shop or the Kaiwhara Café across the street, on Kaiwharawhara Road. I either walk to these or park on the road, if on my way to or from somewhere else. I often take public transport, when practical, and my family own bicycles (not e-bikes). But I find it impractical to take the bus or ride a bike to the grocery store (with us having no grocer nearby), and I also find it impractical to use buses or bikes when I have multiple stops to make, or others to pick up (such as my elderly mother-in-law, across town), or a distant destination (like the hospital, a law firm, the cinema, a bank). I don't cycle at night, at all. I enjoy walking and cycling for fun, but generally avoid cycling in town or in my neighborhood, because of the steep hills, extreme winds, concerns about my bike getting stolen if/when I stop. I am on the verge of retirement and am healthy, but am not getting any younger, nor do I claim to be fit enough to cycle across town or up/down the steep hills in my area.

Our home is located on Sargeson Way, next to the Bridle Track, which allows me to regularly and directly observe the behaviours of cyclists, pedestrians and drivers in the area.

Accordingly, I think I'm in very good position to contribute constructively to the conversation regarding the cycling-friendly and pedestrian-friendly changes, and parking changes, proposed or sought by the Council for Cameron Street, Kaiwharawhara Road and the Bridle Track.

I support the development of cycle lanes and pedestrian paths, and improved safety for them, and changes to the transport infrastructure where they make sense, and where they do not make travel by car impractical or unnecessarily inconvenient or unnecessarily disrupt or harm businesses and the affected communities. I am supportive of the national goal to achieve net-zero emissions, but I am also mindful that the same government that set that goal is also offering substantial monetary subsidies (the "Clean Car Discount") today, to encourage citizens to buy electric vehicles in order to help us reach the net-zero goal. We have a hybrid vehicle and are generally able to travel locally on an electric charge. These are valid forms of transportation, too, and they require parking spaces. (Many more charging stations would be welcome, as well.)

Within these considerations, it is important to find the right balance between the interests of cyclists, public transport, businesses, commercial and private vehicle owners, and residents of all ages and physical conditions. Notwithstanding the movement toward more climate-friendly transport solutions, it is simply not practical for a vibrant, modern country, its tax base and citizenry to thrive while traveling exclusively by public transport and cycling. The solutions chosen by the Council must consider the long-term costs and benefits and be proportionate to the needs of all users and not just a small minority, whether cycling enthusiasts or others.

The following table sets out my comments on the key changes proposed for the Cameron Street and Kaiwharawhara Road area.

Proposed	Comments
<p>Cameron Street uphill cycle shoulder:</p> <ul style="list-style-type: none"> • “Multi Criteria Analysis” dated 23/6/22 describes preferred option as “buffered cycle lane”. • In response to a request for clarification Jonathan Kennett confirmed that the Council is going to work with Waka Kotahi and trial a type of cycle lane that is often used in Europe, and plan to call it a “cycle shoulder” which would be about 1.0m wide with broken white line and green blocks. The intention is that cyclists know they should keep left going uphill, and that people driving know to look out for cyclists but can carefully pull over into the cycle shoulder if they need to allow a vehicle coming downhill to pass. 	<p>I note the “Project details” link has a picture which shows what was described by Jonathan to Rod Crone, but without any dimensions.</p> <p>In response to a request for clarification I now understand that the cycle shoulder will likely be 0.75m wide.</p> <p>Although discussions with several cyclists suggests no cycle lane/shoulder is necessary, I am comfortable with a 0.75m wide cycle shoulder.</p>
<p>Cameron Street uphill cycle shoulder - Vegetation to be cut back up to 2.2m high to improve visibility:</p> <ul style="list-style-type: none"> • “Cameron Street, General Arrangement Plan, Sheet 2” indicates that “Vegetation to be cut back up to 2.2m high to improve visibility”. • In response to a request for clarification in respect of how far the vegetation will be trimmed (kerb line or behind kerb line) Jonathan Kennett confirmed on 4/11/22 that the detail will be decided following advice from horticulture team, but happy to hear what residents would like. He imagines WCC will want to trim 0.5-1.0m from kerb line but will need to ensure it does not damage the shrubs. 	<p>I note that the vegetation was trimmed on Friday 4/11/22 to the kerb line.</p> <p>While some earlier suggested that the vegetation should be cut back to improve visibility, they were thinking that it should be cut back behind the kerb line sufficiently to allow for re-growth before re-trimming once vegetation encroaches on the road.</p> <p>I suggest vegetation should be trimmed to around 0.2-0.3m behind the kerb, and at a frequency to ensure it does not encroach on the road below 2.2m.</p>
<p>Proposal for cycle shoulder and downhill sharrows painted on Cameron Street</p>	<p>I am comfortable with painted sharrows on the uphill cycle shoulder.</p> <p>However, I do not agree with painted sharrows on the downhill section as they are unnecessary and will make it unsafe for cyclists when it is wet.</p>
<p>No Stopping Restriction (broken yellow lines) around the three corners between Cameron Street and the Bridle Track</p>	<p>I agree with the proposal to establish a No Stopping Restriction using broken yellow lines on the three corners between Cameron Street and the Bridle Track.</p>
<p>New Give Ways at bottom of Fore Street, Marsh Way, and intersection with Cameron Street</p>	<p>I am comfortable with the proposal for three new Give Ways to improve safety.</p>
<p>Signs directing cyclists to cross private property to access the Bridle Track from Cameron Street</p>	<p>This needs to be resolved.</p> <p>As the Council has reminded residents on multiple occasions, the section of road between Cameron</p>

Proposed	Comments
	<p>Street and the Bridle Track is private property, with multiple residents having private easement rights. As a Sargeson Way resident, I do not consent to having my access rights compromised or taken from me and my family.</p> <p>If the Council wants to direct cyclists to cross the private property, then it needs to find a solution acceptable to residents who have a direct interest in the private property and the associated easements for the purposes of right of way and services.</p>
<p>Bridle Track:</p>	<p>Anybody who walks up or down the Bridle Track and encounters cyclists will know of the dangers.</p> <p>With the Council promoting greater use of the Bridle Track by cyclists, it must consider and implement improvements in the surface (widening, additional fencing, surface cleaning and maintenance) and signage to make the track safer for pedestrians and cyclists.</p> <p>Often when walking the Bridle Track, I have been 'spooked' by cyclists without any warning, often going too fast. My wife encountered a cyclist recently who had forgotten to retract the metal bar that's intended to maintain distance between cars and bikes. That could seriously injure someone – or worse, if a young child on a walk with family. I also walk our dog on the Bridle Track regularly, and cannot always control which side of the narrow path the dog is going to be on at any moment. With cyclists speeding around blind turns, this is a major safety concern for all involved.</p> <p>It is only a matter of time before there is a serious incident involving a cyclist or cyclist hitting a pedestrian. Increasing volume and speed of movement on the track will only make that more dangerous, unless it's done with great care for ALL concerned. It's also worrisome to contemplate cyclists -- in large numbers -- exiting the Bridle Track onto Sargeson at high speed, with vulnerable pedestrians, cyclists and children, as well as cars, moving about the area.</p>
<p>Cameron Street parking restrictions:</p> <ul style="list-style-type: none"> • Changes to parking restrictions on the downhill side resulting in a combination of P60, P120 and P24hr carparks • "Proposed roll out of changes" includes the words "39x P24-hr spaces except residents". In response to a request for clarification Jonathan 	<p>In relation to the 39x P24hr carparks we agree residents do not want a residential parking scheme, although we did suggest a "P24hr except residents" restriction may be useful (consistent with streets near the airport). In any event, I am comfortable with a P24hr restriction without an exception for residents on the basis we can call the Council and</p>

Proposed	Comments
<p>confirmed that the “except residents” is an error and the website will be updated. If residents get annoyed with long-stay non-resident parking they can call WCC and get the cars ticketed and towed, but if no one calls WCC it is unlikely parking wardens will go to Cameron Street. Jonathan noted that residents did not want a residential parking scheme.</p> <ul style="list-style-type: none"> • “Changes in response to stakeholder feedback” includes a bullet point “Removing two carparks at the bottom of Cameron Street to make it safer for everyone” 	<p>get cars ticketed and towed if we get annoyed with long stay parking by non-residents.</p> <p>Removing two carparks at the bottom of Cameron Street is not identified on the “Cameron Street, General Arrangement Plan, Sheet 2”. I am strongly of the view that the P60 carparks (15m of carparking space, 3x carparks?) at the bottom of Cameron Street on the downhill section between the entrance to Te Rau Ora (2 Cameron Street) and the existing broken yellow lines connecting with Kaiwharawhara Road must be removed and converted to a ‘No Stopping Restriction’ with an extension of the broken yellow lines. This section of road is a hazard for cyclists and vehicles alike as downhill vehicles are forced over the centre line to the opposite side of the road causing many near misses.</p> <p>Finally, I note the parking analysis for Cameron Street has included private rights of way (Curnow Street, Sargeson Way, Marsh Way) and suggest they provide 41x unrestricted parking spaces for non-residents. The rights of way are private property and only property owners (or guests with their approval) may park in these rights of way.</p>
<p>Cameron Street change 50 km/hr to 30km/hr for safer speeds on Cameron Street</p>	<p>I agree this change makes sense.</p>
<p>Kaiwharawhara Road uphill cycle lane proposal:</p> <ul style="list-style-type: none"> • Stage 1 (2023) - create a clearway 4pm-7pm in 2023 • Stage 2 (2024) - extend clearway times to 2pm-9pm • Stage 3 (2025) – replacing clearways with a separated cycle lane 	<p>I support the proposal for a shared cyclist/bus clearway from 4pm to 7pm.</p> <p>My observation is that most cyclists (probably over 95%) are commuter cyclists who bike to work 7am-9am in the morning and home again 4pm-7pm in the evening, while buses using Kaiwharawhara Road are relatively few and rarely affected by peak time traffic. Accordingly, at this point only a shared cyclist/bus clearway from 4pm-7pm makes sense. I do not see these usage patterns changing, certainly don’t think it’s valid to assume they will.</p> <p>Accordingly, I do not support the phasing in of Stage 2 or Stage 3. This would seem to be a death knell for much of our small Kaiwharawhara business district, and an extreme discouragement for any new businesses I may desire to have nearby (rather than having to travel across town to reach!) If a compelling need arises, as determined by stakeholders here in coordination with the Council, then we should address that then and have opportunity for comment then. Otherwise, frankly, it would seem a fait accompli that a substantial majority of roadside parking will be eliminated by</p>

Proposed	Comments
	<p>2025, based on suppositions rather than consensus and evidence.</p> <p>Accordingly, it would be inappropriate for Council to now plan a default to Stages 2 and/or 3.</p>

Feedback

772

114161146354

NAME: Matt	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Love it, the safer we make it for people who bike to/from work the better, particularly for those who would like to but find the Ngaio Gorge intimidating sharing with cars.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

773

114179292227

NAME: Emily	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

it will be harder on kenya streeet for older people to cross because if there on the other side of the road they have to park on the side there not on and the could be slow crossing also there will be less parks

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

774

114179309999

NAME: Elena	SUBURB: Ngaio	ON BEHALF OF: Ngaio School	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

It is fine but I don't want shops to close because there is not many car parks in the area because the bike lane has taken it over.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

775

114181575781

NAME: Zong Chen	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This enables a large catchment area of people living around Ngaio to go into the city by bike, which would reduce emissions and traffic congestion. I've heard many cyclists around town say they use Ngaio gorge road to go uphill.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

776

114173697115

NAME: Ella Borrie	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I strongly support giving more transport options and prioritising public transport and active transport. I would support doing the work in one go to realise the benefits earlier.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

777

114178051083

NAME: Elliot	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Generally supportive of making it easier for cycling - however, have you considered just a wide shared pathway because the nature of the hill means there are very few pedestrians. I am concerned about the width of the road for buses. The existing road feels quite tight with buses navigating the corners.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

778

114167972880

NAME: Emma	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

I support increasing accessibility however I do not believe that implementing this change on Cameron street is necessary nor a good use of this investment. This street is relatively infrequently used, Cameron street residents living on what is a categorically a quiet and unused route will experience significant disruptions and accessibility issues of their own, particularly with severely reduced parking that we must pay for, noting that you can only park on one side of the street. This would be a significant disruption for a relatively average impact of this investment on this particular street.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

779

114179685251

NAME: Emma	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

"Install a new pedestrian crossing outside SWET and Immigrants Son, resulting in removal of 3 carparks." I oppose this as I do not think it will be beneficial for businesses involved or for pedestrians who use the lights at the end of the road to cross instead. I think it will further disrupt the traffic flow also.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

780

114179294179

NAME: Felix	SUBURB: Ngaio	ON BEHALF OF: on behalf of a primary or secondary school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

It is decent. There are some good changes and some bad changes in my opinion.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

781

114179292031

NAME: Fewixe	SUBURB: Ngaio	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I think in the future, biking up up and down Ngaio Gorge will be helpful going to school/work.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

782

114168774755

NAME: Phillip Bolton	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Waste of resources and money

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

783

114181698892

NAME: Finn	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This route will hopefully increase the number of cyclists and make this a safer way to travel.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

784

114182554779

NAME: Fleur Rohleder	SUBURB:	ON BEHALF OF: Fire and Emergency New Zealand	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?
These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Submission%20-%20Wellington%20City%20Council%20-%20Ngaio%20Connection%20Project.pdf

Submission on Ngaio Connection Project

To: Wellington City Council

Submission on: Ngaio Connection Transport Project

Name of Submitter: Fire and Emergency New Zealand

This is a submission on the Wellington City Council Ngaio Connection project. The specific elements of the project that Fire and Emergency's submission relates to is the provision for timely and adequate emergency access through the area.

Fire and Emergency's submission is:

In order for Fire and Emergency to achieve its principal objective which includes reducing the incidence of unwanted fire and the associated risk to life and property, protecting and preserving life, and preventing or limiting injury, damage to property land and the environment, Fire and Emergency requires adequate access to property and structures throughout the area to ensure they can respond in emergencies. In addition to fire safety and response, Fire and Emergency's functions under the Fire and Emergency New Zealand Act 2017 Act (the Act) also include responding to motor vehicle accidents, medical emergencies, weather events and disasters, other rescues and hazardous substances incidents. As such, Fire and Emergency has an interest in transport infrastructure projects and street upgrades to ensure that, where necessary, appropriate consideration is given to operational firefighting requirements.

Fire and Emergency consider it is vital for the health, safety and wellbeing of communities that the needs of emergency services are taken into account as transport network upgrades / changes are developed and implemented.

Adequate access to both the source of a fire (or other emergency) and a firefighting water supply is essential to the efficient operation of Fire and Emergency. The requirements for firefighting access are set out in the New Zealand Fire Service Firefighting Water Supplies Code of Practice SNZ PAS 4509:2008 (Code of Practice)¹ and further detailed in Fire and Emergency's 'Designer's guide' to firefighting operations Emergency vehicle access' (December 2021). This document is attached to this letter as **Attachment 1**.

