



Aro Valley connection proposal

All submitters feedback

31 October – 21 November 2022

Submission	Name	Suburb	On behalf of	Page
1	Mike	Aro Valley	An individual	17
2	Doug	Aro Valley	An individual	19
3	Adam Bennetts	Aro Valley	An individual	21
4	Keith Davies	Aro Valley	An individual	25
5	Adam	Aro Valley	An individual	27
6	Richard Rust	Aro Valley	An individual	29
7	Alastair Stewart	Kelburn	An individual	31
8	Emma Alcock	Aro Valley	An individual	33
9	Alex Gray	Brooklyn	An individual	35
10	Alison Jones	Highbury		37
11	Andrew Bartlett	Miramar	An individual	39
12	Andy Bradwell	Kelburn	An individual	41
13	Anita Easton	Wadestown	An individual	43
14	Anna Delany	Aro Valley	An individual	45
15	Anna Sisley	Kelburn	An individual	47
16	AnneMarie Curtis	Karori	An individual	49
17	Ann Clark	Aro Valley	An individual	51
18	Antony Paltridge	Wellington Central	An individual	53
19	Joe bloggs	Aro Valley	An individual	55
20	Arran Whiteford		WCC Environmental Reference Group	57
21	Arran Whiteford	Melrose	An individual	59
22	Asher Regan	Te Aro	An individual	61
23	Athol McCredie	Aro Valley	An individual	63
24		Karori	An individual	65
25	Ben Jeffares	Aro Valley	An individual	68
26	Ben Sandle	Wadestown	An individual	70
27	Tim Bollinger	Te Aro	An individual	72
28	Brendon	Te Aro	An individual	75
29	Brett Halkett	Miramar	An individual	77
30	Bridget	Aro Valley	An individual	79
31	Bruce Herron	Johnsonville	An individual	81
32	Eric	Mount Victoria	An individual	83
33	Barbara wheeler	Aro Valley	An individual	85

34	C Andersen	Aro Valley	An individual	87
35	Cameron Hart	Karori	An individual	90
36	Cameron Grant	Karori	An individual	92
37	Carolyn English	Aro Valley	An individual	94
38	Cassandra Bahr	Newtown	An individual	96
39	Catherine Monk	Aro Valley	An individual	98
40	Cathy Blakely	Vogeltown	An individual	100
41	Cecilia	Brooklyn	An individual	102
42	Kah	Wilton	An individual	104
43	Andrew Chisholm	Brooklyn	An individual	106
44	Steve	Aro Valley	An individual	108
45	Chris Mitchell	Karori	An individual	110
46	Chun-Lin Lee	Aro Valley	An individual	112
47	Caroline Vincent	Brooklyn		114
48	Callum	Aro Valley	An individual	116
49	Clarry Inwood	Aro Valley	An individual	118
50	Clive Truman	Aro Valley	An individual	120
51	Zane	Aro Valley	An individual	122
52	Jessica Cox	Ngaio	An individual	124
53	Brock	Island Bay	An individual	126
54	Francis Cuncannon	Kelburn	An individual	129
55	Dan Perkins	Newtown	An individual	131
56	Ella	Northland	An individual	133
57	Dan	Aro Valley	An individual	135
58	Dan Byrne	Crofton Downs	An individual	137
59	Darren Young	Vogeltown	An individual	139
60	Raewyn Carmichael	Highbury	An individual	141
61	Dave Allan	Aro Valley	An individual	143
62	David	Vogeltown	An individual	145
63	David	Brooklyn	An individual	147
64	DAVID MCGILL	Aro Valley	An individual	150
65	Chris Owens	Te Aro	An individual	152
66	Jill Robinson	Kelburn	An individual	154
67	Don Holborow	Mount Victoria	An individual	156
68	Don Pettit	Aro Valley		158
69	Dylan Packman	Brooklyn	An individual	160
70	Zong Chen	Other	An individual	163

71	Echo	Aro Valley	An individual	165
72	Emily	Aro Valley	An individual	167
73	Emily Flowerday	Aro Valley	An individual	169
74	Emma Reid	Aro Valley	An individual	171
75	Erina Papp	Brooklyn	An individual	175
76	Felix	Brooklyn	An individual	177
77	Tommy Thomson	Ngaio	An individual	179
78	Kirsten	Island Bay	An individual	181
79	Fiona Gunter-Firth	Aro Valley	An individual	183
80	Fiona K	Northland	An individual	186
81	Fionnghuala Cuncannon	Kelburn	An individual	188
82	Fleur Rohleder		Fire and Emergency New Zealand	190
83	Felix Marwick	Karori	An individual	192
84	Frances Forsyth	Aro Valley	An individual	194
85	Frank Kelly	Aro Valley	An individual	197
86	Freddie Dillon	Aro Valley	An individual	199
87	Jane	Te Aro	An individual	201
88	Garth Bates	Aro Valley		203
89	Georgina preston	Kelburn	An individual	205
90	Giselle Bahr	Newtown	An individual	207
91	Greg Murison	Berhampore	An individual	209
92	Graeme Campbell	Aro Valley	An individual	211
93	Graeme	Newtown	An individual	213
94	Grant Clarke	Mount Cook	An individual	215
95	Annika Green	Aro Valley	An individual	217
96	Phil McCracken	Aro Valley	An individual	219
97	Hamish Gordon		An individual	221
98	Harvey Livschitz	Aro Valley	An individual	224
99	Charles Thompson	Aro Valley	An individual	226
100	Leoni Hawkins	Northland	An individual	228
101	Hayden Hockly	Aro Valley	An individual	230
102	Hayden ricketts	Aro Valley	An individual	232
103	Hector McLachlan	Mount Victoria	An individual	234
104	Helene Pohl	Aro Valley	An individual	236
105	Hiromi Beran	Karori	An individual	238
106	Khoi Phan	Mount Cook	An individual	240

107	Carl Howarth	Newtown	An individual	242
108	Tom Hutchison	Brooklyn	An individual	244
109	Iain Atkinson	Aro Valley	An individual	246
110	Ingrid Downey	Aro Valley	An individual	248
111	Inge Doig	Karori	An individual	252
112	Iona Woodward	Aro Valley	An individual	255
113	I. P.	Aro Valley	An individual	257
114	Mike Fee	Island Bay	An individual	259
115	Ben Carter	Te Aro	An individual	261
116	Jack	Kelburn	An individual	263
117	Jacqui	Highbury	An individual	265
118	Jadene Huff	Aro Valley	An individual	267
119	jh	Newtown		269
120	James Dunne	Karori	An individual	271
121	James Sullivan	Johnsonville	An individual	273
122	james	Brooklyn	An individual	275
123	James kinsella	Lyall Bay	An individual	277
124	Jane Brenan	Aro Valley	An individual	279
125	Jane O'Shea	Highbury	An individual	281
126	Janice Swanwick	Brooklyn	An individual	283
127	Jason	Aro Valley	An individual	285
128	Julia Stace	Aro Valley	An individual	287
129	John Creser	Wellington Central	An individual	289
130	Jeff Montgomery	Te Aro	An individual	291
131	Jenny Livschitz	Aro Valley	An individual	293
132	Jessie black	Kelburn	An individual	295
133	Jeff Flavell	Ngaio	An individual	297
134	Jonathan Markwick	Mount Cook	An individual	299
135	Joan Pettit		An individual	301
136	Jo Carter	Kelburn	An individual	303
137	Joel Cosgrove	Newtown		305
138	Jo Heaton	Highbury	An individual	307
139	John Newman	Highbury	An individual	310
140	George Sederis	Hataitai	An individual	312
141	john whitty	Aro Valley	An individual	314
142	Jonathan	Mount Victoria	An individual	316

143	J	Kelburn	An individual	318
144	Jo Tregidga	Brooklyn	An individual	320
145	Jo white	Aro Valley	An individual	322
146	John Sullivan	Aro Valley	An individual	324
147	John Beaglehole	Karori	An individual	326
148	Jules	Aro Valley	An individual	328
149	Kain	Mount Cook	An individual	330
150	Karl Tiefenbacher	Te Aro	An individual	332
151	Kate Clarke	Mount Cook	An individual	334
152	Keith Clement		An individual	336
153	Kelda Hains		An individual	338
154	Kelvin Aris	Other	An individual	340
155	duncan kenderdine	Kelburn	An individual	342
156	kev O'Donnell	Brooklyn		344
157	Catharine	Brooklyn	An individual	346
158	Kyle Schwartz	Aro Valley	An individual	349
159	Laura	Aro Valley	An individual	351
160	Liam O'Connor	Karori	An individual	354
161	Tim	Miramar	An individual	356
162	Louis	Aro Valley	An individual	358
163	Tristan Campbell	Aro Valley	An individual	360
164	Megan Chrystal	Aro Valley	An individual	362
165	Maree	Aro Valley	An individual	364
166	Maria Cassidy	Aro Valley	An individual	367
167	Magnus McCarthy	Aro Valley	An individual	369
168	Mark Wilson	Aro Valley	An individual	371
169	Matthew Brown	Kelburn	An individual	374
170	Matt Stevens	Aro Valley	An individual	376
171	Matt Shipman	Aro Valley	An individual	378
172	Max Fuhrer	Aro Valley	An individual	381
173	M Dommett		An individual	383
174	Me	Brooklyn	An individual	385
175	Mel Foot	Te Aro	An individual	387
176	Meredith	Aro Valley	An individual	389
177	Mike W	Wadestown	An individual	391
178	Mika Herval	Kelburn	An individual	393
179	M Millen	Aro Valley	An individual	395

180	Mark Johnston	Berhampore	An individual	397
181	Martin	Northland	An individual	399
182	Anonymous	Mount Cook	An individual	401
183	MVH		An individual	403
184	Amanda Shima	Highbury	An individual	405
185	Thomas	Miramar	An individual	407
186	Nadine Dodge	Aro Valley	An individual	409
187	Natalie Whiye	Aro Valley	An individual	411
188	Natalie Wilson			413
189	Bill Nelson	Highbury	An individual	415
190	Nicole	Aro Valley	An individual	417
191	Nick	Northland	An individual	419
192	Nicky	Aro Valley	An individual	421
193	Nicole Duckworth	Owhiro Bay	An individual	423
194	Nigel Charman	Aro Valley	An individual	425
195	Nigel Ramsay	Northland	An individual	427
196	Nikola Andic	Aro Valley	An individual	429
197	Nick	Aro Valley	An individual	432
198	joby	Wellington Central	An individual	434
199	O Peacock	Aro Valley	An individual	436
200	Olivia	Aro Valley	An individual	438
201	Mike Reid	Highbury	An individual	440
202	Pam Ramsay	Northland		442
203	Patrick	Island Bay	An individual	444
204	Paul Schrader	Vogeltown	An individual	446
205	Penny	Aro Valley	An individual	448
206	Peter shaw	Wilton	An individual	450
207	Phaedra Brice-Chen	Aro Valley	An individual	452
208	pippi	Aro Valley	An individual	454
209	Michael	Aro Valley	An individual	456
210	Ron Goudswaard	Aro Valley	An individual	458
211	Pip	Aro Valley	An individual	461
212	Peter Steven	Karori	An individual	463
213	Alan Platt	Wilton	An individual	465
214	Patrick McKenna	Northland	An individual	467
215	Liam Prince	Aro Valley	An individual	469

216	Patrick Lam	Kelburn	An individual	471
217	Peter Ramage	Other	An individual	473
218	Rachel Bisset	Aro Valley	An individual	475
219	Rachel Griffiths	Aro Valley	An individual	477
220	R Hurst		An individual	480
221	Ralph McCubbin Howell	Te Aro	An individual	483
222	John Wilson	Ngaio	An individual	486
223	Rebecca Day	Highbury	An individual	488
224	Jo	Aro Valley	An individual	490
225	Rex Collett	Brooklyn	An individual	492
226	Rhys Hayward	Northland	An individual	494
227	Rich H	Brooklyn	An individual	496
228	Richard Dobson	Brooklyn	An individual	498
229	Rory	Karori	An individual	500
230	Rosalie	Aro Valley	An individual	502
231	Michelle Rush	Ngaio	An individual	505
232	Sean Kells	Mount Victoria	An individual	507
233	Samuel irwin	Te Aro	An individual	509
234	Sarah Bennett	Highbury	An individual	511
235	Sarah	Brooklyn	An individual	513
236	S	Aro Valley	An individual	515
237	Sarah Jane Parton	Aro Valley	An individual	517
238	Sarah	Aro Valley	An individual	520
239	Sasha Vlassoff	Mount Cook	An individual	522
240	Sarah Martin	Aro Valley	An individual	525
241	Shaun Cornelius	Brooklyn	An individual	527
242	John Scrivener	Aro Valley	An individual	529
243	Shannen Petersen		An individual	531
244	Sheila Hart	Southgate	An individual	533
245	Sinny West	Kelburn	An individual	535
246	Sven Ericksen	Aro Valley	An individual	537
247	Steve Naismith	Brooklyn	An individual	539
248	Sonya Hogan	Aro Valley	An individual	541
249	Stefanie	Aro Valley	An individual	543
250	Steven Firth	Aro Valley	Scipio Firth Trust	545
251	Stuart Cunningham	Karori	An individual	547
252	Suraya Sidhu Singh	Other	An individual	549

253	Susan	Aro Valley	An individual	551
254	Taison	Khandallah	An individual	553
255	Lisa Thompson	Aro Valley	An individual	556
256	Emma Mayo	Newtown	An individual	558
257	Emily	Ngaio	An individual	560
258	Sarah Spencer		An individual	562
259	Thomas Adams	Northland		564
260	Tom O'Flaherty	Karori	An individual	566
261	Craig Anderson	Aro Valley	An individual	568
262	Tim	Highbury	An individual	570
263	Tim	Thorndon	An individual	572
264	Tish	Highbury	An individual	574
265	Tegan van der Peet	Wilton	An individual	576
266	Tom Brodie	Aro Valley	An individual	578
267	Thomas bisset	Aro Valley	An individual	580
268	Tom Dench	Aro Valley	An individual	582
269	Louis White	Aro Valley	An individual	584
270	Bill Viggers	Kelburn	An individual	586
271	Vivian Stephens	Aro Valley	An individual	588
272	Marcail Parkinson	Thorndon	VUWSA	590
273	Kelsey Lafferty	Newtown	An individual	592
274	Zachary	Aro Valley	An individual	595
275	Ben	Wellington Central	An individual	597
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277	Nathan Hall	Thorndon	An individual	600
278	Andrew Sussman	Hataitai	An individual	601
279	Abir Rubel	Newtown	An individual	602
280	Alan Heays	Aro Valley	An individual	603
281	Andrew Williams	Southgate	An individual	604
282	Alecia	Northland	An individual	605
283	Alessandro Dal Sasso	Aro Valley	An individual	606
284	Alex Saunders	Kelburn	An individual	607
285	Alex Gray	Aro Valley	An individual	608
286	Keys Family	Aro Valley	An individual	609
287	Alex Brocklehurst	Aro Valley	An individual	610
288	Alex Kinzett	Northland	An individual	611

289	Alice Coppard	Berhampore	An individual	612
290	Alison	Highbury	An individual	613
291	Alistair	Wilton	An individual	614
292	Alley	Aro Valley	An individual	615
293	Andrea Knox	Brooklyn	An individual	616
294	Andre Geldenhuis	Kelburn	An individual	617
295	Emma Barnes	Aro Valley	An individual	618
296	Gus		An individual	619
297	Anke	Karori	An individual	620
298	Anne Jacobsen	Te Aro	An individual	621
299	Alice Donnell	Kelburn	An individual	622
300	Anne Heins	Other	An individual	623
301	Ant Burt	Ngaio	An individual	624
302	Jane		An individual	625
303	A Stillaman	Kelburn	An individual	626
304	A. G.	Other	An individual	627
305	Brad	Crofton Downs	An individual	628
306	Ben and Charlotte Darlow		An individual	629
307	Ben Evans	Aro Valley	An individual	630
308	Benjamin Swale	Karori	An individual	631
309	Elliot Blyth	Newtown	An individual	632
310	Robert Clegg	Brooklyn	An individual	633
311	Luca	Other	An individual	634
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316	Andrew Calder	Brooklyn	An individual	639
317	Cam Macduff	Brooklyn	An individual	640
318	Cameron Trauger	Northland	An individual	641
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320	Laurence Diack	Thorndon	An individual	643
321	Catherine Hay	Brooklyn	An individual	644
322	Cathy Dean	Other	An individual	645
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324	Chelsea Grant	Other	An individual	647
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327	Chris	Khandallah	An individual	650
328	Chris Ford		Disabled Persons Assembly NZ	651
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340	Danyon Graham	Kelburn	An individual	663
341	David	Aro Valley	An individual	664
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350	Sarah	Te Aro	An individual	673
351	Dan	Aro Valley	An individual	674
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361	Esther Albon			684

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366	Fiona McCarthy	Karori	An individual	689
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373	G B	Ngaio	An individual	696
374	Grant Buchan		An individual	697
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383	Max Bloomfield	Aro Valley	An individual	706
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386	Alex Dyer		Cycle Wellington	709
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388	Helen Trustrum	Aro Valley	An individual	711
389	Irene Papp		An individual	712
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391	James E	Aro Valley	An individual	714
392	Joseph Fletcher	Aro Valley	An individual	715
393	Jane Campbell	Karori	An individual	716
394	Jacquie Bown	Brooklyn	An individual	717
395	Jake	Vogeltown	An individual	718
396	Jan Pearson	Kelburn	An individual	719
397	Helen Taylor	Khandallah	An individual	720
398	Janette Sherwood			721

399	Jinnie Potter			722
400	Jasper Kueppers	Brooklyn	An individual	723
401	Jacob Munz		An individual	724
402	Jed Soane	Brooklyn	An individual	725
403	Jeet	Aro Valley	An individual	726
404	Jeff Galt	Hataitai	An individual	727
405	Jessica Matcham	Ngaio	An individual	728
406	Jess	Aro Valley	An individual	729
407	Jez Weston	Brooklyn		730
408	Jill Ford	Newtown	An individual	731
409	Jamie	Aro Valley	An individual	732
410	Joanna		An individual	733
411	Joanne Davidson	Karori	An individual	734
412	Joanne Lentfer	Aro Valley	An individual	735
413	Joe Ballard		An individual	736
414	Johnny Haylock	Karori	An individual	737
415	Glenn Jones	Newtown	An individual	738
416	Jono Wood	Other	An individual	739
417	Josh	Kelburn	An individual	740
418	Judith	Aro Valley	An individual	741
419	Judith	Aro Valley	An individual	742
420	Julian Silver	Aro Valley	An individual	743
421	Justin	Melrose	An individual	744
422	Matthew Eden	Khandallah	An individual	745
423	Kahikatea morgan	Aro Valley	An individual	746
424	Kahu	Aro Valley	An individual	747
425	Karien Mallee		An individual	748
426	K Maclaren	Aro Valley	An individual	749
427	Kate	Brooklyn	An individual	750
428	Katherine Blow	Brooklyn	An individual	751
429	Kathryn Kelly	Highbury	An individual	752
430	Katie Carey	Aro Valley	An individual	753
431	Kenneth Dixon	Other	An individual	754
432	Khiry Hewitt	Karori	An individual	755
433	Kieran Lee	Newtown	An individual	756
434	Kiri	Miramar	An individual	757
435	Kirsten Malpas	Ngaio	An individual	758

436	Michael Flyger	Other	An individual	759
437	K McCaskill	Ngaio	An individual	760
438	Lachlan Ridley	Aro Valley	An individual	761
439	Lachie Philipson	Other	An individual	762
440	Lauren	Kilbirnie	An individual	763
441	Lachlan Patterson	Karori	An individual	764
442	leslie alldridge	Newlands	An individual	765
443	Liam O'Connor		An individual	766
444	Lindsay	Highbury	An individual	767
445	Trapped in garage	Kelburn	An individual	768
446	Xianglin Deng	Highbury	An individual	769
447	Lisa Nickson			770
448	Lisa Mutch	Aro Valley	An individual	771
449	Louis Schmitt	Karori	An individual	772
450	Lucy Weston-Taylor	Ngaio	An individual	773
451	Luke Holden	Te Aro	An individual	774
452	Maddalena Dal Sasso	Aro Valley	An individual	775
453	Simon	Mount Cook	An individual	776
454	Marcia Abell	Ngaio	An individual	777
455	Mark Ramsden	Khandallah	An individual	778
456	mary	Aro Valley	An individual	779
457	Mat	Johnsonville	An individual	780
458	Matt	Hataitai	An individual	781
459	Matthew	Highbury	An individual	782
460	Max	Aro Valley	An individual	783
461	Max Dickens		Bus and Coach Association	784
462	Finn	Aro Valley	An individual	785
463	Dan Barratt	Highbury	An individual	786
464	Matthew Bartlett	Aro Valley	An individual	787
465	Michael Riemann	Aro Valley	An individual	788
466	Michael Ingham	Aro Valley	An individual	789
467	Mike	Aro Valley	An individual	791
468	James	Berhampore	An individual	792
469	Matt McKillop	Mount Cook	An individual	793
470	Mike	Mount Victoria	An individual	794
471	Natasha	Northland		795
472	Neil	Berhampore	An individual	796

473	Nick Rinehart	Oriental Bay	An individual	797
474	Nicole Gaston	Aro Valley	An individual	798
475	Nico	Karori	An individual	799
476	Kathy and Nigel Hughes			800
477	Dale Mctavish		An individual	801
478	Nick Russ	Karori	An individual	802
479	Lynley	Wadestown	An individual	803
480	J	Aro Valley	An individual	804
481	Gabriel Pollard	Brooklyn	An individual	806
482	Andrew Maloney	Aro Valley	An individual	807
483	Odessa Owens	Aro Valley	An individual	808
484	Ollie	Kelburn	An individual	809
485	Olly	Hataitai	An individual	810
486	Owen Matthews	Vogeltown	An individual	811
487	Phil Kendon	Northland	An individual	812
488	Ruth Palmer	Highbury	An individual	813
489	Patricia	Highbury		814
490	Paul Gray	Karori	An individual	815
491	Paul Jones	Mount Victoria	An individual	816
492	Philippa Curthbert		NZAA	817
493	Peter de Boer	Karori	An individual	818
494	Peter Taylor	Other	An individual	819
495	Pam Fuller	Highbury	An individual	820
496	Raj PARBHU	Northland	An individual	821
497	Richard Taylor	Khandallah	An individual	822
498	Todd	Aro Valley	An individual	823
499	Rhedyn Law	Newtown	An individual	824
500	Richard Mansfield	Aro Valley	An individual	825
501	Richard McNamara	Highbury	An individual	826
502	Jim	Johnsonville	An individual	827
503	Geoff Ridley	Northland	An individual	828
504	Rachel Ridley	Northland	An individual	829
505	Robb	Karori	An individual	830
506	Rod Page	Island Bay	An individual	831
507	Roni Alder	Brooklyn	An individual	832
508	Divya Nathu	Hataitai	An individual	833
509	Simon	Karori	An individual	834

510	Ross Carter	Aro Valley	An individual	835
511	Roz Scott	Aro Valley	Tickadeeboo Insideout Design LTD	836
512	Roz	Kelburn	An individual	837
513	Ruby	Kelburn	An individual	838
514	Susannah	Highbury	An individual	839
515	Sam gilkison	Karori	An individual	840
516	Samuel Kempthorne	Brooklyn	An individual	841
517	Sara Bishop	Aro Valley	An individual	842
518	stuart gardyne	Roseneath	An individual	843
519	Simon O'Rorke	Aro Valley	An individual	844
520	Simon Planzer	Kilbirnie	An individual	845
521	Siobhan Oldale	Miramar	Abel Odor	846
522	Matt	Khandallah	An individual	847
523	Carolyn Hagenson	Aro Valley	An individual	848
524	Sophie Yeoman	Newtown		849
525	Stuart	Island Bay	An individual	850
526	Sarah	Brooklyn	An individual	851
527	Stephanie	Aro Valley	An individual	852
528	Steve Bielby	Other	An individual	853
529	Steve Mackle	Wadestown	An individual	854
530	Steve	Other	An individual	855
531	Steve	Hataitai	An individual	856
532	H Brunswick	Aro Valley	An individual	857
533	Howard	Aro Valley	An individual	858
534	Tessa	Kelburn	An individual	859
535	Lorraine Boennic	Seatoun	An individual	860
536	Helen Gear	Other	An individual	861
537	Regan Sayer	Karori	An individual	863
538	Tom Turton	Karori	An individual	864
539	Thomas	Aro Valley	An individual	865
540	Tara	Aro Valley		866
541	Trace	Highbury	An individual	867
542	Tristan	Mount Cook	An individual	868
543	Trudy	Aro Valley	An individual	869
544	P van Dyk	Aro Valley	An individual	870
545	Chandra Perera	Other	An individual	871

546	Vaughn Scott			872
547	V. Watson	Aro Valley	An individual	873
548	Eve	Kelburn	An individual	874
549	Warren Kaler	Ngaio	An individual	875
550	Will Miller	Newtown	An individual	876
551	Will Thomson	Newtown	An individual	877
552	Winston Cochrane			878
553	Nat	Aro Valley	An individual	879
554	Speedy	Aro Valley	An individual	880
555	Zoe	Brooklyn	An individual	881
556	Thomas McGrath			882
557	E Tillot			883
558	Maysie Pyatt			884
559	Robert Fisher			886
560	Ben Young			887
561	A Bramly			888

Feedback**1**

114170567571

NAME: Mike	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly oppose

Why do you think that?

I strongly oppose the plan and believe it's simply to resolve difficulties buses have using Aro st and Raroa rd. The loss of parking will negatively impact residents, local businesses and the community. The plan does little for pedestrian safety and increases risks for users of the park and walking through to Little Palmer.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Does very little to improve pedestrian safety and the bike lane is a convenient solution for what is primarily a bus lane

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Clearly biking is secondary to bus access as a result terms like sharrows and cycling barriers are used. Removal of carparking is a major problem for residents and will negatively impact their lives.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

The term "a quiet way to ride through to the city" is ridiculous! This is a footpath and it's illegal for bikes to ride on it. Encouraging people to ride along the path is dangerous for pedestrians. Why is there both a bike laneway on Aro St and the Aro Park / Little Palmer option.

What do you think of the timing of the proposed approach?

Why do you think that?

I strongly oppose the proposed plan to remove carparks and believe that the current bike lane plan is simply an excuse to have a bus lane.

Feedback**2**

114173887109

NAME: Doug	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

This is a good compromise allowing more space for biking and less space for free car storage. Please don't forget about the pedestrians as you are making these changes. We are the original low emission transport users.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

I don't mind the new cycle lane. Could you extend the car share parks to more than one provider? It seems strange to favor one company. Please consider protection for pedestrians from e-scooters, nothing here addresses this. Will bikes be separated from walkers in Aro Park and the small connecting laneway to Able Smith St? I've been hit by a bike here exiting the laneway at the Able Smith end. I think keeping the parking in the shop area is a good compromise, allowing more short stay rather than people just storing their car on the street.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

An uphill bike lane makes sense to me, you often see lines of cars stuck behind bikers so this would stop this. Please look at how narrow the footpath is between Entrance St and bus stop 7786. I walk this every day and you have to step aside if someone is carrying a bag it's that narrow. One trip up and you're on the road. This is a key pedestrian route that has been forgotten about with all the focus on bike lanes. Also crossing the entrance of Holloway road on foot requires you to cross three separate lines of traffic as so many cars incorrectly use the old bus turning lane which allows them to speed into Holloway road rather than slowing down. With a bike lane here this puts them at risk too. Please close off this disused lane.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Support but please show a side for people and a side for bikes / e-scooters. Consider a barrier at the north end of the laneway so bikes don't cross the Able Smith footpath at high speed and impact pedestrians.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Seems fine

Feedback**3**

114181243786

NAME: Adam Bennetts	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I am generally supportive, but I don't not think the impacts have been accurately determined.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I support the proposal. I would like to have seen the planters removed so cyclists don't need to bend into traffic. These are unsafe choke points today. A single raised pedestrian crossing really should not count as 'pedestrian improvements' in the title of documents. I suggest that the most used and therefore best pedestrian crossing to raise is the one at the shops. The negative impacts on pedestrians (more shared paths) is not included, this unfairly once again ignores pedestrians as the primary users of footpaths.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

I would like to support but I would not feel safe using it. The changes appear to still only suit very confident cyclists. There is not a consistent wide lane the entire way up Raroa. Some of the changes will introduce safety issues. If footpaths are going to increasingly become shared paths then instead of the uphill cycle lane on Raroa serious consideration should be given to widening the footpath and making this a shared uphill path. If Aro park becomes shared path because cyclists already use it (very poor justification in my opinion), then why not the same for Raroa Rd given many cyclists already ride up the footpath? As a pedestrian I have to give way to uphill cyclists/scooter riders. on nearly every pedestrian journey (at least twice a week). Bus Stop opposite 125 Raroa - this does not appear safe. It appears passengers alight onto a cycle lane. There is no footpath, merely steps up to the street above. I can't see how this is safe for cyclists or pedestrians. I recall that this bus stop was proposed when route 25 was changed, why was it not built then? Changes to Raroa Rd indicate improved efficiency for public transport without mentioning that this can only occur by removing obstacles (parked cars) that will result in increased speeds on a tight and windy road. Increased speeds is surely the opposite of other Council and Waka Kotahi changes? Slower speeds are safer. I believe Raroa Rd users will be less safe. Raroa Road near 140 was safer for pedestrians for a couple of months as additional cars parked on both sides of the roads and the choke point reduced speed of vehicles and made it much easier and safer to cross the road. Since then broken yellow lines were painted (non-notified?) and traffic speeds have increased again and it has become less safe to cross the road. There are sections of Raroa Road that do not appear to be getting broken yellow lines but due to the move of the centreline could not be used to park. Examples, from 141 Raroa to 151 in particular. The maps clearly show parked cars taking up the entirety of the modified lane. It is as simple as looking at the cover of https://www.transportprojects.org.nz/assets/Aro-Valley-connection/Aro-Valley-Connection_PMP_P2.pdf to see this. Further south of 151 Raroa should also be Broken Yellow lines all the way to the bottom of Raroa, currently not all is. Given 141 south is not yellow lines this implies these sections are counted as available parking? They should not be. Parking on Raroa Rd has been significantly undercounted. Single partial surveys at 9:00am and 4:00pm completely ignore those who leave for work before 9 and arrive back after 4. I consider this to be inadequate and has resulted in low quality stats that should NOT be used to indicate any level of impact. 30 cars are commonly parked between about 96 and 155. For much of Raroa south there are simply no side streets nearby. Where there are side streets they are already heavily used. I have concerns about the quality of the data reported in the document: "WCC Transitional Cycleway Parking Management Plan Part 2" Stage one raroa road south section 1 mentions 66 parking spaces removed. This is incorrect in as much as some counted "parking spaces" are never used because they are not space and would be very unsafe. This gives a misleading view on total spaces available. the included parking drawing suffers the same issue. Table 39, side streets are a lot further than 1-3 minutes from the 140 Raroa Rd area to Plunkett, Norna, or Mt Pleasant Rd. Table 40. I dispute that there are 121 parks on Raroa Rd after stage 1. If this includes Norna and mt pleasant it is misleading. The counts for norna and mt pleasant should be listed separately so those that live on Raroa can clearly see impact. experience is that norna and mt pleasant are frequently occupied. Parking in Mt pleasant would require two road crossing across an arterial route with increased traffic speeds without pedestrian crossings. Increasing road crossings isn't a safety improvement. Table 42. no side streets within 1-3 minutes of 140 Raroa. I dispute that parking will be easily found. Table 43 Occupancy rates are underestimated by overestimation of available spaces. I that the dispute net loss is correct. Table 38 has an incorrect street name. Is the data in the same line correct or for the wrong street? The appendices are not referenced. Page 71. This is an incorrect map of unrestricted parking in Raroa Rd. Page 82. same problem as Page 71. Page 80. Overestimates removed parking, but also overestimates available parking. no clear map showing remaining parking. There is no clear map showing remaining parking on Raroa rd. a map showing what is left would be very useful (see attached Raroa_Road images). I estimate that Raroa Road South has only 25 car spaces after

stage 1. (150m divided by 6 = 25). However there are frequently 30 cars parked between 93 and 153 (e.g. there are 28 on the night of 20/11). The level of impact on car parking on Raroa Road south will NOT be low. Decisions should not be made on such inadequate data. The current Holloway rd crossing used by pedestrians appears to become a bike lane (a shared space would not be painted green with a bike symbol?). It isn't clear where pedestrians are meant to cross Holloway after stage 1. The uphill cycle lane/path/shoulder is on part of the road that is frequently covered in debris (lots of fist sized rocks are common) from the hills above. This is not well maintained currently and none of the documentation indicates that the council will ensure the lane is cleared more frequently. Without clean lanes (especially narrow shoulders) cyclists will have to ride in the lane. Cycle shoulders are apparently a trial but this is not well covered. They are not bike lanes, I understand that vehicles can and should use them as necessary to avoid oncoming traffic but there will be a significant speed differential on Raroa road heading uphill. Potentially 40km/h. This introduces an obvious safety issue. As well introducing confusion for drivers as to which green paint can be driven on and which can't. 750mm wide is NOT a bike lane and shouldn't be advertised as such. This is narrower than the handlebars of my bike (770mm). I suspect cyclists that haven't read the design in detail are going to be quite surprised by the inadequacy of the uphill lane. The broken yellow lines south of 133 have already been done, this suggests the design is out of date and further invalidates parking data.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Little Palmer St. This has blind corners at each end and I already find cyclists that illegally use this don't slow down for pedestrians now. As both ends have visibility blocked by private walls I don't see how this can be remedied. That cyclists already 'rat run' Little Palmer St is not sufficient reason to make it a shared path. Some footpaths should remain for pedestrians. If cyclists wish to cycle into the CBD the council should install a bike lane down Willis St and connect bike lanes properly. Aro Park should not become a throughfare for cyclists, this is an unreasonable imposition on pedestrians. No cycling signs should be painted on the foot path and cyclists dismount signs installed. If a shared path must go in use the 'driveway' on the eastern edge, not the path through the middle. Joining at the garage project driveway is unsafe. cyclists ride into the driveway on an angle that may not be seen by drivers turning right into garage project and are obscured by the toilets for vehicles turning left. Footpaths should be for pedestrians first, especially heavily used footpaths. To suggest that this design provides pedestrian improvements is disingenuous.