In general, the key access requirements include specific roading and access widths, and surface and gradients to support the operational requirements of Fire and Emergency appliances. This includes, but is not limited to the following:

- The minimum roading and carriageway widths should not be less than 4m for general appliance access. This width is required for firefighters to efficiently work around the fire appliance to access hoses and pumps.
- A clear passageway / vehicle crossing of no less than 3.5m wide should be provided as site entrances, internal entrances and between buildings.
- The maximum negotiable gradient is 1:5, but in general the roading gradient should not exceed 16%.

¹ The New Zealand Fire Service Firefighting Water Supplies Code of Practice SNZ PAS 4509:2008 can be found here:

<https://fireandemergency.nz/assets/Documents/Files/N5a-SNZPAS-4509-2008-NZFS-Firefighting-water-supplies-Code-of-practice.pdf>

- The height clearance along accessways (for example trees, hanging cables and eaves) must exceed 4m.

Importantly, Fire and Emergency must be able to operate pumping appliances from a hard standing. Often, this can be done from the public road, and this is how Fire and Emergency prefers to operate where possible. Pumping appliances are vehicles used to pump water for firefighting (refer Appendix A of the Fire and Emergency's 'Designers' guide). They carry a relatively small amount of water (1,350–2,000 litres) and have a limited length of hose. Accordingly, Fire and Emergency must have access to a water supply and must also be able to base operations near the building, so firefighters can reach the fire with water. Refer Appendix B of Fire and Emergency's 'Designer's guide' that sets out compliant site layout examples. Aerial appliances may be required to respond to multi-storey and / or higher-density developments. These appliances have more complex access requirements outlined in Attachment 1.

Fire and Emergency recognises the significant advantages of prioritisation of street space for pedestrians and cyclists from Hutt Road up Kaiwharawhara Road and Ngaio Gorge Road to Crofton Road, and up Cameron Street to the Kaiwharawhara Bridle Path. However, in prioritising street space for pedestrians and cyclists, care must be given to avoid unintended consequences on Fire and Emergency's ability to respond to emergency events. Prior engagement with Fire and Emergency and other emergency service providers is therefore important before such measures are finalised and implemented.

In some cases, the provision of cycleways on main arterial roads, lowering of speed limits, and incorporation of speed controls as part of traffic-calming programmes may inadvertently block or significantly slow down a route required by Fire and Emergency. If an emergency appliance needs to park on a road, this can mean the street / road is blocked to all other traffic including other emergency vehicles that may need to get past.

Fire and Emergency seeks the following from the consent authority:

- That Wellington City Council engage directly with the Wellington Fire and Emergency team to ensure that the changes proposed in the Ngaio Connection project do not adversely impact Fire and Emergency's ability to respond to emergency events.
- More generally, Fire and Emergency request that on similar, future projects that include the improvement of active transport connections and implementation of traffic calming measures, Wellington City Council considers the impact on emergency service vehicles and consults with the relevant authorities.

Fire and Emergency welcomes any questions on this submission and looks forward to continuing to work with Wellington City Council to provide for the safety of Wellington's communities.



Fleur Rohleder
Planner, Beca Ltd.
on behalf of
Fire and Emergency New Zealand

Date: 21/11/2022

Email: [REDACTED]

Address: PO Box 3942 Wellington

Telephone: [REDACTED]

Attachment 1

Designers' guide to firefighting operations

Emergency vehicle access

F5-02 GD



Document Title: Emergency vehicle access

Published: 10 December 2021

Document review date: 10 December 2023

Status of this document

This document is issued by Fire and Emergency New Zealand.

Recommendations for change

The document, its content and specific processes are not to be altered except through Fire and Emergency New Zealand document management processes.

Requests or recommendations for changes to this material should be sent to National Manager Response Capability.

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1. Context

Scope	We need to be able to reach your building with our different vehicles in a fire or other emergency. This chapter helps you understand the types of vehicles we use, and how you can provide access for them on your site.
Who this chapter is for	<p>This chapter outlines our position on appropriate considerations for building owners, building designers and other building practitioners, on emergency vehicle access to sites, both completed and under construction/refurbishment.</p> <p>It may also provide useful guidance for anyone undertaking planning of any kind that needs to consider emergency vehicle access.</p>
What is not included in this chapter	<p>This chapter is a guide to provide advice to the building industry on Fire and Emergency's operations and recommendations in relation to emergency vehicle access – it does not replace any mandatory/statutory requirements.</p> <p>We recommend you read it alongside other chapters in the guide. This is not an exhaustive guide to Fire and Emergency operations, but an overview of the relevant expectations building industry stakeholders can have of our operations.</p>
Legislative framework	<p>We aim to reduce the risk to both firefighters and building occupants through encouraging appropriate building design which allows us to achieve our statutory objective (under the Fire and Emergency New Zealand Act 2017) to reduce the incidence of unwanted fire and the associated risk to life and property. Our functions include responding to and suppressing fires and attending to other types of emergencies that may occur in a building.</p> <p>Read this guide alongside the:</p> <ul style="list-style-type: none"> • mandatory requirements of the New Zealand Building Code (Building Code) • requirements of New Zealand Standards (Standards), and • Building Act 2004. <p>This guide does not replace any part of the Building Code or Standards or other mandatory building requirements.</p> <p>We note that the Building Code Fire Safety C – Protection from fire clauses C1– C6 define the Building Code performance requirements of the Building Act 2004. Clause C5 is the performance requirement on 'Access and Safety for Firefighting Operations'.</p>

2. Definitions

The following definitions apply for the purposes of this chapter. Defined terms used throughout this document are consistent with the Building Act 2004, Building Code and Acceptable Solutions C/AS2.

Address point	This point is part of the data set administered by Land Information New Zealand, (LINZ). It is the address (point) where the building is commonly known to be located. It can be either a single point or a range of individual points as described on the LINZ data set.
Aerial device	Encompasses all the types of Fire and Emergency aerial components (turntable ladder, elevating platforms, elevating monitors, baskets, cages and booms).

Aerial vehicle	A specialised emergency vehicle that has an aerial device that hydraulically rises to suppress fire and/or effect rescue as well as support other operations.
Allowable bearing pressure	The calculated pressure required to counter compression forces exerted by dead loads (i.e. the minimum strength required to maintain stability under a weight load).
Appliance	An emergency vehicle that provides capability to Fire and Emergency's mandated functions.
Attendance point	<p>The place where the first attending Fire and Emergency pumping vehicle will stop and set up. There is only one attendance point, usually, at the building's primary entry point. Firefighters may be deployed to other firefighter access points from here.</p> <p>A full description of the attendance point can be found within F5-02 GD FFO Emergency vehicle access.</p>
Breathing apparatus (BA)	A device firefighters wear to provide breathable air in an atmosphere that is immediately dangerous to life or health. Also known as self-contained breathing apparatus (SCBA) or compressed air breathing apparatus (CABA).
Building hydrant system	Fixed water main pipe system, normally already charged with water and supplemented by Fire and Emergency pumps. This should not be confused with an in-ground hydrant connected to the town mains.
Canopy	Projecting hood supported on brackets, corbels or columns over a door, window or niche.
Carriageway	The driveable portion of a road (which may or may not include a sealed top surface layer).
Collapse zone	<p>The collapse zone is an area around the building measured as 1.5 times the height of the structure. This is the area which would be considered dangerous in the event of an outward failure of a facade element.</p> <p>In this document, the term 'collapse zone' only applies to pre-cast concrete panel (tilt-slab) and unreinforced masonry type construction.</p> <p>Use a pragmatic approach where practicable when designing, and when in doubt, consult Fire and Emergency.</p>
Fire engineering brief (FEB)	A formal process outlined in the International Fire Engineering Guidelines for all stakeholders to define and agree on the basis and scope of work for fire engineering analysis.
Firefighter access point	<p>The place where firefighters gain access to a building. This must comply with the New Zealand Building Code Clause C5.6:</p> <p>Buildings must be designed and constructed in a manner that will allow firefighters, taking into account the firefighters' personal protective equipment and standard training, to:</p> <ul style="list-style-type: none"> (a) reach the floor of fire origin, (b) search the general area of fire origin, and (c) protect their means of egress.

**Fire and
Emergency
vehicular access**

Vehicular access provided to Fire and Emergency vehicles should be consistent with Acceptable Solution C/AS2 Part 6 (relating to firefighting).

Buildings must be provided with access that allows appliances to reach a position that makes it convenient for firefighters to get into the building and access the inlets to fire sprinkler systems or building fire hydrant systems, where these are installed.

Occupants of risk group SI are more likely to require rescue by Fire and Emergency. An additional recommendation for this risk group is to allow access for the larger size of aerial vehicles to get as close to buildings as possible with space to 'jack' the vehicle.

If a building has a large footprint (which is most likely to occur for a single-storey building such as a warehouse) and is not protected with fire sprinkler systems, access to two sides of the building is required. This gives Fire and Emergency the ability to access the building in a number of places and means that their travel within the building is minimised to reach any fire source.

In addition, for the health and safety of our personnel, this access:

- should not involve a canopy, or other part of a structure to drive or park under
- should be located outside a horizontal collapse zone requirement of 1.5 times the height of a portal frame building
- should be within 135 metres of a firefighting water supply.

Where access meets these recommendations above, and is acceptable to Fire and Emergency, the 75 m hose run may be measured from this hard-standing point.

**Hard-standing
area (for Fire
and Emergency
vehicles)**

A hard (roading) surface capable of withstanding the fully laden weight of a fire appliance from which fire operations for a structure are conducted. A hardstanding should be big enough for the fire appliance to enter, exit and manoeuvre and for firefighters to move around it to connect hose and safely access equipment. In most cases, the hardstanding will be the main road if the structure is close to it.

A full description of the hardstanding area can be found in this chapter (F5-02 GD FFO Emergency vehicle access).

Jacks

External outriggers and jacks fitted to aerial vehicles that extend to stabilise the vehicle when its centre of gravity shifts during the operation of the aerial device.

Overhang

The portion of a vehicle's body that extends forwards past the front wheels or backwards past the rear wheels. It relates to body swing, which is where a set of wheels when turning acts as a pivot point and the bodywork swings past that point. The longer the overhang, the greater the body swing.

3. Our operations

Vehicle types	<p>We use several different types of vehicles (also referred to as appliances).</p> <p>Each type of vehicle carries a different combination of equipment and has a specific function at an emergency incident. Most of our vehicles have a specially built body fitted on a commercial vehicle chassis, normally a truck.</p> <p>Vehicle types include pumping appliances, aerial appliances and a range of specialist function vehicles such as mobile command units, logistics and support vehicles, and water tankers.</p>
Timeliness	<p>To fight a fire effectively, save lives and limit damage to property, we need to respond quickly and start applying water while the fire is still small. While automatic detection systems and good information help us respond quickly, good access is also vital.</p> <p>This highlights the importance of having designated hard-standings with all the provisions for a fast fire attack proximate to as many parts of a building as possible, that are free from obstructions.</p>

4. Challenges

4.1. Fire and Emergency vehicles

Capability of vehicles	<p>Pumping appliances are vehicles used to pump water for firefighting. They carry a relatively small amount of water (1,350–2,000 litres) and a limited length of hose. This is why we must have access to a water supply and must also be able to base our operations near the building, so firefighters can reach the fire with water. Often, this can be done from the public road, and this is how we prefer to operate where possible. However, for large sites, sites with multiple buildings, or sites with large set-backs, our vehicles may have to operate from within your site, which is less favoured.</p> <p>Aerial appliances are larger and heavier than our other vehicles and may be on a two-, three, or even four-axle heavy vehicle chassis. Aerial appliances have limited reach and need to get close to buildings or structures to operate effectively. We will normally try to reverse these vehicles into position beside a building and, where possible, operate from building corners.</p> <p>For these reasons, we recommend that you provide access and working space for Fire and Emergency vehicles on your site.</p>
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Vehicle dimensions	Each vehicle type has different dimensions. Table 1 below shows maximum vehicle dimensions of Fire and Emergency's current fleet of vehicles.
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Table 1 – Maximum parameters for Fire and Emergency vehicles

Dimension	Maximum dimensions
Gross vehicle mass	25 t
Maximum overall length	12.6 m
Maximum overall width	2.55 m (6.5 m when stabilisers are deployed)
Required free height	4 m

4.2. Access requirements

Carriageway widths

Carriageways should be wide enough to allow our vehicles to get through them easily and to allow us to carry out emergency operations. This means that when our vehicle is parked, we can easily open and exit the doors, access equipment from its compartments and safely connect the hose to the pump.



Figure 1 – A pumping appliance showing width required for hose

To accommodate a Fire and Emergency vehicle, carriageways should have a minimum width of 4 m. This can be reduced to a minimum width of 3.5 m at entrances, provided tight turns are not required (see Figure 2 below).

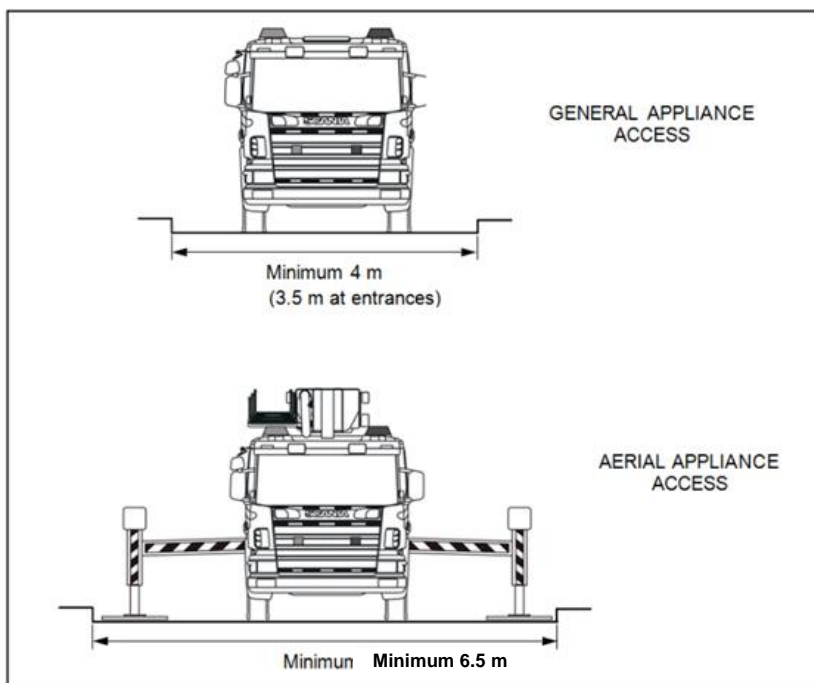


Figure 2 – Minimum carriageway widths along straight sections

Curved carriageway sections should allow for expected vehicle body swing. The minimum distance between the inner and outer arcs should be not less than 5.0 m for pumping vehicles and 7.3 m for aerial vehicles (see Figure 3).

For pumping vehicle access, the minimum inner radius should be 6.3 m and the outer radius 11.3 m. For aerial vehicle access, the minimum inner radius should be 5.2 m and the outer radius 12.5 m (see Figure 3).

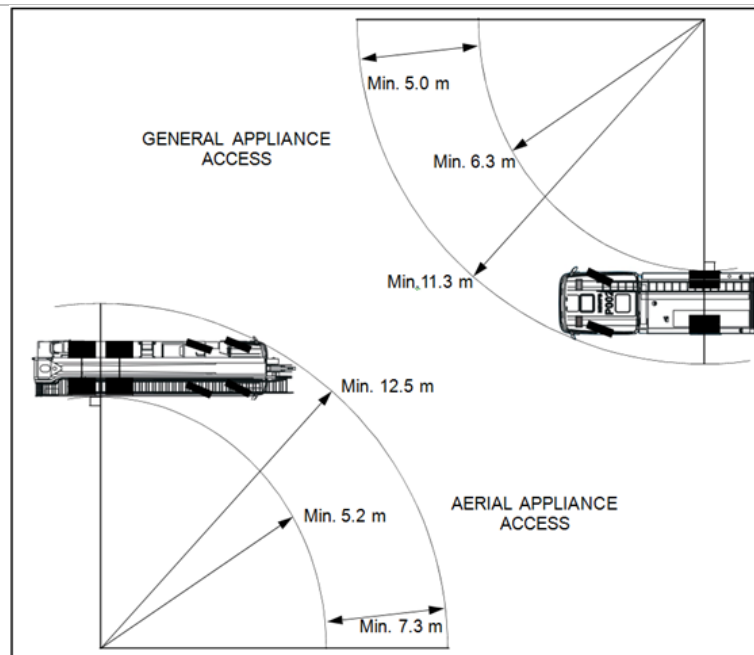


Figure 3 – Minimum carriageway widths – curved sections

The radius dimensions above are for wall-to-wall clearance from body overhang, and do not represent the vehicle's wheel tracks.



Figure 4 – Showing long rear overhang



Figure 5 – Showing long front overhang

Kerb dimensions

Kerbs built along the edges of a carriageway should be no higher than 250 mm and should be free of vertical obstructions at least 300 mm back from the kerb face to allow clearance for front and rear body overhang.

This means that if absolutely necessary, we can mount the kerb with our vehicles, although this is a last resort due to the additional hazards.

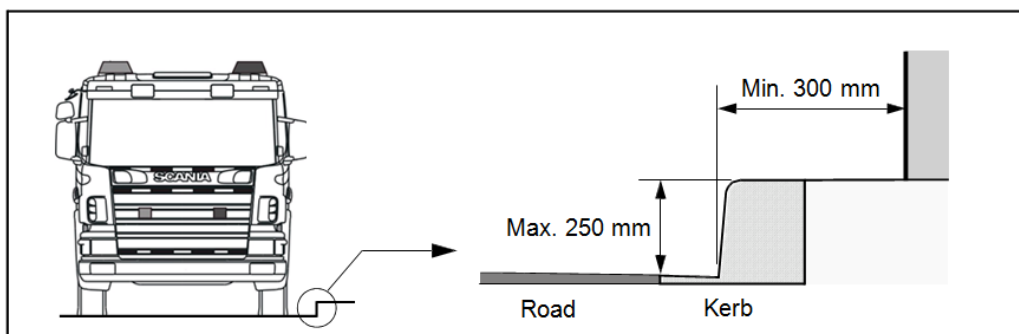


Figure 6 – Carriageway kerb clearance dimensions

Turning areas

Any carriageway with a dead end needs a turnaround area so that our vehicles don't have to do multi-point turns to turn around. This is so we can move our vehicles quickly in an emergency to protect them.

Fire and Emergency vehicles need to be able to turn a full 360° within a 25 m circle (wall-to-wall clearance) to meet Waka Kotahi NZ Transport Agency requirements. The minimum turning radius of turnaround areas should be no less than 11.3 m for pumping vehicles and 12.5 m for aerial vehicles (see Figure 3).

The Waka Kotahi NZ Transport Agency's Road and traffic guidelines for New Zealand on-road tracking curves for heavy motor vehicles (RTS 18) as indicated in Table 1, should be considered. Table 2 below summarises the tracking curves and their radii for design vehicles.

Table 2 – Turn radii and tracking curve sheet numbers for the design vehicles at various radii
(Source: <https://www.nzta.govt.nz/assets/resources/road-traffic-standards/docs/rts-18.pdf>)

Vehicle	Radius of turn				
	10 m	12.5 m	15 m	20 m	25 m
8 m rigid truck	1	2	3	4	5
11.5 m rigid truck		6	7	8	9
Semi-trailer*		10	11	12	13
Tour coach		14	15	16	17

Table 3 – Fire and Emergency fire vehicle types in relation to the Waka Kotahi NZ Transport Agency on-road tracking curves

Fire and Emergency fire vehicle type	Waka Kotahi on-road tracking curve
Pumping appliance	8 m medium rigid truck
Aerial appliance	12.6 m rigid truck

Change of level	The only acceptable means of providing access through a change of level is a ramp that meets the requirements set out below. Fire and Emergency vehicles are not designed to drive up or down steps.
Access ramps	Ramps should not delay vehicle response and should provide entry and exit clearances for Fire and Emergency vehicles.
Gradients for straight ramps	Fire and Emergency prefers a ramp gradient of 1:8 or less for straight ramps. The maximum straight ramp gradient our vehicles can negotiate is 1:5.
Gradients for curved ramps	Access ramps that follow a curved or circular profile in plan view should have a maximum gradient no greater than 1:10 (measured along the centre line). The vehicle chassis will twist and flex when driving up a curved ramp, so we need a lower gradient.
Change of ramp gradients	Access ramps should have a smooth transition between the main ramp gradient and entry and exit gradients. A minimum 4.0 m long 1:15 transition grade is best for both ramp approach and departure (see Figure 7 below).

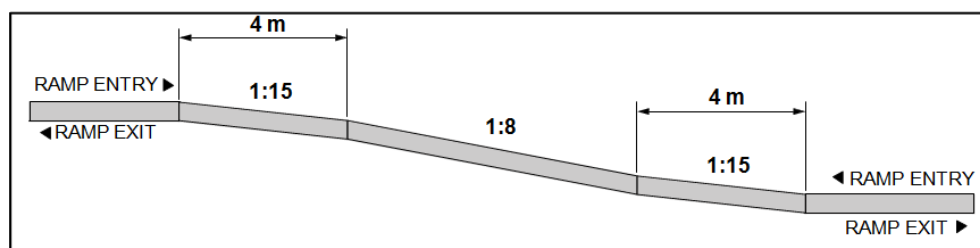


Figure 7 – Maximum access ramp gradients

Reduced gradient clearance

When a change of gradient includes a recessed threshold such as a gutter (e.g. for storm water drainage), the reduced approach and departure clearance should be allowed for in the design of the access way (see Figure 8).

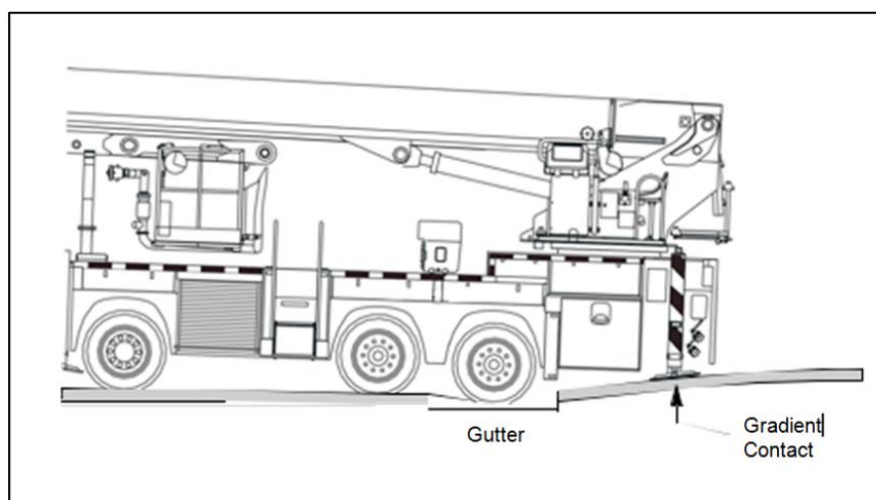


Figure 8 – Reduced gradient clearance due to gutter

When wheels go into a gutter, the body slants downwards, reducing the effective underbody clearance height at both the front and rear overhanging sections. The clearance is even smaller when the gutter is deeper and/or when the overhang is longer.

Building and structure

We need vehicle access routes to have an unobstructed clearance height of at least 4.0 m so that vehicles can pass through openings. This includes clearance from

clearance height

building construction, archways, gateways/doorways and overhanging structures (e.g. ducts, pipes, sprinklers, walkways, signs, structural beams, trees, hanging cables, etc.).

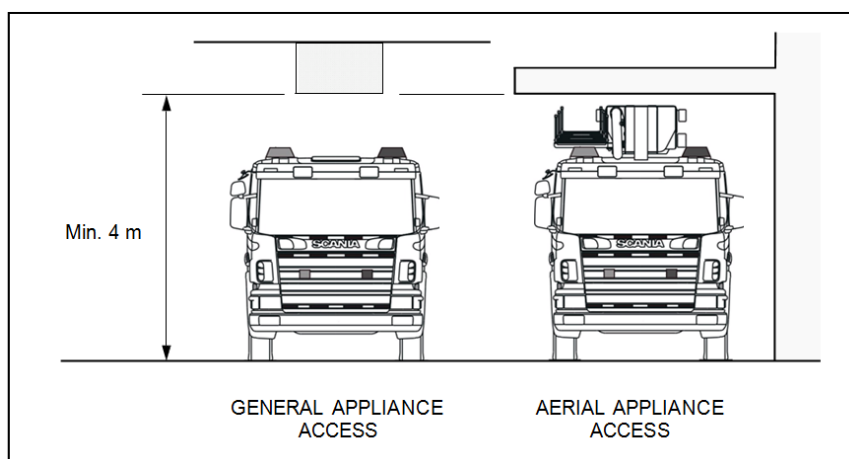


Figure 9 – Building and structure clearance heights

Note: Special considerations apply where there are both height restrictions and gradient changes. In some cases, height clearance will need to be more than 4.0 m so the vehicle can make the gradient change.

Ensuring clear access

We need clear access routes for our vehicles at all times.

Site managers should ensure that nothing blocks or partly blocks the carriageways for our vehicles. We need to be able to drive through access routes during all weather conditions. This means we need some form of hard-standing so our vehicles don't get bogged down. If a vehicle gets stuck, it creates two problems, we can't use it, and it may stop other vehicles getting through.

Perimeter security points (e.g. sliding/swinging gates, boom gates, bollards and vehicle security barriers) should not make it difficult for vehicles to gain access.



Figure 10 – Clear access available to a site

Site entrances, internal entrances and space between buildings should be at least 3.5 m wide and 4 m high.

The following common occurrences often make access difficult:

- Overhanging vegetation which restricts height clearances
- Overgrown vegetation which restricts width access and clearances

- Illegally parked vehicles in long driveways, narrow rights of way or halfway onto kerbs in small streets.

Contact us at designers.guide@fireandemergency.nz to discuss.

4.3. Vehicle weights (loads)

Static loads of vehicles

Carriageways need to be able to withstand the load of a Fire and Emergency vehicle, particularly if they are supported, elevated or reinforced by structural members (e.g. suspended floors, ramps, wharfs, aprons, etc.).

Figure 11 shows the vehicle loads exerted through the wheels that are used to determine forces acting through load-bearing structural members. Wheelbase distances between the front and back axles range from 3.7 to 5.5 m for pumping vehicles and 4.4 to 5.6 m for aerial vehicles. Designers should consider the distances between the wheels – both longitudinal and lateral – when calculating point loads for the wheels.

Note: Axle loads, such as those shown in Figure 11, are not always evenly distributed over all wheels.

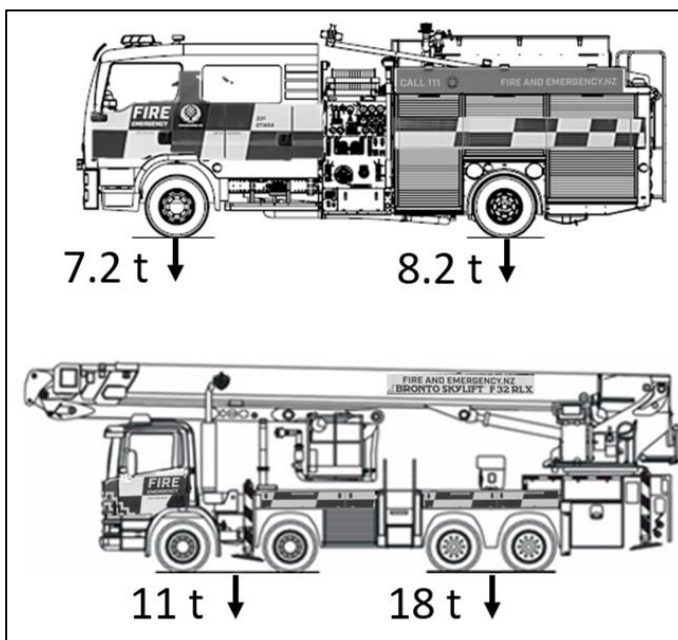


Figure 11 – Axle loads of vehicles

In general, access routes should be able to withstand a laden weight of up to 25 tonnes with an axle load of 8 tonnes or have a load-bearing capacity of no less than the public roadway serving the property, whichever is lower.

Roadway pavements designed for aerial vehicles must withstand a vehicle with multiple axles spaced at no less than 2.5 m centres and each carrying 8.2 tonnes.

The hardness of the carriageway surface should withstand static pressure of no more than 850 kPa from a vehicle's tyres.

Note: Pavements Fire and Emergency vehicles use for access should be designed according to Waka Kotahi NZ Transport Agency's HN-HO-72 traffic loading specifications, to meet the load-bearing requirements.

Dynamic loads (on aerial vehicles)

Aerial vehicles are fitted with stabilisers that prevent the vehicle from overbalancing when the aerial device is operating. Aerial vehicles will either have two stabilisers at the rear only, or more commonly, two front and two rear stabilisers (see Figure 12).