What do you think of the timing of the proposed approach?

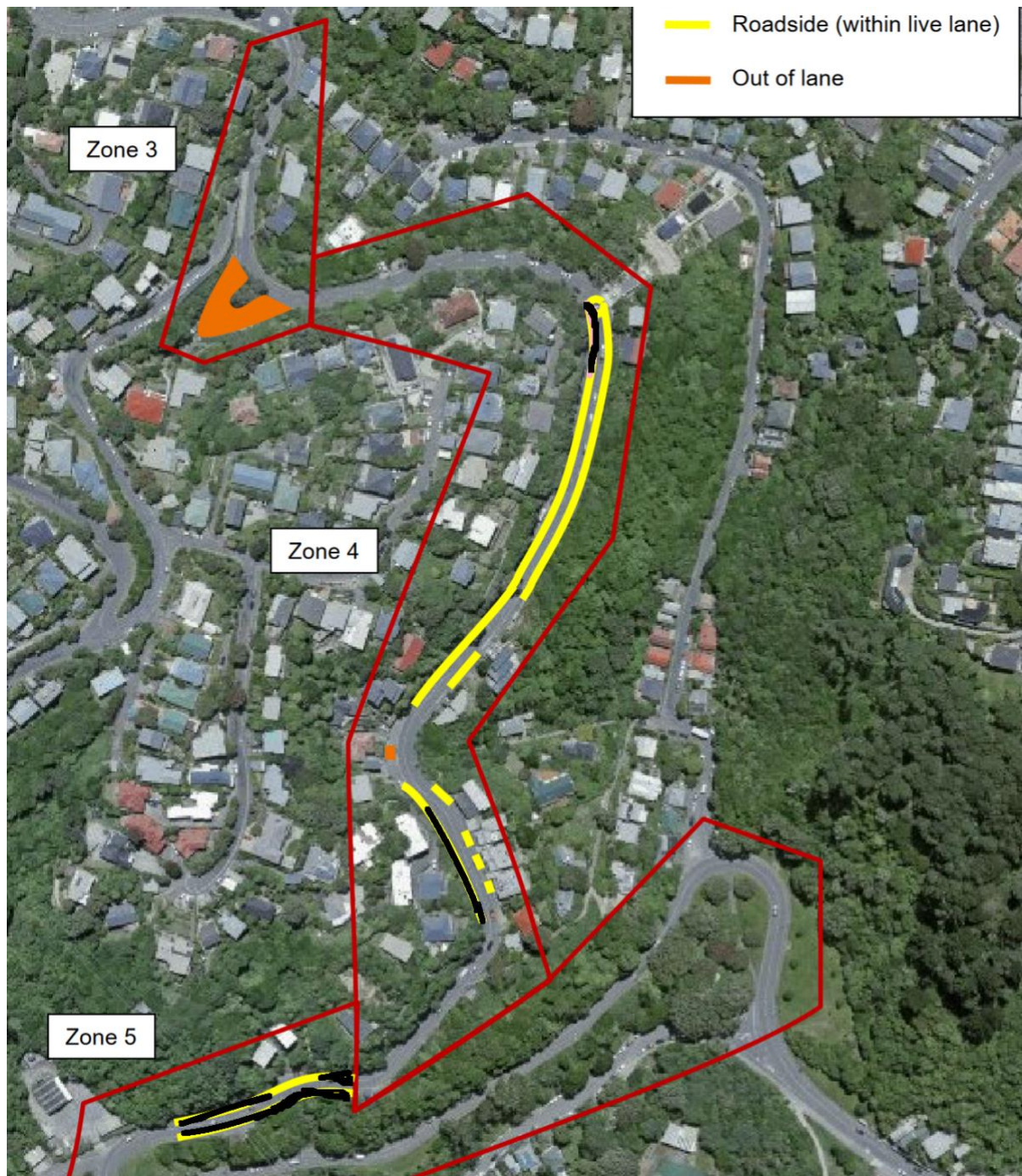
Make the proposed changes more quickly

Why do you think that?

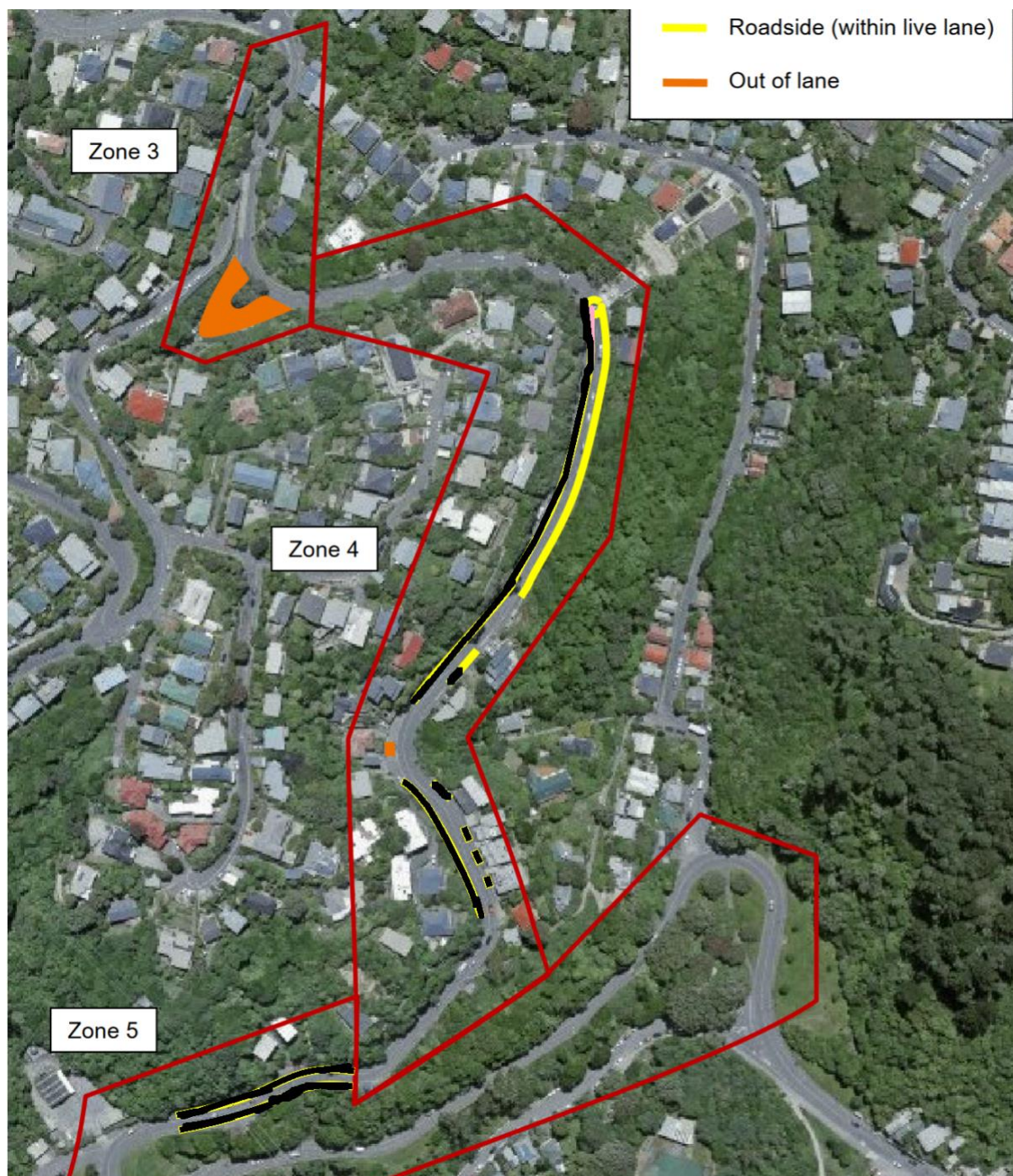
If you're going to do it, do it quickly. Rip the band aid off. If this is completed piecemeal the numbers of cyclists using it won't increase which will be used as justification that it is not needed.

Parking_maps_and_additional_comments.docx

Actual parking available in Raroa Road, I removed the areas that cannot be parked in and are not parked in:



Actual parking available after parking removed. Downhill parking will not be as Council documents indicate:



Additional comments:

I found submitting an interesting process and have the following additional comments:

1. The information is inaccessible to many. The files are large and unreadable without decent computer equipment. Printed material has not been made easily available (e.g. at libraries). The

timeframe is short given the large volume of information to read. This is unacceptable given the demographics of the area.

2. I attended the Aro Valley morning event. I didn't consider this a professional well run consultation event at all. Questions I had could not be answered. I would not bother attending another.

3. Imagery in <https://www.transportprojects.org.nz/assets/Aro-Valley-connection/TR179-22-Aro-Valley-Transitional-Cycleway.pdf> is very low quality. It is not possible to read some writing on some of the road maps.

4. <https://www.transportprojects.org.nz/assets/Aro-Valley-connection/Aro-Valley-connection-designs-Roadmarking.pdf> renders extremely slowly. This could have been split up if you wanted to make it easier for people to view.

5. Format of documents is archaic. where is an online map view of parking changes rather than low quality screenshots with coloured lines embedded in documents?

6. There is no mention of nor consideration of the demographics of the area. Although I and some of my immediate neighbours own at most one car and use off street parking there are plenty of large houses with many residents who individually own and require cars for work. The demographics of the area will not change quickly if at all. This is a high student / young working flat areas and is like to continue to require off street parking simply due to make up of households. More independent adults in a single dwelling equals more vehicles.

7. I can only upload one file? It is 2022. Why do I have to jump through hoops? Not good enough at all. Further restricts who can submit.

Feedback**4**

114177331584

NAME: Keith Davies	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Yes but they should be more bold (cycle lane both sides) and delivered much faster piece mealting it is putting lives on the line.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Would be great to take this further putting a lane on both sides of the road why should cyclists heading into town put there lives on the line as well. Also why is the shopping area exempt? Once again cyclists forced to risk their lives. Are the businesses rates going to increase to accommodate this cycle way free zone for them considering the cafes and driveways they have already take up much of the footpath a cycleway should be fine.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Once again remove more carparks and put a lane on the downhill side as well. Cyclist are asked to risk their lives again for the convenience of storing private motor vehicles.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Widening the paths would be great I walk this route too and from work most days and sometimes its a real squeeze. Would the drainage problem with the Aro Park path be sorted out at this time as well? Someone has cut trenches to stop the path being blocked by the puddle that pulls in the low spot but it is sometimes still hard to navigate after substantial rain. I would also suggest widening the footpath on the North side of Raroa Rd bordering the Pukehinau Kiwifoot Reserve. This section is very narrow only one pedestrian can really occupy it and passing anyone is just about impossible I dread to think how someone who needs a mobility id would navigate this footpath with multiple other users on it. Now with the bush track up to Kelburn having an entrance on this section of footpath I can see it becoming used more.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

We need more cycle lanes much quicker to encourage people to move away from cars. I am a walker but would love to cycle but am very scared to try as the lack of cycle lanes with physical barriers and the behavior of motor vehicle drivers worries me a lot. I cant see why it takes so long for this stuff to happen and bowing to the pressure of anyone who makes a negative fuss about them before they are even installed makes me feel like cyclist or non motor vehicle users are 2nd class citizens.

Feedback

5

114166786591

NAME: Adam	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**6**

114178408493

NAME: Richard Rust	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly oppose**Why do you think that?**

Strongly disagree - Aro residents deserve to have parking if they want a vehicle. You have not considered the cars from out of the area who park around Aro and walk to work, or the students who park in the area and walk to university. Many of our local streets are clogged with cars belonging to people who do not live here. Suggest you could concentrate on this issue. Your plans leave no room for locals, trade vehicles, district nurses, friends and family. Do not consider a bike lane will solve any problems and will cause major disruption to all in the area with the removal of parks.

Do you support the proposed speed changes on Aro Street?**Strongly oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Oppose your plan to eliminate car parks for residents in Aro Street which will cause them to move their vehicles to surrounding streets. Strongly oppose a cycle lane in Aro St - do not believe the number of cyclists warrant removing residents carparks.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose**

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Very negative

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Thought bikes were on the roads? There is a driveway to Aro Hall which can be used by bikes. The path is good now for pedestrians.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I disagree with the bike lane so hope that by delaying the changes a more sensible approach that takes into consideration residents will prevail.

Feedback**7**

114160136847

NAME: Alastair Stewart	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I support the plans, but would strongly ask the council to consider extending the current coupon parking zone that ends at plunket St to include Raroa Road to Raroa Crescent. As a resident, it is plain to see the significant number of non-resident commuters who park here for free - volumes of parked cars drop significantly on weekends and during the lock downs.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

There is a narrow pinch point at the corner of the basketball court by the kind that would be nice to widen too.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback

8

114171697043

NAME: Emma Alcock	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Negative
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Oppose	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**9**

114175541441

NAME: Alex Gray	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I do not consider Council has demonstrated the need for the proposed changes compared to the negative effects they will have on businesses and residents.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I support extending the 30km/h speed limit through to Willis Street and removing the bus stop outside 47b Aro Street. As a cyclist I am opposed to the uphill bike lane as there are only 147 cyclists per day and the removal of parking on the uphill side of Aro Street will cause considerable inconvenience to residents who own cars who are unable to park off the street. If Council wants to remove car parks here they should provide alternative parking close by. Providing resident car parks in Holloway Road does not meet that criteria. I oppose the raised platform at the pedestrian crossing as this will cause discomfort to bus passengers. Speed cushions will slow down cars but not bikes, trucks or buses going downhill who can avoid the cushions.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

As a cyclist I am strongly opposed to the proposed uphill cycle lane. The main reasons for this are that Raroa Road is a windy narrow road and there is not enough width for a separate cycle lane. This is confirmed by page 11 of the 90% design audit which listed 7 corners where a bus or commercial vehicle would have to use the cycle lane to avoid crossing the road centre-line. Consequently the cycle lane can only be marked with audio tactile markers and is not separated from traffic. This may give cyclists a false sense that this is a safe route-- it is not. I would never cycle up Raroa Road not now or if a cycle lane was added. Instead I prefer to ride up Kelburn Parade and Upland Road which are generally much wider with no sharp corners.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**People using Aro Park for recreation (e.g. picnicking)?**

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?**Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?****Oppose****Do you have any comments to make about the proposed design?**

2.5 metre wide shared path is not really wide enough for cyclists and pedestrians. Cyclists should stay on the road.

What do you think of the timing of the proposed approach?**Don't know****Why do you think that?**

As I oppose most of the changes this question is irrelevant to me.

Wellington

Google

Street View - Jun 2021



Google



Figure 2.4-1: Cycle lane on inside of curve, Raroa Road

Locations of particular concern include the curves in the vicinity of:

- 150 Raroa Road
- 146 Raora Road
- 82 Raroa Road
- 44 Raroa Road
- 2 Cluny Avenue
- 6 Raroa Road (see Figure 2.4-1)
- 1 Moana Road

Motorists cutting the corner into the cycle lane will be a frequent occurrence, and while most motorists will adjust their course if a cyclist is present, crashes may still result occasionally. Such crashes are likely to result in serious injury to the cyclist.

This issue was raised in the 30% CASA, with the recommendation to install either flexiposts or audio-tactile profiled (ATP) line-marking leading up to and around a cycle lane on the inside of a curve. The designer noted that buses would need to encroach on the cycle lane around the corners. The decision from the safety engineer and client was to install ATP, but these have not been indicated on the plans.

Recommendations

2.4.1	Install audio-tactile profiled (ATP) line-marking leading up to and around a cycle lane on the inside of a curve.
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Responses

Designer	ATP to be added to the drawing package at locations identified above.
Safety Engineer	Agree with CAT and Designer.
Client	Agree.

Feedback**10**

114161504789

NAME: Alison Jones	SUBURB: Highbury	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Yes too many cars on Raroa Road now, and is dangerous for pedestrians crossing the road now. This will make it better for cyclists and bus users. Alison Highbury

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support**Do you have any comments to make about the proposed design?**

No

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

I am an ex-cyclist and high frequency bus user and its very narrow presently for buses to pass each other on the narrow road with cars parked on both sides. This may make me cycle again, but I can also cycle down from Mt Pleasant/Raroa Rd corner.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I think the proposed widening is good and will not impact unduly on Aro Park users also Little Palmer St.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

a query on residents car parking in Holloway Road is that 68 total residents car parks in total?

Feedback**11**

114180482623

NAME: Andrew Bartlett	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This is a tricky set of compromises right on the bare minimum of what is required, but I do support. I wish parking was on the up-hill side, against the uphill lane rather than being a downhill dooring risk with the sharrows. Ideally there would be more separation (and not just paint), particularly for the sections where the lane narrows to 75cm including the gutter.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Parking changes on the south side of Aro Street rolled out over two years should be rolled out faster, slow phased changes just prolongs the pain for all involved. The parking that remains should be on the uphill side, next to the bike lane to remove the dooring risk to users sharing the lane with downhill traffic.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

The bus stops should not be in cycle lane, but in the traffic lane with red paint on the cycle lane, and a build-up if practical. It is hard to tell, but these locations look like the bus couldn't pull to a curb and footpath anyway, so there isn't the need to have flat boarding and bus passengers can just cross the cycle lane at road level. I'm most concerned however about the thin cycle space (not a full lane). Experience tells us that cars will pass a cyclist: - at the same distance as given to the left gutter - at the edge of any marked lane. This thin dashed lane could well be quite unsafe. I suggest a second line on the road side of this that pushes traffic further to the middle of the road, so you have two dashed lines, the one cycles should say in and one that cars should stay in (except for large oncoming traffic).

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Don't know

People walking on the paths through Aro Park and Little Palmer Street?

Don't know

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

An innovative response to real-world desire lines. Well done!

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think the phased approach just prolongs the pain for all involved. The cycle connection as far as the Aro Park path should be in the first stage.

Feedback**12**

114181163351

NAME: Andy Bradwell	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Neutral**Why do you think that?**

At either end of the cycle path in question there is no connecting to another cycle lane so you could ask what's the point, in fact either end is dangerous. Will this plan encourage cyclists to go faster down hill will if so it will lead to accidents in bad weather.. The road in question has limited space and nothing can change that so options are very little. Residents and visitors will just park in neighbouring roads just moving the problem we have today. When the weather is bad will people cycle or just revert back to cars. I am in favour of making the road safer and less cars hence I suggest residents parking is first action but not sure this plan will do the job feels like a plan for a few people who don't live in this street and those that do will be disadvantaged.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Neutral****Do you have any comments to make about the proposed design?**

Im not sure if plan been thought threw in detail. survey of cars on the streets was done in May so for Raroa road the volume of cars parked would be lower than normal with University shut.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Oppose**

Do you have any comments to make about the proposed design?

I live in Raroa Raod Id be happy to pay for residents parking and think this shoud be done first to reduce number of cars in the street. All i can see happening is people will move their cars intro neighboruing streets and block them up and camplaints from residents there eg Moana St or Cluny St. Many park in Raroa Raod and go to the University I am a cyclist and even with these proposed changes i would not cycle that road its just too narrow a road and nothing you can do can change that with the topography there also at the bottom you connect to Willis St which has no cycle path and at the top you have to get across that busy road/intersection leading to karori. so it feels an orphan. Have you thought about making the cycle lane divert onto Plunket to get easier access to the road to Karori via tunnel

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**People using Aro Park for recreation (e.g. picnicking)?**

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?**Do you have any comments to make about the proposed design?**

Think you need ctycle lane once in Willis St

What do you think of the timing of the proposed approach?**Don't know****Why do you think that?**

For us in Raroa thsi plan will create a real issue, We just have one car to get to visit elderly family and friends we take the bus or walk into city for work I dont see action that will reduce cars on street and it will be impossible to find a park its tough now so reducing the parks will make it near on impossible. Probably think about leaving area which is a shame as been here for over 15 years I support making the bus trip easier but doing all this for scooters I dont agree. I find it hard to agree with all plan, the only people who will benefit will be cyclists living in Karori, residents on these streets will have little to benefit. Im not convinced you will see a massive increase in cyclists biking down that hill. In the rain its dangerous you will get people going faster and i sure it will cause an accident or two

Feedback**13**

114164319742

NAME: Anita Easton	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?**

I would prefer a proper bike lane all the way up, including all the corners. One of the things that makes the uphill ride tricky at the moment is gravel washed down from the hillside. More street sweeping will make this change much more effective :)

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Do the full Aro St bike lane in one tranche. It's really needed! I have ridden through there with a not very confident older child, and sharing with traffic and parks was pretty scary for her.

Feedback**14**

114174329455

NAME: Anna Delany	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**15**

114179602907

NAME: Anna Sisley	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Will still be a very unsafe hill to ride a bike up even with cycle lane with huge negative impact on parking for residents

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Don't know

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Serious concerns re loss of parking on Raroa Road all the way up the hill and the loss of all parking at the area between Plunket St and Cluny Ave with the bus stop moves. Support moving bus stops for safety. Concern re visibility from my garage at 44 Raroa with bikes feeling more confident at being in a lane. Concern at the narrowness of the winding Raroa Rd hill area and that with no parking one side and cycle lane the other, the road is still too narrow for improved safety of

cyclists at the loss of so much parking. No where for cars to park. Bus service unreliable all over Wellington at this point in time

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Don't know

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

I don't think it is a good idea at all

Feedback**16**

114161623037

NAME: AnneMarie Curtis	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Balanced and practical proposal, good work!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Balanced and practical, well done! Not sure about stopping the cycle lane through the shops though, could cause confusion and be unsafe for cyclists. Speed bumps are a pain for cyclists, maybe build them so they're flat where cyclists ride?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Will be a significant improvement.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Great idea.	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
The proposed timing will give the Aro Valley community time to adjust.

Feedback**17**

114175804517

NAME: Ann Clark	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose**Why do you think that?**

I think more work is required to understand the needs of the older and less mobile members of the community. Having read the impact on the disability community I despair at the analysis

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose**Do you have any comments to make about the proposed design?**

I do not support the removal of bus stop, I don't support City hop only for carshare I support extension of 30 kph but from Durham St to Willis St. I support more mobility parks. I expect a rationing of residents parking permits to one per rateable unit

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support**Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

We already have courier drives driving through, deliver easy motorbikes and other motorised vehicles this will just make it worse. i was told by a pedal cyclist to get off the Palmer st connection as I was a fucking witch and in his way, we have one cyclist who rides his bike and walks the dog at same time totally blocking path. So when are the cycling community going to learn some manners and realise that not all of us have all our faculties and that some need more time to react.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I cannot see how you are going to manage down parking, I am a former car owner who could never find a park, car now sold but I am alone in this. Public transport and City hop or other car share do not support my needs, so when will they

Feedback**18**

114174947508

NAME: Antony Paltridge	SUBURB: Wellington Central	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It should happen faster

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

It should happen faster

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

It should happen faster

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
It should happen faster	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It should happen faster

Feedback**19**

114172405072

NAME: Joe bloggs	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

As per my earlier comments.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Ridiculous and totally unworkable- another Island Bay

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose****Do you have any comments to make about the proposed design?**

A difficult narrow road with hardly any residents parking made even narrower. Crazy stuff driven by ideology and special interest groups not a real desire to listen to the people.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Why do you think that?
Don't make the changes- listen to what the silent majority actually think other than your own echo chamber

Feedback**20**

114181696640

NAME: Arran Whiteford	SUBURB:	ON BEHALF OF: WCC Environmental Reference Group	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Let's reclaim Aro Street for people. It's currently pretty awesome, but also clogged with cars.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

The proposed changes will greatly improve cycling and transport in the area. Pending local support, there should be an option to accelerate the roll out of the three stages. Long term, we should be looking for cycling options safe enough for children to use, this design is a massive improvement, but still clearly not safe enough for children. The public generally do not understand what a sharrows is, you should educate the public or replace the sharrows sign with 'take the lane' or better alternative.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

The proposals will make cycling in the area massively more appealing. Improvements: Bus stops should block the lane of uphill traffic to prevent dangerous passing. The council must listen closely to feedback on the 'cycle shoulder', we are skeptical about this design - equivalent designs result in bike being cut off by wide cars.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

We should first trust that bikes will recognise this area is a slow area, and if that doesn't work, later move to slow bikes.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The aro valley community is strongly supportive of active transport. A faster transition offers certainty to those affected.

Feedback**21**

114174187059

NAME: Arran Whiteford	SUBURB: Melrose	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We need two way cycleways so that kids can bike to school, and so that downhill bikes don't have to wait in the traffic jams caused by cars every morning.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Generally great improvement, strongly support! The downhill side needs a bike lane too, as it's gridlock every morning. The current design forces bikes to wait in car traffic. We need to give people incentive to get out of their cars and bike. Noone understands sharrows. If you want to use a sharrow sign, you must first educate the public as to what they mean. Alternatively you could use something more intuitive e.g. 'bikes take the lane' I have quizzed many of my cycling friends and noone knew what they meant. I bike this route a lot very frequently have drivers tooting at me for taking the lane - the new uphill cycle way will make me feel much safer.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support**

Do you have any comments to make about the proposed design?

The uphill bike lane must have physical dividers - like the brooklyn one. I ride the Point Jenningham bike lane daily, and cars always cross into the bike lane on corners.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?**What do you think of the timing of the proposed approach?**

Make the proposed changes more quickly

Why do you think that?

Aro street does not need or want to be car-centric! Aro has the least commuting by private car in NZ! Aro is ready for large change now. Parked cars add nothing to the area.

Feedback**22**

114160296909

NAME: Asher Regan	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is a busy narrow road with a lot of bike traffic. Too many close calls.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Why does it need to be done over 3 years? Can it not be done faster?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

I would like to see the 30kph zone extended up raroa rd as well

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
I think most people using little Palmer continue through inverlochy and oak park rather than going down able smith. Getting through from Inverlochy to oak park has a major narrow pinch point that could be improved.	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The road is dangerous to ride now. It needs these changes asap.

Feedback**23**

114181252343

NAME: Athol McCredie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Traffic can go very fast up and down Aro St but it is a narrow road and there is a lot going on around the village area, so vehicles need to be slowed and separation created from them for cyclists.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Overall I think it is great. This is a very difficult area for cycling but there are also a lot of people who cycle through it.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

The blind corners are what worry me most as a cyclist because you never know when a vehicle behind you is going to come round it close to the bank.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

I support widening the Aro Park path to the proposed 2.5m, provided it is better drained than the current one which ends up as a big puddle in the middle, with muddy ground either side, in winter. The exit from Aro Park to Palmer St needs to be better handled for cyclists. It is too narrow for both cycles and people to go through at once and cyclists don't have a way to get onto the street without first going on the footpath past the Aston Towers drive, which has resulted in near misses with cars and pedestrians due to lack of visibility into the drive. And I am concerned about Little Palmer St. I am very strongly against cutting down any trees to enable the path to be widened there. The other issue here is at the Abel Smith St end where there are many near misses between cyclists and pedestrians due to 2 poorly placed power poles and vegetation at the corner. It is a blind turn here and cyclists have to turn sharply to avoid the pole on the Abel St footpath. A route straight onto Abel Smith St would be ideal but would mean the loss of a carpark. Widening the exit onto the Abel Smith St footpath from Little Palmer St would help reduce the risk of collisions here, but first you would need to shift the two electrical service boxes.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**24**

114171879810

NAME:	SUBURB:	ON BEHALF OF:	ORAL
	Karori	An individual	No

General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

While this plan leaves a real lot desired, it's better than the current state.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

The safety report says "The proposed treatment for the Aro Valley route, whilst being an improvement on the existing situation, is only expected to attract cyclists of the "strong and fearless" or "enthused and confident" categories." This does not help kids ride to school, or their grandparents. As such this falls far short of what the climate emergency requires. A downhill lane is sorely required. Only a small fraction of people on bikes -- like myself -- will ride with the cars. None of my family will, for example. Prioritising parking goes against WCC's own priorities (pedestrian > person on bike > driver). Cutting the cycle lane off through the shops is dangerous and weird. People shop -- cars don't. Prioritising parking goes against WCC's own priorities (pedestrian > person on bike > driver). At the very minimum, a 30kph limit with a handful of speed cameras is required all along Aro if you don't want to build cycle lanes both ways. Sharrows are sh*t. There are buildouts and planters at 49, 72 and 73. Seems people on bikes will just disappear for a couple of seconds (sarcasm -- these are really really bad.)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Sharrows are sh*t. When a planner uses sharrows, they've handed in their card. Some of the lower bits are OK, like legalising the currently common footpath cycling up Raroa. The cycle shoulder reduces down to 71cm where 78cm handlebars are the norm. How you think a 2.5m bus overtakes a person on a bike with the safe 1.5m is beyond... anyone but the planners. The safety audit agrees: "This would be likely to result in serious injury to the cyclists, and is expected to occur occasionally." So you are here deciding to seriously injure cyclists. This is not acceptable in any way or form. WRT to Raroa the audit says: Motorists cutting the corner into the cycle lane will be a frequent occurrence, and while most motorists will adjust their course if a cyclist is present, crashes may still result occasionally. Such crashes are likely to result in serious injury to the cyclist. This issue was raised in the 30% CASA, with the recommendation to install either flexiposts or audio-tactile profiled (ATP) line-marking leading up to and around a cycle lane on the inside of a curve. The designer noted that buses would need to encroach on the cycle lane around the corners. The decision from the safety engineer and client was to install ATP, but these have not been indicated on the plans. Completely unacceptable. Of course many people on bikes will continue to illegally ride on the footpath. A 30kph limit is absolutely required on Raroa anyway, with a handful of speed cameras.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Three years for just a few km is weird. Aro Valley is not that special. Apparently you want to appease car owners. This is an illusion. You're risking to deliver bad looking intermediate work, then having things cut short by a possibly unsupportive new council. Please just do it all very quickly.

Feedback**25**

114173593100

NAME: Ben Jeffares	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**26**

114160221272

NAME: Ben Sandle	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Making major routes safer on bikes should help encourage more people to ride bikes over taking car

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support**Do you have any comments to make about the proposed design?**

Aro valley is narrow currently with parking on both sides, removing parking for bike lane would make much safer when cycling and should give cars and buses more space. Residents might not like reduced parking though

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support**Do you have any comments to make about the proposed design?**

Will the bike lane be protected from traffic? Currently some cars don't give much space around corners pushing cyclists close to the bank. Removing all uphill parking should make much safer

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

The connection through to little Palmer street is currently narrow and can mean pedestrians block bikes or if two come in opposite directions not much space to pass, widening of the path would be great improvement as it's much nicer and safer route on bike than down to the busy Willis street intersection

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The faster you can make the changes the better as would encourage more people to ride bikes over using cars

Feedback**27**

114181553570

NAME: Tim Bollinger	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

The bus stop outside 47b Aro Street - the aro cafe, in the middle of the shopping centre should remain, as it is central and logical, and VERY well utilised. Keep it please. It is the heartbeat of the village. And surely the shop owners that are worried about losing parking spaces for cycleways and public transport, want people to gather there and not further up or down the street. There also needs to be a continuous cycle lane on the North side of Aro street all the way to the Willis Street intersection, instead of deliberately sending bicycles down the middle of the park. This is a footpath - not a road. While currently, a lot of bike traffic carries on through the park, this is simply because it is a friendly community shared space. It was never designed to be THE dedicated route from Aro Valley to the city - simply an alternative route. As cycleways were invented to separate cycles safely from cars, and because the purpose of this exercise is to manage traffic, traffic managers should not be allowed to designate traffic directions through areas that are not their responsibility. It is a park, not a road. Babies should be able to safely sit on this pathway. It should not become a freeway for bicycles. That is the place for a road - which should be able to be shared by cars and bicycles alike. What is lovely about the whole Aro Valley Community Centre

area is that it is indeed a shared space. As a result the pathway is often, and should often be, interrupted by fairs, concerts, and other social activities that happen in the middle of the park, which the pathway runs through. It is not a roadway. In fact, a bollard at the West end of the park, missing for some years, should be restored to prevent vans and maintenance vehicles pulling up into the grass and blocking the whole area for the community - which they now regularly do.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

These are naturally evolved shared spaces and should not be 'redesigned' by top-down traffic engineers for their own transport benefits. If the Council wants to develop little country byways within the city - and I agree there should be more like this - then, turn some of the roads into pedestrian and cycleways instead. Not turn the ones that we already have into roads by proxy, for the benefit of cars. One of the biggest problems with the Little Palmer Street access way is that over the years trees have died and had to be removed from their roots being damaged by sort of industrial pathway 'maintenance' that widening suggests. I recommend instead that streets like Abel Smith Street be narrowed to prevent cars speeding up it after coming directly off Highway 1 up Karo Drive. We could grow more trees along the side, create a new pedestrian crossing at the top of the street, and make it safer for school children who have to cross that street twice each day to get to and from Te Aro school. While on the subject of traffic management, there should also be yellow cross hatching markings on Willis street at the entrance to Palmer Street, where the four lanes of one-way traffic backs up so much that it is often impossible for drivers to exit safely.