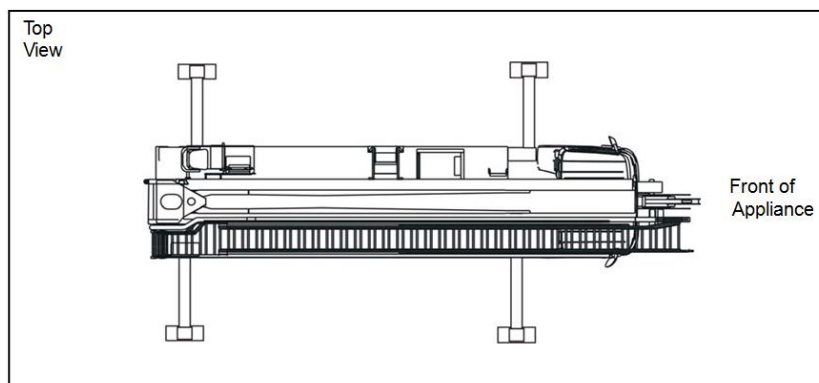


Figure 12 – General stabiliser arrangement on aerials

Extending and rotating the aerial device changes the vehicle's weight distribution and creates other forces, such as torsion moment forces. These exert dynamic forces through the stabiliser.

Note: The changing distribution of weight can cause up to 70 percent of the total vehicle weight to be borne by a single stabiliser.



Figure 13 – Highlights the space requirements for jacking stabilisers

The maximum dynamic loads and pressures exerted through a single stabiliser of the Bronto Skylift F44 RLX, with a fully loaded cage (500 kg), at maximum extension/outreach and under worst-case rotation angle, are:

- maximum stabiliser force: 200 kN
- maximum footplate pressure: 11 kg/cm² (1079 kPa)
- maximum bearing plate (block) pressure: 2.8 kg/cm² (274 kPa).

Consider the maximum exerted pressures above when calculating the minimum Allowable Bearing Pressure (ABP) for the carriageway or hard-standing area.

4.4. Site access and security features

Security features can delay site access

Many sites have security measures in place that restrict public access. These are to meet legal requirements for health and safety in the workplace and to keep the site and its staff secure.

However, enhanced security measures often delay firefighters when they investigate fire calls. Features such as security gates, high fences and bollards delay our vehicle access.

Security features can also translate to issues with physical access to buildings, including to locations where firefighters are required to interface with fire systems. Where enhanced security measures are present, this is likely to delay our investigation of the fire call.

There are solutions to overcome the issues presented by enhanced security and these include automatic unlocking or opening of security features upon a fire alarm activation. These may also have a time delay built into the system, so the site remains secure for longer, accounting for our response time. A master lock control switch could also be provided for our use in an area we can access such as a fire control centre (FCC).

Alternatively, where the building fire alarm is connected directly to Fire and Emergency, keys to the site may be provided to us.

Another option is a lockbox on site provided that information regarding its location and its access is provided to us ahead of time. On-site security staff, or contracted security staff who respond automatically in the event of a fire alarm activation, may also be able to provide access for us.

If you have any concerns about responding Fire and Emergency crews having timely access to a site, contact designers.guide@fireandemergency.nz to discuss options.

4.5. Vehicle hard-standing

Vehicle hard-standing requirements

A vehicle hard-standing is a designated area that can withstand the laden weight and associated loads of the Fire and Emergency vehicle and its crew and facilitate firefighting operations.

For our vehicles to work effectively, the hard-standing must be as close as possible to both the water supply and the structure to be protected. We encourage you to follow the guidance within this document. If you can't meet the criteria in this chapter, email designers.guide@fireandemergency.nz for help.

Under Clause C5.3 of the Building Code:

Buildings must be provided with access for fire vehicles to a hard-standing from which there is an unobstructed path to the building within 20 m of:

- (a) the firefighter access into the building, and
- (b) the inlets to automatic fire sprinkler system or fire hydrant systems, where these are installed).

This is to enable firefighter to get into the building and to move freely around our vehicles.

Under Clause C5.4 of the Building Code:

Access for fire vehicles in accordance with clause C5.3 must be provided to more than 1 side of firecells greater than 5,000m² in floor area that are not protected by automatic fire sprinkler system.

The hard-standing should:

- comply with Section 4.2 of this chapter regarding access requirements
- enclose a rectangle at least 4.0 m wide and 11 m long
- not have a gradient of more than 1:50
 - Stabilisers used on aerial vehicles limit hard-standing gradients. Aerial vehicles can only use their stabilisers and operate if the ground slope is within +/- 5°
- be outside the collapse zone (see 'Collapse zone' in the Definitions section for details)
- be in the open air and have no overhead obstructions along its entire area
- be within 135 m of a pressurised water supply, or within 6 m of an open water source, due to equipment limitations (supply hose)
 - This distance should not include any sharp angles
 - This distance should be measured taking into consideration obstructions such as buildings, fences, waterways and storage or parking areas. See [Appendix B](#) for examples.

Note:

- Hose runs can be measured from this point, provided all the requirements above are satisfied.
- The above hard-standing requirements do not apply to the following classified uses (as defined in Clause A1 of the Building Code):
 - backcountry huts
 - detached dwellings
 - within household units in multi-unit dwellings
 - outbuildings
 - ancillary buildings.

Attendance point

Our policy is to respond to a single attendance point. The attendance point is generally at the building's main entrance and is often (but not always) the same as the address point. This location should include the alarm panel, building hydrant/sprinkler inlets, a suitable firefighter access point, etc.

If there is a remote place within the building which cannot be reached by hose within 75 m of the attendance point, a common solution is to provide a building hydrant system. In certain situations, this is even mandated by prescriptive guidance (for example Acceptable Solution C/AS2, paragraph 2.2.1 and associated tables)).

Note: This attendance point should not be confused with a firefighter access point or vehicular hard-standing, which may be remote from the attendance point and provided with a building hydrant outlet. It may also provide a mimic fire alarm panel or other fire safety features.

The attendance point should also include all the requirements for a hard-standing area and meet clauses C5.3 and C5.7 of the Building Code.

When identifying an attendance point, factor in the following:

- Operational procedures do not allow firefighters to drive vehicles down narrow lanes, under canopies or through flood water
-

- It is our policy not to park a vehicle under a canopy, or within the collapse zone in certain circumstances (see 'Collapse zone' under Definitions)
- Location of and ease of access to fire alarm panel
- Location of and ease of access to the controls for fire safety systems
- Inlets for fire sprinkler and/or building hydrant (riser) systems.

See [Appendix B](#) for examples.

Safer siting areas

We often strategically place our vehicles at building corners, particularly our aerial vehicles.

This is because the corners are generally safer if the building collapses outwards, and we can usually use our aerials across two faces of the building providing for better coverage and observation.

5. Recommendations

Fire and Emergency recommended approach

We need you to consider how you could provide access for firefighting vehicles in the course of your work. Our requirements may differ case by case, basis and you should discuss any queries you have with us.

We recommend you consider the following points:

Consider the dimensions of our vehicles

- Access gates, driveways should meet the minimum dimensions outlined.
- Driving surfaces should be designed to support the weight of our vehicles.

Consider the manoeuvrability of our vehicles

- Dead ends and turning circles should meet the requirements discussed in this chapter.
- Straight ramps designs should take in into account our vehicles' needs, particularly at ramp entry and exit points.
- Curved ramps should be carefully considered in relation to our vehicles' weights and clearances including vehicle overhangs.
- Recesses such as storm water drains should be carefully placed to consider our vehicle movements.

Consider hard-standing recommendations

- Hard-standings should be at the correct distance from building, firefighting systems/inlets and firefighting water supplies.
- Consider vehicle loading requirements for attendance and hard-standing points.
- Allow working space for firefighters in and around our vehicles. Consider:
 - doors opening
 - firefighters exiting vehicles with PPE and BA on
 - whether firefighters can access important equipment around our vehicles, such as ladders and hoses.
- Allow working space for the deployment of stabilisers on our aerial vehicles.

Consider how we will access the site in an emergency

- Consider how any site security could affect our access, particularly outside business hours.

- Keep access routes always clear, particularly from vegetation, parked cars and temporary structures, etc.
 - Speed is critical – the sooner we start firefighting operations, the more likely we are to limit the consequences.
-

5.1. Completing the firefighting facilities checklist

Completing the checklist

When completing F5 SC Part C: 2 Access to site and 3 Access to building of the firefighting facilities checklist (FFFC), you should state what access you have given us to key facilities and the attendance point. This will allow us to understand the proposed layout and ensure that this access meets our operational needs for firefighting.

Remember that facilities are put in place for our use in emergency situations and the location of those facilities should be decided in consultation with us.

6. Related information

6.1. Designers' guide to firefighting operations

- F5 01 GD FFO Introduction
- F5-02 GD FFO Emergency vehicles access
- F5-03 GD FFO Radio communications
- F5-04 GD FFO Fire alarm panels
- F5-05 GD FFO Building hydrant systems
- F5-06 GD FFO Automatic sprinkler systems
- F5-07 GD FFO Stairs in buildings
- F5-08 GD FFO Lifts
- F5-09 GD FFO Fire Control Centres
- F5-10 GD FFO Evacuation and rescues
- F5-11 GD FFO Water supplies
- F5-12 GD FFO Construction, refurbishment and demolition sites
- F5-13 GD FFO Multi-tiered vehicle stacking buildings
- F5-14 GD FFO Firefighting shafts in taller buildings

6.2. Legislation

- [Fire and Emergency New Zealand Act 2017](#)
- [Building Act 2004](#)
- New Zealand Building Code (Building Regulations 1992 > New Zealand Building Code > [C Protection from fire](#))
- [Health and Safety at Work Act 2015](#)

6.3. Standards

- SNZ PAS 4509:2008 Firefighting water supplies code of practice
- NZS 4510:2008 Fire hydrant systems for buildings
- NZS 4512:2021 Fire detection and alarm systems in buildings
- NZS 4541:2020 Automatic fire sprinkler systems

6.4. References

- Waka Kotahi NZ Transport Agency guidelines:
 - [Vehicle mass and dimension rules](#)
 - HN-HO-72 – [Waka Kotahi NZ Transport Agency Bridge manual \(2013\)](#)
Note: This standard covers the requirements for all pavements bearing a heavy load such as a fire appliance.
 - [Road and traffic guidelines – New Zealand on-road tracking curves for heavy motor vehicles \(RTS 18\)](#)
- Acceptable Solution C/AS2 > [New Zealand Building Code Compliance C Protection from fire](#)
- [New Zealand Building Code handbook \(third edition, amendment 13\)](#)

Note: The legislation, standards and references referred to in this guide (including those listed above) are relevant at the time that this document was published. Note however that the legislation/links may have been updated since this document was published.

Appendix A – Images

Pumping appliances

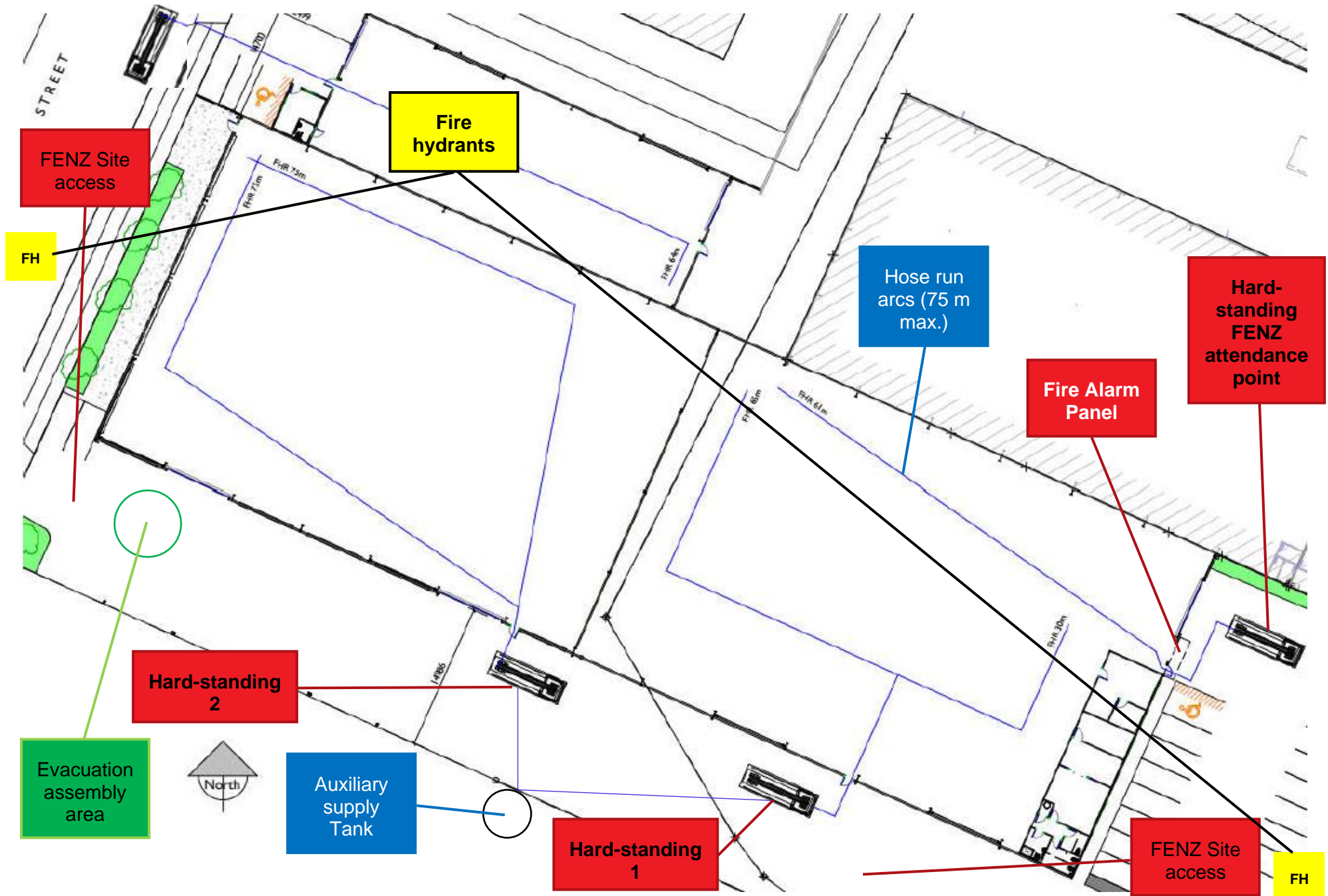


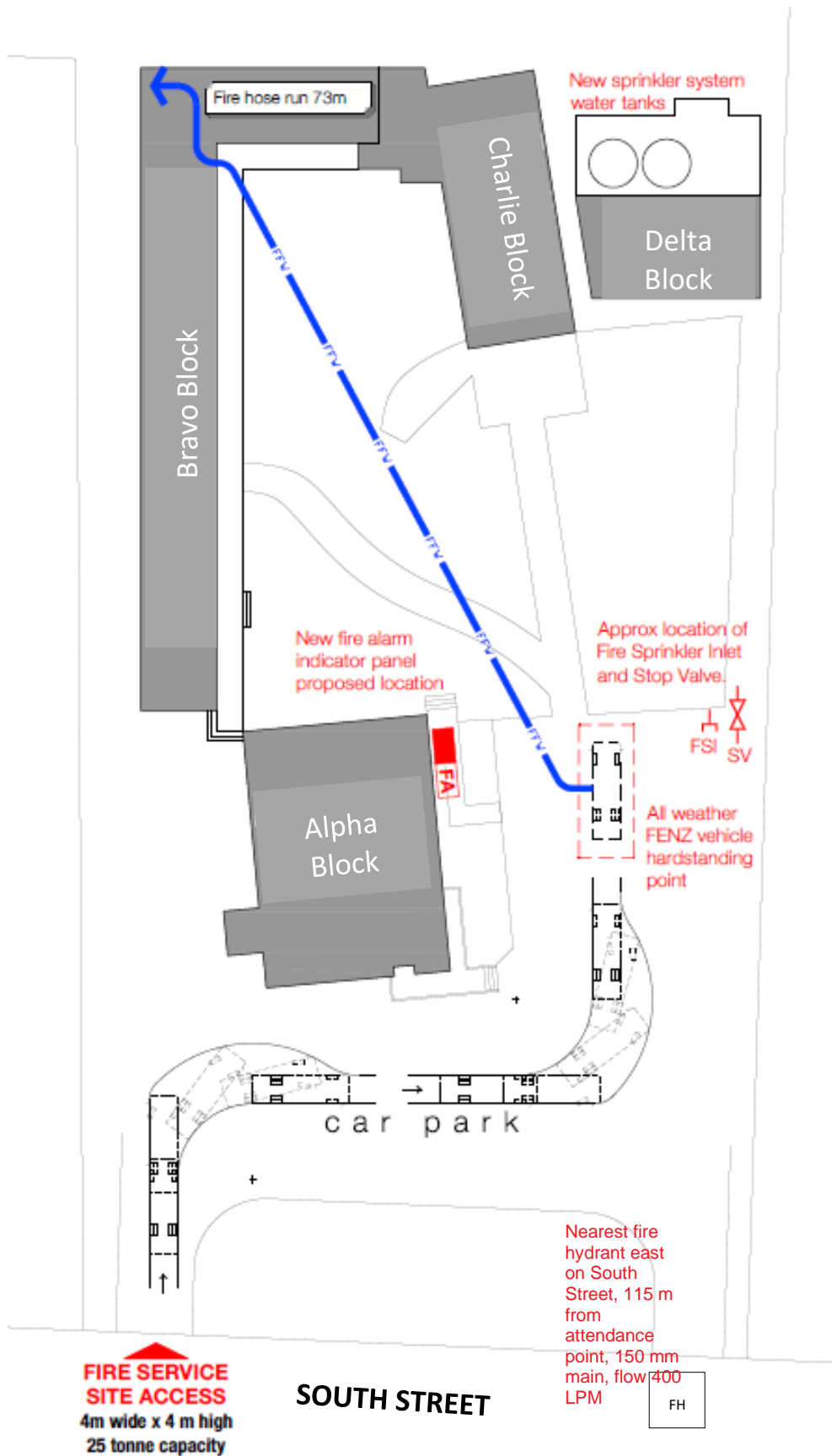
Aerial appliances





Appendix B – Site layout examples





Document information

Owner	National Manager Response Capability
Last reviewed	10 December 2021
Review period	Every second year

Record of amendments

Date	Brief description of amendment
December 2021	Format update and SME content review
March 2018	Initial version

Feedback

785

114181562680

NAME: Gayathiri Ganesan	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I want to bike but the current lack of bike paths makes me too scared to do so.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

786

114179693325

NAME: Geoff Martin	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

It's important to balance the needs of cyclists with other road users and the community as a whole

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

787

114164347634

NAME: Gerrard Smith	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

There are huge benefits to cyclists, scooterers, pedestrians, and bus users. There are only minimal implications for drivers and locals

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

788

114165653822

NAME: Giselle Bahr	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

789

114171709282

NAME: Graeme Campbell	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

790

114177331836

NAME: Graeme	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Having more cycleways and constricting private car road use will cause more congestion for the majority of road users, but won't dramatically reduce car use in or through this area.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

791

114161427203

NAME: Greg Downs	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

792

114167824838

NAME: Glenn	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

For the very small number of cyclists this is total overkill. Extending the 30KMPH zone is also ridiculous and it is too large.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

793

114179304208

NAME: Hunter	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Because it makes people get to work and school slower. Also the roads are in great condition

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

794

114167641247

NAME: Charles Thompson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Ngaio Gorge has always been the major access route to the area and has always been a geographic challenge. The extended slip works have not done any favours to the cycling & walking community. Let's get past this for a long term future in this incredible neighbourhood.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

795

114179813605

NAME: Leoni Hawkins	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

796

114165244940

NAME: Hector McLachlan	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

797

114180498816

NAME: Nicole	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Going to make it hard to attend local businesses as it is already difficult to get a park in the area. I feel as if there is already bike lane for bikers

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

798

114173438279

NAME: Carl Howarth	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

A well thought out and designed proposal, though I'm not in favour of the staged approach as it risks discouraging people from using the cycleway due to gaps. More people biking means less traffic congestion for those who really need to drive, and it's better for our health, climate and wellbeing.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

799

114167921514

NAME: Hannah Zwimpfer	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I cycle this road one a week for commuting. My children are also too scared to cross at the crofton rd crossing. Raising this crossing and reducing the speed is such a win.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

800

114179295878

NAME: Ivy	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I want to add bike lanes but don't want the shops to close

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

801

114176787788

NAME: Louise Lin	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

For safety and to reduce carbon emissions

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

802

114182551013

NAME: Alex Dyer	SUBURB:	ON BEHALF OF: Cycle Wellington	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Ngaio%20Connection%20-%20Cycle%20Wellington%20Submission.pdf




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Ngaio Connection Transitional Design

Cycle Wellington submission

We would like to make an oral submission. Please contact info@cyclewellington.org.nz

Key points of our submission

- We strongly support creating this connection
- We agree with the overall layout of this important cycling connection
- Cycle shoulders and missing protection may lead to close passing by drivers
- Staging sections will leave gaps at dangerous points that undermine take-up
- Downhill shared spaces need more work to protect riders from car door zones
- Part-time clearways help 9–5 commuters but not other riders
- We've also given feedback on some specific design elements

We strongly support creating this connection

This route connects Ngaio and Khandallah to the Hutt Road and Thorndon Quay corridor, forming an important part of the bike network. The long, steep hill and twisty road lead to unpleasant cycling conditions, especially uphill. Riders using ebikes find hills easier, but are still significantly slower uphill than cars and buses, leading to conflict when drivers must wait before passing.

Everyone in our community deserves safe and attractive streets. Bike lanes are climate action.

We are grateful for the effort of the Council to accelerate work on this infrastructure. We look forward to insights from this 'transitional' project being embraced and addressed in the future 'transformational' change project for this route when appropriate.

We strongly support the repurposing of street space as much as possible as infrastructure to enable the public to choose to travel by public transport and active transport. We support the changes to on-street parking and all speed reductions to 30k/ph.

We agree with the overall layout of this important cycling connection

This project follows a similar principle to the Brooklyn hill transitional cycleway – implementing an uphill-only cycleway. This layout will help cyclists in the direction where they currently face more conflict due to larger speed differences. We hope the planned cycleway will make the uphill ride much safer and more pleasant, as the Brooklyn cycleway has.

However, downhill riders will face a similar situation to today. Only the fastest and most confident cyclists can keep up with traffic downhill on this route. The sharrows will make little difference.

Cycle shoulders and missing protection may lead to close passing by drivers

Cycle shoulders attempt to provide some space in constrained settings. We're concerned they may actually increase risk in some situations.

While cycle shoulders provide some visual space for cycling in, drivers will not understand the difference between cycle shoulders and standard bike lanes. Drivers may be reluctant to slow down and wait for clear space before passing safely, reasoning that the 'bike lane' should be sufficient space for the rider.

Cycle shoulders may also lead to an expectation that riders must remain at the far left edge of the road at all times – making it harder for riders to temporarily ride closer to the centre of the lane according to the Code for Cycling: [Take the lane if you need to](#)

We support the use of physical separators wherever possible. The proposal uses a mixture of physical separators and 'buffer space'. Buffer space does not provide the feeling of safety, or the actual safety, of physical separators. Audio tactile profile pavement markings will not provide a sense of separation or effectively keep cars out of the bike lane.

Staging sections will leave gaps at dangerous points that undermine take-up

We understand the pressure to minimise effects on parking. We are doubtful that delaying some sections of the plan will make those parking changes easier. In the meantime, they will undermine the safety of the bike lane at key points.

Under the staged plan, riders heading towards Ngaio in the morning or early afternoon (including people heading home in time for school end) or at weekends will have to leave the bike lane and pull out to pass parked vehicles. Uphill drivers will be reluctant to leave space, particularly where corners prevent a clear line of sight.

Downhill shared traffic spaces need more work to protect riders from car door zones

We do not believe sharrows make a significant improvement in the higher speed sections of this plan.

In several places, downhill riders will pass parked vehicles. Rider speeds will be high – due to the hill, but also among less experienced riders who fear the response of drivers they ‘hold up’. The safety of these riders depends on them staying 1.5m or more out from parked vehicles, to avoid any suddenly opening doors. Looking into the backs of vehicles to spot drivers is impossible at speed, or with commercial vehicles or those with tinted windows.

Riders (especially those less confident) will feel pressure to ride closer to parked vehicles, to allow traffic to pass. This will place them in the dangerous ‘door zone’ at high speed. The need for assertive road positioning as a cyclist – in the face of daunting traffic volumes, extremely large and heavy vehicles, and highly variable and often aggressive motorist behaviour – makes cycling less intuitive and attractive for many.

The worst part of a ‘dooring’ incident is often not impact with the door but falling into the path of a following vehicle. That is why we prefer parking retention (where necessary) on the uphill side of the road than the downhill side. This reduces the speed difference, helping drivers spot approaching riders and giving riders more time to react and avoid a crash.

Where there’s an uphill bike lane, placing the parking between the bike lane and the traffic lane helps further:

- low car occupancy means dooring incidents from passengers are less frequent
- any fall will be away from following traffic, not towards it.

Part-time clearways help 9–5 commuters but not other riders

An equitable approach to bike lane design needs to work not only for those in 9–5 employment but also for off-peak trips, for example:

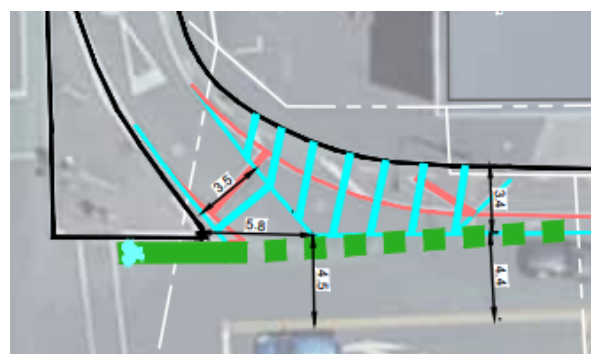
- people with childcare duties such as school drop-offs and pick-ups
- people who work part-time, shifts, or at weekends

The part-time features also undermine bus improvements.

Feedback on specific design elements

Hutt Road intersection slip lane

What else can be done to improve safety at the main intersection with Old Hutt Road. Traffic turning on a slip lane will be fast, and may have blind spots for people on bikes.



Improved usable width on Kenya Street

We are pleased to see the plans have been updated to reduce the pinch-point effect on Kenya Street.

More raised pedestrian crossings

We applaud the addition of raised pedestrian crossings. These are safer for people crossing the road on foot, and help slow traffic in areas where that makes biking safer.

Nothing at Crofton Road / Ottawa Street roundabout?

This roundabout has been left out of the project scope – but is a significant hazard for riders. The road layout means some riders could be allowed to bypass the roundabout (as on Crawford Road in Kilbirnie).

About Cycle Wellington

Cycle Wellington is a voluntary, not-for-profit organisation aimed at improving conditions for existing cyclists and encouraging more people to bike more often. We advocate for cyclists who use their bikes for recreation and transport. Since 1994, we've worked constructively with local and central government, NZTA, businesses, and the community on a wide variety of cycle projects. We represent around 5,000 members and supporters.

Nā mātou noa, nā Cycle Wellington

21 November 2022

Feedback

803

114160132836

NAME: Bridie	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Why such a permanent change to parking in this area when the cycle way is mainly used during peak times? Leaving us with no options when the kaiwharawhara train station is no longer in use and there is no carparkinf facilities close by.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

804

114165747367

NAME: Mike Fee	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Cycle infrastructure is important for cycle safety. Bikes see a great alternative option to motor vehicles for many journeys in and around town. Bikes lanes etc make that a more practical option for many cyclists

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

805

114179295563

NAME: Jess	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Don't know

Why do you think that?