What do you think of the timing of the proposed approach?

Why do you think that?

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Feedback**28**

114160247086

NAME: Brendon	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I am very excited about this. Currently I ride this route 2-3 times a week and I am terrified that someone will open a car door on me and I will be knocked off and run over. Through the worst sections I ride on the footpath if there are no pedestrians. These changes will mean I won't be forced to break the law for my own safety.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

This is very exciting. I love walking/cycling through cities where you don't have to share the route with cars by walking alongside roads.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I can understand this to prevent blowback but please remember that cyclist lives are in danger in the mean time. Please proceed with cycleways as soon as you can.

Feedback**29**

114160330168

NAME: Brett Halkett	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**30**

114160250777

NAME: Bridget	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Yes, lots of people bike around here already due to the reserve, and Aro Valley is so close to the city that making it easier for more people to bike safely and have to rely on cars less is a great idea. There are also many students and kids in the area. More people biking means less people driving cars on the roads, so this benefits drivers too, and even if nobody used it (which I highly doubt), it is not that hard or expensive to revert back to road space for driving/parking, especially compared to the cost of maintaining roads that cars and other heavy vehicles use. Aro Valley is a beautiful place to bike with all the birds and trees around too! If we don't provide alternative transport solutions to cars, Aro St and the rest of Wellington will only get more congested.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Having a bike lane (and a lower speed limit on aro st) will be fantastic, it is a narrow road so this will really make things safer for bikers. I do not currently bike but would like to, and having more dedicated bike lanes in the city would make me feel much safer and encouraged to start biking in town. I have seen cars honk/yell at cyclists (particularly uphill aro st/raroa rd), and also a few cyclists on the narrow footpaths, so having a dedicated lane is much needed I think. I currently live in Aro Valley.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Yes, I frequently use these paths to walk. It seems like a non-disruptive change with positive outcomes to everyone.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Generally I think it's great and a good timeline, although it would be nice if the stage 3 part of the bike lane was done sooner, as there is busy traffic/intersections down there- though there isn't much bike infrastructure there already, ideally it would coincide with improvements to Willis St (such as a dedicated bike lane). It's not a big deal though (in terms of the timeline).

Feedback**31**

114171725983

NAME: Bruce Herron	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Any shared footpath with pedestrians has a white line in the middle separating cyclists and pedestrians.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Any where there is a shared footpath with pedestrians a white line is painted down the middle with cyclists on one side and pedestrians on the other. Intermingling is a accident waiting to happen. I have ridden in many cities in Europe where this is done and it works really well

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Any shared foot path has a white line separating cyclists from pedestrians

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

There is a white line in the middle of the footpath separating cyclists and pedestrians. Have lives in cities where this is done and it works really well

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**32**

114161319825

NAME: Eric	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Great improvements to make Wellington a more pleasant place to live.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Installing the bike lane in a disconnect manner is counter productive to inducing usage

Feedback**33**

114169216176

NAME: Barbara wheeler	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This area is long overdue for road user safety improvements. Most residents in this neighbourhood regularly walk or cycle and their safety needs to be addressed

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Aro street has been unsafe for pedestrians and cyclists because of vehicle speed and the complexity of the intersections and street furniture at the aro valley shops

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Don't know
People walking on the paths through Aro Park and Little Palmer Street?	Don't know
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
I live on little palmer street and believe these changes will improve access and safety of all users, IF communication about desired user behaviours and signage is clear	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
These issues are overdue for attention and road user safety in these busy and congested areas is important. Most people walk or cycle in this neighbourhood and their needs are not being addressed quickly enough

Feedback**34**

114168798862

NAME: C Andersen	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I do not own a car, but understand that parking availability is already a nightmare for residents. The reduction of overall parks, and consolidation of parks into areas that may be a significant distance from people's homes will cause issues. There is also the issue of parking access for local business. The expectation that people visiting these will be happy to park elsewhere and then walk seems optimistic. Economic impact would be noticeable. Deliveries to local residents would also become impossible, unless couriers and tradies would be willing to park elsewhere and walk the delivery to it's destination. I live in one of 6 households located on 3 sections on Raroa Rd (soon to be 7 on 4, with a new development currently underway at 160 Raroa). These households are reliant on the street parking located beneath 162 and 164 Raroa Rd. The only other parking available is 1 deck space per household, located across the road and 50-60m offset, which will remain as 6 spaces for 7 households once the new build at 160 Raroa Rd is complete. Not only is this a bare minimum for any household currently, it will shortly mean less parks than households, which seems to assume that no household of the current 6 will ever need to receive guests, as no parking would be available for them, and will not need to receive deliveries either. Any increased need to cross the road will put people in danger, as there are blind corners both above and below the path access at 162/164. So while the bike lane proposal would make things easier and probably safer for cyclists living further up Raroa Rd, it would be a nightmare of reduced parking, reduced safety, no visitors, no deliveries, and reduced property values for the residents of 164/162/158 and soon 160. Perhaps the cycle lane could be moved out from the bank at this point where the road becomes wide, with some parking then still being available, and the cycle integrity being maintained via the reflective batons I see in some of the concept renders.

Do you support the proposed speed changes on Aro Street?

Support

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I do not own a car, but understand that parking availability is already a nightmare for residents. The reduction of overall parks, and consolidation of parks into areas that may be a significant distance from people's homes will cause issues. There is also the issue of parking access for local business. The expectation that people visiting these will be happy to park elsewhere and then walk seems optimistic. Economic impact would be noticeable. Deliveries to local residents would also become impossible, unless couriers and tradies would be willing to park elsewhere and walk the delivery to it's destination.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

I live in one of 6 households located on 3 sections on Raroa Rd (soon to be 7 on 4, with a new development currently underway at 160 Raroa). These households are reliant on the street parking located beneath 162 and 164 Raroa Rd. The only other parking available is 1 deck space per household, but located across the road and significantly offset, which will remain as 6 spaces for 7 households once the new build at 160 Raroa Rd is complete. Not only is this a bare minimum for any household currently, it will shortly mean less parks than households, which seems to assume that no household of the current 6 will ever need to receive guests, as no parking would be available for them, and will not need to receive deliveries either. Any increased need to cross the road will put people in danger, as there are blind corners both above and below the path access at 162/164. So while the bike lane proposal would make things easier and probably safer for cyclists living further up Raroa Rd, it would be a nightmare of reduced parking, reduced safety, no visitors, no deliveries, and reduced property values for the residents of 164/162/158 and soon 160.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support
Do you have any comments to make about the proposed design?
Broader pathways seem a logical solution to pedestrian and cyclist congestion, while reducing the risks cyclists are currently taking around the base of Aro Street. I see no downsides to this.

What do you think of the timing of the proposed approach?
Don't know
Why do you think that?
I oppose the changes, so the speed at which they occur isn't relevant

Feedback**35**

114161615043

NAME: Cameron Hart	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I currently avoid cycling on Raroa road because it's scary. This should improve things although paint is not protection. I think it would be good to reduce the downhill speed limit from 50 to 40 km/h. While I can go 50 on a bicycle it doesn't feel that safe, especially if I have my kid on the bike. Reducing the speed would hopefully mean less tailgating from cars. Also driving that stretch of road you are hard pressed to go 50 km/h anyway so it seems like it wouldn't be that controversial.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

While I would like to see changes ASAP I think it's reasonable to give residents time to adjust.

Feedback**36**

114161295080

NAME: Cameron Grant	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

New Zealand has declared a climate emergency. Wellington has declared a climate emergency. Transport options that have the least effect on climate change must be have the highest priority. Transport options that have the most effect on climate change must have the lowest priority. Therefore bike lanes must have a highest priority; and cars the lowest priority. While it is a step in the right direction. This plan does not go far enough. The bike lane must be physically separated from the cars for 100% of the journey. Anything less will not encourage the majority to trust the cycling is safe for them and for their children. Half done cycling lanes are not good enough. Please, I beg of you. Be more bold. This current plan is not committing to the climate emergency that has been declared at multiple levels of government.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

New Zealand has declared a climate emergency. Wellington has declared a climate emergency. Transport options that have the least effect on climate change must be have the highest priority. Transport options that have the most effect on climate change must have the lowest priority. Therefore bike lanes must have a highest priority; and cars the lowest priority. While it is a step in the right direction. This plan does not go far enough. The bike lane must be physically separated from the cars for 100% of the journey. Anything less will not encourage the majority to

trust the cycling is safe for them and for their children. Half done cycling lanes are not good enough. Please, I beg of you. Be more bold. This current plan is not committing to the climate emergency that has been declared at multiple levels of government.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

no cycle 'shoulders'. anything less than 100% protected bike lane is not acceptable

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Negative

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

30kph the whole way. Cars must yield to bikes. encourage bikes, discourage cars

Feedback**37**

114169286710

NAME: Carolyn English	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Feels like cyclists, pedestrians, bus users and car owners have all been considered for this plan - I really like it.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

This is a great compromise for the bottleneck that happens in Aro St as traffic backs behind and then tries to pass bikes (often dangerously)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Great to know the bus drivers have had their say on what would make it save for their passengers.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Great plans. The widening of the path in Aro Park may also help with the puddling and poor drainage on the end closest to the hall too.	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Raroa Rd residents are dealing with the change straight away so other residents could too.

Feedback**38**

114165652432

NAME: Cassandra Bahr	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I like the clever thinking for our windy, steep road. Putting a bike lane on the up and letting bikes keep up with the cars going down is a good compromise! I do wonder though if a 10y/o kid would be safe going down.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Removing so many parks will make people upset but they gotta cope! Bikes need to come first. Also, I suspect many of the car owners there are students flatting, who do NOT need a car if they're living in Aro Valley, so close to town!

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What a lovely idea! (also, the tone of this survey is GREAT)

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Maybe make it a bit faster, if it's working? Climate change accelerating etc

Feedback**39**

114180596672

NAME: Catherine Monk	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Don't know**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

OPPOSE - New bike lane on the uphill side, except through shopping area (It makes no sense to remove parking in an area where there is not enough.) - Four speed humps along Aro Street for safer speeds (this will impede busses more than slowing people down) - Parking changes on the south side of Aro Street rolled out over two years. SUPPORT - Raising the pedestrian crossing by Aro Park - Better bike parking in the shopping village - Removing one bus stop outside 47b Aro Street - Introducing car share (one car, one van) and a mobility park on Epuni Street. NUETRAL -Extending the 30km/h speed zone down to Willis Street (suggestion: instead of going down to Willis, why not take this up Aro Street further?)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

OPPOSE - Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction PARTIAL SUPPORT - Removing all on-street parking space on the uphill side, and putting broken yellow lines. (Suggestion: only remove off street and replace with broken yellow lines from 134A Raroa Road continuing uphill. DO NOT REMOVE between 140A to 134A Raroa Road) SUPPORT - Putting broken yellow lines on the corners in the downhill direction to improve safety and visibility - Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Street path) -Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15m to make it easier to access buses.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**People using Aro Park for recreation (e.g. picnicking)?****Positive****People walking on the paths through Aro Park and Little Palmer Street?****Very positive****People riding bikes along this route?****Very positive****People driving vehicles on the surrounding streets?****Very positive****People with disabilities or accessibility issues?****Very positive****Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?****Strongly support****Do you have any comments to make about the proposed design?**

I hope improvement of the water drainage in Aro Park is also on the agenda so the footpath doesn't gather puddles and flood whenever it rains like it does now.

What do you think of the timing of the proposed approach?**Make the proposed changes more quickly****Why do you think that?**

Why does it need to take three years when most of this is painting lines on the ground?

Feedback**40**

114165734131

NAME: Cathy Blakely	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

I'm a cyclist, and have noticed that lots of drivers are not aware of sharrows and how they work. More info and guidance needs to be provided to people so that cyclists don't bear the brunt of frustration and anger.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It is so important that we have safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Feedback**41**

114178449499

NAME: Cecilia	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I think you should also: 1. Raise the pedestrian crossing in the Aro shopping village - as a pedestrian, the amount of time I almost got ran over in that spot is ridiculous! 2. Introduce scooter share hub in Aro park (e.g. Flamingo or Beam). As a pedestrian, getting to and from the city is fine but walking up the steep hill is intense (and at least I'm young and fit so can afford to do it). There are few or no buses going uphill on Durham street, Epuni street or Raroa street so that doesn't make up for a very inclusive access

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I also think you should: 1. Increase the frequency of the bus 25. 2. Increase the network to cover more areas in Brooklyn, Karori etc. I'm not not sure if Metlink or LGWM owns this, but this should absolutely be part of your scope. A lot of WCC initiatives are great but not coordinated very well with other stakeholders, leaving Wellingtonians in limbo. I'm all for reducing cars on the road but at the moment the alternatives are just unrealistic; bikes and walking are limited to able-bodied people (and dependent on Wellington weather) and the current public transport system is too limited in the areas it covers, and too infrequent to be a viable daily option.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Maybe some painting on the path would be helpful to keep the bikes/scooters controlled and reduce the risk of them zigzagging around pedestrians?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**42**

114181677320

NAME: Kah	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Aro Valley is an extremely vibrant part of the city blighted by being a commuting chokepoint to and from the western suburbs. How might we incentivise car traffic through to the Terrace and Tinakori Road instead where it's wider?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Currently the mix of buses, cars, bicycles, parked cars, the few pedestrians and general road layout is not sustainable. As a regular user of Raroa Rd on a bicycle and by car, reducing parking on the uphill AND downhill sides need to be considered - particularly on some of the tighter corners. Downhill sharrows isn't really going to help a bicycle squeeze past parked cars and an

uphill moving car. Regular policing of cars parked on the footpath is also required. How might we maximise the existing space more rapidly (e.g. can we share the footpath at the uphill side of Raroa Rd, near the bottom?)

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

In my opinion widening the path here is going to cause more conflict between modes with cyclists travelling in both directions faster. Also unless it's made easier for a cyclist to emerge and join uphill traffic on Aro St (e.g. by funnelling them to the pedestrian crossing closer to Willis St instead of exiting by Garage Project) you essentially create a very confused point of traffic flow. There are so many more things that can be done to this make this space better for more recreational usage (e.g. as you say, picnicking) and to be a more attractive area to spend time in rather than making more room for cyclists. Make cyclists WANT to slow down and enjoy the space.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think there are lessons that can be learnt in the Brooklyn Road cycle lane "experiment" - how might we try some of these changes in a lighter weight fashion more quickly?

Feedback**43**

114179873597

NAME: Andrew Chisholm	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Can we calm traffic somehow without big speed humps that put wear on suspension?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

This is a major improvement for cycling as the road is narrow and cars weaving around parked cars makes it dangerous for cyclists. Can these changes also enable two cars to pass easily for most or all of the route, as this would be an additional benefit to the work.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
Will it be safe enough for pedestrians, especially those with disabilities, without specifically marked spaces for bikes or people?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
We need to urgently make it easier for people to have low-carbon ways of getting around. Full use of cycleways will lag their availability due to time taken for people to get use to the change from cars, and we need to start this change ASAP

Feedback**44**

114181153222

NAME: Steve	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Whilst I understand the concept of improving traffic flow for all road users in this area, the negatives far outweigh the positives in terms of the impacts upon local residents and ratepayers in the affected areas. Parking is already severely limited and there appears to be no suitable logical alternative for residents to park their vehicles.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

Where do people park their cars when its already at a premium ?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

We park directly where your photo has the new bus stop on Raroa Rd at the Harrold Street path as this is the only access to our property at 94 Raroa Rd. If all parking is removed here with the bus stop and cycle way, then there are no parks on the left hand side of the road within

kilometres. This simply won't work for us transporting children to and from school and other activities, grocery shopping, visits from friends relatives, and any sort of deliveries. Also no parking for tradespeople as required. The only way this would work for us is if council integrate a 2 car pad adjacent to the one at 92 Raroa Rd into the design, which would be on council road frontage. We could then run access down from our boundary to this car pad. That would be a preferred solution. Alternatively, council could look at building car pads or carports over the vacant land directly opposite our property which would allow us access to our property from across the road. That would be a secondary solution. Thirdly, they could instigate Residents Parking across the road as far down as the bustop for ratepayers in that vicinity. That would still mean our vehicles are subject to random theft and/or vandalism by passersby on that sidewalk, as has occurred in the past. Then concern would be the multiple residents in the area trying to park in this limited section of Raroa Rd. If the parking on the left hand side is removed, then clearly as a 20 year plus ratepayer in this location, we have genuine concerns about the long term valuation of our property.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Seems good as long as there is no detrimental effect to existing ratepayers in that area

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The lack of consultation with the impacted residents in the area indicates a rushed proposal for what are serious long term implications for the entire area. We would question the data used in making these decisions, given the vagaries of car parking numbers, particularly on Raroa Rd, at certain times of the day.

Feedback**45**

114160393069

NAME: Chris Mitchell	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I would love to feel safe to bike through the area.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I think we need to act as quickly as possible to prioritise the safety of our people and to encourage people to drive less.

Feedback**46**

114181507100

NAME: Chun-Lin Lee	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The changes will make it safer and more comfortable to travel by bike along Aro Street.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I support the addition of a bike lane / space for people on bikes to move along Aro St. Please ensure that the speed humps and raised platform are cycle friendly (ramps that aren't too steep) and that there are good transitions from the cycle lane onto the footpath to access the cycle parking in the village. Some of the surface finishes along Aro St are currently quite bumpy/jarring for cyclists travelling at speed.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

If possible I would support these paths being widened further to 3m or even wider to provide additional space between users and improve comfort and safety.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

As a cyclist I would love to see the changes implemented as soon as possible. I don't think the changes are that drastic so don't think it needs to be drawn out, however, I understand you may have a lot of opposition from residents to do with parking.

Feedback**47**

114174268926

NAME: Caroline Vincent	SUBURB: Brooklyn	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

As a resident of Brooklyn and an employee in Karori I use this route on bike or car multiple times a week. I believe these changes will be positive for everyone, while also giving current on street parkers time to make alternative arrangements.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Particularly happy about the extension of the 30km zone, should lessen drivers attempting ridiculous passes at the end of the rd. Although I would support the changes all happening asap I can appreciate the reasons for delay helping people mode shift.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Nice to see council upholding it's parking policy and making the bold step of removing car parking to make the transport of people easier and safer.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Great idea, a lot of people on bikes already use this route, makes sense to improve the space for everyone

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I would love it to be quicker but appreciate we also need to give people time to get used to reduced free parking and mode shifting.

Feedback**48**

114165654796

NAME: Callum	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Current cyclist and car interactions are very dangerous and there needs to be intervention. With the increased prevalence of ebikes this commute is more viable for cyclists as the hill is less of an obstacle. Only having an uphill cycle lane was well thought out.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

The speed humps aren't needed as they'll make journeys unpleasant for all road users. I'd like improvements for crossing Devon Street at Aro Street. Very poor layout and unmarked crossing. Need to strictly enforce the parking changes as we at Holloway Road end regularly get strange cars left parked for very long periods.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

There have been a few jalopies left on Raroa Road which I'm sure the residents would like removed. Blind corner bus encounters are very dangerous here so I'm glad they're going to be fixed.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Would suggest painting a centre line for bicycles, which would also encourage pedestrians to walk courteously to one side.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Priority is Raroa Road due to dangerous blind corners.

Feedback**49**

114173786393

NAME: Clarry Inwood	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The whole approach seems to be one of imposition without consultative approach to communities along the routes. This piecemeal approach will alienate people. Any transition needs very careful planning over at least a 10year period .

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

It appears the concept of not consulting local residents before the planning begins is not on the Council agenda. Building cycleways on existing narrow roads does not cover alternatives. Eg. completely new routes for them.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Cannot see how this would be safe once in practice. Further design thought is required or abandon the idea.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Very negative

People driving vehicles on the surrounding streets?

Very negative

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Any cycleways through existing pedestrian ways should separate pedestrians from cycles, scooters etc

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The residents of Aro St and Aro Valley should have been consulted as our environment will be degraded by this action to encourage more traffic from Karori, Kelburn etc . Instead why not encourage traffic through wider streets past the University and Glenmore St

Feedback**50**

114177271329

NAME: Clive Truman	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

The negative impact on residents accessibility to their properties, to maintain them and travel to and from them will far outweigh the positive impact to a relatively small number of cyclists that use this route.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

The removal of car parks towards the top end of Aro st where the new apartments are being built will cause great difficulty for the houses to be maintained(as they are all over 100 years old) as there will be nowhere for tradesman to park their vehicles and drop off equipment. There is already a shortage of car parks for residents and this will create almost nowhere for residents to park. How will residents drop of shopping or have furniture delivered ? There will literally be nowhere for vehicles to even stop nevermind park. It will be a disaster.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Neutral**

Do you have any comments to make about the proposed design?

No problem with this.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

No problem

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Give me more time to sell my house as it will be worthless once this is completed

Feedback**51**

114178670758

NAME: Zane	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I support this. I regularly walk, cycle and drive this route... it's a bit tight in a lot of places. This proposal would solve some of that. And, yes, we need to reduce emissions - this may promote safer, more attractive walking & cycling.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**52**

114179503034

NAME: Jessica Cox	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Cycleways are taking far too long to put in, it is just drawing out a painful process for both parties. Pedestrian and cyclist impacts from cars are going up and better infrastructure needs to be put in. As fuel prices go up more people will be seeking this as an alternative, with safety being a main concern.

Feedback**53**

114181683353

NAME: Brock	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Much needed and long-awaited improvements, which will improve the safety of myself and my family when visiting businesses in the village and friends nearby – though unfortunately, it looks like we will need to keep waiting a few years longer yet to get there. The village needs more work than is in this proposal to lower the speeds cars currently blast through at, which make it a lousy place to walk around safely and enjoy sitting outside cafes.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I don't love the shopping area – I don't think the traffic calming measures here are going far enough to make this a safe space to mix cars and bikes together. The crossing at Devon needs to be raised – I've seen countless cars blow through here without seeing or waiting for pedestrians. Thank you for bike parking!

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Please install the kerb crossing near the bottom of Raroa ASAP (and cycle markings on the footpath), as it's a quick win that will allow me to ride at least part of this route more safely with little work. I really like how you're realigning road markings to better follow the shape of the road and create more space for bikes. I'd really like more protection – I understand the areas around bus stops and at tight bends barriers can't be used because of buses, but at every other opportunity – no matter how tiny – barriers and posts should be added, as they make such a huge difference to perceived safety while biking. It looks like there should be room to better handle the bike lane entering the roundabout at Plunket – I just know cars are going to push up against bikes there.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**People using Aro Park for recreation (e.g. picnicking)?****Very positive****People walking on the paths through Aro Park and Little Palmer Street?****Very positive****People riding bikes along this route?****Very positive****People driving vehicles on the surrounding streets?****Very positive****People with disabilities or accessibility issues?****Very positive****Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?****Strongly support****Do you have any comments to make about the proposed design?**

Would love you to add the two kerb ramps at Little Palmer (and remove the parking that blocks them) ASAP, as they are quick wins that don't require the remainder of the work be completed first. Longer term, Abel Smith needs to connect better with the shared path on the south side of Karo, as the transition from shared path to crossing to footpath then road (when you need to swerve onto Abel Smith somewhere) is not great, and isn't improved with this work.

What do you think of the timing of the proposed approach?**Make the proposed changes more quickly****Why do you think that?**

The 30kph zone should be extended up to Holloway at least until phase 2 is completed, to attempt to mitigate some of the risk to cyclists in the interim (ideally the whole area permanently becomes 30kph). I admire the idea of a staged approach as a way to reduce backlash, but given experience to date, I find it unlikely to have the desired effect – and instead just prolong the backlash over many years, delay the response to very real pedestrian and cyclist safety issues, and slow our urgent transition to a low carbon transport system. Your parking survey data shows residents will largely be unaffected once residents parking is added to other areas – go hard and early on that approach and messaging. Your data also shows how coupon parking and Holloway parking is being used, and the low level of compliance within any type of parking, which distorts the actual parking needs of residents and businesses. Your data should give you the confidence to

remove this parking immediately. It's clear that expectations for parking in the area have been warped by years of comically poor compliance, which inflates the actual parking requirements of local residents and businesses.

Feedback**54**

114169041879

NAME: Francis Cuncannon	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

I live at [REDACTED] Raroa Road. It is proposed to shift the bus stop from being in front of 38 Raroa Road to be in front of 40 Raroa Road. I oppose that. The bedrooms at [REDACTED] Raroa are all on the road side of the house. I am often woken as it is by the late buses and early buses when they pull up in front of 38 Raroa. The bedrooms for [REDACTED] Raroa are all at the back of the house, and the occupants (my daughter's family) are not woken by the buses. Moving the bus stop will have a significant impact

on the quality of sleep those living in our house get. I strongly oppose the shifting of the bus stop. Other than on that issue, I support the other changes proposed.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**55**

114180825052

NAME: Dan Perkins	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Don't know
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**56**

114165357978

NAME: Ella	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I'm looking forward to feeling safer on my bike in this area

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

I'm looking forward to feeling safer on my bike

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**57**

114160118862

NAME: Dan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

Consultation needs to be made about what is the best action. I do not believe we can install a cycle lane and squeeze the roadway - its not safe and we will have an ugly, confusing and unsafe roadway like Island Bay. Either get rid of parking all together, or have the cycle way but unbarriered.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Remove all parking along Aro Street if there is going to be a barriered cycle lane. The street is already dangerous enough with its current width. I am for a cycle lane in Aro valley - but not by making the road a more dangerous place. The side roads of Aro Valley should suit parking needs - and if people don't have parking at their properties it becomes a case where the street parking is a privilege not a right.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Negative

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

Cyclists are road users - the park is home to a preschool and childrens playground - as well as a basketball court and a community space. I do not believe childrens safety should be put at risk for a 5 minute shortcut for cyclists.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**58**

114173827942

NAME: Dan Byrne	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Cyclists should be protected from traffic.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Like how the bike lane is protected, but seems narrow.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

This bike lane should be protected. Maybe reduce the width of footpath on opposite side.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Maybe some lighting needed along this route.	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Doing the stages together would be less disruptive overall.

Feedback**59**

114174273831

NAME: Darren Young	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

You need to have provision for trade and delivery vehicles in the parking spaces that are going to remain. Loading zones are vital here to make the dwellings liveable.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Oppose	
Do you have any comments to make about the proposed design?	
Due to Aro Valley have a large proportion of walkers widening the path to accommodate the likes of cyclists and e scooters might discourage people walking this route	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Do stage 1 and the stage 2 part by polhill reserve together otherwise you have a gap in the middle of each stage 1 part.

Feedback**60**

114160396591

NAME: Raewyn Carmichael	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I have lived in Kelburn for 50 years and I know these changes will just make the traffic delays bigger and cause more accidents.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose**Do you have any comments to make about the proposed design?**

Aro Street is way too narrow now for the buses. Adding a bike lane will just make it far worst and cause huge delays.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose**Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)? People walking on the paths through Aro Park and Little Palmer Street? People riding bikes along this route? People driving vehicles on the surrounding streets? People with disabilities or accessibility issues?	Positive
	Negative
	Positive
	Neutral
	Negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Oppose	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**61**

114181509895

NAME: Dave Allan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I support all the proposed changes except the bike lane/removal of parking as there will be simply not enough room for residents to park. Aro St, Devon St and Epuni St parking already is full after 10pm every day already. These are residents not coupon parkers.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I support all the changes except the bike lane/removal of parking on Aro St South side. I live in Aro Valley and there are virtually no car (if any) parks available in Aro St, Epuni St and Devon St on any day of the week after 10pm already. This is not Coupon parkers it is residents cars, Removal of this many car parks is not fathomable. People who live in Aro Valley, do actually have cars and they need to be able to park them. As for Raroa Road it is already mayhem. The best route to put a bike lane from Karori would be Glenmore Street/Tinakori Road it is far wider.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

As per my comment for Aro St I agree with all but the bike lane there is simply not enough room. glenmore St/Tinakori Road is far wider.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

To be honest I agree with all the changes except for the bike lane which I oppose as there is simply not enough space left for residents to park. Glenmore St/Tinakori Road is far wider.

Feedback**62**

114174302149

NAME: David	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Don't know
Why do you think that?

Feedback**63**

114180537204

NAME: David	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Removal of parking on the uphill side will move cyclists off the road into their own (safer) space and allow me to drive at a more reasonable speed, without the frustration of trying to get past on a road with lots of bends. It will also mitigate the current gridlock with buses unable to pass due to cars on both sides.

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support**Do you have any comments to make about the proposed design?**

I think you can make the changes more quickly than 3 years. Specific feedback points: - please don't add speed bumps into this main road. They are annoying on a bus (and this is a bus route) and also for uphill cyclists. use speed cushions if you must. Or something temporary to get some data Signage map 6: please add some green paint to denote shared space at the pinch point between Aro playground basketball court and Palmer St Signage map 11: again, add paint on the pavement so that pedestrians are aware that cyclists will be crossing; check that the kerb is lowered so that cyclists can come off the road onto the shared path in the right place; check that parking doesn't impeded sight lines for those entering or exiting the path through the park. Please REMOVE one parking space on the downhill side directly in front of the zebra crossing exiting the park. Vehicles there block sight lines for kids exiting the park onto the zebra crossing. (you could convert it to another bike park). I love the idea of bike parking outside the Garage

project bar. Again - I strongly oppose the raised pedestrian crossing and speed bumps, but love the rest of the plan for this section. Please can you add Sharrows to the uphill side of raroa cres (travelling from karori to aro st), so that drivers are aware that there may be slow moving bikes going up. Main comment from me is that irrespective of a bike lane, please remove all on street parking from the uphill side to manage the gridlock; please DO NOT implement speed bumps – these should never be implemented on ANY bus route; and please remove a parking space from the approach side of any pedestrian crossing (even if it's in a layby), there should be no parking within 6m of a crossing to manage sight lines). Thanks David [REDACTED]

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Love it. I drive this route most days, and removal of car parks on one side will massively improve the flow. Specific feedback points: 1. Signage Map 24 - make the footpath from opposite Norway St to Old Bullock Rd (172 Raroa Rd) a shared path. 75 cm unprotected bike lane is not wide enough to be safe. There is little pedestrian traffic on that section (as much of this goes down old bullock rd to holloway road), so slower "acoustic" (i.e. non electric) bikes would be much safer on a shared path; and it wouldn't slow vehicle traffic on an uphill section. 2. map 31 - remove parking from the downhill side where there is a 75cm unprotected bike lane on the uphill side. 3. map 34 - add "Please indicate" on the roundabout W-8 warning signs.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Please remove the parking space on Palmer st at the exit of little palmer st, so that bikes heading south will exit to the road, rather than stay on the footpath on Palmer st. Please make sure there are dropped kerbs at all the entrances / exits Please have paint or other visual signals at entrances/exits so pedestrian traffic crossing the exit is aware of the possibility of bikes crossing.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Two stage is OK, but make them 6 months apart, not 2 years. From experience elsewhere, people will park to block cycle lanes until the last possible legal moment, impacting the network effect. 2 year delay also gives opportunity for "review" and possibility of never making the changes; which again impacts on the benefits of the installed changes due to lack of continuous network - so, please just get on with it. why should the public road be free storage? it should facilitate movement.

Feedback**64**

114181663873

NAME: DAVID MCGILL	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

30km whole of Aro St Humps Speed cameras Public transport

Do you support the proposed speed changes on Aro Street?**Oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?**

Uphill bike lane a FAKE lane compromised completely with dual use by buses, waste skips, trade vehicles. It is plain dangerous for all road users with the bikes obliged to weave in and out of the dedicated lane. Currently 4 construction sites on Aro St. More trade vehicles and waste skips expected with the intended general increase in local building density. Whole of Aro St at 30km. I note on the Oriental Bay bike counter: High user numbers on fine weather days to very low users on windy and rain days. These poor weather users still require public transport. Bike lane for Aro/Raroa is 'cream on top' change, improved public transport reduces vehicle use and in doing so allows bike users a legitimate space on the road

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Oppose**

Do you have any comments to make about the proposed design?

No bike lane Speed limit set at 30km for Raroa Rd

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

Think a little into the future.. a cargo bike+a buggy+a walking child+an adult and going the other way a cargo bike+a buggy+a walking child+ an adult. Or.. repeat with electric assist cargo bike at 20km. Leave Little Palmer St out of the route, get the bike traffic into Palmer St. Bike speed restrictions where dual bike/pedestrian.

What do you think of the timing of the proposed approach?**Why do you think that?**

30km speed limit full length Aro St. Speed humps spaced on full length of Aro St. Speed cameras too to ensure compliance. Public transport and traffic calming. Very easy to install, no fuss with already difficult side street roadside parking.