I think it is bad for some shops because if they don't have car park's then they will get out of business. I do think its is safer if we lower speed limet's.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

806

114170829286

NAME: Jacquie Bown	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Increased connectivity across Wellington central is a key shift in us becoming a more sustainable and environmentally conscious city. These proposals benefit everyone and reduce barriers to the necessary shifts we collectively need to make.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

807

114180609820

NAME: Jacqui McVie	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Fully support making roads safer for all users and encouraging a range of transport modes

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

808

114181653620

NAME: Jake	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

A good balance of providing for safe cycling connection between Ngaio and the city while maintaining parking facilities for Kaiwharawhara businesses

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

809

114161063779

NAME: Jane O'Shea	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Wellington desperately needs better low emissions transport options.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

810

114162363193

NAME: Helen Taylor	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

There is an alternate route for cycling, scooting & walking. The disruption to many far outweighs benefits for a few.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

811

114160246551

NAME: James Mowat	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

812

114179307302

NAME: Joel	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

It was fine before and it will be slower and more inconvenient for cars.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

813

114167309098

NAME: Joanna	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Safe spaces for cycling and alternative modes of transport need to be provided for along this route.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

814

114166867916

NAME: Joel Cosgrove	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

815

114169318483

NAME: John Gregson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

It's an important cycle route, and it's important to get more Wellingtonians using carbon friendly transport.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

816

114160363060

NAME: Jono Wood	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

817

114164039012

NAME: Josh	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

818

114171753820

NAME: Jan Pieter de Jong	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Ngaio Gorge Road is used by many cyclist to commute to and from work. It is great to see this proposal, which will make the journey much safer. I support doing the work in one stage, rather than spread out over three stages.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

819

114160387534

NAME: Matthew Eden	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

This is the main road for all the northern residents Kenya resident's will car park's where are they going to park, older residents live here, think about them. cycle lane's work at in the morning and in afternoon the rest of the day they don't Get the rest of the world to stop polluting we are a drop in the bucket

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

820

114171533357

NAME: Kate W	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This will make cycle commuting, walking and travel by bus much safer and more attractive for northern suburbs residents. Thanks.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

821

114174265853

NAME: Kain	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

822

114174278022

NAME: Katherine MacGregor	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

At the moment Ngaio Gorge is the one thing preventing me from wanting to commute via bike. It feels to treacherous in its current form. It needs dedicated bike lanes (ideally in both directions) up to Ngaio Village.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

823

114164228581

NAME: Katherine Blow	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

824

114167865567

NAME: Katie Benson	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

The proposal sounds safer and more convenient for most people than the current route, but I would prefer to see more separated cycle ways used. Sharrows will not be much use without a significant education campaign for all road users.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

825

114175737612

NAME: Stacey	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I'm worried about all the carparks being lost on Kenya Street. As a Trelissick Crescent resident we already get a lot of Ngaio Gorge people parking in our street. We also get people parking all day and commuting into the city. This will overload our street even more.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

826

114178512093

NAME: Kelvin Aris	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

827

114162992941

NAME: Kenneth Dixon	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

828

114179711821

NAME: Kevin	SUBURB: Newlands	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Some make sense, but removal of carpark will make it harder to get to local business in Johnsonville

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

829

114179292645

NAME: Katie	SUBURB: Ngaio	ON BEHALF OF: Ngaio primary	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I think it will be helpful to have a bike lane for everyone although I do not support moving the speed limit down.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

830

114161352514

NAME: Kim	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

831

114180616488

NAME: Kirsten Malpas	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Busy traffic area but best bike access to many northern suburbs.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

832

114162917041

NAME: Kura	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I think improving infrastructure for public transport and bikes is one of the most beneficial things WCC can do right now. AUT did a study a while back that found for every dollar invested in bike infrastructure, there was a 20 dollar return. That came from reduced healthcare costs, increased local spending and because bikes are one of the cheapest forms of transport to cater for. We are going to start seeing the effects of climate change more and more in the coming years. Besides all of the other relevant talking points relating to this, the council is probably going to have less money to play with after things like excess storm damage has been accounted for, and I would really like us to still be able to afford things like annual wastewater maintenance or hospital upgrades. It's good business sense to only buy what you can afford, and personal vehicles are notoriously expensive both for the council and as individuals. I did a rough budget and found that at my most conservative estimate, when I was working I was spending more on running a vehicle than food annually. Now that I am on the sickness benefit, not only is it not an option to pay that, but it's also not really safe enough to bike, and public transport has been gradually gutted over the last 10 years making that close to unusable too. I used to work in Petone and the 10 minute commute from Old Porirua road turned into an hour and a half when I had to take the bus after my van was stolen. I count that as a big part of why I lost my job. Properly investing in public transport and cycle ways will have huge benefits to the community. I strongly support this.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

833

114179298250

NAME: Lucy	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Lucy-Ngaio I think it will be great for cyclists but a little inconvenient for drivers because if you have to go slower you might be late to where you have to go but it will be safer for the cyclists.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

834

114179296562

NAME: Lockie	SUBURB: Ngaio	ON BEHALF OF: ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I think that this is the safest way to prevent car accidents and it is also cool.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

835

114179293580

NAME: Lucas	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

My dad cycles into work and will use this cycle track a lot.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

836

114172777927

NAME: Lachie	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

837

114162433749

NAME: Lauren	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Creating safe ways for Wellingtonians to cycle and use other low-emission transport options is key to combating climate change and improving quality of life for everyone!

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

838

114161170531

NAME: Lachlan Patterson	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

This route has really poor infrastructure for bikes and pedestrians, and this will make it much safer for all street users, especially people riding bikes. I am unsure about the two staged system relying on clearways in the meantime. I am worried that that means the route will remain unsafe in parts for riders who are not riding at peak, so this disadvantages riders who aren't commuters. I also do not see the benefit in an extra year or two of retaining parking if the intention is still to remove the parking in favour of safer infrastructure later. This ought to be one intervention.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

839

114170398450

NAME: Annika Philipp	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

840

114173607000

NAME: Lisa Julian	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I strongly support any move to make our roads safer for pedestrians and cyclists. I had to give up cycling to work after three road accidents. I am desperate to commute on my bike safely for my physical and mental health and to support a reduction in carbon emissions.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

841

114170282687

NAME: Lindsay Meehan	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Ngaio%20submission%202.PNG

Kia ora

As a long-term Kaiwharawhara resident and avid cyclist I would like to lodge my strong objection to the proposed alterations to Cameron St/Bridle Path. By encouraging greater cycle traffic

through this route the Council would, unwittingly, be creating a significant Health & Safety issue. The simple reason being is that the Bridle Path is not designed to accommodate a mix of cyclists and pedestrians.

Currently this pathway is used primarily by pedestrians, many of whom are dog walkers. However, the volume of cycle traffic has increased over the past several years, especially since the introduction of E-bikes, resulting in injury or near injury to walkers and/or their pets along the course of this pathway. As a cyclist myself, I am extremely vigilant around the Cameron St area, especially when driving my car, as I regularly encounter over-confident cyclists paying scant heed to the road rules.

I can see the logic in having the Ngaio Gorge upgraded to cater to the increasing volume cyclists in the Ngaio/Khandallah area, however offering the Bridle Path as an alternative route could potentially open the Council up to severe criticism from rate payers who find that walking access to the Bridle Path becomes a danger to life and limb!

Feedback

842

114162569908

NAME: Lorraine Phillips	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

Suggestion: Several car parks (six?) on the northern side of Kaiwharawhara Road, outside NZ Post building, be allocated 120 minutes. This would be useful for people with an appointment in the city. They can drive and park there, and walk to the bus stop on the Hutt Road, where there is a frequent bus service to and from the city. Currently, there is not a problem parking here for an hour or two, but with the removal of car parks on the southern side of Kaiwharawhara Road, I suspect there will be an extreme shortage of car parks for medium term parking, making it extremely difficult to drive and then bus into the city from Ngaio.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

843

114179292535

NAME: Madi	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

wile I support adding bike lanes I do not want the local buisineses to close due to the lack of parking space.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

844

114179299538

NAME: Mj	SUBURB: Johnsonville	ON BEHALF OF: ngaio school	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

i think this is a good idea only because my sister gose to onzlow so it would be not to waste gas money

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

845

114179293133

NAME: m.t	SUBURB: Ngaio	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I think that they should reduce the speed limit because then it would reduce the chance of a crash.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

846

114170186089

NAME: Susan McIntosh	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

The creation of this cycle lane is at least a better visual experience than that of Island Bay and Ridderford Streets - I am not comfortable with the consistent approach of banning vehicles in Wellington, not everyone can bike, walk or take public transport. The painting, curbing and general look is too busy and is visual pollution.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

847

114170061402

NAME: Simon	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Currently the route is risky and unpleasant to travel

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

848

114171794829

NAME: marilyn hester	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

i feel neither strongly opposed or strongly supportive and prefer to see how the proposed changes work out.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

849

114160165694

NAME: Mark Ramsden	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I commute by bike 4-5 days a week from Khandallah to the CBD. These changes would make that much safer. At present, I experience a dangerous close pass by vehicles on the uphill section of Kenya St, or on the uphill ride home up the Ngaio gorge, several times a week. This proposal would change that and make cycling much more attractive.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

850

114181261139

NAME: Mark Jacob	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I'm a cyclist and would like safer roads. The impact on drivers is minimal.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

851

114174297948

NAME: Danielle Connoles	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Neutral

Why do you think that?

I agree with the need for safer cycleways but DO NOT AGREE with the plan if it includes the REMOVAL of the car parks outside the existing business's along Crofton Road. Especially Cameron Harrison Butchery (4 Crofton Road).

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

852

114174953395

NAME: Aroha Mead	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

There are many areas of Ngaio, especially in the hills, without good access to public transport. Before reducing options for cars to park and use roads, please increase coverage of public transport as a higher priority than cycle lanes.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

853

114181450529

NAME: Matt	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

854

114179669170

NAME: Bus and Coach Organisation	SUBURB:	ON BEHALF OF: Bus and Coach Organisation	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?
These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Bus%20and%20Coach%20-%20Ngaio-Connection-paper-consultation-form.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
 - Install a bike lane on the southern side in the uphill direction
 - Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
 - Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.
- We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.
- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
 - In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☒ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☐ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

We support the clearway proposal. However, the northbound clearway should extend to 33 Kaiwharawhara Road. This should be usable by both cycles and buses. The clearway should have no loading zones as they will force buses and cyclists back into the slow of traffic. All clearway times should also have Stage 3 implementation times - local businesses have adequate off-street parking.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trellisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support

☒ Support

☐ Neutral

☐ Oppose

☐ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

We support the installation of a separated, protected bike lane. However, we have concerns about a raised pedestrian crossing as they can damage buses and make passenger's rides uncomfortable.

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☒ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☐ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

We strongly support the removal of car parks, particulalry when it will benefit PT users.

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

Do you support the proposed changes on this section on Cameron Street (between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

We support changing the parking on Cameron Street as it will potentially accomodate some overflow parking from local businesses.

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

<input checked="" type="checkbox"/> Make the proposed changes more quickly	<input type="checkbox"/> The timing of the stages is about right	<input type="checkbox"/> Make the proposed changes more slowly	<input type="checkbox"/> Don't know
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Why do you think that?

As stated in our Aro Valley submission, local businesses will never support the proposed changes - even though Stage three will not require particularly large changes. Opening up the potential for public pushback and legal challenges can result in compromises that negatively impact the changes

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Main thoroughfares should not be clogged by on-street parking, particularly at peak hours. These proposals do not go far or fast enough.

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Lower urban speeds increase pedestrian safety. Public transport users will often walk first and last miles. Therefore this encourages PT uptake by proxy.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☒ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

☐ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☐ An individual ☒ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation Bus and Coach Organisation

Name of school

Feedback

855

114160227175

NAME: Mathew Hartley	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

As a cyclist, making this route safer through more separation is a great thing, more and more people are choosing to ride up the gorge, especially with such unreliable public transportation.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

856

114180820680

NAME: James	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

These changes will encourage more cycling, which will be better for everyone.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

857

114181497087

NAME: S Naismith	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

We own █ K rd and a part share in █ K Rd with a total of 7 tenancies – 6 residential and 1 commercial. If the suggested removal of the existing parking options and subsequent addition of restricted parking and clearways goes ahead, we would like to know where the residential tenants are going to be able to park and also where the employees and more importantly where customers for the Tile World shop will be able to park. The removal of the existing car parks is going cause a huge issue for our residential tenants (as the number of parking options would be significantly reduced) and it will also cause a very large downturn in business for the Tile World shop as potential customers will have nowhere to park to access the shop. We run a building business out of █ K rd so there are vans and small trucks coming in and out of the building during the day and on weekends. Where are we going to be able to park whist moving vehicles out of the building to allow loading and unloading of gear inside the building? Has there been a survey to establish how many vehicles come and go from numbers █ and █ K rd ? as we have not heard from anyone associated with WCC or LGWM. Is there a health and safety plan? Where are trade's people supposed to park when working inside buildings? Where do delivery vehicles and couriers park whist delivering goods? These are things you people need to consider before you take away car parking. Let's get wellington moving is an absolute joke - lets congest wellington and turn it into a ghost town.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

858

114160311133

NAME: Matt	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

A real game changer in terms of cyclist safety plus also will improve vehicle traffic flow and safety too. After some near misses and seeing the aftermath of crashes along this route, this is a big step forward.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

859

114178463049

NAME: Morris Robertson	SUBURB:	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

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PROPOSED NGAIO CYCLE WAY – COMMENT AND SUGGESTIONS

1. GENERAL

I appreciated the opportunity for the small group discussion at Ngaio Library. While there were a lot of questions of a concerned nature, I have taken on board the basic concept and, with the benefit of local experience, I have made a number of suggestions which could resolve some of the major concerns.

Recognition of the importance of Ngaio Gorge as an alternative access to the city when Ngauranga Gorge is blocked needs to be taken into consideration as does the volume of heavy vehicles using the route for access to the north-western suburbs.

2. NGAIO GORGE/KENYA ST./TRELISSICK CRESCENT WEST INTERSECTION

This intersection appears to be the most critical part to resolve if the scheme is to succeed as it needs to safely resolve the merging of both formed cycle ways, foot/scooter traffic and right turning traffic.

My suggestion, if it has not already been explored, is to place a small roundabout at the intersection, similar to the one at the junction of Glenmore St. and Kelburn Viaduct. The configuration and the space available is about the same.

A roundabout would have a major effect on slowing traffic at this critical point, would allow design of a safer merge both sides from dedicated cycle ways to shared downhill lanes, and allow for safe turns into and out of Trelissick Crescent

3. PROPOSED PEDESTRIAN CROSSING 5 NGAIO GORGE ROAD

I have stood at the corner where Kenya St. becomes Ngaio Gorge Road and timed vehicles rounding the corner from the point at which a pedestrian crossing would be first sighted. The average time was 3-4 seconds with vehicles travelling at about 30 kph or less, having been slowed by the carriageway narrowness of the top of Kenya St. I doubt if a hump would make much of a difference and it could even be a distraction.

My route from Trelissick Crescent to the bus stop below Perth St. is to cross the main route at a point between the top two bollards, from where there is the most available visibility in both directions. On the return journey from the Perth St bus stop, I find that the safest crossing point, with the best viewing distance both ways, is direct from the bus stop to the opposite point on the east corner of Ngaio Gorge Road and Trelissick Crescent.

If a pedestrian crossing was to be placed at that point, it would have several benefits and take advantage of the situation whereby Perth St. and Trelissick Crescent East do not align. It would provide the opportunity for uphill cyclists to cross over into Perth St.

Related to the above the owner of land on the corner of Ngaio Gorge Road and Trelissick Crescent [East], five years ago, applied for a resource consent to build 19 houses in the site he owned. On behalf of interested neighbours, I examined the application drawings in detail and ascertained that the details regarding vegetation and slopes had been fudged. Also, the scheme relied at least 5 vehicles needing to park off-site.

The consequence of the above, information passed to WCC resulted in questions being asked of the developer, which weren't answered and the application lapsed. The property is now on the market with the original plan shown as the potential for development.

4. DISPLACED CARPARKING

The 5-minute centroid in Kenya St. shows the alternatives of Trelissick Crescent, Abbott St. and Crofton Road. [uphill] as alternative parking options.