Feedback**65**

114171447608

NAME: Chris Owens	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

As a cyclist, it can be a bit scary navigating through these streets, especially when cycling uphill, avoiding parked cars, while motorists impatiently try to overtake you. I think installing an uphill bike lane, and have sharrows for the downhill as very positive and fully support this. It will make cycling safer and more attractive.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I think it is vital to redesign streets to allow active road users (cyclists, pedestrians) an easier and safer way to navigate this part of the city. I support the changes to the on-street parking and the reduction of the speed limit to 30km/h. If people are concerned about the loss of parking, alternative transportation modes need to be strengthened, and that includes reliable, convenient, and more frequent bus services. Please be mindful of people with sensory or physical disabilities in the design so that their safety is prioritised, as well as more able-bodied cyclists and pedestrians.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support**Do you have any comments to make about the proposed design?**

I think it is vital to redesign streets to allow active road users (cyclists, pedestrians) an easier and safer way to navigate this part of the city. I support the changes to the on-street parking and the construction of an uphill bike lane up Raroa Road.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?**Strongly support****Do you have any comments to make about the proposed design?**

I support these changes, however I hope the disruption to the flora on Little Palmer Street will be minimised as much as possible. Perhaps any significant plants/trees that are affected could be transplanted to Aro Park? People riding bikes through this area are welcomed, however, cyclists should be encouraged to slow down and be mindful of children and those with disabilities. Traffic is generally low with some of the surrounding side streets, however I support mitigation (efforts to slow traffic) if the design warrants it. I support measures that provide greater safety for those with disabilities.

What do you think of the timing of the proposed approach?**Make the proposed changes more quickly****Why do you think that?**

Fundamentally, I think the timing is too long, however I can understand some wanting more time. Overall, I wish to avoid bikelash as much as possible and support measures that help mitigate this from happening.

Feedback**66**

114160264086

NAME: Jill Robinson	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Since the introduction of the bus route up through Raroa Rd this has become very congested. The number of cars on Raroa Rd has increased significantly over the last few years. Where will they go. Epuni Street is already hard to find a car park in. While I support the car share arrangement, the removal of car parks in Aro Street will put pressure on car parking spaces in Epuni Street. Will there be a lot more resident only spaces made in Epuni Street?

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

There are too many people dependent on parking a car near their house. Epuni street parks are already full. I agree with carshare. The E station has proved popular. But removing car parks for residents there will be even more demand in Epuni St. Devon St already full with cars.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

The number of cars on Raroa Rd has increased exponentially over the last few years. Where will these cars go? It was a poorly conceived idea to have buses coming on this route. I believe this has contributed to the difficulties on this route. The proposed changes will do nothing to address the fact that on some parts of Raroa road two buses are unable to pass causing build of cars in both directions

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

This sort of improvement will be very beneficial. It will also be likely to make these routes safer for people walking through in evenings

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

WCC needs to ensure that the public transport in Aro Valley is efficient, reliable, regular and cheap (ie WCC subsidised) if it wants to get people out of cars. This needs to be embedded before starting these changes which could make this route even more congested and dangerous

Feedback**67**

114167251099

NAME: Don Holborow	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

This is unnecessary change which will create hassles for everyone living along the route. You must accept that vehicle use (including electric vehicles) will continue, and people need places to park. I actually ride up Aro Street (to Polehill Gully) for recreation more often than I drive up Aro Street. I never have any problems, as drivers in Wellington are generally courteous and understanding. This proposal is likely to induce road rage in everyone who sees it, making things less safe!

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

The bike lane is just not needed. I regularly ride up to polehill with no difficulty - cars are understanding and it works fine. Getting rid of the carparks will be a disaster for all the student flats which rely on them, and this far outweighs any perceived benefits of the cycleway. I also drive to Island Bay a bit and have never seen ANYONE on that cycleway. It's just safer and faster to ride on the road.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Again, flats on Raroa Rd (there are many flats up the hill between the road and Highbury) rely on the parking. Cars can get past bikes at appropriate places. It is just unnecessary.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is an unnecessary waste of money - what's there works fine as far as I have experienced it (for over 30 years!).

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

In fact just don't make them. It ain't broke so don't try to fix it. You are doing changes just for the sake of it, with no identifiable benefits.

Feedback**68**

114172948207

NAME: Don Pettit	SUBURB: Aro Valley	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

Need to support bike and other transport for climate improvements. Need to move faster!

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Need to move faster and make the lane continuous on the uphill side through the village

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**69**

114181659087

NAME: Dylan Packman	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I support all these changes and am very grateful to the council for the commitment to improving transport choices and safety. I grew up in Aro Valley. Freedom was riding with school friends on bikes around the city, including up Raroa Road, down through the Botanical Gardens, around the waterfront, wherever we wanted. I want the same freedom, along with greater safety, for my kids. We need to move more people more efficiently. I think these changes are necessary for the growth of our city and for our climate commitments. I am disappointed by the extended phased approach. We need change asap, and I would welcome these being implemented much faster than currently suggested. I support the extension of the speed reductions to 30km/h. I note this leaves a gap of around 120m between the bottom of Aro Street and the lower speeds on Willis Street past Abel Smith Street. I look forward to this being fixed in future along with many many other streets. We need safe streets with safe speeds. If the 10 minute parking is going to be kept at the bottom of Aro Street I think it would be good to consider reshaping the traffic island at the intersection of Willis and Aro with the intention of having a protected cycle lane between the 10 minute parking and the footpath. As a bare minimum it could be good to add some sharrows at the bottom of Aro Street. I like the bike parking and support it being in a prominent position in the Aro Street shops. There has been a big issue with bike theft in Wellington and having it in a highly visible position will be very beneficial. I would support covered lockable cargo bike parking for residents on side streets as used overseas. I support the changes to parking in Holloway Road - I wonder if it should be coupon parking for the remainder of the street and suggest the demand be monitored and evaluated later. I think the intersections of Ohiro Road and Aro Street, Durham Street and Aro Street, and Norna Crescent and Raroa Road, should be changed from Give Ways to Stops to help people in cars turning from the side streets be more aware of people on bikes. Norna Crescent joins Raroa Road immediately before a pedestrian crossing so changing this Give Way to a Stop may also increase pedestrian safety here. I appreciate the creative solution of the "cycle shoulder" but think they should be used only where absolutely necessary as a last

resort. For example on Raroa Road between Mt Pleasant Road and 151 Raroa Road there seems to be parking allowed on the downhill side of the road despite the uphill cyclelane being a cycle shoulder. There should be yellow dotted lines on the downhill side and the cycle shoulder should be a cycle lane. Because cycle shoulders are only paint, and reduce separation and protection, the speed limits should be reduced where they are in use. I don't understand what's happening at the Chaytor Street end of Raroa Crescent. There are sharrows downhill on Raroa Crescent towards Chaytor Street, should there be sharrows uphill as well? That intersection is particularly cursed, and I'm aware there is targeted work on that intersection in parallel to this process. Thank you.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly
Why do you think that?

Feedback**70**

114181575781

NAME: Zong Chen	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This enables cycling from home into the city centre for a large area all around Aro Valley and Kelburn. This would also build up momentum to increase cycling infrastructure in other parts of the city.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I know it's planned for stage 2 but: it's better to let the bike lane continue through the shopping area unbroken. The busy road conditions makes it naturally a dangerous part of the cycling journey, with increased accident risks.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Yes, removing the uphill side parking is very important. Public roads should be used for transport, not for storage of personal properties (here, cars).

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Yes, creating a pathway here makes sense, since people (myself included) already use this route. I would, however, be cautious about widening the path through Aro Park: we want more grass, more greenery, less concrete; but I can see how cycling traffic can pose a danger to pedestrians on the current narrow path.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

For the (very small) amount of work and changes that this proposal includes, a two-year timeframe is very long. In terms of construction, I expect this project to be fully done within one season. In two years, there should be many more bicycle lanes set up around the city, not just one or two projects like Aro St and Ngaio gorge. Do compare "2 years" with goals like reducing carbon emissions by 45% by 2030: you don't have all that many years left!

Feedback**71**

114169080535

NAME: Echo	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

Support all EXCEPT: - New bike lane on the uphill side, except through shopping area -Parking changes on the south side of Aro Street rolled out over two years. I am a resident in Aro Valley with a young child. It will be very difficult to get her across town to activities without a car (hard to have a car without a park)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I have previously indicated that I don't support the Parks along Aro to Holloway Road being removed

Feedback**72**

114160386062

NAME: Emily	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The proposals support safer, more accessible sustainable transport options for people in Aro Valley and surrounding areas.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I strongly support protected cycling facilities on Aro Street to give people greater travel choice. In addition I would like to see the 30kph zone extended to the base of Raroa Road to improve safety for pedestrians and cyclists along the length of Aro Valley, particularly to support access to schools and around Waimapihi Reserve entrance. Raising the crossing adjacent to Aro shops would also be beneficial as there is often a lot of activity and visual distraction for vehicles.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

I'm concerned that poor sightlines, particularly around the Community Centre and Palmer St, make this an unsuitable route for cycling. Cyclists and scooters already using this route often travel at speed (a real issue around the Community Centre and creche) and merge poorly with traffic on Aro St. The Park and community area should remain a quiet recreation space where elderly, parents and those with visual or physical impairments don't have to remain vigilant for bikes/scooters.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think the proposed timing will enable residents to get used to parking change or adjust their transport preferences.

Feedback**73**

114175806634

NAME: Emily Flowerday	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

My only concern is parking availability, it's already very difficult to find a park as a resident. And to lose so many more along Aro street will only make it harder. I would suggest making more incentives to limit the number of cars per household including shared rentals.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Don't know
Why do you think that?

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As stated in the survey I am concerned about parking. As a resident with a young family, we are already finding it increasingly more difficult to find a park close to our house. As a family we are very supportive of creating safer cycle and walkways and are keen to increase the use of our bikes and to feel safe cycling with children on the bike.

Unfortunately at the present time we are unable to live without a car, our local bus route does not access a large supermarket, and public transport is not substantial enough to rely on getting to work on time. One of our household works as a doctor attending clinics across the Wellington and Wairarapa region which has also made us a bit more reliant on a car. There is already a lot of competition for parking including the many trades people working in the area at present building houses and apartments, more car share vehicles with Mevo now able to access residential parks, those parking to exercise in local popular trails, and the many shared rental houses with multiple cars.

A few suggestions/ considerations:

- Increase the cost of parking for more than one car per house- combined coupon and residential.
- Those who have an off street park are essentially removing a parking space for another vehicle- do those with off street parking pay for access to that road space?
- Increase the number of residential parks (I see this is part of the plan) and stop car shares parking in residential spaces.
- Could you have a segment of the route as a cycle freeway for a period of time, but allow some parking during off peak times such as over night when residents can park.

I just wanted to express one concern, but overall it is very encouraging that Wellington is making changes to improve access to safer cycling for its population.

Feedback**74**

114174883527

NAME: Emma Reid	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

As a long time resident, I do not support the bike lane initiative with 48+ lost carparks. I do not see the evidence of a demand for a dedicated biking lane given the very limited number of bikers using this street. I do see the very real need to keep the existing carparks while also accommodating the increasingly popular service vehicles such as the carshare Uber and food deliveries. I see the bike lane as discriminating against the families with young children, the older and those with limited mobility who'll be unable to continue to enjoy their residences safely and easily without pickup/drop off access. I see a very real threat to the unique heart & soul and the vibrancy of the Aro Valley community village with less visitors being able to park to enjoy the popular cafes and local businesses. I see the safety and convenience of the residents being compromised with emergency services such as ambulances, fire engines and the police not being able to park, along with the trade services. I think other options could be explored such as removing the trees on the Aro Street south side to enable a shared bike/scooting and walking lanes. Other options include a car toll tax to help reduce the increased car usage into the city whilst increasing the safety of bikers. I do not believe that the residents of Aro Valley should lose our enjoyment of our residences without access to carparks for the sake of the fit & healthy able to ride up/down a steep valley road.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street is a residential area with many people already enjoying the opportunity to walk, scooter and bike, along with the use of public transport. There is evidence of the pavement already being shared between walkers and those on wheels. As a long term resident, I am supportive of many of the proposed initiatives but am very much concerned with the loss of 48+ carparks on the South side of Aro Street. Aro Valley is a vibrant community with cafes and other shops. With the reduction of valuable carparks, it is less likely that the existing village community will survive without visitors to the area being unable to park. Having lived here for many years, I do not see the evidence and demand for increased bike usage at this stage that would warrant and justify the residents losing the high-in- demand, valuable and necessary car parks. It is already very difficult to find a park on Aro Street, Epuni Street and Holloway Road. The popular services such as ride share (Uber), courier/grocery drops offs, as well as tradespeople are also already struggling to find suitable car parking spaces. With an aging population, it is a very real concern to safely transport elderly family members with limited mobility. There is also a concern that emergency services such as ambulances, police and fire engines will be unable to safely park in an emergency and this would certainly compromise the safety of the residents. Local young families are also compromised and will be forced to cross busy streets or walk a kilometre before a street crossing, should they even be able to locate a carpark. Other solutions could be explored such as removing the old trees on the pavement on the southside of the Aro St. Then the walkway would be wider for shared use. There could also be a line in the middle allowing with foot and wheel traffic on either side. Some of the trees have already been removed for safety reasons. This would mean that the carparks would remain. As your proposal already states, there is an increased growing need for more car parking and not less (i.e. not being able to accommodate the increased need for parking spaces) so the proposal of limiting the needs of thousands of existing members of the community does not make sense? Also, the addition of the electrical charging car space on Epuni Street is an existing energy saving initiative. There is no mention of increased charging stations. This proposal seems to outline a severe and brutal negative impact on the residents of Aro Valley that will lower the quality of life and enjoyment of those living here. The loss of 48+ carparks will mean that thousands of the residents won't be able to easily enjoy their homes and visitors, where those who are unable to use bikes will also be disadvantaged. There is no allowance for residents using popular services to safely disembark or unload given the severe reduction of carparks. This proposal does not provide logical suggestions for the modern world in which we live, but compromises on the safety of its inhabitants with the expected rise in illegally parked service vehicles for the sake of bikers.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Where do the residents park? Many will have similar needs to those who live lower in the Valley. Not all residents will be able to travel to work on a bike or take a bus. In terms of safety - why do the buses need to be so big/wide? They're very often not full at all - and if there is increased need - perhaps there should be more buses available during rush hour traffic times.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**People using Aro Park for recreation (e.g. picnicking)?****Very negative**

People walking on the paths through Aro Park and Little Palmer Street?	Very negative
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly oppose	
Do you have any comments to make about the proposed design?	
<p>There are many elderly residents with mobility issues. There is also the preschoolers. The bikers are already using these spaces and they are FAST and it is often a shock for bikes to weave past without notice or a speed limit. I cannot see how much wider the Aro Park & Little Palmer Street could be to accommodate both walkers and bikers, looking at the provided image against my existing experience of walking this alleyway almost daily? The park is a recreational area in a valley with limited flat land. The green spaces and basketball court are regularly used by many, including children, for ball games and ride-ons. There is no mention of widening the gap from Palmer Street onto the basketball court - this is the bottle neck with a very narrow entry where I have already often felt uncomfortable walking thru this area with the existing recreational use and number of pedestrians. Would it not make much more sense that this area is kept for those using the space recreationally and walkers, rather than being another major thorough fare for fast bike riders, who could just as easily use Aro Street onto Willis Street (where changes are also being proposed to accommodate bikers)?</p>	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
<p>In the opening statement, I am confused by the mention of 'better transport' options being provided? What are these better transport options, apart from adding the bike lane and reducing/increasing bus stops. There is no mention of smaller (& safer) buses (particularly on Raroa Road) with more buses on the schedule. There is no mention of easy pick/up drop off points for the popular carshare options (especially with half of the carparks supposedly vanishing). I also do not understand why this intermittent bike lane on Aro Street is being introduced slowly in a phased approach. How is that safe where the confusion is more likely to compromise resident safety? There is no mention of the existing properties with driveways, carparks or garages. Are these also to go? What support is available for those with these conveniences when desperate carparkers will be forced to use what is available to them? What about the safe turn around areas for those who miss a carpark and need to drive up and down the area looking for a park? I do not believe that this bike lane is a fair and accessible option across the breadth of a community. This proposal seems to favor the strong and fit of a certain age who're able to bike up/down a steep hill. The older, younger and those with mobility restrictions are disadvantaged, as are those with young families. I do not believe that this community and the surrounding areas are ready for this sudden change, even if implemented within a mere few years. The evidence of heavy bike use to warrant such a drastic change is not evident. I believe that there is a better opportunity to educate the public on bike safety and the opening of car doors. The use of smaller, safer and</p>

more regular bus services also makes sense. More car share spaces, rather than just the two proposed on Epuni Street would make more sense. More crossings further up Aro Street are also needed (esp between Durham and Holloway Road) which would also encourage slower traffic. Two lanes at the Aro St/Willis St intersection also makes sense to help keep traffic moving (for those going onto the motorway vs those intending to go down Willis Street). This would also make it safer for bikers, who'll more likely be going down Willis Street. It would make better sense to add more Resident Parking on Aro Street, before removing the coupon parking. The coupon parking process needs to be better implemented as it is obvious that this is being abused. The current use of bikes on Aro Street should be published and readily available as evidence of this perceived demand. This would not include those recreational riders who use the Polhill Mountain Bike track. Speaking of the Polhill Reserve, there is no mention of the mountain bikers who park on Aro Street and unload their bikes. Where will they now park, as many do not bike to the tracks, but drive too.

Feedback**75**

114181659439

NAME: Erina Papp	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

Fix public transport first. The online feedback lumps different aspect together rather than making it easy to comment in each individual aspect. It is not clear how it will make walking easier or safer.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

Fix public transport before meddling with infrastructure- we need reliable, affordable public transport. There are no motorcycle parks provided. The parking changes mean that living in Brooklyn, I will no longer be able support the businesses in the Aro Valley or visit family living there as I will be able to park.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

I oppose installing uphill bike lanes and cycle shoulders on Raroa Road as it is too narrow. Why not create a shared footpath for bikes and people instead. Cycling uphill is not viable for large portions of the population. Wellington's hills and weather, wind and rain, does not make cycling comfortable or safe for much of the year. We have an aging population, for whom cycling is not an option.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Public Transport is unreliable and expensive, fix this first. Cycling is not an options for large portions of the population. Consider those in surrounding suburbs who will be affected.

Feedback**76**

114173895146

NAME: Felix	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly oppose

Why do you think that?

The negative aspects to this proposal strongly outweigh the positive - potential devaluation of property, accessibility issues for the disabled, and a negative impact on local businesses. The road is simply an inappropriate place for a cycleway.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

The parking facilities on this road are already woefully inadequate for the amount of traffic and business in the area. (Observe any weekend afternoon). Changes should not happen at the expense of further cutbacks to parking facilities.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Similar to Aro Street, the parking in this area is also already inadequate. Removal of these parks degrades the value of existing properties and the lifestyles of existing residents.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Sounds like a great use of space.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I don't think it's realistic to expect this degree of lifestyle change in the proposed amount of time. If there is a transition to smaller uncovered transport it will need to happen culturally over a period closer to 10-15 years. People will not give up their cars in this city easily (especially with the Wellington climate). Forcing the change will only lead to resistance.

Feedback**77**

114162965802

NAME: Tommy Thomson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

It's a great start, but it should continue through the shopping area. Having to swerve into the traffic lane is unsafe and makes the cycle lane much less useful. I'm concerned that in the pictured proposal, those poles don't look sturdy enough to prevent a car from entering the cycle lane. Better bike parking is sorely needed.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

The proposal looks good, this route is fast and busy so the cycle lane is definitely needed. The picture doesn't include any protection for the cycle lane. Without protection, it's not really a cycle

lane, just a shoulder for the car lane that happens to be painted green. A proper cycle lane needs protection from cars.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Wider paths are better for all users of those paths. However if cyclists travel too fast there are potential safety concerns, and these paths should be primarily for pedestrians, so it would be preferable if a dedicated cycle lane was constructed

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

This is crucial infrastructure that needs to be constructed as soon as possible. WCC has declared a climate emergency and it is your duty to encourage people out of cars and onto bikes.

Feedback**78**

114161436697

NAME: Kirsten	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

This will significantly improve safety for cyclists and improve uptake of cycling/cycle commuting to these suburbs.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Uphill bike lane takes priority over downhill. Generally I am a huge supporter of bike lanes, especially separated protected ones. I do wonder about your analysis of the impact on visitors though: this will remove a very large number of parks currently used by weekend visitors to Waimapihi. Most recreational users either mountain bike (and thus drive with their bikes) or walk/run and follow with coffee in the village. I don't think 2hrs is anywhere near long enough for a parking space for a weekend user. Waimapihi is my favourite place in Wellington to run and I would often run for longer than 2hrs. A P120 means no extended runs and no time for coffee afterwards. Please consider P180 for visitors at the weekend.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

I cycle commute daily and excluded Karori/Northland as places to live purely based on this road and its hazards for cyclists. I would actively consider cycling to Karori if it were safe to do so.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Don't know

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Shared paths generally don't work for users of any mode in my opinion, however this is safer for cyclists than current options.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

This has a big impact on residents of Aro Valley. Options to go car-free in Wellington are few and properties that don't have off-street parking also don't have good storage for e-bikes, which are the only viable car substitute. Improvements to car share programmes also need to be substantial before going car-free is a true option.

Feedback**79**

114181470518

NAME: Fiona Gunter-Firth	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The lack of consultation by council officers with the residents adversely affected by the removal of car parks and the instillation of a bike lane means there are major faults in the design and reallocation of parking. The plastic bollards running along the side of the bike lane will lock people out of access to their homes. Bollards next to bike lanes are not a common thing elsewhere in NZ or Wellington city why have they been included here? And have the designers considered the negative affect they will have on the house that will no longer be accessible? If there were no bollards next to the bike lane people could load and unload after hours at night and when the bike lane is not busy. Otherwise, people are completely blocked from accessing their homes. I am questioning the legality of the council blocking peoples access to their properties. There is not enough new residents parking being offered particularly in the middle section of Aro street. More of the coupons parks in Epuni and Adams Tce need to be converted to residents parking. There are many of us who need vehicles for our work. Why should people form out of town have rights over the residents who have been adversely affected by this proposal. The council transport policy states that's resident parking is a priority, this needs to be applied by way of making more coupon parking resident parking.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Please add this to my earlier submission. The council design team did not consult with the rate paying property owners. A number of homes will become inaccessible with this proposed design. Asking councillors to vote on a completed design without proper consultation of those affected is unacceptable. This proposed design will stop us and other properties on the South side of Aro street being able to access their properties to load and unload materials etc. Legally Aro street is not a limited access road, private properties can not have their access blocked permanently by an action of the council. Our rear section only has a one-meter-wide access point to the street which runs between two houses for 40 meters until our house then another 20 meters to the house behind us. There are trees on the footpath planted by the council some years ago, these trees are not maintained and also impinge on our ability to access our properties. I have included 2 photos of our frontage to show this issue. With the addition of a bike lane with bollards we will no longer be able to load and unload materials we use in our home-based business. Let alone take delivery of anything larger than a handheld bag. We question the legality of this proposal.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

Below images of Aro street south side; show how the 1 meter wide access point to our property will be blocked even further by bollards on the proposed bike lane. Currently access is difficult with the council trees obstructing the entrance and with the addition of the proposed bike lane with bollards, access to deliver or remove large materials will be impossible.



Feedback**80**

114167326984

NAME: Fiona K	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

You need to consider the people who live in Raroa Road, and have to park their cars on the street. Their rights should be considered before those who will be travelling through Raroa Road.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

You need to provide more parking spaces for residents. They bought/ rented these properties in good faith and you are taking away their rights to park their cars.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

You note that upwards of 60 car parks will be removed, but these cars already park there. Many properties are too high up the hillside for them to dig out their own off-street parks. You need to provide assistance to these people, not arbitrarily require them to park their cars somewhere distant from where they live. At the same time, you need to provide some group carparks at

various locations up Raroa Road. If you think that is too hard to do, perhaps you should rethink the proposal??

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Is there actual space available on Little Palmer Street to widen it as you have illustrated?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

You are requiring people to change their lives significantly. We supposedly live in a democratic society not a dictatorship.

Feedback**81**

114167761788

NAME: Fionnghuala Cuncannon	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Subject to the proposed change I have commented on about the bus stop on Raroa Road I support the changes.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

I strongly support the proposed changes, except for the proposal to move the location of the bus stop in front of 38 Raroa Road to be in front of 40 Raroa Road. This is not sensible. The stretch of road from the roundabout (Raroa/Fairview) to Moana Road is not at all safe at the moment because it is narrow and winding and with carparking a number of cars travel too quickly and cross over the centre line. For that reason I would like to see the speed dropped for that stretch

to 40km/h, or even 30km/h. Even if these changes resolve that somewhat, the bus stop should be left in front of 38 Raroa where the road is slightly wider. It would be dangerous to move it to 40 Raroa which does not leave much space between it and the proposed new bus stop on the other side of the road. Cars will seek to sneak between the gap when two buses happen to be at both bus stops simultaneously. As an owner of both 38 and 40 Raroa I have a strong preference for the bus stop to be left where it is. What is proposed is dangerous, and there is no reason why it should not be where it currently is in front of 38 Raroa.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**82**

114181665173

NAME: Fleur Rohleder	SUBURB:	ON BEHALF OF: Fire and Emergency New Zealand	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	
People walking on the paths through Aro Park and Little Palmer Street?	
People riding bikes along this route?	
People driving vehicles on the surrounding streets?	
People with disabilities or accessibility issues?	
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Why do you think that?

Submission%20-%20Fire%20and%20Emergency%20-%20Aro%20Valley%20Project.pdf

Submission on Aro Valley Connection Transport Project

To: Wellington City Council

Submission on: Aro Valley Connection Transport Project

Name of Submitter: Fire and Emergency New Zealand

This is a submission on the Wellington City Council Aro Valley Connection project. The specific elements of the project that Fire and Emergency's submission relates to is the provision for timely and adequate emergency access through the area.

Fire and Emergency's submission is:

In order for Fire and Emergency to achieve its principal objective which includes reducing the incidence of unwanted fire and the associated risk to life and property, protecting and preserving life, and preventing or limiting injury, damage to property land and the environment, Fire and Emergency requires adequate access to property and structures throughout the area to ensure they can respond in emergencies. In addition to fire safety and response, Fire and Emergency's functions under the Fire and Emergency New Zealand Act 2017 Act (the Act) also include responding to motor vehicle accidents, medical emergencies, weather events and disasters, other rescues and hazardous substances incidents. As such, Fire and Emergency has an interest in transport infrastructure projects and street upgrades to ensure that, where necessary, appropriate consideration is given to operational firefighting requirements.

Fire and Emergency consider it is vital for the health, safety and wellbeing of communities that the needs of emergency services are taken into account as transport network upgrades / changes are developed and implemented.

Adequate access to both the source of a fire (or other emergency) and a firefighting water supply is essential to the efficient operation of Fire and Emergency. The requirements for firefighting access are set out in the New Zealand Fire Service Firefighting Water Supplies Code of Practice SNZ PAS 4509:2008 (Code of Practice)¹ and further detailed in Fire and Emergency's 'Designer's guide' to firefighting operations Emergency vehicle access' (December 2021). This document is attached to this letter as **Attachment 1**.

In general, the key access requirements include specific roading and access widths, and surface and gradients to support the operational requirements of Fire and Emergency appliances. This includes, but is not limited to the following:

- The minimum roading and carriageway widths should not be less than 4m for general appliance access. This width is required for firefighters to efficiently work around the fire appliance to access hoses and pumps.
- A clear passageway / vehicle crossing of no less than 3.5m wide should be provided as site entrances, internal entrances and between buildings.
- The maximum negotiable gradient is 1:5, but in general the roading gradient should not exceed 16%.

¹ The New Zealand Fire Service Firefighting Water Supplies Code of Practice SNZ PAS 4509:2008 can be found here:

<https://fireandemergency.nz/assets/Documents/Files/N5a-SNZPAS-4509-2008-NZFS-Firefighting-water-supplies-Code-of-practice.pdf>

- The height clearance along accessways (for example trees, hanging cables and eaves) must exceed 4m.

Importantly, Fire and Emergency must be able to operate pumping appliances from a hard standing. Often, this can be done from the public road, and this is how Fire and Emergency prefers to operate where possible. Pumping appliances are vehicles used to pump water for firefighting (refer Appendix A of the Fire and Emergency's 'Designers' guide). They carry a relatively small amount of water (1,350–2,000 litres) and have a limited length of hose. Accordingly, Fire and Emergency must have access to a water supply and must also be able to base operations near the building, so firefighters can reach the fire with water. Refer Appendix B of Fire and Emergency's 'Designer's guide' that sets out compliant site layout examples. Aerial appliances may be required to respond to multi-storey and / or higher-density developments. These appliances have more complex access requirements outlined in Attachment 1.

Fire and Emergency recognises the significant advantages of prioritisation of street space for pedestrians and cyclists between the city and Aro Valley. However, in prioritising street space for pedestrians and cyclists, care must be given to avoid unintended consequences on Fire and Emergency's ability to respond to emergency events. Prior engagement with Fire and Emergency and other emergency service providers is therefore important before such measures are finalised and implemented.

In some cases, the provision of cycleways on main arterial roads, lowering of speed limits, and incorporation of speed controls as part of traffic-calming programmes may inadvertently block or significantly slow down a route required by Fire and Emergency. If an emergency appliance needs to park on a road, this can mean the street / road is blocked to all other traffic including other emergency vehicles that may need to get past.

Fire and Emergency seeks the following from the consent authority:

- That Wellington City Council engage directly with the Wellington Fire and Emergency team to ensure that the changes proposed in the Aro Valley Connection project do not adversely impact Fire and Emergency's ability to respond to emergency events.
- More generally, Fire and Emergency request that on similar, future projects that include the improvement of active transport connections and implementation of traffic calming measures, Wellington City Council considers the impact on emergency service vehicles and consults with the relevant authorities.

Fire and Emergency welcomes any questions on this submission and looks forward to continuing to work with Wellington City Council to provide for the safety of Wellington's communities.



Fleur Rohleder
Planner, Beca Ltd.
on behalf of
Fire and Emergency New Zealand

Date:	21/11/2022
Email	[REDACTED]
Address:	PO Box 3942 Wellington
Telephone:	[REDACTED]

Attachment 1

Designers' guide to firefighting operations

Emergency vehicle access

F5-02 GD



Document Title: Emergency vehicle access

Published: 10 December 2021

Document review date: 10 December 2023

Status of this document

This document is issued by Fire and Emergency New Zealand.

Recommendations for change

The document, its content and specific processes are not to be altered except through Fire and Emergency New Zealand document management processes.

Requests or recommendations for changes to this material should be sent to National Manager Response Capability.

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1. Context

Scope	We need to be able to reach your building with our different vehicles in a fire or other emergency. This chapter helps you understand the types of vehicles we use, and how you can provide access for them on your site.
Who this chapter is for	<p>This chapter outlines our position on appropriate considerations for building owners, building designers and other building practitioners, on emergency vehicle access to sites, both completed and under construction/refurbishment.</p> <p>It may also provide useful guidance for anyone undertaking planning of any kind that needs to consider emergency vehicle access.</p>
What is not included in this chapter	<p>This chapter is a guide to provide advice to the building industry on Fire and Emergency's operations and recommendations in relation to emergency vehicle access – it does not replace any mandatory/statutory requirements.</p> <p>We recommend you read it alongside other chapters in the guide. This is not an exhaustive guide to Fire and Emergency operations, but an overview of the relevant expectations building industry stakeholders can have of our operations.</p>
Legislative framework	<p>We aim to reduce the risk to both firefighters and building occupants through encouraging appropriate building design which allows us to achieve our statutory objective (under the Fire and Emergency New Zealand Act 2017) to reduce the incidence of unwanted fire and the associated risk to life and property. Our functions include responding to and suppressing fires and attending to other types of emergencies that may occur in a building.</p> <p>Read this guide alongside the:</p> <ul style="list-style-type: none"> • mandatory requirements of the New Zealand Building Code (Building Code) • requirements of New Zealand Standards (Standards), and • Building Act 2004. <p>This guide does not replace any part of the Building Code or Standards or other mandatory building requirements.</p> <p>We note that the Building Code Fire Safety C – Protection from fire clauses C1– C6 define the Building Code performance requirements of the Building Act 2004. Clause C5 is the performance requirement on 'Access and Safety for Firefighting Operations'.</p>

2. Definitions

The following definitions apply for the purposes of this chapter. Defined terms used throughout this document are consistent with the Building Act 2004, Building Code and Acceptable Solutions C/AS2.

Address point	This point is part of the data set administered by Land Information New Zealand, (LINZ). It is the address (point) where the building is commonly known to be located. It can be either a single point or a range of individual points as described on the LINZ data set.
Aerial device	Encompasses all the types of Fire and Emergency aerial components (turntable ladder, elevating platforms, elevating monitors, baskets, cages and booms).

Aerial vehicle	A specialised emergency vehicle that has an aerial device that hydraulically rises to suppress fire and/or effect rescue as well as support other operations.
Allowable bearing pressure	The calculated pressure required to counter compression forces exerted by dead loads (i.e. the minimum strength required to maintain stability under a weight load).
Appliance	An emergency vehicle that provides capability to Fire and Emergency's mandated functions.
Attendance point	<p>The place where the first attending Fire and Emergency pumping vehicle will stop and set up. There is only one attendance point, usually, at the building's primary entry point. Firefighters may be deployed to other firefighter access points from here.</p> <p>A full description of the attendance point can be found within F5-02 GD FFO Emergency vehicle access.</p>
Breathing apparatus (BA)	A device firefighters wear to provide breathable air in an atmosphere that is immediately dangerous to life or health. Also known as self-contained breathing apparatus (SCBA) or compressed air breathing apparatus (CABA).
Building hydrant system	Fixed water main pipe system, normally already charged with water and supplemented by Fire and Emergency pumps. This should not be confused with an in-ground hydrant connected to the town mains.
Canopy	Projecting hood supported on brackets, corbels or columns over a door, window or niche.
Carriageway	The driveable portion of a road (which may or may not include a sealed top surface layer).
Collapse zone	<p>The collapse zone is an area around the building measured as 1.5 times the height of the structure. This is the area which would be considered dangerous in the event of an outward failure of a facade element.</p> <p>In this document, the term 'collapse zone' only applies to pre-cast concrete panel (tilt-slab) and unreinforced masonry type construction.</p> <p>Use a pragmatic approach where practicable when designing, and when in doubt, consult Fire and Emergency.</p>
Fire engineering brief (FEB)	A formal process outlined in the International Fire Engineering Guidelines for all stakeholders to define and agree on the basis and scope of work for fire engineering analysis.
Firefighter access point	<p>The place where firefighters gain access to a building. This must comply with the New Zealand Building Code Clause C5.6:</p> <p>Buildings must be designed and constructed in a manner that will allow firefighters, taking into account the firefighters' personal protective equipment and standard training, to:</p> <ul style="list-style-type: none"> (a) reach the floor of fire origin, (b) search the general area of fire origin, and (c) protect their means of egress.

**Fire and
Emergency
vehicular access**

Vehicular access provided to Fire and Emergency vehicles should be consistent with Acceptable Solution C/AS2 Part 6 (relating to firefighting).

Buildings must be provided with access that allows appliances to reach a position that makes it convenient for firefighters to get into the building and access the inlets to fire sprinkler systems or building fire hydrant systems, where these are installed.

Occupants of risk group SI are more likely to require rescue by Fire and Emergency. An additional recommendation for this risk group is to allow access for the larger size of aerial vehicles to get as close to buildings as possible with space to 'jack' the vehicle.

If a building has a large footprint (which is most likely to occur for a single-storey building such as a warehouse) and is not protected with fire sprinkler systems, access to two sides of the building is required. This gives Fire and Emergency the ability to access the building in a number of places and means that their travel within the building is minimised to reach any fire source.

In addition, for the health and safety of our personnel, this access:

- should not involve a canopy, or other part of a structure to drive or park under
- should be located outside a horizontal collapse zone requirement of 1.5 times the height of a portal frame building
- should be within 135 metres of a firefighting water supply.

Where access meets these recommendations above, and is acceptable to Fire and Emergency, the 75 m hose run may be measured from this hard-standing point.

**Hard-standing
area (for Fire
and Emergency
vehicles)**

A hard (roading) surface capable of withstanding the fully laden weight of a fire appliance from which fire operations for a structure are conducted. A hardstanding should be big enough for the fire appliance to enter, exit and manoeuvre and for firefighters to move around it to connect hose and safely access equipment. In most cases, the hardstanding will be the main road if the structure is close to it.

A full description of the hardstanding area can be found in this chapter (F5-02 GD FFO Emergency vehicle access).

Jacks

External outriggers and jacks fitted to aerial vehicles that extend to stabilise the vehicle when its centre of gravity shifts during the operation of the aerial device.

Overhang

The portion of a vehicle's body that extends forwards past the front wheels or backwards past the rear wheels. It relates to body swing, which is where a set of wheels when turning acts as a pivot point and the bodywork swings past that point. The longer the overhang, the greater the body swing.

3. Our operations

Vehicle types

We use several different types of vehicles (also referred to as appliances).

Each type of vehicle carries a different combination of equipment and has a specific function at an emergency incident. Most of our vehicles have a specially built body fitted on a commercial vehicle chassis, normally a truck.

Vehicle types include pumping appliances, aerial appliances and a range of specialist function vehicles such as mobile command units, logistics and support vehicles, and water tankers.

Timeliness

To fight a fire effectively, save lives and limit damage to property, we need to respond quickly and start applying water while the fire is still small. While automatic detection systems and good information help us respond quickly, good access is also vital.

This highlights the importance of having designated hard-standings with all the provisions for a fast fire attack proximate to as many parts of a building as possible, that are free from obstructions.

4. Challenges

4.1. Fire and Emergency vehicles

Capability of vehicles

Pumping appliances are vehicles used to pump water for firefighting. They carry a relatively small amount of water (1,350–2,000 litres) and a limited length of hose. This is why we must have access to a water supply and must also be able to base our operations near the building, so firefighters can reach the fire with water. Often, this can be done from the public road, and this is how we prefer to operate where possible. However, for large sites, sites with multiple buildings, or sites with large set-backs, our vehicles may have to operate from within your site, which is less favoured.

Aerial appliances are larger and heavier than our other vehicles and may be on a two-, three, or even four-axle heavy vehicle chassis. Aerial appliances have limited reach and need to get close to buildings or structures to operate effectively. We will normally try to reverse these vehicles into position beside a building and, where possible, operate from building corners.

For these reasons, we recommend that you provide access and working space for Fire and Emergency vehicles on your site.

Vehicle dimensions

Each vehicle type has different dimensions. Table 1 below shows maximum vehicle dimensions of Fire and Emergency's current fleet of vehicles.

Table 1 – Maximum parameters for Fire and Emergency vehicles

Dimension	Maximum dimensions
Gross vehicle mass	25 t
Maximum overall length	12.6 m
Maximum overall width	2.55 m (6.5 m when stabilisers are deployed)
Required free height	4 m

4.2. Access requirements

Carriageway widths

Carriageways should be wide enough to allow our vehicles to get through them easily and to allow us to carry out emergency operations. This means that when our vehicle is parked, we can easily open and exit the doors, access equipment from its compartments and safely connect the hose to the pump.



Figure 1 – A pumping appliance showing width required for hose

To accommodate a Fire and Emergency vehicle, carriageways should have a minimum width of 4 m. This can be reduced to a minimum width of 3.5 m at entrances, provided tight turns are not required (see Figure 2 below).

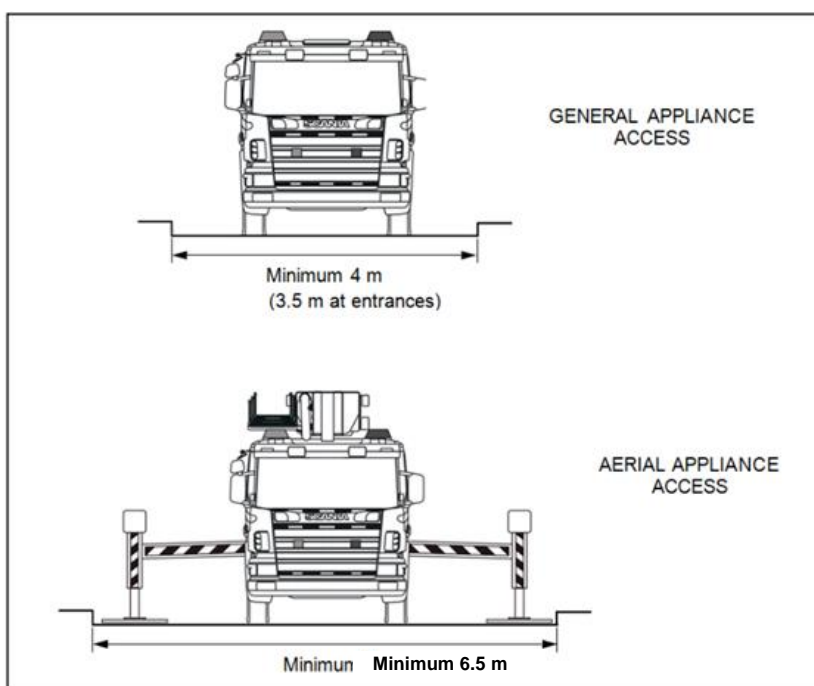


Figure 2 – Minimum carriageway widths along straight sections

Curved carriageway sections should allow for expected vehicle body swing. The minimum distance between the inner and outer arcs should be not less than 5.0 m for pumping vehicles and 7.3 m for aerial vehicles (see Figure 3).

For pumping vehicle access, the minimum inner radius should be 6.3 m and the outer radius 11.3 m. For aerial vehicle access, the minimum inner radius should be 5.2 m and the outer radius 12.5 m (see Figure 3).

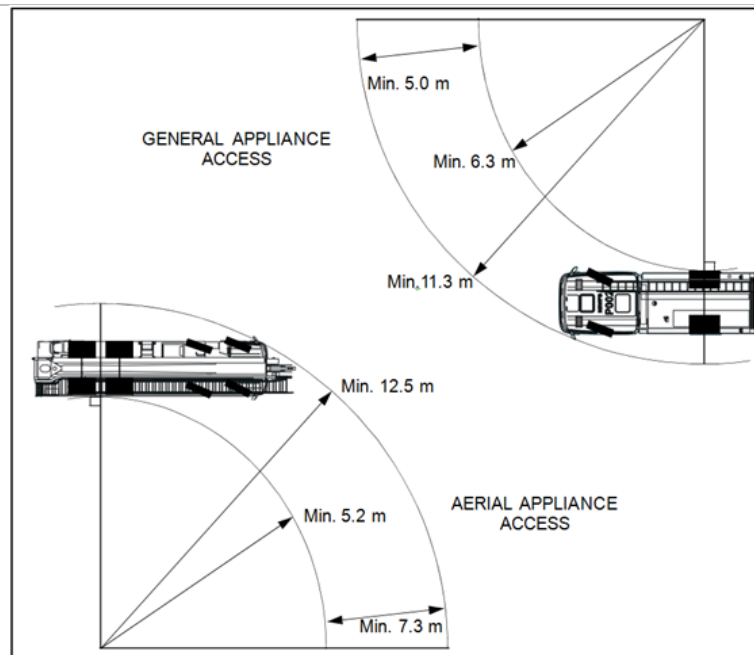


Figure 3 – Minimum carriageway widths – curved sections

The radius dimensions above are for wall-to-wall clearance from body overhang, and do not represent the vehicle's wheel tracks.



Figure 4 – Showing long rear overhang



Figure 5 – Showing long front overhang

Kerb dimensions

Kerbs built along the edges of a carriageway should be no higher than 250 mm and should be free of vertical obstructions at least 300 mm back from the kerb face to allow clearance for front and rear body overhang.

This means that if absolutely necessary, we can mount the kerb with our vehicles, although this is a last resort due to the additional hazards.

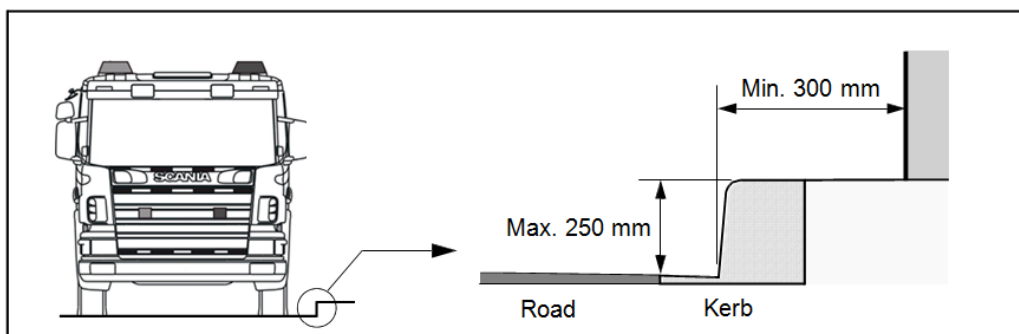


Figure 6 – Carriageway kerb clearance dimensions

Turning areas

Any carriageway with a dead end needs a turnaround area so that our vehicles don't have to do multi-point turns to turn around. This is so we can move our vehicles quickly in an emergency to protect them.

Fire and Emergency vehicles need to be able to turn a full 360° within a 25 m circle (wall-to-wall clearance) to meet Waka Kotahi NZ Transport Agency requirements. The minimum turning radius of turnaround areas should be no less than 11.3 m for pumping vehicles and 12.5 m for aerial vehicles (see Figure 3).

The Waka Kotahi NZ Transport Agency's Road and traffic guidelines for New Zealand on-road tracking curves for heavy motor vehicles (RTS 18) as indicated in Table 1, should be considered. Table 2 below summarises the tracking curves and their radii for design vehicles.

Table 2 – Turn radii and tracking curve sheet numbers for the design vehicles at various radii
(Source: <https://www.nzta.govt.nz/assets/resources/road-traffic-standards/docs/rts-18.pdf>)

Vehicle	Radius of turn				
	10 m	12.5 m	15 m	20 m	25 m
8 m rigid truck	1	2	3	4	5
11.5 m rigid truck		6	7	8	9
Semi-trailer*		10	11	12	13
Tour coach		14	15	16	17

Table 3 – Fire and Emergency fire vehicle types in relation to the Waka Kotahi NZ Transport Agency on-road tracking curves

Fire and Emergency fire vehicle type	Waka Kotahi on-road tracking curve
Pumping appliance	8 m medium rigid truck
Aerial appliance	12.6 m rigid truck

Change of level	The only acceptable means of providing access through a change of level is a ramp that meets the requirements set out below. Fire and Emergency vehicles are not designed to drive up or down steps.
Access ramps	Ramps should not delay vehicle response and should provide entry and exit clearances for Fire and Emergency vehicles.
Gradients for straight ramps	Fire and Emergency prefers a ramp gradient of 1:8 or less for straight ramps. The maximum straight ramp gradient our vehicles can negotiate is 1:5.
Gradients for curved ramps	Access ramps that follow a curved or circular profile in plan view should have a maximum gradient no greater than 1:10 (measured along the centre line). The vehicle chassis will twist and flex when driving up a curved ramp, so we need a lower gradient.
Change of ramp gradients	Access ramps should have a smooth transition between the main ramp gradient and entry and exit gradients. A minimum 4.0 m long 1:15 transition grade is best for both ramp approach and departure (see Figure 7 below).

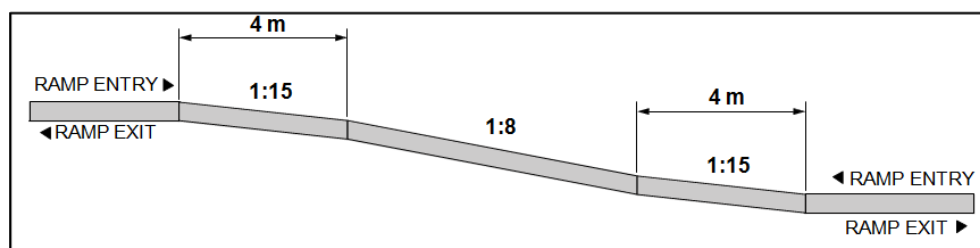


Figure 7 – Maximum access ramp gradients

Reduced gradient clearance

When a change of gradient includes a recessed threshold such as a gutter (e.g. for storm water drainage), the reduced approach and departure clearance should be allowed for in the design of the access way (see Figure 8).

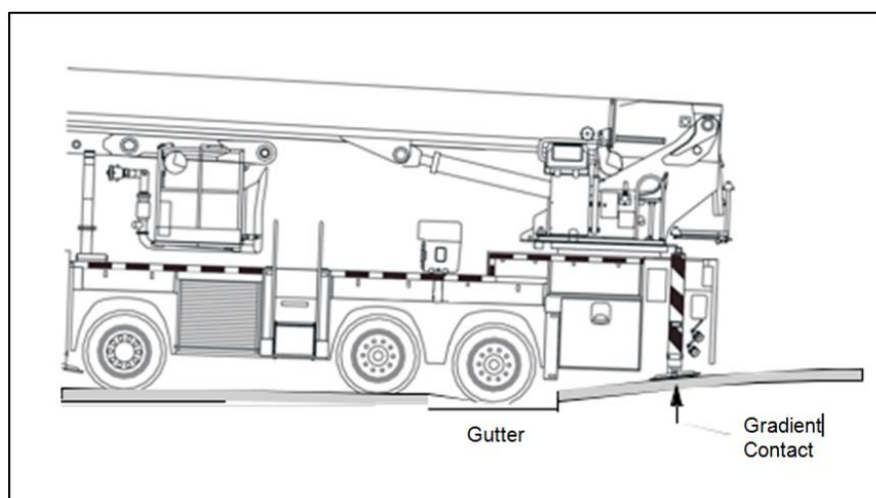


Figure 8 – Reduced gradient clearance due to gutter

When wheels go into a gutter, the body slants downwards, reducing the effective underbody clearance height at both the front and rear overhanging sections. The clearance is even smaller when the gutter is deeper and/or when the overhang is longer.

Building and structure

We need vehicle access routes to have an unobstructed clearance height of at least 4.0 m so that vehicles can pass through openings. This includes clearance from

clearance height

building construction, archways, gateways/doorways and overhanging structures (e.g. ducts, pipes, sprinklers, walkways, signs, structural beams, trees, hanging cables, etc.).

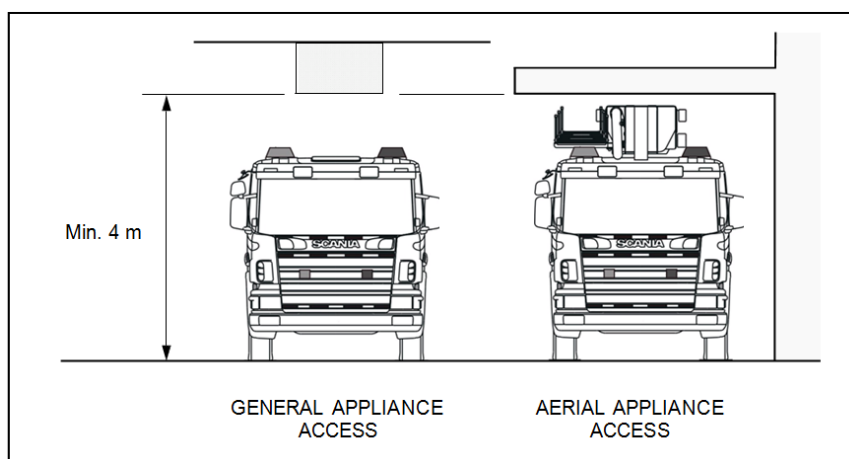


Figure 9 – Building and structure clearance heights

Note: Special considerations apply where there are both height restrictions and gradient changes. In some cases, height clearance will need to be more than 4.0 m so the vehicle can make the gradient change.

Ensuring clear access

We need clear access routes for our vehicles at all times.

Site managers should ensure that nothing blocks or partly blocks the carriageways for our vehicles. We need to be able to drive through access routes during all weather conditions. This means we need some form of hard-standing so our vehicles don't get bogged down. If a vehicle gets stuck, it creates two problems, we can't use it, and it may stop other vehicles getting through.

Perimeter security points (e.g. sliding/swinging gates, boom gates, bollards and vehicle security barriers) should not make it difficult for vehicles to gain access.



Figure 10 – Clear access available to a site

Site entrances, internal entrances and space between buildings should be at least 3.5 m wide and 4 m high.

The following common occurrences often make access difficult:

- Overhanging vegetation which restricts height clearances
- Overgrown vegetation which restricts width access and clearances

- Illegally parked vehicles in long driveways, narrow rights of way or halfway onto kerbs in small streets.

Contact us at designers.guide@fireandemergency.nz to discuss.

4.3. Vehicle weights (loads)

Static loads of vehicles

Carriageways need to be able to withstand the load of a Fire and Emergency vehicle, particularly if they are supported, elevated or reinforced by structural members (e.g. suspended floors, ramps, wharfs, aprons, etc.).

Figure 11 shows the vehicle loads exerted through the wheels that are used to determine forces acting through load-bearing structural members. Wheelbase distances between the front and back axles range from 3.7 to 5.5 m for pumping vehicles and 4.4 to 5.6 m for aerial vehicles. Designers should consider the distances between the wheels – both longitudinal and lateral – when calculating point loads for the wheels.

Note: Axle loads, such as those shown in Figure 11, are not always evenly distributed over all wheels.

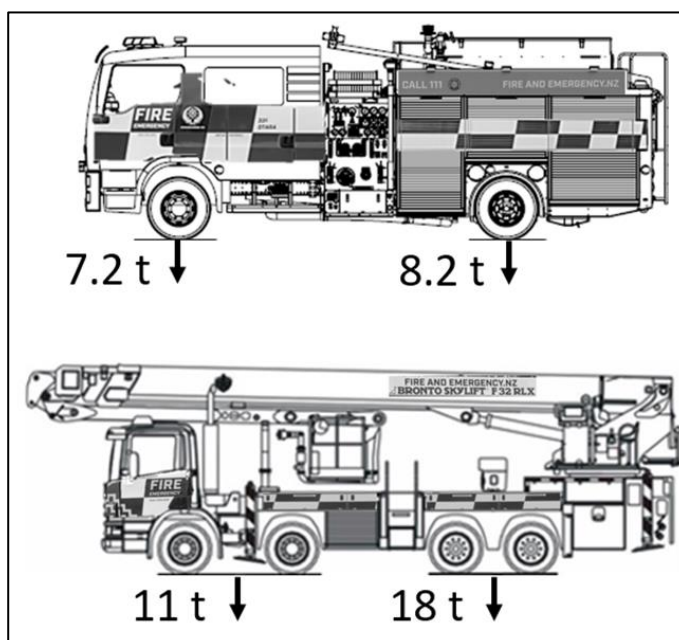


Figure 11 – Axle loads of vehicles

In general, access routes should be able to withstand a laden weight of up to 25 tonnes with an axle load of 8 tonnes or have a load-bearing capacity of no less than the public roadway serving the property, whichever is lower.

Roadway pavements designed for aerial vehicles must withstand a vehicle with multiple axles spaced at no less than 2.5 m centres and each carrying 8.2 tonnes.

The hardness of the carriageway surface should withstand static pressure of no more than 850 kPa from a vehicle's tyres.

Note: Pavements Fire and Emergency vehicles use for access should be designed according to Waka Kotahi NZ Transport Agency's HN-HO-72 traffic loading specifications, to meet the load-bearing requirements.

Dynamic loads (on aerial vehicles)

Aerial vehicles are fitted with stabilisers that prevent the vehicle from overbalancing when the aerial device is operating. Aerial vehicles will either have two stabilisers at the rear only, or more commonly, two front and two rear stabilisers (see Figure 12).

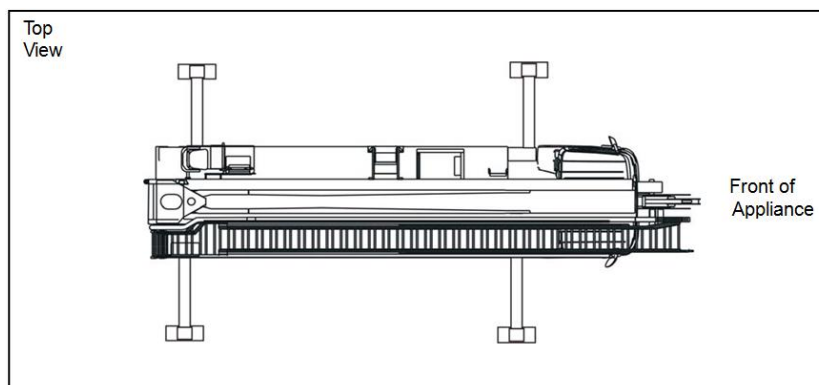


Figure 12 – General stabiliser arrangement on aerials

Extending and rotating the aerial device changes the vehicle's weight distribution and creates other forces, such as torsion moment forces. These exert dynamic forces through the stabiliser.

Note: The changing distribution of weight can cause up to 70 percent of the total vehicle weight to be borne by a single stabiliser.



Figure 13 – Highlights the space requirements for jacking stabilisers

The maximum dynamic loads and pressures exerted through a single stabiliser of the Bronto Skylift F44 RLX, with a fully loaded cage (500 kg), at maximum extension/outreach and under worst-case rotation angle, are:

- maximum stabiliser force: 200 kN
- maximum footplate pressure: 11 kg/cm² (1079 kPa)
- maximum bearing plate (block) pressure: 2.8 kg/cm² (274 kPa).

Consider the maximum exerted pressures above when calculating the minimum Allowable Bearing Pressure (ABP) for the carriageway or hard-standing area.

4.4. Site access and security features

Security features can delay site access

Many sites have security measures in place that restrict public access. These are to meet legal requirements for health and safety in the workplace and to keep the site and its staff secure.

However, enhanced security measures often delay firefighters when they investigate fire calls. Features such as security gates, high fences and bollards delay our vehicle access.

Security features can also translate to issues with physical access to buildings, including to locations where firefighters are required to interface with fire systems. Where enhanced security measures are present, this is likely to delay our investigation of the fire call.

There are solutions to overcome the issues presented by enhanced security and these include automatic unlocking or opening of security features upon a fire alarm activation. These may also have a time delay built into the system, so the site remains secure for longer, accounting for our response time. A master lock control switch could also be provided for our use in an area we can access such as a fire control centre (FCC).

Alternatively, where the building fire alarm is connected directly to Fire and Emergency, keys to the site may be provided to us.

Another option is a lockbox on site provided that information regarding its location and its access is provided to us ahead of time. On-site security staff, or contracted security staff who respond automatically in the event of a fire alarm activation, may also be able to provide access for us.

If you have any concerns about responding Fire and Emergency crews having timely access to a site, contact designers.guide@fireandemergency.nz to discuss options.

4.5. Vehicle hard-standing

Vehicle hard-standing requirements

A vehicle hard-standing is a designated area that can withstand the laden weight and associated loads of the Fire and Emergency vehicle and its crew and facilitate firefighting operations.

For our vehicles to work effectively, the hard-standing must be as close as possible to both the water supply and the structure to be protected. We encourage you to follow the guidance within this document. If you can't meet the criteria in this chapter, email designers.guide@fireandemergency.nz for help.

Under Clause C5.3 of the Building Code:

Buildings must be provided with access for fire vehicles to a hard-standing from which there is an unobstructed path to the building within 20 m of:

- (a) the firefighter access into the building, and
- (b) the inlets to automatic fire sprinkler system or fire hydrant systems, where these are installed).

This is to enable firefighter to get into the building and to move freely around our vehicles.

Under Clause C5.4 of the Building Code:

Access for fire vehicles in accordance with clause C5.3 must be provided to more than 1 side of firecells greater than 5,000m² in floor area that are not protected by automatic fire sprinkler system.

The hard-standing should:

- comply with Section 4.2 of this chapter regarding access requirements
- enclose a rectangle at least 4.0 m wide and 11 m long
- not have a gradient of more than 1:50
 - Stabilisers used on aerial vehicles limit hard-standing gradients. Aerial vehicles can only use their stabilisers and operate if the ground slope is within +/- 5°
- be outside the collapse zone (see 'Collapse zone' in the Definitions section for details)
- be in the open air and have no overhead obstructions along its entire area
- be within 135 m of a pressurised water supply, or within 6 m of an open water source, due to equipment limitations (supply hose)
 - This distance should not include any sharp angles
 - This distance should be measured taking into consideration obstructions such as buildings, fences, waterways and storage or parking areas. See [Appendix B](#) for examples.

Note:

- Hose runs can be measured from this point, provided all the requirements above are satisfied.
- The above hard-standing requirements do not apply to the following classified uses (as defined in Clause A1 of the Building Code):
 - backcountry huts
 - detached dwellings
 - within household units in multi-unit dwellings
 - outbuildings
 - ancillary buildings.

Attendance point

Our policy is to respond to a single attendance point. The attendance point is generally at the building's main entrance and is often (but not always) the same as the address point. This location should include the alarm panel, building hydrant/sprinkler inlets, a suitable firefighter access point, etc.

If there is a remote place within the building which cannot be reached by hose within 75 m of the attendance point, a common solution is to provide a building hydrant system. In certain situations, this is even mandated by prescriptive guidance (for example Acceptable Solution C/AS2, paragraph 2.2.1 and associated tables)).

Note: This attendance point should not be confused with a firefighter access point or vehicular hard-standing, which may be remote from the attendance point and provided with a building hydrant outlet. It may also provide a mimic fire alarm panel or other fire safety features.

The attendance point should also include all the requirements for a hard-standing area and meet clauses C5.3 and C5.7 of the Building Code.

When identifying an attendance point, factor in the following:

- Operational procedures do not allow firefighters to drive vehicles down narrow lanes, under canopies or through flood water
-

- It is our policy not to park a vehicle under a canopy, or within the collapse zone in certain circumstances (see 'Collapse zone' under Definitions)
- Location of and ease of access to fire alarm panel
- Location of and ease of access to the controls for fire safety systems
- Inlets for fire sprinkler and/or building hydrant (riser) systems.

See [Appendix B](#) for examples.

Safer siting areas

We often strategically place our vehicles at building corners, particularly our aerial vehicles.

This is because the corners are generally safer if the building collapses outwards, and we can usually use our aerials across two faces of the building providing for better coverage and observation.

5. Recommendations

Fire and Emergency recommended approach

We need you to consider how you could provide access for firefighting vehicles in the course of your work. Our requirements may differ case by case, basis and you should discuss any queries you have with us.

We recommend you consider the following points:

Consider the dimensions of our vehicles

- Access gates, driveways should meet the minimum dimensions outlined.
- Driving surfaces should be designed to support the weight of our vehicles.

Consider the manoeuvrability of our vehicles

- Dead ends and turning circles should meet the requirements discussed in this chapter.
- Straight ramps designs should take in into account our vehicles' needs, particularly at ramp entry and exit points.
- Curved ramps should be carefully considered in relation to our vehicles' weights and clearances including vehicle overhangs.
- Recesses such as storm water drains should be carefully placed to consider our vehicle movements.

Consider hard-standing recommendations

- Hard-standings should be at the correct distance from building, firefighting systems/inlets and firefighting water supplies.
- Consider vehicle loading requirements for attendance and hard-standing points.
- Allow working space for firefighters in and around our vehicles. Consider:
 - doors opening
 - firefighters exiting vehicles with PPE and BA on
 - whether firefighters can access important equipment around our vehicles, such as ladders and hoses.
- Allow working space for the deployment of stabilisers on our aerial vehicles.

Consider how we will access the site in an emergency

- Consider how any site security could affect our access, particularly outside business hours.

- Keep access routes always clear, particularly from vegetation, parked cars and temporary structures, etc.
 - Speed is critical – the sooner we start firefighting operations, the more likely we are to limit the consequences.
-

5.1. Completing the firefighting facilities checklist

Completing the checklist

When completing F5 SC Part C: 2 Access to site and 3 Access to building of the firefighting facilities checklist (FFFC), you should state what access you have given us to key facilities and the attendance point. This will allow us to understand the proposed layout and ensure that this access meets our operational needs for firefighting.

Remember that facilities are put in place for our use in emergency situations and the location of those facilities should be decided in consultation with us.

6. Related information

6.1. Designers' guide to firefighting operations

- F5 01 GD FFO Introduction
- F5-02 GD FFO Emergency vehicles access
- F5-03 GD FFO Radio communications
- F5-04 GD FFO Fire alarm panels
- F5-05 GD FFO Building hydrant systems
- F5-06 GD FFO Automatic sprinkler systems
- F5-07 GD FFO Stairs in buildings
- F5-08 GD FFO Lifts
- F5-09 GD FFO Fire Control Centres
- F5-10 GD FFO Evacuation and rescues
- F5-11 GD FFO Water supplies
- F5-12 GD FFO Construction, refurbishment and demolition sites
- F5-13 GD FFO Multi-tiered vehicle stacking buildings
- F5-14 GD FFO Firefighting shafts in taller buildings

6.2. Legislation

- [Fire and Emergency New Zealand Act 2017](#)
- [Building Act 2004](#)
- New Zealand Building Code (Building Regulations 1992 > New Zealand Building Code > [C Protection from fire](#))
- [Health and Safety at Work Act 2015](#)

6.3. Standards

- SNZ PAS 4509:2008 Firefighting water supplies code of practice
- NZS 4510:2008 Fire hydrant systems for buildings
- NZS 4512:2021 Fire detection and alarm systems in buildings
- NZS 4541:2020 Automatic fire sprinkler systems

6.4. References

- Waka Kotahi NZ Transport Agency guidelines:
 - [Vehicle mass and dimension rules](#)
 - HN-HO-72 – [Waka Kotahi NZ Transport Agency Bridge manual \(2013\)](#)
Note: This standard covers the requirements for all pavements bearing a heavy load such as a fire appliance.
 - [Road and traffic guidelines – New Zealand on-road tracking curves for heavy motor vehicles \(RTS 18\)](#)
- Acceptable Solution C/AS2 > [New Zealand Building Code Compliance C Protection from fire](#)
- [New Zealand Building Code handbook \(third edition, amendment 13\)](#)

Note: The legislation, standards and references referred to in this guide (including those listed above) are relevant at the time that this document was published. Note however that the legislation/links may have been updated since this document was published.