With regard to Trelissick Crescent parking is available on one side only due to the narrow carriageway. I have passed photos, via Liz in your team, showing fully occupied parking spaces on a Sunday afternoon. It is similar over night with all parking spaces from Kenya St. to Jacobsen Lane/Hewett Way intersection fully occupied. Often there is overflow parking illegally on the opposite footpath, as parking to the kerb would leave a carriageway width of about two metres.

Beyond Hewett Way, the road narrows and there are several blind corners, no footpath until near the far end of Trelissick Crescent. Street lighting is minimal and does not have full coverage. There is bush on outer side, with a steep fall into Ngaio Gorge. Houses on the inner side general are higher up and do not have sightlines to the road below.

With regard to Abbott St. the stretch illustrated is steep with the uphill side hard against a bank and lacking a footpath. With entrances the available parking on the downhill side is limited. This leaves Crofton Road from the junction up to Orari St. With the number of entrances and residents parking there is little scope to accommodate many displaced vehicles.

5. NGAIO UNION CHURCH [Cnr. Kenya St./Crofton Road]

One of my local roles is as Project Manager for the church and the adjoining house. When the current projects are completed there will be about 12 carparks on site. There is an arrangement for, Sundays only, for the Church users to park on the Motor Doctors forecourt across the road.

However, the Church is increasingly being used for a range of community activities throughout the week, including public meetings, Ngaio/Crofton Downs Residents Association, recycling collections, repair café, indoor bowls and a local choir. All of these generate a short-term parking demand, particularly in Crofton Road.

6. PUBLIC TRANSPORT

The cycle way route is served by Route 25 as far as the turn off into Perth St. and Route 26 the full length, rush hours only. The uphill route has a difficult turn both into and out of Perth St. The footpath at the Ngaio Gorge Road/Perth St. peters out before the position of the bus doors. A number of school busses are users of the full route.

7. SUMMARY

I trust that you will consider the suggested ideas included in this response. I would welcome the opportunity to meet with you on location.

Kindest regards



Morris Robertson QSM FNZIA [Retd.]

Trelissick Crescent, Ngaio 6035

Email:

Phone:

Feedback

860

114178553050

NAME: Thomas	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

That's a really silly question. There are so many different aspects to the proposal. I support some, strongly oppose others. I've also lived here long enough to know that this proposal is what's going to happen, and this "consultation" is a complete sham. You will only listen to the people who agree with you. 30km/h is way too slow, even on a bike. 40km/h is a much better compromise, one I'd be happy with across the city, except main arteries. You're proposing to spend millions on a few km of road, when the biggest danger to cyclists are the crappy roads everywhere else, full of potholes and covered in loose gravel, and the horrifically bad drivers in Wellington. Sort the normal roads first, educate drivers, then look at fancy bike lanes. The Hutt Road bike path is a death trap, and I refuse to use it. Cobham Drive was good, then you dug it up for a crossing nobody wants, and put a huge bump on the bike path. You're not making cycling safer, it's quite the opposite. I cycle 300-400 km a week, and I felt much safer before you started "fixing" things

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

861

114181610950

NAME: Mike Woodruff	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

As a user of local businesses this will affect them and my ability to park. Cyclists are already a liability on this road, and having a cycle lane up Cameron Street as well as parked cars will be too tight a squeeze with oncoming traffic on such a steep hill

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

862

114164345947

NAME: Neil	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

A popular commuting route for cyclists

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

863

114160137714

NAME: Nick Rinehart	SUBURB: Oriental Bay	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

864

114181699545

NAME: Nico	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Because it means that bikers like me will travel safe and won't hold up traffic.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

865

114179753844

NAME: Adam Eruera	SUBURB: Kaiwharawhara	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

removal and reduced parking impacts local businesses

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

866

114181529147

NAME: joby	SUBURB: Wellington Central	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

huge waste of money on the private transport needs of a few people. No alternatives considered

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

867

114175945428

NAME: Dale Mctavish	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

15112022144058-0001.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
- Install a bike lane on the southern side in the uphill direction
- Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
- Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.

We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.

- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
- In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

THE CONVOLUTED NATURE OF THE TERRAIN MAKES THIS AREA UNSUITABLE FOR BIKEWAYS; NEVER MIND PEDESTRIANS. WILL MORE PEOPLE REALLY WANT TO WALK, RIDE & BUS INTO THE CITY VIA HUTT RD? BUSINESSES SHOULD NOT BE AFFECTED IN ANY WAY, WHAT WILL THE COST OF PATROLLING THE CLEARWAYS BE FOR RATEPAYERS?

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trelisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

FURTHER NARROWING OF THE ROADS

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

FURTHER NARROWING OF THE ROADS —
MORE 30 KM SPEED LIMIT MEANS CONGESTION —
MORE CARBON EMISSION FROM THE PUNTERS
WHO ARE IN THE MAJORITY

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

**Do you support the proposed changes on this section on Cameron Street
(between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?**

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

NONSENSE

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☐ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

Don't make the changes at all.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

- 1 30 km-CONGESTION OF TRAFFIC
 - 2 WASTE OF FUEL
 - 3 MORE CARBON INTO THE ATMOSPHERE
 - 4 CYCLISTS WILL BE CHOKING ON THE EMISSIONS AS THEY SPEED DOWNHILL AT 50 km.
- WELLINGTON IS STRANGLING & GOING BACKWARDS

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☒ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

868

114179292987

NAME: Oskar	SUBURB: Kaiwharawhara	ON BEHALF OF: Ngaio school	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I think that it will be beter for the comonity and globle worming coss it will incareg peope to bike to school and there work.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

869

114178504822

NAME: Mike Reid	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

870

114178557556

NAME: Sarah	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I highly support an uphill cycle lane.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

871

114167958924

NAME: Murray Pascoe	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

These designs will make it easier and safer to cycle this route, and easier to drive on Kenya St. At the moment, its difficult to know when its safe to pass. Reducing the speed limit on Ngaio Gorge from 50 to 40 would make it even better, the current speed restrictions have made it feel much safer to ride downhill.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

872

114169344318

NAME: Paul Merwood	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

873

114174937742

NAME: Paul Gray	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

874

114181452839

NAME: Paul Jones	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

875

114181795933

NAME: Philippa Curthbert	SUBURB:	ON BEHALF OF: NZAA	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Letter%20to%20WCC%20Re%20Aro%20Valley%20and%20Ngaio%20Gorge%20Cycleways%2021%2011%2022.pdf



New Zealand Automobile Association Inc.

342-352 Lambton Quay, Wellington 6011
PO Box 1, Wellington 6140

21 November 2022

Submission for WCC Aro Valley and Ngaio Gorge Cycleways

Wellington City Council

Emails arovalleyconnections@wcc.govt.nz

ngaioconnections@wcc.govt.nz

This submission is made by the Wellington District Council of the New Zealand Automobile Association (AA).

1. The District Council represents over 200,000 members. Although we were founded as an organisation representing motorists, we are moving to becoming a mobility organization as all of our members are on occasions pedestrians and an increasing number are cyclists. Furthermore, two of our Council members are commuter cyclists and two more are recreational cyclists.
2. The AA supports separate cycle and bus lanes to encourage mode shift as long as the design is safe for all users. However, we have a number of mainly safety concerns on these proposals as follows:
3. In principle we support the uphill cycle lane up Aro Street to Holloway Road including the extension of the 30km/h speed limit and the raised platform to enhance safety at the pedestrian crossing. However, we request that the approach ramps to this platform be as flat as possible to avoid “bumping” bus passengers on this route.
4. We have concerns regarding the effects on residents on Aro Street especially where there will be no parking on both sides of the road. One of our Councillors visited Aro Street at 7.45 am one morning and noted that most of the coupon parking parks were occupied with residents’ cars as no coupon was displayed. Consequently, the removal of 48 coupon parks will disadvantage residents as these have not been replaced with alternate residents’ parks. We also note that the replacement residents parks in Owhiro Road and Holloway Road are a long way from many Aro Street houses and may be too far away for elderly or disabled residents.
5. We wondered if Council has actively advised all the Aro Street residents of the bike lane proposal? We acknowledge that a drop-in session has been held but for such



a major change we would expect at the very least an individual letter drop plus possibly even door knocking every resident or property owner. Otherwise, the Council could be seen to be imposing significant change on residents without adequate consultation of the proposed changes.

6. We have major safety concerns with the proposed uphill bike lane on Raroa Road. We acknowledge the challenge of providing bike lanes in Wellington, but in our view this narrow windy road with poor visibility on corners is not really wide enough for an additional bike lane.
7. This is confirmed by page 11 of the safety audit (enclosed) which identified 7 locations where a bus or long vehicle would have to cross into the cycle lane to avoid crossing onto the other side of the road. This is potentially unsafe and may give cyclists a false sense of security that this is a safe bike lane. Photos are also enclosed of a bus struggling to stay on the correct side of the road going around one of the sharp corners. Consequently, we support the statement from Cycle Wellington advocate Patrick Morgan that a painted cycle lane is not protection. To enhance safety and encourage greater cycle use the AA supports separated cycle lanes which means installing devices to prevent accidental impingement by vehicles.
8. We also have issues with the staged approach to the implementation of this bike lane up Aro Street and Raroa Road. By proposing to construct the upper section of the bike lane first, cyclists will have to negotiate the lower section with no bike lane. This is probably OK with experienced cyclists but not suitable for new or inexperienced cyclists. We would prefer the whole scheme to be implemented in a short space of time (e.g., 6 months). If this is not possible, we consider the bike lane construction should begin at the Willis Street end.
9. In comparison, we have fewer concerns regarding the proposed cycle lane to Ngaio. This is because the road is generally wider and the houses in Ngaio mainly have off street parks.
10. To minimise the severity of “dooring” accidents we suggest that any parking retained be on the uphill side and possibly offset like the Island Bay Cycleway.
11. For Kaiwharawhara Road we wondered if Council had considered a peak hour clearway (say 3pm to 6pm) instead of a bike lane? This is because there is limited business parking and most cyclists would be using this route in the evening.

We request the opportunity to present our submission to Council.

Yours sincerely

Geordie Cassin

Chairman – Wellington District Council

NZAA

Feedback

876

114181537673

NAME: Peter Taylor	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Safer to bike through the area

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

877

114170218082

NAME: Peter Keller	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Anything to encourage people to try out bicycle commuting must be a good idea. Unfortunately I have been riding bikes continually for 65 years, and will continue to do this in spite of any "changes" to the Ngaio Connection route.

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

878

114161632212

NAME: Peter Steven	SUBURB: Karori	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

More people would ride bikes (and especially e-bikes) if they didn't have to share a lane with high speed traffic and heavy vehicles.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

879

114165547222

NAME: Alan Platt	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

A route almost impossible to ride safely by bicycle.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

880

114160155823

NAME: Paula	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

The roads are narrow and dangerous enough for pedestrians and cars to also include cycleways now, it simply makes no sense. There is not enough parking and yet the proposal is intending to eliminate some? The footpaths along the marked area go from super narrow and crowded with vegetation to non-existent and there seem to be no indications that that issue is going to be revised/improved.

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

881

114164198935

NAME: Peter Ramage	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

The cycleway as designed is inconsistent with WCC's 2020 parking policy, that says the safe movement of people and goods is more important than car parking, and the Paneke Poneke network principles, including that "The network needs to be safe and to feel safe for everyone who wants to get around on a bike or scooter" This plan will not deliver a cycleway suitable for all ages and abilities.

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

882

114179303725

NAME: R.S	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

because i don't like bike Rubus,ngaio

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

883

114174308034

NAME: Raj PARBHU	SUBURB: Northland	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

No room for cars to move freely and planned layout is terrible!

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

884

114161580032

NAME: Ramona Redfern	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

I think these changes are long overdue and will make my life easier and safer, but I'd like to point out that a painted line isn't protection, make cycle lanes physically separated as much as possible.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

885

114178469045

NAME: Richard Findlay	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

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Have your say on pedestrian and bike improvements in Ngaio

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
- Install a bike lane on the southern side in the uphill direction
- Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
- Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.

We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.

- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
- In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

See attached.

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trelisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

**Do you support the proposed changes on this section on Cameron Street
(between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?**

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

- | | | | |
|---|--|---|-------------------------------------|
| <input type="checkbox"/> Make the proposed changes more quickly | <input type="checkbox"/> The timing of the stages is about right | <input checked="" type="checkbox"/> Make the proposed changes more slowly | <input type="checkbox"/> Don't know |
|---|--|---|-------------------------------------|

Why do you think that?

See attached.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

See attached.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☒ I own or manage a business in the area
- ☐ I go to school or education in area
- ☒ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☒ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Kaiwharawhara Rd Section

The proposed changes to the Kaiwharawhara Rd section are not based on any evidence (actual or likely bike volume) supporting the need for a separated cycle lane priority over a non-separated cycle lane with clearway times matched to the needs of commuter cyclists.

The proposal to automatically transition to a cycle lane with excessive clearway times (2pm-9pm) after one year (in 2024), and/or a separated cycle lane after two years (in 2025), are a disproportionate response to the needs of cyclists versus businesses who depend on on-street parking for staff and customers when there is little or no practical, alternative parking.

Some of these businesses will need to plan for closure, the consultation documentation uses the word “adapt” but this is not commercial thinking. Those with leases (which are likely to have a personal guarantee from the business owner) will be faced with significant personal losses. Commercial and residential properties will become vacant, with limited alternative use in the short to medium term.

The objectives of the changes can be achieved through having a 4pm to 6pm clearway for exclusive cycle use on this relatively short stretch of road. While Council officers are unable to provide current cycle usage data, through discussions it appears that uphill use outside the hours of 4pm to 6pm is very low (fewer than an average of 20 cyclists per day).

This small section of the overall cycleway should have a clearway from 4pm to 6pm to create space for cycle use and outside this time period be available for on-street parking, with a mixture of loading zone, short term and longer-term options. This is a sensible compromise which better balances the needs of existing users and the objectives of a connected bike network. It is also consistent with what is proposed on the downhill section of the road.

Staged Approach for Ngaio Connection

In the absence of usage data, a staged approach is sensible. However, Stage 1 (amended as per the earlier comment) should be longer to allow time to assess how those changes work and to collect actual usage data. There should be at least three years of usage experience and data collection which can then be assessed before any further changes are proposed. Any further substantive changes (such as extending the hours of the clearway or installing a permanent separated cycleway) need to be subject to further public consultation.

Do You Support the Overall Proposed Changes to the Ngaio Connection Route

My opposition to the proposal primarily relates to the proposed changes to the uphill section of the lower part of Kaiwharawhara Rd where the proposed changes do not balance the objectives of the changes with the likely damage to businesses and property owners. For this part of the route the uphill section needs to be treated in the same way as the downhill section.

Feedback

886

114161217300

NAME: Jim	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

It's about time the car-hating council minions stopped stuffing with our roadways and paid some attention to what the majority of people who pay their rates want. Only a small minority of users of this route are cyclists or people who take public transport.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

887

114173624630

NAME: C. Pa'o	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Accommodation of buses, cars and cycles on the road. Safer environment for cyclists going up and down the gorge.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

888

114160397889

NAME: Simon	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Uphill bike lanes critical on this route. They should be delivered ASAP. Do not support the interim step of time limited clearways. Go straight to the separated bike lanes along the full length of the route.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

889

114176177289

NAME: Roz Scott	SUBURB: Aro Valley	ON BEHALF OF: Tickadeeboo Insideout Design LTD	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The businesses will die in Kaiwharawhara Rd it is mostly commercial buildings with business for Wellington, the public by the dog. Food, dogs get groomed, coffee get bought, coffee machines get sold, people drop off courier packages, vehicles get fixed, Home staging, health and safety items for sale, home interiors for customers to buy beautiful gifts the list goes on - all these businesses have suffered heaps since COVID and are trying to survive- some silly road person decides I know what let's take all cars off the road and make a mess and kill these businesses so we can cycle to work which we have done for many years. Save the business and Kaiwharawhara Ngaio

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

890

114174257610

NAME: Sean Kells	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Another slow trudging hill that would benefit from a separation of concerns for newer riders. With weather and gradient already hampering the route accessibility, investing in rider infrastructure would make it a much nicer and much safer trip

Do you support the proposed speed changes on the Ngaio route?

Neutral

Decision made by Waka Kotahi

Feedback

891

114161365555

NAME: Sam	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

I support the idea of the cycle way, just unsure where it is expected the cars that park in these parks will be displaced to?

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

892

114181201050

NAME: Samuel Kempthorne	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Good project for this route, I think this project should be completed in 2023, don't drag out this for three years, the true benefits only exist once the whole project is completed.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

893

114181680261

NAME: Sam A	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

The negative impact this will have on our business is massive. After a tough last 2+ years, this is another blow to us. Our business requires carparks to operate. Our customers come from all over the Wellington region so bikes and public transport are not an option for them.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

894

114168802587

NAME: Samuel irwin	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

We need more diversification of traffic and commuting.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

895

114175979368

NAME: Matt	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

I believe that these proposed changes aren't well thought out and could create a more dangerous road in an area where the road is already dangerous

Do you support the proposed speed changes on the Ngaio route?

Oppose

Decision made by Waka Kotahi

Feedback

896

114181496057

NAME: Steve Naismith	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

897

114161266491

NAME: Matt	SUBURB: Strathmore Park	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

I cycled the route for years and the only place where things got tight sometimes was Kenya Street heading downhill into Ngaio. I managed this ok by pulling over for a couple of seconds at the top of the hill to let cars pass then it was ok. The plan to have an uphill cycle lane is not good on Kenya Street- it is easier for cars to pass you as a slow moving cyclist in the mornings going up the hill- the dangerous bit is cars passing you at speed as you go down the hill in the evening. The proposed design will just make this more dangerous as it narrows the road. Like I say, I never had a problem slowly cycling uphill on Kenya (cars can pass easily because you are slow and there is little oncoming traffic in the morning), the problem was cars following you closely down the hill in the afternoon because, while you are moving quickly, you are not travelling as fast as them. At times you get passed at speed as they try to get around (along with swearing and honking). I don't think there is an easy answer, but I think leaving it as it is would be better than narrowing the road. Leaving in a narrow shared downhill lane with parked cars is going to continue to put off reluctant cyclists and so I don't think you will increase cycling uptake enough to justify the cost and disruption to residents' parking. Maybe you lower the speed limit down Kanya Street so that car and cyclists downhill speeds are closer- although that is pretty annoying for cars when there are no cyclists around!

Do you support the proposed speed changes on the Ngaio route?

Support

Decision made by Waka Kotahi

Feedback

898

114162671374

NAME: Steve Mackle	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Make cycling safer, encouraging more people to do so. This will help normalise it as a form of transport and recreation, helping reduce carbon emissions.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

899

114164230294

NAME: Steve	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Good for countering climate change and reducing carbon footprint.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

900

114181501755

NAME: Stuart Cunningham	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

901

114168748659

NAME: Tex Graham	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

The improvements to the Ngaio Gorge section will be great. The Cameron Street upgrade is a complete waste of money. I ride up and down this street often and it is the safest street i ride on for my entire commute. To implement restrictions on parking for residents on this street is not a good reflection on what this overall project is trying to implement. There is no need for it and it will leave a sour taste in peoples mouths. This street has just had surfacing work done on it in the past year and now you are going to be doing more work on it? Waste of money, spend it where it will actually make a difference.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

902

114164126373

NAME: Lorraine Boennic	SUBURB: Seatoun	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

903

114164407974

NAME: Tim	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

904

114165686662

NAME: Trevor Butler	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

905

114160174590

NAME: Victoria Rogers	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Support

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

906

114181436196

NAME: Vaughan Renner	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

Given climate change, raised pedestrian crossings are idiotic. Every vehicle has to slow then speed up again, thereby increasing carbon emissions.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

907

114180441761

NAME: Glenda	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Oppose

Why do you think that?

I think there should be a cycle way both up and down the Ngaio gorge for safety. Cars often try to pass when coming downhill and it's too narrow.

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

908

114161144967

NAME: William Turner	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly oppose

Why do you think that?

Kenya Street is already far too narrow, who in their right mind is biking up Cameron Street?
Indifferent to changes in the Gorge as long as traffic is not effected, already bad enough.

Do you support the proposed speed changes on the Ngaio route?

Strongly oppose

Decision made by Waka Kotahi

Feedback

909

114164321402

NAME: Will Thomson	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Strongly support

Why do you think that?

Cycling needs to be an accessible and realistic option

Do you support the proposed speed changes on the Ngaio route?

Strongly support

Decision made by Waka Kotahi

Feedback

910

114184540983

NAME: A Bramly	SUBURB:	ON BEHALF OF:	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Avryl%20Bramly%20Ngaio%20Submission.pdf

Have your say on pedestrian and bike improvements in Ngaio

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses between Ngaio and Hutt Road.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Ngaio Connection

The Ngaio Connection project will make it safer and easier for more people to walk, bike, and bus into the city via Hutt Road from Ngaio, Crofton Downs, Khandallah and Johnsonville.

The street changes planned for this route include:

- Better access to bus stops
- New raised pedestrian crossings
- Uphill bike lanes (one-way)
- Sharrow road markings going downhill
- Safer speeds around Ngaio Village and on Cameron Street
- Some parking removal and parking changes along the route and on side streets, with a staged approach through the business areas on Kaiwharawhara Road.

Changes proposed for the Kaiwharawhara Road section

We are proposing changes to the road layout along Kaiwharawhara Road to:

- Install a new pedestrian crossing outside 24-28 Kaiwharawhara Road

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Ngaio Connection at transportprojects.org.nz/ngaio

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at ngaioconnections@wcc.govt.nz if you have any questions or you can visit a Ngaio drop-in session if you need help filling out a submission.

- Install a clearway from Hutt Road to 25 Kaiwharawhara Road between 4pm-7pm, seven days in 2023
- Install a bike lane on the southern side in the uphill direction
- Install four P30 car parks and a P10 30-metre loading zone with a clearway from 4pm-7pm seven days, outside 53-57 Kaiwharawhara Road
- Install a bike lane and broken yellow lines for the remainder of the southern side to the slip stabilisation work, where 480 metres of bike lane is being built as part of the Ngaio Gorge slip stabilisation work due for completion December 2022.

We are proposing a staged approach to changes on Kaiwharawhara Road to increase the safety benefits to people on bikes while continuing to give businesses time to adapt to the changes.

- In January/February 2024, we propose to extend the clearway times along this stretch from 4pm-7pm to 2pm-9pm
- In January/February 2025, we propose to install full-time bike lanes on the two sections of clearway outlined above.

Do you support the proposed changes for this section of the route on Kaiwharawhara Road?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Changes proposed for the Ngaio Gorge Road section

We are proposing to change the road layout along Ngaio Gorge Road to:

- Install an uphill bike lane with broken yellow lines up to the intersection with Trellisick Crescent at Kenya Street
- Paint sharrows in the downhill direction
- Install a raised pedestrian crossing between 5-7 Ngaio Gorge Road.

Do you support the proposed changes for this section of the route on Ngaio Gorge Road?

- ☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

No options to separate out parts of project
Cyclery / versus other road changes
loaded question

Changes proposed for the Kenya Street and Crofton Road section

On Kenya Street and Crofton Road, we are proposing to change the road layout to:

- Install a bike lane in the uphill direction from Waikowhai Street roundabout to Crofton Road and along Kenya Street
- Remove 59 unrestricted car parks on Kenya Street, taking occupancy from 41% to 87%
- Remove one P30 car park by 2 Kenya Street
- Install one P10 car park by 6 Kenya Street
- Remove four unrestricted car parks outside 54-58 Kenya Street and installing broken yellow lines to improve access to the bus stop for buses
- Extend the 30km/h speed limit in the Ngaio Village from 11 Ottawa Road to 6 Kenya Street
- Raise the existing pedestrian crossing on Crofton Road to improve safety.

Do you support the proposed changes for this section of the route on Kenya Street and Crofton Road?

- ☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

as above

Changes proposed for the Cameron Street section

On Cameron Street, we're proposing changes to the road layout to:

- Connecting to the Bridle path, install a cycle shoulder in the uphill direction and a shared bike and vehicle lane downhill
- Change four unrestricted car parks to four P60s outside 6 Cameron Street
- Change seven unrestricted car parks to six P120s outside 8-14 Cameron Street
- Change 39 unrestricted car parks to 39 P24hr car parks outside 16-60 Cameron Street
- Change four angle P60 car parks at the bottom of Cameron Street to two parallel P60 car parks
- Install no stopping restrictions (broken yellow lines):
 - around corner of Cameron Street into Sargeson Way
 - around corner of Sargeson Way into Marsh Way
 - around corner of Marsh Way into Cameron Street
 - on both sides of narrow section of Cameron Street from 60 Cameron Street to Brasch Way.
- Reduce speed on Cameron Street from 50km/h to 30km/h.

**Do you support the proposed changes on this section on Cameron Street
(between Kaiwharawhara Road and the Kaiwharawhara Bridle Path)?**

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Cycleways waste scarce rates money
as above.

Staged approach for Ngaio Connection

To help businesses in the Kaiwharawhara area adapt to the change, we are proposing a staged approach to parking changes in this area over two years.

Most of the proposed changes for the route will be installed in stage 1. We would install most of the proposed uphill bike lanes, greatly increasing safe separation for people on bikes and making it easier for people driving along the route. Work to improve sight lines will improve safety for people driving and those riding bikes.

Reducing speeds and installing raised pedestrian crossings will help make things safer for all road users, especially people walking to and from bus stops, providing better transport options for local residents and visitors.

Two sections of the uphill bike lane planned for Kaiwharawhara Road will happen in stages 2 and 3, using clearways over the first two years to help manage the removal of car parking.

Stage one - 2023

- Clearway operating from 4pm-7pm, parking available at all other times.

Stage two - 2024

- Clearway operating from 2pm-9pm, parking available at all other times.

Stage three - 2025

- Uphill bike lane fully installed and in use, with the remainder of the car parking on the south side of Kaiwharawhara Road removed.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

Wasting money on a greenwash middle class vanity project.
Don't make them at all. Promote public transport.

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

☐ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☒ Strongly oppose

☐ Don't know

Why do you think that?

Needs consultation separate from cycle ways

Do you support the proposed speed changes on the Ngaio Connection route?

The existing 30km/h zone on Ottawa Road will be extended 320m to Ngaio roundabout and up Crofton Road. The speed limit on Cameron Street would be reduced from 50km/h to 30km/h. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☒ Strongly oppose

☐ Don't know

Why do you think that?

Needs consultation ~~and~~ apart from cycleways

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Ngaio route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van *Hybrid*
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport? *Slanted question. Some modes only may be important.*

- ☐ Very important
- ☐ Important
- ☐ Moderate importance
- ☐ Low importance
- ☒ Not important
- ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No
 - ☒ Yes
- If yes, please provide your contact number: [REDACTED]

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

*Do not publish this
for privacy reasons*

Feedback

911

114187771462

NAME: Akane Sandom	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?

Ngaio Connection

Ngaio connection

Do you support the overall proposed changes to the Ngaio Connection route?

These include traffic resolution TR180-22

Why do you think that?

Do you support the proposed speed changes on the Ngaio route?

Decision made by Waka Kotahi

Ngaio%20sub%20late.PNG

Submission on Ngaio Connection Project

Akane Sandom

Ngaio resident

Overall, I believe the Ngaio Connection project is important and urgent, and I strongly support it. I am grateful for the effort of Council to accelerate work on this infrastructure.

Improvements made through this project are crucial for people who may be considering cycling to and from the city, if the journey was safer than it is now. The more people who feel safe enough to travel into the city by bike will mean less cars on our already congested roads, making greater journeys for all.

Separated cycleways are imperative

There are some real limitations on the improvements that can be made along the route through this project, which I understand have been expressed as limitations of the narrow already existing road design. This project’s design needs to ensure anyone (between the ages of 8 and 80 years) wanting to cycle, is safe and feels safe on their journey. I don’t believe this project can achieve this with its current design - separated cycle ways are imperative for safer journeys for all cyclists.

However, in the absence of separated cycle ways being possible for the entire route, in particular for the downhill section of Ngaio gorge, I would like to see:

- Slower speed limit for cars travelling down the Gorge, so that cyclists do not feel pressured to travel at 50km an hour
- Increased signage to warn cars of cyclists taking the lane going down the gorge, in addition to sharrow markings.

Safety for cyclists at Hutt Road/Kaiwharawhara Road traffic lights

I consider the current road layout is hazardous for both cyclists and drivers. The left-turn give way has visual obstructions that makes it hard for vehicles turning left to see cyclists coming from their right (which have an unorthodox light sequence when crossing from Spotlight). For the same reason, vehicles turning right from the southbound lane of Old Hutt Rd into Ngaio Gorge can often be hard to see by the left-turning vehicles. Lastly, the left-turn lane allows for relatively high-speed entry and exit which I think contributes to the driver blindness. I would like to see a redesign of the left hand turn lane for cars coming from the south turning into Kaiwharawhara Road to make it safer for all users of the road, especially cyclists.

Improvements are needed to the Bridle Track

The Bridle Track that connects Khandallah and Kaiwharawhara is a great alternative for many cyclists who may not be comfortable using the gorge. However improvements to the path are needed to ensure that cyclists ride at appropriate speeds, and to make the path feel safer for pedestrians. I note that the project includes some improvements to the path - however I believe specific consultation and a focus on this path’s improvements could benefit the community. As a start, I would like to see improvements to signage for both cyclists and pedestrians (especially on blind corners) and improvements to the surface of the path itself to reduce slips and subsidence.