Appendix A – Images

Pumping appliances

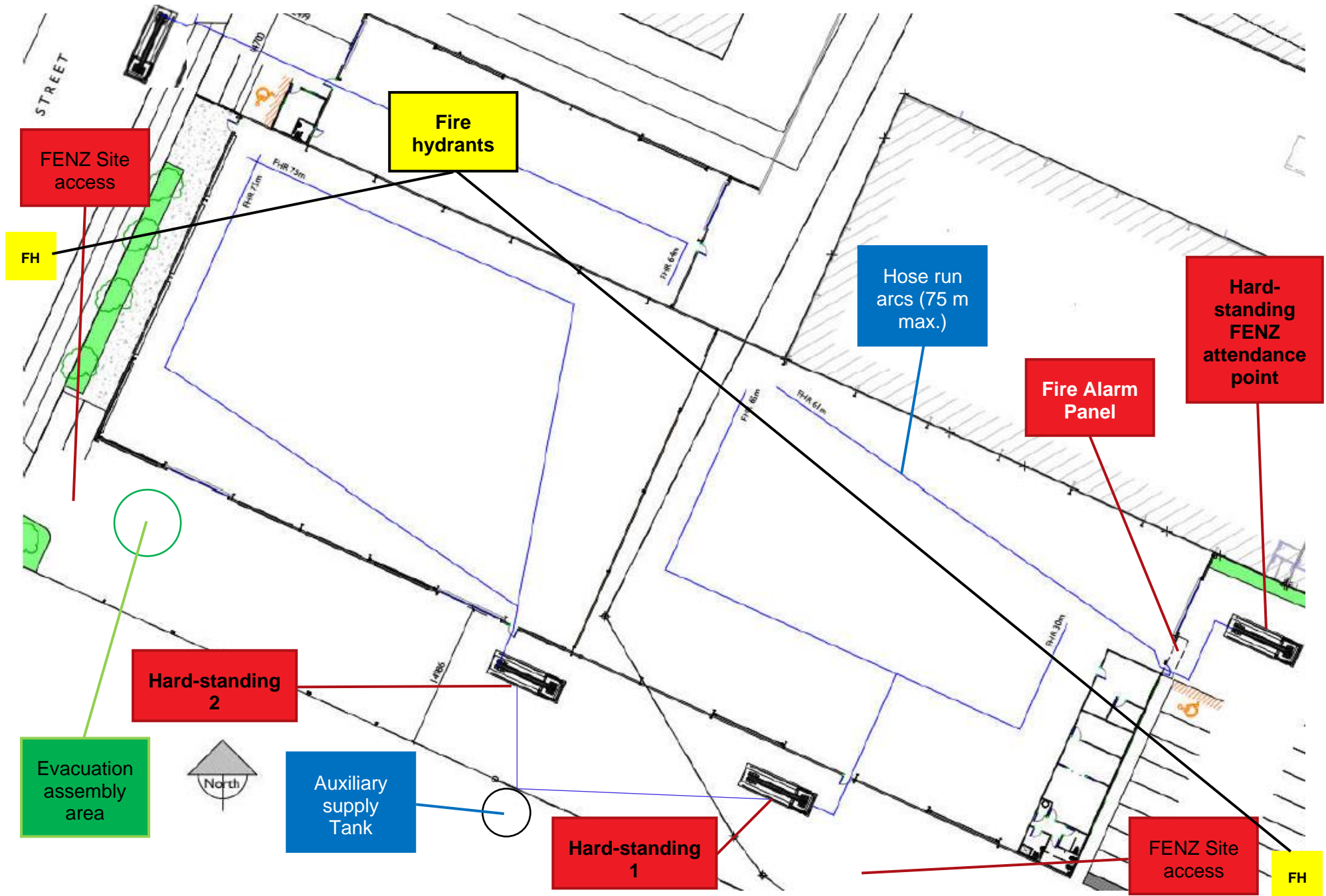


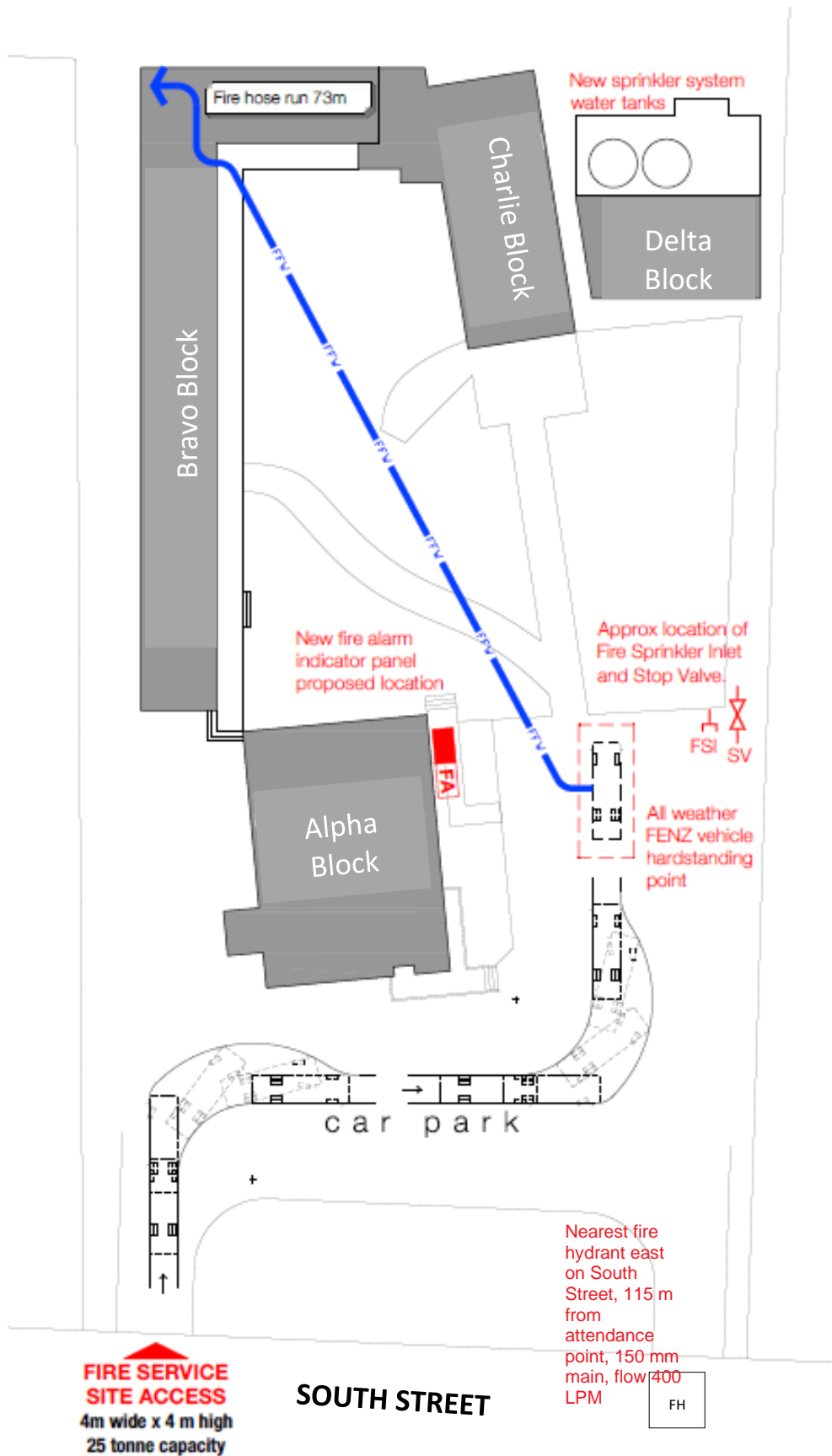
Aerial appliances





Appendix B – Site layout examples





Document information

Owner	National Manager Response Capability
Last reviewed	10 December 2021
Review period	Every second year

Record of amendments

Date	Brief description of amendment
December 2021	Format update and SME content review
March 2018	Initial version

Feedback**83**

114163989149

NAME: Felix Marwick	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

for too long this carriage way has been a storage area for private vehicles - parking should be a lower priority on arterial roads that have higher use

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

expecting huge push back from residents losing car parking - can you offer them consent fee waivers to facilitate off street parking changes on their sections?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
make sure a shared pathway is wide enough to accommodate all users - especially wheelchairs and mobility devices	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
too often these projects are stalled by legal challenges and lengthy redesigns. Do it right, do it quickly and save \$\$ on legal matters and construction costs

Feedback**84**

114181054536

NAME: Frances Forsyth	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The overall effect of the proposed changes is to provide a faster and safe route for people travelling through Aro Street to suburbs like Karori. It strongly disadvantages residents in Aro Street and is advantageous only to the commuters. Karori residents have rejected higher density housing and have rejected the building of a public co-ed secondary school. The carbon footprint of the suburb of Karori is massive compared with Aro Valley on a per head of population basis. Allowing a few more eBikers to feel safe is insufficient justification for the proposed changes

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

To quote the WCC parking policy: For those who find active and public transport does not meet their needs, such as disabled people, older people, and parents with young children, their expectation is for an accessible city where they can readily access facilities, goods and services when and where they need to. Aro Street has high levels of residential housing with no off street parking and pre 1930s homes. Moving all residential parking from the south side of Aro Street completely ignores the needs of people unable to use active or public transport, and WCC policy for such people.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

There has been an increase in housing along this route and parking needs to be adjusting using yellow dashed lines so that parked cars are not reducing the road to one way. Introducing a 30kph shared bike and car zone would mean that cars will have to slow down. This road is too narrow for a mix of public and private vehicles. Slowing the speed will encourage drivers to use a more suitable route or to move to public transport.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Park is a green space and community hub which the community fought long and hard for. We spent a considerable proportion of the budget for the hall restoration on moving the building away from the accessway. This improves visibility and is all that is needed. Simon Kennett stated that Waka Kotahi will be improving Upper Willis St in the next couple of years so there is no further need to widen any FOOT paths. The area is used by hundreds of Aro Valley residents walking to work, school and pre-school. During the day and evening the hall is usually heavily booked for yoga, sit and be fit, and choir etc. Many people attending events at the hall are disabled, elderly, or have small children. The focus should be on foot traffic not on speeding bicycles. It is highly unlikely that if cyclists are given permission to whiz through the park, they will return to the road once Willis Street is improved. This will be a permanent loss of green space in the Park and Little Palmer Street at a time when housing density is increasing

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

There is no option for not making the improvements. We had a raised pedestrian crossing at the shops and it was a failure and had to be removed. Mountainbikers see such humps in the road as something to jump over. I agree that there should be spaces for City Hop vehicles. There is no need to place cycle parks in the heart of the village. If you can ride a bike you are also likely to be able to walk. Why not put cycle parking for 12 bikes, and mobility parking, at the bottom of St

John Street beside Aro Park. Mobility parking is only useful for people visiting the Aro Valley not for disabled residents. The focus of this entire plan is on making it easier for cyclists and motorists to pass through Aro Valley, and almost the entire cost of this will be borne by local residents. We currently have a number of construction projects on Aro Street which are taking more than a year to complete. with new DP rules there will be a lot more construction there. Permits will be required to block the cycleway which will add considerable costs to the price of construction. Ms Pascoe seems to be under the mistaken idea the tradies only go to a job for 120 minutes and that they can park their vehicles and all of the tools a few hundred metres away in a side street. I have spoken to the tradies currently working in Aro Street and they are furious with the idea that they will need permits to park during major build projects. I agree that coupon parking must go. It is rarely enforced and very few people pay for coupons. There is no valid reason for commuters to be encouraged to park in Aro Street, especially when it is at the expense of disabled, elderly and people with small children. This demographic need safe parking close to their homes as described in the WCC parking policy

Feedback**85**

114177388769

NAME: Frank Kelly	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Its going to make life worse for the people who live there. They already cant get parked and this will make it kuch worse

Do you support the proposed speed changes on Aro Street?**Stongly oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly oppose****Do you have any comments to make about the proposed design?**

If you remove all the parking then people will park on the already congested side streets. People park here everyday for work and this will absolutely overrun the side streets. I live on one of those streets and can hardly get parked as it is, this would make it much worse. Im sick of havin car parking spaces that people wctually need being removed for cyclists, its not necessary and its not

the right thing to do. Cyclists should not be priority, it should be fair on everyone and this is just another attempt to make life harder for drivers who need to use their cars.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Don't know

People driving vehicles on the surrounding streets?

Very negative

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Because it shouldnt be happening atall so the longer it can be pushed out the better

Feedback**86**

114161530651

NAME: Freddie Dillon	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Get us to a low carbon future, and gives children and workers more options to get around.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**87**

114160289641

NAME: Jane	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Removing this number of car parks is going to negatively impact on residents, there is no space to park on side streets so this is treating Aro Valley as a thoroughfare, it does not benefit residents at all.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Removing car parks when there is no other options to park is going to negatively impact residents. Creating an uphill bike lane on a steep street is foolish.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback**88**

114173905287

NAME: Garth Bates	SUBURB: Aro Valley	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**89**

114174935869

NAME: Georgina preston	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I don't think the bus stop should change. Get rid of coupon parking straight away. Convert some to 5 min parking

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Negative
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	
I'm concerned you will be removing the trees and shade in little Palmer st	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
As a driver I want bike lanes sooner. I am terrified of knocking a cyclist going up arrow. Once there are bike lanes I will consider getting a bike

Feedback**90**

114165653822

NAME: Giselle Bahr	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Climate change, safe active transport, lots of us people.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

The design and plan look excellently appropriate for the difficult sitting.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
I like that you are acknowledging how hard it is to mode shift. However if the community finds that the transition goes smoothly and they want to implement the later changes more quickly you could be a bit flexible about that?

Feedback**91**

114181467991

NAME: Greg Murison	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Making Wellington's transport system work for our city is key to making it liveable.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

This is a difficult and narrow section of road. Providing more space for people to move will make it safer. While this feels only halfway done, I appreciate how difficult it will have been to fit it all in there with limited budgets.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The lower section, though Aro Valley, is already the most difficult and congested part. Buses cant pass each other, bikes are getting squeezed. 3 years is too long to wait.

Feedback**92**

114171709282

NAME: Graeme Campbell	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Don't need to take too long to do this sort of thing. Cheaper. Get the benefits sooner

Feedback**93**

114177331836

NAME: Graeme	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly oppose

Why do you think that?

The proposed changes will negatively affect the majority of residents and visitors to this area who have or need a car. Cyclists and walkers currently have easy access and flexibility on the roads and paths.

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Dedicated cycle lanes will cause more congestion and narrow the road even further. Bikes using the road currently do not have an issue and can easily navigate the current road without incident. Disagree with the car share only spaces they should be treated as per private cars.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Dedicated cycleways are not required along this road and severely limit safety due to further constricting this road especially around bus stops.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Do you have any comments to make about the proposed design?

Providing walking and cycling alternatives to get people away from the road is a good option for road users and creates a better riding and walking experience.

What do you think of the timing of the proposed approach?

Why do you think that?

Don't make the changes at all. It is costly and will negatively affect users of the area. Removing the car parking will really affect residents and people visiting the area.

Feedback**94**

114178554216

NAME: Grant Clarke	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**95**

114180755779

NAME: Annika Green	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Getting rid of parking spaces in an area where there is already terrible parking issues makes no sense. The majority of the houses on the impacted streets do not have off-road parking solutions for this and you're going to make the issue SO much worse. We live in Wellington which is windy, rainy, and often cold. I don't want to give up my car to walk or cycle in the rain. What I do use but often fails me is the bus service. Invest in making that better, not taking away our cars. The majority of car owners in Wellington don't want to give up their cars and this forces people to move out of a great suburb. Further, most Aro Valley car owners might not use their cars for their commute into the city but a lot of don't work in the city, for example, my flatmate works at the airport. It is not practicable for her to cycle to the airport. And, again, we live in Wellington where it is windy and cold. The weather is enough of a put off for living in Wellington let alone saying we can't have car access. This is ridiculous and out of touch with the city we live in.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Cycling up hill in windy, rainy conditions is a bad idea. Don't take away people's car parks.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

This is a good idea for it has positive impacts without taking away our car parks.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Don't make any changes that necessitate the removal of any car parks. If anything, sort out the issuance of resident and coupon permits. Clearly too many people have them for the parks available at the moment. Further, people already have to park on Holloway Road when there's nothing available on Aro Street. Just because you're labelling Holloway Road that way doesn't solve the problem for residents on Aro Street as the majority of the time Holloway Road is already full.

Feedback**96**

114161592207

NAME: Phil McCracken	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The cycling lobbyists are the equivalent of Grima Wormtongue whispering into the councils ear. They need exorcising from the fabric of our society.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback**97**

114163839028

NAME: Hamish Gordon	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I do not support the overall proposal as I think it will have far too much impact on the majority of road users on this route. WCC traffic data has a vehicle count of 312,000 PCM, vs 3,300 bicycles, so only 1% of road traffic is bicycles. In particularly the proposed changes to Raroa Rd seem unjustified and the WCC appears to have no data on cycle use on this route that justifies this large change to the route. I proposed a better alternative for Raroa Road is to put a shared cycle path on the current footpath (widening it if nessscenary to fit Transit NZ guideline). I currently cycle on that footpath and note that other cyclists do the same thing. Raroa Rd is narrow and twisty and it is much more pleasant to be off the inside uphill lane hard against the bank. I do not think a cycle lane on the inside uphill lane will be any more pleasant to ride up

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I don't live in the area, but I regularly commute through the area mainly by bicycle and car (70:30). I agree that Aro St is narrow for cycling and maybe an uphill cycle lane with no parking from Aro park to Holloway road might work, but I have concerns for residents who have parking along there. My other issue is Raroa Rd/Aro St are one of the 3 main arterial routes into the city from the western suburbs. Based on the WCC traffic counts there are 10,400 vehicle movements on Aro per day, so 312,000 per CM. Cycle traffic has been counted at 3,300 per CM, so that is 1%

of the traffic is cycles, so this proposal seems a massive change that will only benefit the 1% of users (cyclists) and negatively impact on residents and the 99.99% of vehicle users on this route.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

My main use of this road is cycling to and from town via this route. I oppose the cycle lane proposal on Raroa road as I do not think there is the cycle traffic using this route that justifies such a major change to this route. This road is narrow and twisty and the proposed changes will just make it more unsafe to use as a cyclist or for cars, Heavy vehicles and buses. My other issue is Raroa Rd/Aro St are one of the 3 main arterial routes into the city from the western suburbs. Based on the WCC traffic counts there are 10,400 vehicle movements on Aro per day, so 312,000 per CM. Cycle traffic has been counted at 3,300 per CM at Aro St, but there is no cycle data collected from Raroa Rd. Based on the Aro St count only 1% of the traffic is cycles, but as a regular cyclist using this road I don't see many cyclists on it. I would propose that the WCC consider widening the existing foot path and making this a shared cycle/pedestrian path with pedestrians having the ROW. This works for Birdwood Street where there is a similar existing path.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Do you have any comments to make about the proposed design?

I cycle through this park as do many others. However, the proposed design risks turning the park into a cycleway with a park around it. It is a small area as it is and you risk changing the current amenity focus of the park.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I think the whole proposal seems rushed and the cycle planners have admitted that they don't have particularly accurate data on cycle movements: -The current cycle meter is on Aro St below Aro Park so cyclists using the Aro Park/Little Palmer St aren't counted -Many cyclist using Aro St

are mountain bikers accessing the Polhill trails, so only riding as far as Holloway Road. -Cycle meter does not count carbon fibre bikes (~40% of mountain bikes are made of this) -No cycle meter or count on Raroa Road so there are large assumptions on actual cycle usage.

Feedback**98**

114164046518

NAME: Harvey Livschitz	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

do this sooner

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**99**

114167641247

NAME: Charles Thompson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It's great to see WCC getting serious about encouraging cycling. With the availability and popularity of electric bikes, our geographic challenges are lessened... now we need more safe places to ride.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

It will be very important to use signs extensively to make it clear to all road users in the shopping district that there is likely to be cross traffic.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Where Raroa Rd narrows above Mt Pleasant Rd is currently the #1 worst experience for cyclists and buses. I understand that widening the road is expensive and risky so a dedicated bike lane is a must.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Park also needs some drainage improvements. It is often boggy. In a couple of places, this route could also benefit from a convex traffic mirror.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think the priorities are right. I personally feel that Raroa Rd is the most urgent and probably most expensive, so it's good to see that included in the 2023 plan.

Feedback**100**

114179789543

NAME: Leoni Hawkins	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I use the Aro Valley route into the city a lot and it has become a busier through route for cars and tradie vans adding buses into the mix, it can be quite hair raising some days so a separate bike lane will be a great improvement to the safety of the road. I will also appreciate and use the bike parking at the Aro shops.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

This is a good short term change until the intersection of Aro St and Willis st is improved for biking.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I think the changes for residents are about right but i think that the coupon parking could be removed sooner.

Feedback**101**

114181625367

NAME: Hayden Hockly	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The safety of our community and reducing emissions are of paramount importance. People living in Aro Street are close enough to the city to not need a car and these changes will make it easier and safer not to have one.

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

The parking should be deleted from the down hill, North side of the road leaving parked cars next to cycle lane. This will be safer for cyclists, will mean less debris on the cycle way and will mean fewer parks removed. Cycle lane should also be raised to footpath height - It will be safer, mean less debris and fewer puncture and remove need for dangerous (to cyclists) bollards. The footpath from Entrance Street to Adams Tce needs to be improved and widened also as the cars really wizz past here. Quite terrifying for a parent of small children being sometimes only inches from disaster. The speed limit should be lowered to 30km for all of Aro Street - up to Entrance street. This will make it safer for Families and children going to school and living their lives to cross the street.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Please make the cycle lane raised to ensure cars do not encroach and that the surface doesn't get littered with glass and stones thrown off the road by cars. Safer and fewer punctures. Raroa road from Cluny Ave to Moana Road is also dangerously narrow with parked cars creating a single lane bottle neck - I hope this section of road will also have the uphill side of parks removed also.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Yes this path should be widened but it should only be for pedestrians and (perhaps uphill cyclists) only - like some trails around the city. There seems to be little benefit to going through park and Palmer street for cyclists riding down Aro street and they should continue all the way to Willis street and not ride through the park. The corner of the path passing near the hall and preschool is a frequent point of near misses and collisions and the combined approach speed of bikes in both directions increases this risk.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The sooner you make all these changes the better. The safety of our community and reducing emissions are of paramount importance. People living in Aro Street are close enough to the city to not need a car and these changes will be make it easier and safer not to have one.

Feedback**102**

114169286294

NAME: Hayden ricketts	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It'll make the road much safer for cyclists and easier for motorists without so much car storage along the route.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I fully support the removal of car parks along here. Cannot wait for this to be a safe place to bike with my children.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

My car was recently parked at 93 Raroa Road (parked by ex-wife after she borrowed it). It was hit by a kid going too fast downhill. The volume of people parking up there (I live on Norway street) has become a major hazard on such an arterial route.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I often cycle this route with my kids. It's a great safe route away from traffic.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I'm not in favour of speed humps or raised pedestrian crossing. I think the same thing can be achieved with an enforcement campaign or a fixed 30kmph speed camera. Then we won't wreck drivers vehicle suspension.

Feedback**103**

114165244940

NAME: Hector McLachlan	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

These proposals are generally positive but need to be improved and done faster. People living in a suburb as central as Aro Valley should have no need to own cars, let alone park their second and third cars on a major transport artery. The shops should be reconfigured for pedestrians and the route should have protected cycleways on both sides of the road, with no on-street car parking.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

This is a major transport artery, with countless buses, cyclists, pedestrians and cars. So major that it used to have a tram line, which the Council fought residents to remove. There should be protected cycle lanes on both sides of the road, going uphill and downhill. We can't allow our bottlenecks to be clogged with parked cars forever. Without a protected downhill bike lane, less confident riders will not make the journey into town, making the uphill lane less used. In the shops there should be no regular parking, the parallel parking vehicles regularly hold up traffic, and make the area feel claustrophobic and anti-social. I agree with the speed bumps and the 30km/h extension. We should get rid of all regular parking on the street and only have loading zones and some disability parking. The empty car parks could be turned into footpath extensions like on Dixon Street. The overwhelming majority of people travel to Aro Valley by foot, bike and bus. The layout of the shops needs to reflect this.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

The uphill cycle lane needs to be protected the whole way and there should be no on-street parking on the downhill side. Most of the properties on this stretch have off-street parking and parked cars pose a danger to the buses and cyclists that rely on this road. This is a major transport artery that needs to be treated as such. I used to live in Highbury and commuting to and from town by bike was very dangerous. There should be at least one more bus stop on this road just before the turn off to Norna Crescent.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Looks good. The problem is the blind corner between Abel Smith Street and Little Palmer Street. We should get rid of one or two car parks on Abel Smith Street to allow cyclists to leave and enter the path without disturbing pedestrians or each other.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The residents of Aro Valley need all of these improvements done as quickly as possible to be able to appreciate their value. If you just have years of road cones and disruption in the valley people will dislike the changes, even though they won't have happened yet. The experience of cities around the world that have made similar changes to these ones, is that you've got to act quickly and decisively, with the ability to change if anything doesn't work. e.g. the flimsy plastic used to separate cycle ways.

Feedback**104**

114174603294

NAME: Helene Pohl	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
Please also introduce residents parking on Durham Crescent. As it is residents with no garages are having a tough time parking and this will only get worse when there's less parking on Aro St

Feedback**105**

114177208592

NAME: Hiromi Beran	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

These changes will make the route much safer for cyclists, pedestrians, buses and drivers. It would encourage a lot more people to cycle who currently do not due to safety concerns, and will reduce the number of cars on the road (which in turn will make it safer for the remaining cars).

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Fantastic improvements for cycling, walking and public transport, which need to be prioritised for lower income residents, students and in terms of climate change mitigation.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

I avoid Raroa Road when cycling now as the few times I used it felt so incredibly dangerous. It would be fantastic for that to open up, and making it safer and easier for buses will be great as well.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Widening the paths will make it easier and safer for pedestrians and cyclists, and having more people moving through the park regularly will help make it feel safer as a picnic spot.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I'd like to be able to use the new cycle lanes as soon as possible, and a lot of new potential cyclists are very hesitant if a lane stops halfway to their destination. However, it looks very promising, and I'm excited that the council is doing this.

Feedback**106**

114170024030

NAME: Khoi Phan	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This allow a safe cycle connections between Karori, Aro Valley and the CBD. Cars have always had these options but never cyclist. Cyclists deserve safe transport routes ! Also we need to ACCELERATE these project instead of installing them in stages. Any hesitation and delay will affect the rest of cycle network implementation !

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Please add concrete separator instead of plastic one.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Please install cycleway down hill as well as uphill.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The sooner we establish these changes, the better rather than settling them in stages.

Feedback**107**

114173383678

NAME: Carl Howarth	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This looks to be well thought out and will make travel by bike from the city to Aro Valley much better. Love that some bike parking will be provided in the shopping area. I'm looking forward to riding from the city to Waimapihi Reserve and Zealandia with my 3 year old and not worrying so much about the traffic and safety. If the project is to be staged then it makes sense to compete the city to Waimapihi section first as this will be the most popular part.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Would prefer to have the bike lane extended through the shopping area, even if it results in removing parking for cars. Please enforce the 30km/hr speed limit. A speed camera would be good.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Thos sounds well thought out. Cyclists can travel quickly in the down hill direction. It's a good idea to have yellow broken lines on the corners to prevent vehicles parking there and creating hazardous pinch points. I've been dangerously overtaken by speeding cars on this road and forced to the kerb on a corner - it could've been nasty if there had been cars parked on the corner.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I use this route when cycling with my kid. It's more pleasant than being on the road, safer, but also slower. I think that it'll be great for kids and anyone not in a hurry or cycling to the park. Given this I feel that having a shared path is appropriate and won't cause conflict with pedestrians

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I don't agree Aro Valley is so unique that it justifies a staged approach, and I am concerned that having gaps in a safe route over what should be the most popular and important section, will discourage people from using it. This will shake community confidence and provide ammunition for those that want to discredit providing cycling infrastructure. One of the biggest problems with existing cycle infrastructure is that the network is fragmented. This section is the key connection as it's flat and includes the shops, mountain bike track, and the city. It's not unique for an inner city suburb to have a higher density of people living in the area and low off street parking. These characteristics justify doing this section as a priority so residents don't need as many cars, or at least can handle parking further away. I don't think an over-allocation of resident parking permits for the on-street parking available in the area is a problem that should be addressed by delaying or compromising safe cycle infrastructure.

Feedback**108**

114161190534

NAME: Tom Hutchison	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

As a driver; I will appreciate having a space that is allocated to cars and buses with less need to overtake cyclists. As a pedestrian on Raroa road; I will be grateful not to see cyclists on the footpath. As a Cyclist; I appreciate anything that will improve my safety and the safety of my family. This will decongest the area, improve business to our shop, and increase the chance of many residents moving away from their car dependence.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Please consider not elevating the crossing into a speed bump outside our shop at 97 Aro. The street is prone to becoming a river two or three times a year. We have experienced minor flooding in the building during these events. A speed bump outside the front door would certainly redistribute flood waters in our direction, It could devastate the shop.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

As the street is a regular thoroughfare for the 9000 residents of greater karori, this is a necessary change. Please survey to ensure that residents with physical disabilities can keep convenient access to their vehicles. I do not sympathise with residents who "Need their van for work". Nobody else is allocated public space to store their work tools.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**109**

114181683345

NAME: Iain Atkinson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

No to speed bumps and raised crossing

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Sensible solution for bike lane - ie nothing like the mess at Island Bay. No to speed bumps. Traffic already travels at a reasonable rate and there is no need for any car wrecking bumps.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Where are people going to park - they live there and own cars. Public transport is unreliable and not everyone can cycle. Bikes need a way through when bus stops are in use. Having to overtake is an unsafe option.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

How do you manage the road crossing from Aro Park onto the uphill cycle lane? Cycle lanes hat start and stop are pointless, it needs to be a continuous route.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

NO to the raised crossing and speed bumps. Short term parking is required for local shops and cafes for people driving in both directions

Feedback**110**

114174856419

NAME: Ingrid Downey	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

So many more details need to be worked out with individual Aro Valley side streets in order to manage the repercussions of changes to Aro Street/Raroa Road. Please start working street by street and continue this involvement as changes happen, so they can be improved with time. The side streets can work to help the residents on Aro Street who are losing their parking, but it won't be clear for awhile on what this needs to look like to be fair to all parties. I'd personally like to see a special "Aro Valley Residents' Parking" system that recognises the unique needs of Aro Valley car owners with no off street parking. (much like a special arrangement was made with Miramar around the airport). And I would like to see more car share, mobility, and daytime tradie parking made available, as these are real needs. Also, ride hire (Uber) drop off/pick up points should be specifically designed in the Valley, otherwise, we will get a lot of stopping in the road and cycle lane. It would be great for someone to develop an Aro Valley only parking app to allow residents to offer/find parking for visitors and tradies, so that life is easier for the residents of the Valley to get the vehicle access they need, when they need it. And I would love to see a Locky Docky station in the St. John's Street "paper road" between the Garage Project and the Park Toilets. Remove those free car parking spaces (and the danger they are as they come in and out of that tight space) and have this be the main bike parking area for the shops and the Community Centre.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

1. I want traffic to go 30 km or less on this stretch, but I would like to achieve that without the speed humps. I fear this is not possible, because once parking is removed, car drivers will feel more confident to drive faster. It is important to continue a dialogue with the community on how to keep this stretch slow for cars. Maybe temporary speed measuring signs every once in a while may help? 2. All other side streets (besides Epuni) need at least one mobility park and a car share park (but you should really meet with each side street separately to work with them to create the best solution for their street over time. They will be affected by some big issues as a result of this change). 3. I strongly suggest not using one of the potential car parking/loading/drop off space on Aro Street at the shops for bike parking. Instead you need to be really bold and solve the issue with St. John's Street paper road. The cars that park there now for free need to be removed and Locky Docky bike parking installed. I can imagine this might be hard. WCC has taken a hands off approach to getting involved with St John's Street, but now is the time get this excellent linear space used for mass bike parking - the benefit will be safer for cars and pedestrians who drive in to park for Garage Project. Space for vehicle to stop along Aro Street at the shops is too important to use for bike parking - move it to St John's street. 4. The whole stretch needs thought given to where ride hail cars will pull over to pick up and drop off. Will we all be OK with them constantly in the bike lane? Because that is exactly where they will stop. Or do we try to be more proactive and create spaces for them - this would mean a change in ride hail behaviour, i.e. there would be known pick up points along Aro Street and users would have to walk to them to await their car. This really is how it needs to go everywhere - like the addition of taxi stands, we need less chaos with these drivers just pulling over anywhere to let people in and out. 5. With the uphill bike lane, it must be clear that scooter and skateboards need to use it and be kept off the footpath. So it must be swept regularly for safety. NOTHING with wheels should be on the footpath. 6. Start a inventory of land to purchase to create micro parking garages. Either WCC owns them, or you work with a community enterprise to own and operate them. This creates available for hire off street parking, loading and unloading and tradie parking, but also a space for community facilities like compost & recycling stations, toilets, community gardens (on the roof) etc. The economics of building and managing such a structure with just income from the cars that pay to park would be hard, but it must be explored as a way for the future. See the micro parking lot at 74 Aro Street and imagine that if it was a bigger piece of land, it could hold a 3 story parking building that also contained other community features. 7. All the residents along Aro Street need to be offered the opportunity to buy Residents Parking on their nearest side street. Again, this will need a side street by side street approach to ensure the right use and balance for the street. Also, there needs to be a new category of Residents Parking just for Aro Valley. Many people cannot afford the cost and 2 cars per household will not work for flatting situations. Just like for the Miramar Airport parking zone, you will need to work with the neighbourhood to create a special new kind of zone that meets Residents' needs.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
<p>1. please cut down those 2 trees on Little Palmer to make the path even wider. They are non-native and their leaf litter is toxic to other plants and the roots are a hazard. I'm sure the house there would love them gone and get more sun and less leaf litter. PLEASE remove them! 2. The shared path through the park needs to be even wider with a line down the middle, in hopes of keeping people on the correct side when walking/riding. Remove those unreliable lights in order to widen the path (they are always going out and take so long to get working again), remove the wood in the path which is a slip hazard, remove the curb in front of the Community Centre at the end of the path which mountain bikers love to launch off. 3. Expand the opening onto Little Palmer from the Community Centre (move the playground fence). Create a curb cut so bikes can go immediately onto the street and not ride on the footpath, paint a line and add a bollard in the middle to keep the flow and prevent cars from driving into the Community Centre.</p>	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
<p>Holloway Feedback 1. Holloway Road needs to be consulted with more detail about parking. Plan a meeting with the residents. 2. How far up does the Residents' Parking go? I would suggest all the way to Brosnahan Tce. 3. 120mins is too short for people enjoying a hike or bike in the Waimapihi Reserve (or tradies), it should be 240mins, but only from 9am to 4pm M-F and all day on the weekends. It should be Residents only on week nights. 3. The Residents' Parking on Holloway should include those living on Aro (from Durham up) and also should be a special rate (lower than the current). 4. You will also probably need to mark out each parking space, since people park very poorly on Holloway. 5. Holloway needs to be made 30 km for the whole length, with a temp speed sign installed at various times to help drivers keep to it. Other comments: 1. What happened to the plan to put a EV charging station in front of Aro Park? Has there been any research on the need for additional EV charging stations in Aro Valley? 2. Again, No bike parking on Aro Street at the shops - move to St John's (paper) Street. Put a Locky Docky or 2 there. 3. Work with Adams Tce (and ALL other side streets) to discuss changing their parking to Residents' - - Norway should at least go to full coupon parking or they will be flooded with people who used to park for free on Holloway. 4. And again, remove the 2 big trees on Little Palmer to get a bigger path. And open the pinch point between the wall of Ashton Towers and the playground (but add a removable bollard to control vehicle access.) 5. Not in favour of speed bumps, so maybe a speed sign?</p>

Feedback**111**

114178593166

NAME: Inge Doig	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Great, especially where bike lane is separated from the traffic. Care still needs to be taking to ensure cars do not pass cyclists unsafely, or cyclists are needing to bike close to parked cars, running the risk of having car doors opened onto them.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Thank you for the opportunity to respond to the proposed bike lane installation. This is an exciting development and one we have been looking forward to for some time. We live in Karori and hope that our teenagers can cycle safely to school at some point – Wellington College. Much as they'd like to, we don't allow this currently because the risk to their safety is too high,

and we believe this applies to all cyclists that currently use this route. While the proposal is a step in the right direction, it does not go far enough to ensure my children's safety along this route, so won't be useable for our rangatahi at this stage. Some points: 1. There will be no bike lane on the downhill stretch. Whilst, understandably, this is not feasible, marking this as a shared space (sharrow) does nothing to make it any safer than it currently is for cyclists i.e. still not safe. Bicycles and cars will still be using the same space as it is not wide enough to do anything else. This is worse if there is a car parked in the same lane, and worse again if there is oncoming traffic at the same time. Cars will and do try to overtake cyclists going downhill – some cyclists go at the speed of cars, some do not (including younger children). To overcome this the following could help mitigate the issue and are ones I hope will be considered: a.

30km/h speed limit on Raroa Road – I note that when driving this road safely currently, you are actually only doing about 30km/h. Anyone going 50 is hoofing it, and running the risk of hitting stationary cars or moving traffic. b. No passing of cars or bicycles on downhill along Raroa Road. c. Consideration given to installing 'bicycle passing bays' i.e. allow slower bikes to use the footpath at designated points as a passing bay (of say around 10m) if there are no pedestrians using it, with appropriately marked on and off 'ramps' onto the footpath along the route (so both pedestrians and cyclists are aware of the shared space). Whilst I understand you are hoping to encourage increased walking too, in the short term anyway, the pedestrian foot traffic along Raroa Road is very, very low. d. Broken yellow lines at corners on Raroa Road is not enough, there are many points along Raroa Road that are narrow (including straight parts), where currently cars need to wait to give way to oncoming traffic – this is usually due to parked cars on one or both sides of the road – note that restricting parking to one side of the road still won't alleviate this issue especially if the bike path is being used. Raroa road needs to be analysed fully to identify these pinch point, including those along the straights, and be yellow lined on both sides of the road. 2. An uphill bike lane on Raroa Road is great progress, but the same safety issues exist if it is not separated from motor vehicle traffic. • In places where there won't be a barrier, cyclists going uphill run the same risk of being hit or driven very close to as they do currently. • If cars are able to use the bike lane they are more likely to do so going around corners or along narrow parts of the road, especially if there is a car parked on the downhill side of the road and there is on-coming traffic. If they round a bend at 40-50km/h in the bike lane and there is a slow cyclist in front of them, they will likely hit them. • Options: reduce road speed to 30km/h (and monitor it?!), yellow line all bends, and narrow parts of the road, both sides. Use barriers where possible, or where not possible perhaps cats eyes (helps in the dark too), or a rumble strip. 3. Whilst I am a big advocate of the cycle lane I do empathise with the people living along Raroa Road that do not have off-street parking. • Families with children, older people, disabled people may find it hard to manage without a car at this stage in Wellington. Buses are not reliable or take much, much longer than a quick trip would. It is not really feasible for most people to carry a week's grocery shopping a long way, or at all, on a bus or walking. And especially up the quite long, fairly steep hill that is Raroa Road. Families may manage fine with not using a car during the week, but the way kids' sports and other activities work are likely to have to use a car in the weekends. How many households do not have off-street parking on Raroa Road? How will they be catered for in the short term? Will individual circumstances be taken into account? Thank you for the opportunity to provide input :)

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	
It is hard for blind or partially sighted people to use shared foot/bike paths, having some sort of physical indication on the path, so they can ascertain where they can walk safely could help this. Cyclists often rely on people hearing or seeing them come and d/deaf, partially sighted or blind people cannot do this.	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

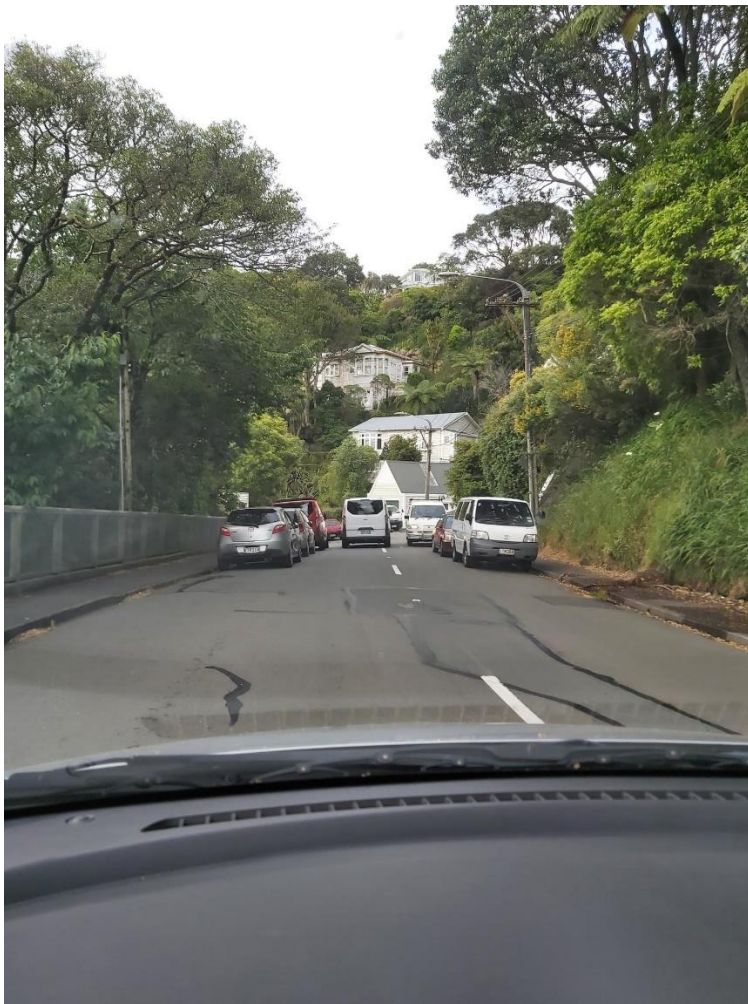
Raroa%20Road%20photos.pdf



Raroa Road - even with cars parked on only one side traffic would still need to wait for on-coming traffic, unless that traffic uses the bike lane, which puts cyclists at risk.



Raroa Road - narrow along straights even without parked cars or a bike lane! Yellow lines required.



Raroa Road - waiting for oncoming traffic, it is indeed narrow this street! A parked car, standard car driving, standard car coming the other way, or a bus or other large vehicle definitely wouldn't work most of the way along Raroa Road. Add in a bike, hhm? Waiting for traffic coming the other way currently happens at least twice down this road on each journey.

Feedback**112**

114181588155

NAME: Iona Woodward	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I bike up here most days. The changes will make cycling hugely safer and less stressful, and also allow motor vehicle drivers to travel up the route without getting stuck behind or dangerously overtaking cyclists.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

I'm so happy you're tackling this! Every time I bike up Aro St (nearly daily), I think about what a difference this is going to make and how much safer it will be. Upper Aro St (above the corner of Durham) is particularly dicey at the moment, since cars want to speed up there. Your proposals will make a huge difference for everyone.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

1. In the picture above, if the bus is stopped to take on or let off passengers, where is the cyclist supposed to go? What are they supposed to do? 2. I'm worried about the idea of cycle shoulders. I know lack of space is a problem, but it's really unsafe to signal to motor vehicles that they can swerve into the bike lane "to avoid oncoming traffic". How about avoiding the cyclists?! It sounds like a recipe for serious injury. 3. Motor vehicles all travel at more or less the same speed, but bikes and scooters go uphill anywhere from 10km/hr to 35km/hr or more for some ebikes. It's tricky to bundle such a wide range of speeds into a regular-sized bike lane, eg, during rush hour. Given this, I'm extra worried about narrowed cycle lanes and "cycle shoulders". 4. It's critically important for the success of the bike lanes that they be kept clear of debris (rocks and plant matter) falling off the bank. Raroa Rd is especially prone to slips which make riding the shoulder hazardous. If the bike lane isn't kept clear, especially in winter, it will be unusable. 5. Even as the proposal stands, cycle shoulders and all, this would be a vast improvement over the existing lack of infrastructure. I'm so relieved you're tackling this!

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

So very pleased you're including Little Palmer St in this!

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

It seems fair to give car owners time to adjust. Very happy you're prioritising the uphill bike lane from the corner of Holloway to Raroa Crescent!

Feedback**113**

114165596586

NAME: I. P.	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

The raised crossing should be at Aro Mart as this is used all the time whereas the crossing by the park is rarely used. Implementation time is too slow, while Wellington may be scared of another island bag stand off, it shouldn't take 2 years

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
Something better than a cheap skim of bitumen please. Cyclist (and scooters) can zoom through these a bit fast	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It shouldn't take this long. Why only 30kph on your selected streets, there is no why that 50kph is sensible on Devon St, Durham Crescent or Mortimer Terrace, they are virtually single lane with blind bends. If the 'main road' is 30 it will encourage people to tear through side streets, all of them need to be 30

Feedback**114**

114165747367

NAME: Mike Fee	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Good cycle infrastructure is vital for safety of cyclists

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Would like to see it happen faster.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Need to ensure that the inside corners on the uphill lane are well protected from motor vehicles cutting too close.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Because the existing route is not very safe for cyclists

Feedback**115**

114173734995

NAME: Ben Carter	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Bike lanes should be fully separated and not just painted so that cars cannot drive or park on them

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**116**

114166227116

NAME: Jack	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support**Do you have any comments to make about the proposed design?**

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support**Do you have any comments to make about the proposed design?**

Raroa Rd is not wide enough to support parking on both sides. It is however wide enough to support a bike lane on one, two lanes of traffic and parking on the downhill side

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**117**

114174235705

NAME: Jacqui	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This is a part of the city that was never designed for modern traffic - we need to make it safer for all members of a very connected community

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

This seems like a really well-conceived option, balancing out the needs of the whole community as much as possible.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

The uphill on-street parking is currently a real hazard, particularly as you come around corners. It feels like it has got worse recently.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I know I have cycled through here in the past and I'm always acutely aware of pedestrians. If we could make it safer for both, that would be a real improvement (particularly with the number of people using e-scooters as a commuting option now)

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

We need to give people time to get used to the changes and realise the actual impact on them isn't as severe as they might believe. Behaviour change also takes time so we let people get used to the situation until it's their new normal.

Feedback**118**

114174855697

NAME: Jadene Huff	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I support every change expect Aro Street. Without allocating resources to either public transport (more buses on the route, less cancellations and actually stopping in Aro Valley stops and not filling up by the time it reaches Durham) and more parking then this will fail. I understand we're trying to reduce cars but until that happens you can't remove resources that people are dependent on.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Firstly the idea for this connection is great however how this would genuinely work has not been thought through. You cannot remove car parks down Aro street without offering alternatives first. I don't drive, however I would frequently have coffee with my grandparents at Aro cafe and they have no choice but to drive. I would support changing one side of the sidewalk down to the church/new housing to be a bus / cycle lane but Aro Valley doesn't have the resources to remove car parks. We only have one bus route (25) that is frequently late or cancelled or worse would be fill by the time it reaches Durham and therefore NO ONE in Aro Valley can actually get on (at least once a week, and this happens constantly throughout 2022). You can not remove cars, without improving public transport to this area. Aro Valley is also the end of the coupon parking for the city. This means that students frequently park on side streets. This will only continue however will

not be possible if you remove car parks. The uni does not offer enough parking options so unless one of this is solved, I suspect that there will be more issues. Finally, I don't know if you're teams live in Aro Valley but the cyclists in this area (especially the weekend) are rude as hell and I've been almost hit by cyclists about 8 times since living in Aro valley. Personally I don't want more in the area unless council has a clear policy as to how to cycle in Ponake.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

This road is terrible. Removing car parks here is a good idea since it's so narrow and windy however I suspect you'll have the same problem.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Firstly Aro park is a crack park. No one sits there when they're drinking or smoking. Changing the paths will not make people more comfortable. However this is an excellent idea. I would suggest making a cycle path similar to Oriental Bay in Aro Park (the only problem with just widening is people somehow always find a way to walk in the middle) by making a seperate path it'll make it more clear. Love the idea on little Palmer street.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The longer you drag it out the more pissy residents would be. Making this a two year project instead of three would be more beneficial (wrap it up in 2022)

Feedback**119**

114165665154

NAME: jh	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

uphill bike lane will make the road much more usable for cyclists and cars Cycle parking likely to draw me in for shopping errands

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

very difficult road to improve! Uphill cycle lane is a big improvement. Consider encouraging kids to cycle uphill on existing footpath?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
excellent idea - creating space will make it better for all, whether on foot or on wheels	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
installing stage two before summer will draw in more early users; also will be ready before start of new school year

Feedback**120**

114160189924

NAME: James Dunne	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This makes the road better to drive on. It should never have been used as a parking lot for private vehicles.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

As a motorist who doesn't cycle and doesn't intend to, these changes make me more comfortable driving through Aro Valley safely.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Parking should have been removed from Raroa Road years ago.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
These changes are worth doing and there is no reason to stall to make whingers happy, because you won't make them happy and everyone else will have to wait while they bleat.

Feedback**121**

114181249840

NAME: James Sullivan	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

For all it is missing it is still a massive improvement over the status quo. As scary as that is. Reducing the speed to 30kph where cyclists have no protection against motor vehicles would be a fast and cheap way to address many of the remaining issues. Especially in the context of an interim plan intended to encourage uptake of cycling.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

The downhill section should have the speed reduced to at least 40kph and ideally more like 30kph to align with the aim of Paneke Pōneke to encourage cycling. This partially matches guidance in both the Paneke Pōneke plan as well as Austroads guidance where cycle and micromobility traffic must coexist in the same roadscape.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

To align with the aim of Paneke Pōneke as well as guidance in both that document and from Austroads. Locations where cycle traffic and general road traffic mingle need to have slower speed limits. Locations such as bus stops where cyclists will have to pull out and enter the main roadway or sections where the physical protections have been removed.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

I am wary of some cyclists using these paths as if they were dedicated cycleways. The process of building that culture where cycling on shared paths means operating at the speed of pedestrians will take some time to happen. Also I fear that council will use these as excuses to not build more appropriate micromobility focused infrastructure to placate drivers.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

This change as a whole is intended to be interim. To be able to gather the most useful data all the changes should be emplaced as quickly as feasible. Major issues can be addressed through removal of features during the period if needed. Implementing all the changes quickly will also help incentivize users of the roadway to find how they would adapt to a more permanent implementation.

Feedback**122**

114161241540

NAME: james	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**123**

114167241508

NAME: James kinsella	SUBURB: Lyall Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Active transport is the future for a thriving vibrant green future city. Having connected infrastructure is essential for making this possible

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

It's a good plan prioritising streets to move people rather than a slave to motor vehicles and car storage (which is a very expensive subsidy on public land)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
It's a good idea but getting a safe and direct route on the streets should not be sidelined by making a route only used because of the inadequate road provisions currently	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The change is coming doing it slowly will only lead to more delays. Make on big change with all the adaptations needed with one disruption than do little disruptions for a longer time. Also putting all the changes at once will allow those who are going to mode shift to active transport the best opportunity to see how good it is. (Unlike the horrible not finished of Evans bay which put many people off after trying it once)

Feedback**124**

114175995384

NAME: Jane Brennan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I am still unclear where bikes will be when come down Aro Street?

Do you support the proposed speed changes on Aro Street?**Oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

You seem to assume that inner city dwellers are "evil" to own and use a car, be it petrol, hybrid or ev; while those living in outer suburbs still have roads and parking for their vehicles. I strongly disagree with taking away residents parking. For many years it has been those living outside of the valley who work in the city, and those attending university who have, and still do, use carparks in our local streets. Surely they are the ones that need to change their transport patterns and not "punish" locals. Re your planned design I fail to see how the bike lane will operate as a 2-way bike lane. Most bikes in the valley are mountain bikes on their way to or from Pohill Reserve - I do not see many local cyclists. Do you have statistics on the number of cyclists using their bikes to commute to work through Aro Valley? I would be interested in seeing these statistics. Is it possible to have a cycle "lane" on one footpath area and pedestrians to use the other side of the road? I think the speed limit restrictions should be sufficient without the need for speed bumps in Aro - speed bumps are expensive to install and maintain and they make driving difficult due to some cars slowing down to almost a stop in order to "climb" over them. Aro is close enough to the city for pedestrians to walk to work if they choose, or bus (now that we have buses that

operate after 6pm in the evening and on weekends). I strongly disagree with the need for removing parking outside house just for a bike lane.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Don't know

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Are you saying that cyclists should be able to sue pedestrian walkways? I thought they were all to be using new bike lanes? Do you have statistics on how many cyclists will be using these areas?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I say this because I think that you have these changes wrong. I do do consider these changes being down for the majority of people of live and work in this area. It seems to be more for people from out of the area making decisions for those who live and work here. We do not all have or want bikes so I fail to see why the bike lanes and the reason to remove parking for residents. Once again I ask for numbers to support your plans.

Feedback**125**

114161007877

NAME: Jane O'Shea	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I live in Highbury and rode an electric bike as my main form of transport for five years. I stopped four years ago for safety reasons. These changes mean I will joyfully get back on my bike again.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I live in Highbury and wrote an electric bike as my only form of transport for five years. Four years ago I stopped because it became too dangerous. If these proposals go ahead, I will joyfully be back on my bike again.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

The sooner the better

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

When I used to ride my bike (stopped 4 years ago because became too dangerous) I used these two paths almost every day. Excellent idea

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The sooner these changes are done, the better for everyone.

Feedback**126**

114181291303

NAME: Janice Swanwick	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

Qualified support because of the conflict between a safer route for cyclists and amenities for residents and business patrons.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

I ticked support because I mostly support making it safer for cyclists, speed bumps and extending the 30 kmh zone. However, I would like to see WCC acquire some land for off-street parking for shoppers and cafe goers. Businesses' bottom lines will be affected if patrons can't easily park.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

Again my support is qualified. At present I irritate drivers behind me by going slowly enough to stop suddenly if I come across a cyclist just around a blind bend and I can't pull out. Then there are the joys of meeting large buses unexpectedly so a cycle lane that is narrower than a car park

would be helpful. It would be even more helpful to make Raroa Rd 30kmh immediately. And if an uphill lane makes cyclists safer, how is it they still have to mix with the traffic downhill. I also empathise with residents with no offstreet parking who will be penalised by the removal of all uphill carparks. Not everyone can cycle, or bus, and many residents will rely on their cars for work, school and shopping. Like Aro Street, there should be some off-street parking provided at several locations - or financial help to householders to create their own off-street parking.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Negative
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Yet again qualified support. A win for cyclists but scary for pedestrians particular for adults shepherding small children, and people with disabilities, when the occasional cycle hoon whizzes past. The path should be separated, and scooter riders should also have to use the cycleways. In fact scooter riders should have to use cycleways wherever they are in Wellington, and not zoom, sometimes dangerously, past unsuspecting pedestrians.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

On one hand, as a former cyclist (too unsteady now) I support safety measures for them. On the other hand, residents with cars are going to need time to adjust to the very limited parking, and probably in some cases move.

Feedback**127**

114160299469

NAME: Jason	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

These changes are well thought through and will improve safety for cyclists. Hopefully they will encourage more people to ditch their cars and get on their bikes. The timing is generous to allow people to adapt. The consultation is informative and easy to engage with.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Until the separated cycle lane is built, I think it would be a good idea to put double yellow lines on Aro St from about 155 Aro St through to the bus stop on the corner with Durham St as there is currently some very dangerous overtaking of cyclists by cars heading into that blind corner, with a danger of head on collision with cars heading downhill towards Willis St

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**128**

114181453177

NAME: Julia Stace	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

No provision appears to have been made for necessary support services of the dense housing stock of Aro Valley. Where do the daily tradespeople park their vans? How do the rubbish collections occur outside the cycle lanes without blocking the road? There is no spare overnight parking in Adams Tce, Durham St and Epuni St already. Expecting the elderly to carry loads from their cars parked on steep streets some distant away is unrealistic. People living on the cycle lane side of Aro St need to access transport on the south side, be it taxis, lifts or whatever. Crossing the street because parking will only be provided on the opposite side is dangerous enough now, let alone when traffic pressure is condensed, after these changes.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Tradespeople are not taken into account. Hard enough nowadays for them to park near the house on which they are working. No provision at all for them in new plan. Housing stock here is old and needs constant maintenance. Most houses don't have any off street parking. Tradies need access to their vans for tools. Having to park in side streets, some distant away, will mean they won't come to Aro St jobs. Garbage trucks picking up, taxis to airport will be similarly disadvantaged with no parking on south side of Aro St once cycle lanes go in.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Cycle lane will be against the bank on which there have been many big slips this year. Same issues of how are tradies & garbage collection vehicles accommodated, outside the houses they need to service?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This route will be wonderful for bikes and terrible for pedestrians. Already the blind exit on Little Palmer St to Abel Smith St is a danger point. People riding cycles and scooters crash into slower moving pedestrians.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

There will not be enough parking for residents of Aro Valley after these changes are implemented. The side street are already full at night. Those who need a car to get to their work, which is beyond walking or bus use, will have to move from the area. Most people shop infrequently now and need a car to carry goods home. Parking far from their house adds to difficulties for the elderly and the less mobile. Again, what provision will be made for vital and frequent casual workers like tradespeople?

Traffic calming flower beds in the heart of Aro Valley

These mature planter boxes 'bookend' the Aro Valley shopping precinct.

These were created some years ago for traffic calming purposes.

The plants are mainly our attractive natives and the planter boxes now contain mature trees and ground covers. It would be a tragedy to remove this attractive vegetation which defines the shopping centre and enhances the atmosphere of the Aro Valley so the cycle lanes to other suburbs can take over and occupy the road here instead.

There are mini planters at either end of the pedestrian crossing on Aro St with lancewood trees and native ground covers that are smaller but are also very attractive and streetscape enhancing.



Aro St Western end Outside Numbers 109 116 ?



Aro St shopping precinct Eastern end, raised flower beds.

Outside 72 & 73 Aro St

Photos included in submission on proposed changes for Aro St traffic.

It would be a huge loss to our streetscape and wellbeing if these features were sacrificed to the proposed cycle way.

Julia Stace



21 Nov 2022

Feedback**129**

114181630140

NAME: John Creser	SUBURB: Wellington Central	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Its a case of special interest groups being given priority over businesses & residents

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

The design is based upon the premise the changes are being made for the "greater good" of the community, which ignores the practice needs of residents.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Very negative
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback**130**

114174882195

NAME: Jeff Montgomery	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

Change Abel Smith Street to 30km/hr between Willis Street and The Terrace. Cyclists and pedestrians will be killed otherwise.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Don't know****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Don't know
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	
The 30km/hr setting needs to also be applied to Abel Smith Street as this is where pedestrians and cyclists will be "ejected" from the safe space. Currently it is 50km/hr and cars hoon up Abel Smith Street from Karo Drive towards the The Terrace and Te Aro School. The speed humps dont work - make it 30km/hr too	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Make Abel Smith Street from Willis Street to The Terrace and Buller Street 30km/hr. Cyclists and pedestrians using the proposed changes will be killed otherwise

Feedback**131**

114160310124

NAME: Jenny Livschitz	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**132**

114178481692

NAME: Jessie black	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I'm very keen to make it safe to get from aro to Karori. My big concern is the end of this is at the most dangerous point- trying to cross Chaytor Street. Perhaps very shortly after finishing this section an interim solution would be so helpful as hopefully numbers of cyclists would ride

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

I think it is very helpful to have more space for cyclists to feel safer going through a place where they make up such a high proportion of road users

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

I think the education of drivers is crucial in making this safer

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**133**

114181440165

NAME: Jeff Flavell	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I cycle this uphill 2-3 times most weeks (commuting and shopping) and will be safer if these changes are implemented in my trips there. Slowing traffic through the valley and up Raroa Rd will make it safer for everyone - pedestrians and residents crossing the road, cyclists and vehicle drivers.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

I'm a little concerned about the raised ped xing and speed humps - making them so a bike with narrow high pressure tyres can go over then without a big jolt is important. Some changes in road surface angle are too abrupt in the humps around Wellington - eg Tasman Street. The humps that don't fill the whole road width are best in my view as a cycle can cross the edge (eg Duthie St Karori)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support**

Do you have any comments to make about the proposed design?

It's a narrow road with poor sightlines on many of the left hand uphill corners. I consider the uphill lane will make my journey up thru the valley to Northland safer - in my experience the biggest thing apart from a separate lane is slowing vehicle speeds. I've had close calls when drivers overtake in the face of downhill traffic - if drivers travelled more slowly then overtaking would not be a dramatic involving fast acceleration and sudden pull-back to the left - the close calls have involved very close pull-backs some almost hitting my bike.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I cycle thru this route at present - it is much safer for me from Willis Street going up to Aro shops than if I went up Vic St and had to cross Brooklyn and Mt Cook Traffic to get to the top of Willis St. Widening the path and making it properly shared should improve the experience for everybody. I'd love a "softer" kerb crossing please off Able Smith St road across the kerb into Little Palmer St : -) Kerb crossings without smooth cut-outs make a jolt that is not as safe as a shallow smooth junction. Putting the entry at Little Palmer St would be an improvement - currently the kerb crossing is down AS St a bit.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I find one of the more dangerous things on my bike is transitions from lanes - having a more continuous lane up the Valley to Hollow Rd would reduce the transitions from the left back into traffic when cycling uphill. I'd support an additional couple of changes: installing racks for cycle parking in the shopping precinct would be a big plus one on the uphill side and one downhill. They could be parallel to the kerb to not take up lots of space.

Feedback**134**

114181171945

NAME: Jonathan Markwick	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This area is very hostile to cycling. Providing a space where people won't get killed for riding a bike is more important than convenient car parking.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I support the replacement of parking spaces with safe infrastructure. Please either install a downhill cycle lane or reduce the speed limit to 30kph for the whole of Aro St (from Holloway Rd down to Willis St). The gradient is not steep enough for bikes to safely share the road with 50kph vehicles. Currently it's very difficult to make a right turn onto Aro St from Aro Park. I'd prefer that a pedestrian/bike refuge was installed so people can cross one lane at a time, or install a speed bump where the Aro Park path joins Aro St.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I strongly support the removal of parking spaces for safe cycling infrastructure, especially uphill. While I'm aware of the narrow road width in some places, I am unlikely to use the 750mm "cycle shoulder" as it does not provide cyclists who will be riding at close to walking space any protection from 50kph motor traffic. I would prefer that slow uphill bikes be able to share the footpaths where they exist on the left-hand side of the road going uphill (eg. from Holloway Rd to Mt Pleasant Rd). The speed differential between uphill bikes and pedestrians is much more comparable than the differential between uphill bikes and motor traffic. There are also lots of pedestrian short-cuts in the area meaning the meandering Raroa Road footpath is lightly used by pedestrians.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I support both of these widened paths. When approaching Aro Park from Karo Drive on a bike, I prefer to use Palmer Street instead of Abel Smith and Little Palmer St because there is too much traffic on Abel Smith Street. It does mean using a short section of footpath on Willis St to get to Palmer St but it is safer and easier than using Abel Smith Street.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

the stage 1 improvements are unlikely to be used by nervous cyclists as you need to bike through the stage 2 area to access stage 1. the stage 2 area is a very hostile and dangerous area to bikes. I am OK with delaying stage 3 as this stage will be of little use until the "downstream" routes are made safer (Victoria/Webb/Willis St)

Feedback**135**

114172679931

NAME: Joan Pettit	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Bike use is currently dangerous.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Please don't wait 2 years to roll out parking changes. Needed now.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

Hope the cycle lanes will be protected.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I don't think time will make a difference in people's adapting to the changes in parking. More important to be clear on timing and transition options.

Feedback**136**

114181640934

NAME: Jo Carter	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

More survey and design work required before any decision plus more consideration needs to be given to all road users. See my detailed submission

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Do you have any comments to make about the proposed design?

See comments in my submission on existing use rights that are also applicable in this area.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

See detailed attachment

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	
People walking on the paths through Aro Park and Little Palmer Street?	
People riding bikes along this route?	
People driving vehicles on the surrounding streets?	
People with disabilities or accessibility issues?	
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Do you have any comments to make about the proposed design?	
No. While I have walked through this park I do not feel I use it enough to be able to comment	

What do you think of the timing of the proposed approach?
Why do you think that?
There needs to be more preparatory work done before any decision is made - see comments about survey methodology and other options in my detailed submission

Submission%20to%20the%20Aro%20St%20Raroa%20Road%20Cycleway.pdf

Submission to the Proposed Transistional Aro Valley Raroa Road Cycleway

Since 1989 I have resided in Raroa Road in the hilly climb section between Entrance Street and Plunket Street. This submission relates to that part of the planned cycleway.

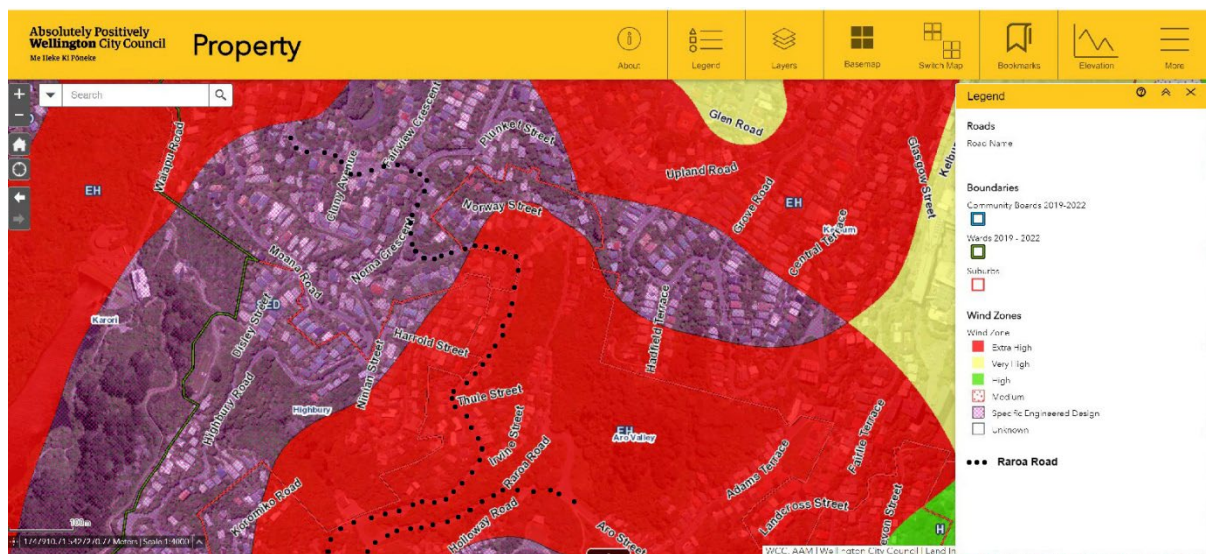
The Raroa Road Environment

Steep, narrow, winding road.

Raroa Road is a long narrow winding steep road, with steep banks on the uphill and downhill sides.. The first Council parking survey noted there were only 24 on-site parks in this area, 114 parks were on-street. Most cars parked on this road are on the uphill side of the road, where most of the houses are located. It is difficult to provide off-street parking on this side of the road due to the topography (steep bank rising up from the road). The only accessible side streets are at the far ends of this area and have little to no available parking. i.e. Mount Pleasant Road at the bottom, and Norna Cres and Plunket Street at the top.

Parts of Raroa Road have extreme wind ratings.

Raroa Road is exposed to very high wind conditions, more than other main roads where cycleways have been provided. The top end of Raroa road is rated for Extreme wind conditions – which require specialised engineering input in house design. The purple area denotes extreme wind conditions



Other cycleways in Wellington are in less exposed areas and do not face extreme wind conditions. The environment, topography and width of Raroa Road are probably the factors that lead the Auditors of this proposal to say:

“The proposed treatment for the Aro Valley route, whilst being an improvement on the existing situation, **is only expected to attract cyclists of the “strong and fearless” or “enthused and confident” categories (according to the Geller classification).**”

Vehicle Parking

Vehicle parking has existing use rights

Roads are zoned Residential in both the Operative and proposed district plans and are subject to residential rules. **The District Plan and Council through its Encroachment Licence system and Residents parking zones has historically provided for parking and parking structures on the road recognising the difficulty that many Wellingtonians face in constructing off-street parking.** On Raroa Road there are garages dating back 110 years. The encroachment system and residents parking zones have led to a long-standing expectation – **I would argue existing use right - that if you buy a property in Wellington you will be able to park your car on the road directly in front of your property** unless it is located on a corner. Council parking policy needs to take this into account. This applies to all cycleways in Wellington.

There are no alternative parking areas in the side streets of Raroa Road.

The only side streets that connect to Raroa Road in this area are:

- Thule, Irvine and Harold Streets which are very steep and unformed legal roads that have houses located on them. These streets have no parking.
- Norna Cres and Mt Pleasant Road. These are steep narrow roads with high banks and have inadequate on-street parking
- Plunket Street at the top of the road. This has controlled coupon and residents parking only. It has a large number of dwellings with no off-street parking (hence the residents parking zone).

Survey Methodology

Chosen survey times do not reflect true parking use in the area.

The parking surveys that were undertaken in this stretch of Raroa Road were undertaken at the wrong time of day and the wrong period of the year and therefore do not accurately reflect the true use of the road for parking by residents.

In other parts of the proposed cycle route, parking surveys were undertaken at a wide range of times. However, in this area of Raroa Road surveys were narrowed to only weekdays at 9am and 4pm on weekends in June and July 2022, and only one day in the first study on May 13th 2022.

The original parking survey recommended that a survey for Raroa Road be undertaken at 7pm to get a better snapshot of parking, but this was not done for this section of Raroa Road.

This area has a very high resident student population. No cognisance was taken of the University calendar. June and July are the period where university students have exams and midterm break so there is a period of at least 4 weeks where many students are not in town and thus the demand for on road parking is significantly reduced. The greatest demand for parking is at the beginning of the university year in March before the Easter break.

Cycleway Design

The design is likely to generate more problems than it solves.

In the lower part of Raroa Road, the proposed cycleway will necessitate the moving of the middle white line to the downhill side of the road. Locating the cycleway on the uphill side of the road will mean that cars will be forced off the uphill side and forced to park on the downhill side of the road. This will have the following results in terms of traffic safety: -

1. On the downhill side there will be less room to accommodate parked cars due to the white line being moved towards the downhill side. Downhill travelling vehicles will be forced to cross the white line to pass parked vehicles on this side of the road. *At the moment it is possible to pass parked vehicles on the uphill side of the road without having to cross the white line.*
2. Occupiers on the uphill side of Raroa Road will be forced to cross Raroa Road, this increase of pedestrian movement across roads is not desirable for traffic safety.
3. The Audit Report notes that cyclists will be in danger due to the blind corners in parts of the road. When vehicles on the downhill side of the road are forced to cross the white line, vehicles on the uphill side of the road will be forced to use the cycleway to avoid likely oncoming traffic around blind corners.

The location of the cycleway will create more vehicle to vehicle conflicts and more pedestrian to vehicle conflicts.

In the last 5 years 2017-2021 (inclusive) the Waka Kotahi accident figures for this area show there were 72 crashes **of which 48 were non injury**. Of the 24 **injury crashes 3 were cyclists**. Of the 48 non-injury crashes 3 were cyclists

Additional parking loss not included in parking analysis.

After the initial design and parking analysis, the audit report recommended additional removal of parking and this was adopted in the revised cycleway design, but a subsequent parking analysis was not performed.

The cycleway will remove parking from 138 – 140 Raroa Road, but in the original report this was shown as unaffected by the cycleway (highlighted in orange). This shoulder normally accommodates about 6 vehicles after hours in addition to those in the garages and driveways.

Planning

The cycleway has been designed with reference to only the existing situation, service vehicles and intensified residential use have not been considered.

Raroa Road is categorised as a principal road / urban connector. *“Principal Road: roads that provide access to motorways and to arterial roads having a dominant through-traffic function and carrying the major public transport routes (primary road)”*. The road needs to take a high number of vehicles. The greatest proportion of those are private motor vehicles due to poor, inflexible bus services and the pandemic. Unlike a lot of Principal Roads in the city the carriageway of Raroa Road

does not extend to the full legal road width, but the road still needs to provide access for all forms of transport – private vehicles, trade vehicles, large service vehicles (rubbish and fire and emergency trucks), couriers, buses, pedestrians, and cyclists. Because Aro St and Raroa Rd are through routes, these vehicles are in higher numbers than in lower order streets.

Residential areas require servicing, but little thought has been given in the parking surveys to providing for vehicles that service the houses in this area. At present there is a new house being built on the uphill side of Raroa Road, a large bin is located on the uphill side of the road and up to four vehicles are associated with this development. At the beginning and end of the year there are a high number of furniture removal operations when the relatively large student population change flats.

All residents have visitors – I do not see any consideration given in the parking surveys to visitor parking. Good cycleway design necessitates safe planning for all road users which include service, private and shared vehicles, and pedestrians.

The parking survey was based on existing residential use without consideration for greater intensification as a result of the Government changes that permit three units per site as of right, creating more parking demand, and greater traffic through to Karori.

Raroa Road and Karori are zoned Outer Residential in the operative district plan, thus allowing for two units per site and up to two storeys height as of right. In the new proposed district plan, these areas are zoned Medium Density Housing, now allowing for three units per site and up to three storeys in height. ***New units that have been developed in Raroa Road over the last 10 years with off-street parking have not meet the occupants' demand as witnessed by the number of vehicles parked on the road in front of these new units.***

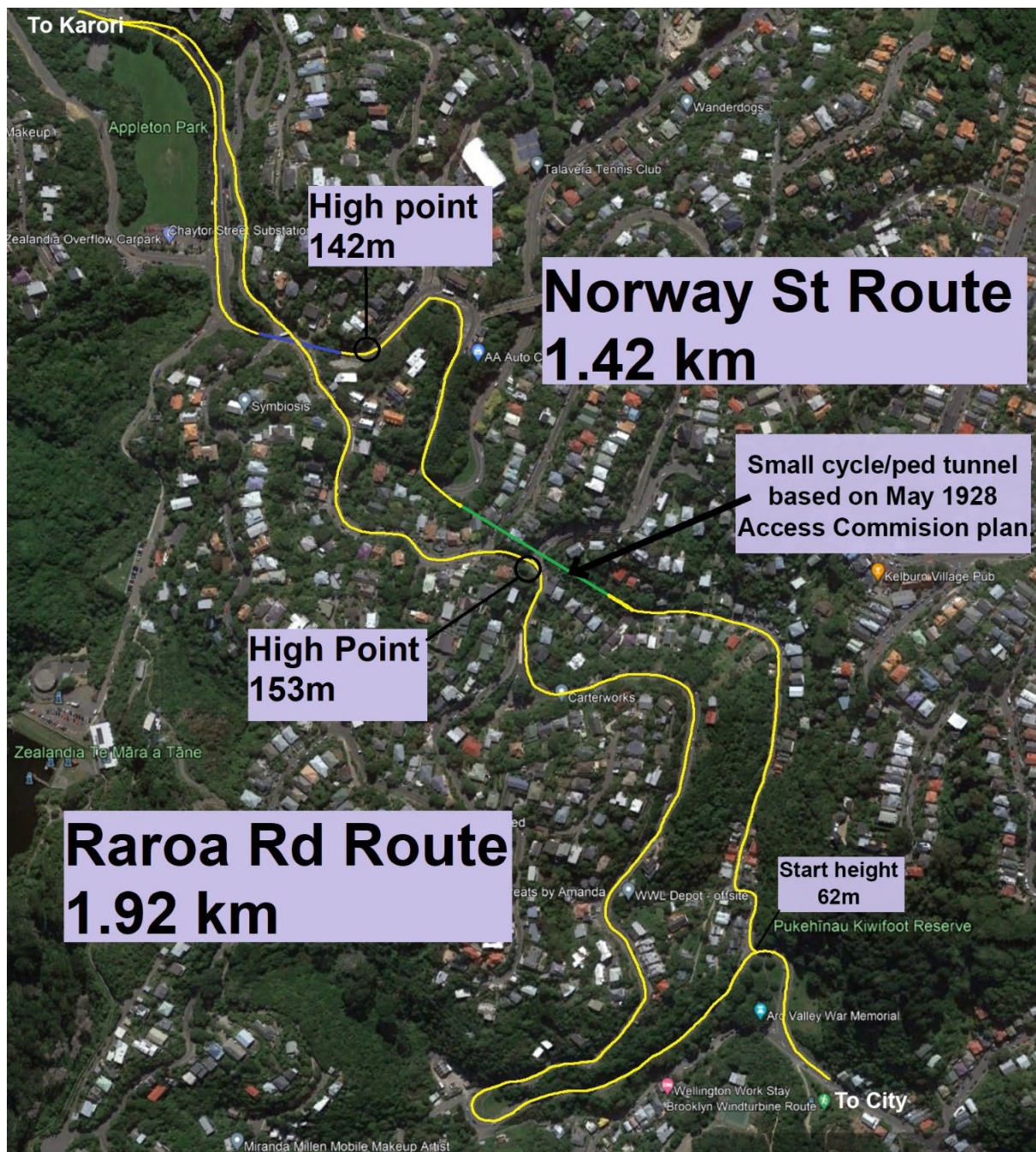
Conclusion and Solution: Norway Street Tunnel to Glenmore Street

Raroa Road is a narrow winding, windy and inhospitable road that struggles to accommodate existing users. Putting a cycleway, even a transitional cycleway along this route is unwise. The Auditors concluded that a cycleway on Raroa Road is for the “fearless”. The main reason for developing cycleways is to encourage wider use of cycling, but this plan will not achieve that.

There is a better option. In the original report, Norway Street was considered as an alternative to this stretch of Raroa Road, however it was excluded because the end of Norway Street leads to a steep public switchback walkway.

It is apparent that those involved in the cycleway design were not aware of the plans that have been on the Council books since 1928 that creates a tunnel from Norway Street through to Glenmore Street. This has been seriously proposed and examined several times by the Council for trams and other vehicles (1930s through to 1950s). In the past this was deferred awaiting greater demand. There is now practical justification for a smaller pedestrian/cycle/micro vehicle tunnel. This elegant solution will provide a better route for cyclists and will provide greater alternative access for walking and better access to bus services on Glenmore Street. Norway Street being a no-exit residential street has low vehicle numbers so would not require dedicated cycleway marking or parking loss.

For cyclists, the new Norway Street route is half a kilometre shorter than the Raroa Road route, involves less of a climb, is sheltered from the wind, and has fewer and safer corners. See following diagram.



Feedback**137**

114166867916

NAME: Joel Cosgrove	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Aro St is such a busy street, attempts build and improve on the safety of the street are a positive thing.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Aro is a narrow space, it's good that it's being organised for wider use than just cars.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Buses and bikes will still be in conflict, but setting up an uphill bike lane will go a long way. Some of those corners are pretty hairy at the moment, so anything to ease that is good.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
The wider the better	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**138**

114181206386

NAME: Jo Heaton	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

I am supportive of the Aro Valley changes if residents agree, however I do not believe that Raroa/Plunket to Raroa/Chaytor section is safe, nor will be safe for cyclists under the current proposal.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

I am supportive of the section from Holloway Road to Chaytor Street, however feel the Raroa/Plunket to Raroa/Chaytor section is too narrow to ever make safe for cyclists. I support parking on one side of Raroa Road only and think this change should have been made when the Bus Service 25 was changed. I am not opposed to the installation of a new bus stop but believe

that current proposal opposite 125 Raroa Road is unsafe - there is poor visibility for uphill traffic of all types behind and downhill. Although not directly opposite, I suggest reviewing the proposed location and consider around 136 Raroa Road instead where passengers would at least be able to disembark onto a flat surface and there is the possibility for a bus to pull to the left. I would like the proposal to move the bus stop near Cluny/Fairview reviewed; the current location provides some visibility for drivers exiting Plunket St to Raroa, whereas a bus on a stop moved closer to the roundabout would probably impede the view of any cyclists or vehicles travelling west from Moana Rd towards Raroa on the downhill side. This section of road is also highly prone to slips and I would like to hear more about retaining improvements to this section so users could feel confident in using the cycle/bus lane without encountering slip hazards. I am not confident that the parking count at the top of Raroa Road is accurate and think it was probably done during a holiday period. As a resident with no off-street option, it is generally impossible to take the car out during the weekdays and find a park nearby on the return. The parking estimate of 45% usage currently seems low and increases to 99% once the changes are made. To mitigate the loss of parking in this stretch of Raroa Road should the proposal proceed, I would like to see elements of the WCC Parking Proposal implemented concurrently i.e. Resident's Parking implement, no permits for those with existing off-street parking, no parking for oversize vehicles or only one park per household. A 2-3 minute walk from the residence maybe fine on the flat, but it's particularly challenging between Raroa/Plunket and Raroa/Chaytor where so many properties are up dozens of steps - groceries, pushchairs and garden waste become really challenging in this scenario and will also increasing parking pressure on residents in surrounding streets such as Plunket and Moana. There is no proposal to insert speed bumps or lower speeds into the Raroa to Chaytor section - there would be a good case to lower vehicle speed to 30km in this section which has several blind corners. Should the proposal proceed, another reason to introduce Resident Only parking in this section would be residents will be better aware of the cycleway and hopefully more careful when opening car doors into the cycleway compared to day-parkers. The safety audit mentions several examples where mixed-used with poor width could result in serious crashes. I simply do not believe that Raroa/Plunket to Raroa/Chaytor is safe for cyclists and the proposal notes that the route would suit 'confident cyclists' only. An alternative route could be Raroa then right at Plunket Street roundabout, left into Upland Road, over the Kelburn Viaduct and then through the tunnel onto Chaytor Street. This may offer better options to link up with the cycling corridor Botanic Gardens to City where the road is considerably wider and which already has better road controls. I am supportive of the implementation of cycleways, but having been a long term resident of Raroa Road I think the top section to Chaytor Street is fundamentally unsafe for a cycleway due to traffic volume, road narrowness, parking constraints and the number of visibility hazards for all traffic, plus the accident black spots at Northland Tunnel/Raroa Crescent and Chaytor Street.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?
People walking on the paths through Aro Park and Little Palmer Street?
People riding bikes along this route?
People driving vehicles on the surrounding streets?

Neutral

Neutral

Neutral

Neutral

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

If it proceeds at all - and I do not think it is safe - I would prefer changes made from Raroa/Plunket to Raroa/Chaytor be made in conjunction with changes to Residents Parking implementation in this section.

Feedback**139**

114161161840

NAME: John Newman	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

What is the solution for parking for access to Waimapihi Reserve? Is creating parking at the old dog training area on Holloway Road an option

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

There still needs to be sufficient parking for residents of Raroa Rd on Raroa Rd. How does this proposal cater for residents of other streets in the area ie Norway, Mt Pleasant, Zetland, Harold, Highbury, Moana which will be impacted by the parking spillover? Do these streets need residents parking if impacted?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**140**

114167199512

NAME: George Sederis	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

We need to make it safer for cyclists on Aro St/Raroa Road. Roading improvements for cyclists will introduce more people to cycle around the city. Very exciting change!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

It's great these cycleway improvements are being rolled out in Aro St. It will make it safer for cyclists to have a separated cycleway, and enable more new cyclists to take advantage of the positive changes. Aro St is very close to the city, so it will be easy (and safer) for cyclists to get from Aro St to the city, or across the city. Long term it would be good to have concrete lozenges rather than plastic sticks as this would be a safer way to separate cars from cyclists, and reduce accidents.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Endorse improvements to make it safer for cyclists on Raroa Road.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Good to improve existing throughfares for cyclists thorough Aro Park/Little Palmer St. Safer for cyclists on this route, rather than being jettisoned onto Willis St at the end of Aro St.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The quicker we can introduce these changes the safer it will be for cyclists.

Feedback**141**

114169247773

NAME: john whitty	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Why do you think that?

I have been going round and round on the website looking for somewhere to make a comment. I want to submit regarding Holloway Road. Car parks will be lost on Aro St and Raroa Rd so must presume those will gravitate to Holloway Road, Which has recently had an upgrade regarding our parking, including yellow lines etc At the beginning of Holloway Rd it is intended to put a 120 minutes limit which knocks out the park and ride people. I don't agree with that. The problem at the beginning of the road is people who park for extended periods- like months. They take parks from the park and ride people and those who want to go for walks and those bikers who travel a distance to bike the tracks in the area. We should have rules similar to those at Ohiro Bay that long-term parkers should have proper toilet facilities etc Also as cars from Aro St and Raroa Rd converge on Holloway Rd should they be entitled to residential permits to park on Holloway Rd. It is for sure that the demand for parking will shift further up the street as parking in Aro St and Raroa Rd disappears. Finally I think your document seems to want to solicit certain responses rather than a real opportunity for comments

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Do you have any comments to make about the proposed design?**

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

People walking on the paths through Aro Park and Little Palmer Street?

People riding bikes along this route?

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

Feedback**142**

114161574605

NAME: Jonathan	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

I don't know what cycle shoulders are, but there needs to be protection of some sort on the corners of Raroa road to stop drivers from cutting corners- it's currently terrifying because of that..

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
Considering this is for a "test and see how it works" approach, there is no need for it to take 3 years! A piecemeal solution will deter people making transition to active Transport

Feedback**143**

114161719663

NAME: J	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I would love to see the cycle lane continue right through the shops down to Willis street. Possibly more parking near the park instead.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

As a Kelburn resident, heading up the hill can be dangerous for both cyclists and drivers. With free parking up Raroa road, both sides of the narrow road are constantly filled with parked commuter vehicles. This means many blind corners become a single lane. I would strongly suggest continuing the cycle lane all the way from the bottom of aro street, up through the shops, and right up raroa road to the roundabout at the top. This will make good use of the hill side of

the road and prevent accidents from happening. The wider inside corners could be utilised for bus stops, allowing space for cyclists to safely go around them. Raroa road could also become a coupon parking zone to reduce the number of vehicles on the street and make more room for buses.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Double the width of the path and make half of it a cycle lane!

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The sooner the better. It is dangerous currently and in desperate need of change

Feedback**144**

114167961377

NAME: Jo Tregidga	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
We deserve to have streets that make it safer to choose active forms of transport, and 3 years from now is too long to wait for this while we are in a climate crisis and have a congested and fume engulfed city

Feedback**145**

114179908588

NAME: Jo white	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly oppose****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Negative
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Oppose	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback**146**

114178472451

NAME: John Sullivan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I support any move to make walking, cycling and other non-motorized transport safer and more convenient, provided it does not detract from the natural and built environment.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I approve of the uphill cycling lane and agree that it is not necessary downhill. Will scooter riders be encouraged/obliged to use it rather than the footpath? While it is important the lower speed through Aro Street I am concerned that adding bumps on a narrow straight street, where speed automatically builds up running downhill, would create noise issues for residents, and pose a risk of cars hitting them at speed and losing control. You might also consider doing something to improve visibility for cyclists and drivers turning from Devon Street on to Aro Street. There is very little warning of cars coming down Aro Street, and the need to keep watching for them compromises ones ability to watch for pedestrians on the crossing immediately to the left.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Works well on Crawford Road and should work well here. Will scooter riders be encouraged/obliged to use it rather than the footpath?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Don't know

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I use the Aro Park and Little Palmer Street paths in both directions walking, and from CBD to Aro Valley cycling. Widening the paths would enhance shared use. There is natural flow on to Inverlochy Place, and the chokepoint between Octa Associates and the apartment block should also be addressed. Pedestrians must always take priority, and the speed of cycles, scooters, skateboards and mobility scooters should be limited to ensure their safety. Cyclists, scooterists etc must be encouraged to slow when entering Little Palmer Street from Abel Smith Street. There is an issue for cyclists etc exiting on to Aro Street from the park and wishing to cross over to the uphill bike lane. The conjunction with the Garage Project carpark is also an issue. The Aro Park path is impassible due to flooding in heavy rain (see photos) and this should be remedied.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

It gives affected parties time to make other arrangements for parking etc, but is still complete within the term of this council.

Flooding in Aro Park

The path through Aro Park is impassable in heavy rain due to flooding in these two areas.



The area around this tree is regularly boggy and flooded in heavy rain



Feedback**147**

114181681064

NAME: John Beaglehole	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Sensible proposals that will greatly aid active transport uptake on a key route to and from the city.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

At the bottom of Aro St, the road widens as it joins Willis St, and occasionally cars stack up two wide -- this makes it quite unsafe on a bike. It would be good to narrow this area, to prevent that from happening. It would also be good to change the camber of the bottom 200 - 300m of Aro St, so that on the downhill side, it doesn't fall off quite so steeply. I entirely support the extension of the 30 km/h speed zone to Willis St. If the uphill bike lane does not extend through the shopping area, you may want to think how you will ensure traffic remains calm.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

I support the addition of uphill bike lanes on Raroa Road (the uphill section). For the section of Raroa Road between Plunket Street and Moana Road, I am not sure that a single-direction bike lane is particularly useful -- drivers can be difficult in either direction. I would favour a combination of a 30km/h speed limit, traffic-calming measures, a slight reduction in parking spaces, and sharrows in the middle of each lane. For carparking on the downhill side of Raroa Road, it will be critical to reduce some of the areas where people are trying to park -- there is a lot of parking too close to corners, and it makes it very difficult in morning traffic. That won't change under the current proposals.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Particularly through the park, I would look to take measures that force cyclists to reduce speed.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

It is great that much of the work is front-end loaded. This work, however, is part of a system that will encourage more people to use active transport, and getting more of the system in place will be positive. Getting it done faster will be net positive.

Feedback**148**

114173811490

NAME: Jules	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Components of the proposal are great (sending bikes through Aro Park etc). Others don't contemplate the realities of living in this particular city and the lack of facilities available for kids/families in the city eg no public swimming pool, no public gymnasium etc. Rather than channeling bikes from Karori through Aro Valley into the city, how about channeling bikes down through Kelburn?

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

There's a baseline assumption with this design that people who live in the city don't need a car. It doesn't consider that the facilities that people need to be able to access aren't available within walking/biking distance from their Aro location - and certainly aren't available in the city. There also seems to be an assumption that public transport will get you where you need to go by when you need to be there. And a final assumption seems to be that those living in Aro are fit and able such that walking/biking are an option. I think these assumptions are flawed.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

The road is narrow with two cars passing, dangerous with a bus.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

How will we manage the Aro fair etc when this pathway becomes more prominent?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

There's an assumption on timing that people will be able to transition away from a car so won't need car parking. That assumption needs to be challenged - unless a related assumption is that Aro is populated by fit single people, rather than young families. Buses don't take dogs (and dogs aren't welcome on the city beach anyway), buses can't get Aro kids to gym/swimming/athletics/dancing/sports in Newtown/Kilbirnie etc after school in time for these classes at 3.30/4pm. And it's not feasible to bike to these locations either. I live in Aro, with 2 small school aged children and a dog. The kids walk to school and I walk to work. However we need the car for their activities after school. We carpool where we can. Ride share vehicles don't permit dogs. And they are not always available in the time you need them. On the design of the cycleway - there's no mention of the impact on the street drainage and drains/sumps aren't identified on the maps. Aro Valley receives a massive amount of rainwater from Kelburn/Brooklyn etc and it's common for the street to flood. The sumps are often blocked which obviously doesn't help drainage. Yet the cycleway is proposed to be built over these sumps. What consideration has been given to the drainage impacts of the cycleway design? Will the Aro stormwater infrastructure be upgraded to accommodate?

Feedback**149**

114174265853

NAME: Kain	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Any improvement is good, but this isn't enough...

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I'm happy for any progress, but this really doesn't go far enough. If I wanted to ride here with a child (or someone justifiably scared of riding in NZ) this wouldn't improve things terribly much. What happens at the shops, or downhill? 'Share' the road with cars? They and their behaviour is the problem...

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

As before, progress is good, but half measures aren't really sufficient. Does anyone really think sharrows make a blind bit of difference to the stress and (perceived) danger of riding?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

This seems like it's not addressing the actual problem, which is the lack of proper infrastructure...

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The state of cycling in Wellington is embarrassingly bad; piecemeal and slow changes won't change that

Feedback**150**

114180690400

NAME: Karl Tiefenbacher	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Making a narrow street even narrower while disadvantaging those who currently need the parks outside their homes is ridiculous and will create more accidents

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

The only thing I agree with is the speed bumps

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Continuing to develop poorly thought out and designed projects in an effort to reduce carbon emmissions instead on peoples ability will result in deaths in the future. This is an incredibly narrow road already and making it even narrower with a cycleway will make it a deathtrap

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

This makes sense

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback**151**

114160113956

NAME: Kate Clarke	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It is very dangerous as a walker/cyclist through this area now.

Feedback**152**

114181449019

NAME: Keith Clement	SUBURB:	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

My concerns are as previously detailed. It is woefully ill-considered

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

I have already submitted once but further detail requires that I modify some of that submission. This proposal is ill considered, poorly communicated and consulted on. Many business people feel sidelined The social engineering implied by trying to force people to relinquish car ownership is inappropriate at this level. Insufficient consideration seems to have been given to all other the street activity apart from cycling. Delivery and emergency vehicles apparently, have no place in this proposal. Humps are not appropriate on an arterial road. Kerb-side access is a real necessity for many activities. The Council District Scheme now proposes Six story building both sides of Aro St.....If parking is only possible on the North side how is it going to be possible to pump concrete across two lanes of carriageway for any development on the South side ? Not to mention daily deliveries, trash and recycle collection, ambulances and furniture moving trucks. The design fails to consider wider use and application of "street" It should be reconsidered

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Park is the only open space in the floor of the Valley. It took an enormous battle between the Community and the Council to get this meagre space. Having it is precious and it should NOT be compromised by promoting the routing of wheeled traffic through it. The Preschool/Community hall will be compromised by this 'higher speed' facility. There will be a greater risk for the young and the elderly. This route is already used by an occasional motor scooter...(I have almost been run down on more than one occasion)....Its further development will only make this worse.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Should not proceed in this form

Feedback**153**

114160151982

NAME: Kelda Hains	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It feels like a dangerous road for cyclists and pedestrians right now. Ready for some changes now!

Feedback**154**

114178512093

NAME: Kelvin Aris	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Make it safe for people of all ages to cycle and people will have a genuine choice as to how to reduce their impact on climate change and the unnecessary dominance of single occupant vehicles clogging up our city. This route by bike or scooter is currently not for the faint hearted. WCC, please hold your line on the wider cycle network and the next generation of wellingtonians will enjoy the many fold benefits of a healthy happy connected city.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I would prefer the 30kmh speed limit to be extended all the way to Chaytor street to avoid any confusion that there will be all types of road user on this windy and narrow section.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Let's get on with it!

Feedback**155**

114160393332

NAME: duncan kenderdine	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

great effort on making this more cycle and bus transit friendly - please note where the west bound cycleway merges with the westbound traffic will need management and clear transition as the two modes are at quite different speeds

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

there are a couple of areas where you are taking a short cut to make a save and usable route - dumping the bikes back onto the road with no protection after the big curve is nonsense as this is where cars go very fast - either widen the footpath or narrow the lanes. then towards the top at what is now one of the more dangerous areas - you are again shrinking the cycle path to nothing

all the way up to the intersection with norna cres - this are is really important to encourage people into bikes as it is very off putting as it currently is

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

i think you should take cyclists straight to aro st rather than through the park as the increased numbers will start to be come a problem at the end of the proposed path meets aro at with the garage project entrance for cars just there

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

go hard and go fast with the end solution - temporary and part done options just create confusion

Feedback**156**

114172433210

NAME: kev O'Donnell	SUBURB: Brooklyn	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Brilliant. I have had three near misses as a cyclist on this road and the slower pace of the change allows residents time to adapt

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

it helps makes the city more liveable and safe for cyclists and walkers

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

will slow traffic down which is a good thing

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	
It is already well used by cyclists- including me. As a cyclist I know that if u widen a path cyclist will ride faster- a narrow path slows them down	

What do you think of the timing of the proposed approach?	
The timing of the stages is about right	
Why do you think that?	
brilliant- I have had three near misses cycling on this stretch of road- but doing it slowly helps with people who own houses	

Feedback**157**

114162882600

NAME: Catharine	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

If you are going to do these changes and I am sure it is a done deal, you must have cycle counters at the beginning and end of all sections to monitor usage. At present the cycle counters are random and don't support much of the proposed projects. I think this goes too far in favour of cyclists. I say the council should finish the Brooklyn Road Cycle lane before it starts stuffing up the rest of the city. I'd like to see more sharing of the road space and cyclists taking more responsibility for their own safety i.e. reflective clothing, bike lights, I'd like to see escooter riders have to wear a helmet and have better lights. There are a few parts of this proposal I agree with but not enough to give the proposal the thumbs up. I'd like to see the mobility park on Aro Street and the bike parks on Devon St or Epuni Street. There is no need for the cycle parks to be outside the cafes. But mobility drivers do need to be close to their destination. There is no need for speed bumps if there is a separated cycle lane. This is overkill. 30kms or speed bumps - not both. I wonder how the residents will feel having to listen to the noise created by driving over speed bumps. They are very noisy. I support the removal of on street parking on the uphill side of Raroa Road. It has become very dangerous for all users with the introduction of this as a bus route. It is hard to drive/ride in the centre of your lane without the possibility of a bus coming at you in your lane.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose**

Do you have any comments to make about the proposed design?

It is too messy. It will make the streets ugly and be confusing. With the removal of the parking, there will be enough room to share the road with bikes on the left. There won't be enough room for two way traffic if they are both buses. With a wider road and cyclists taking responsibility for their own safety there will be room for all with no parking in the left. I'd like to see cyclists wear reflective clothing, have lights. Not wear black. I'd like to see scooters have to use helmets and have lights. If I cycle at 30kms do I have to use the cycle lane?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

I agree with the removal of all on street parking on the uphill side of Raroa Road. I don't support the installation of an uphill bike lane. This restricts the usage of the area, limits passing of others on the cycle lane - think ebike/escooter vs human powered bike. With no designated cycle lane there is room for all. And if all cyclists had lights and wore a \$2 orange reflector vest from the \$2 shop we'd all be able to see them. Don't agree with sharrow. Road pollution. Again, if cyclists took a course on how to ride on the road, they would be as fast as the cars and be seen. No need for them. Stop the visual pollution.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

If Aro street is being made safer then there is no need for them to cycle through a picnic park, past a pre school. I've watched cyclists go through here in the past few months and few give a toss about other users. In a perfect world you could have shared spaces but there are many that don't think of others and who has the right of way. Pedestrians are at the top of the councils transport hierarchy so respect them and leave cyclists off pedestrian spaces.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The council has screwed Island Bay Parade with its cycle lane. It has installed a crap cycle lane on Brooklyn Road. Why can't you finish something, do it properly and learn from it instead of installing these cycle lanes where there is no 'action taken based on instant feedback'. The city is being stuffed by a few righteous cyclists that aren't prepared to compromise. (I cycle all over wellington on a daily basis. I feel safer on the road with cars and trucks than I do with other cyclists on cycle lanes). I do agree with the mobility park but it should be on the main street. If you want a 30kms speed limit don't have speed bumps. You can't have both. With a dedicated cycle lane the cyclists are safe so no need for speed bumps. They wreck cars costing more money for replacement parts, they are ever so noisy for those living nearby. Put the bike parks on Epuni Street and put the mobility park on Aro Street. These cyclists are always saying drivers don't need to park right out front - neither do cyclists. Use a car park in Epuni St or Devon Street for bike parking. Not the main road.

Feedback**158**

114174261258

NAME: Kyle Schwartz	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I very much approve of any lowering of the speed limit, increased signage for shared usage, speed humps, crossings, efficient bus stops, etc. I have low support for dedicated bicycle lanes in mostly flat areas in an inner-city district with no other parking available for residents and their visitors.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

My comments are primarily concerned with the proposal for a "New bike lane on the uphill side, except through the shopping area." I oppose this design where there is a necessity to remove parking spaces. I think the same benefit could be obtained by lowering the speed limit in the mostly flat areas and marking the road as a shared space. I am unsure about how many parking spaces exist as the road curves upward, if that area does not have many parks on it I could see making a dedicated lane there because the cyclists will be very slow. But I do not see that route as a particularly commuter-friendly route to Karori on bikes or scooters. The road is very steep and I think only the most confident and athletic of cyclists could tackle this road. Unfortunately, parking is very limited in Aro Valley, and the removal of spaces will make it especially hard for visitors, making trades, or residents whom depend on vehicles like the elderly or disabled.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

With regards to the very steep uphill portion of this connection, I could see supporting this depending on the number of parks removed here. If there are not many parks removed I could see the use here, though I am not convinced that this is a commuter-friendly route.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I support any changes to lowering the speed limit, installing speed bumps, raising the crossings, etc. I do not support the elimination of existing parking spaces in favor of dedicated bicycle lanes in this area. Lowering the speed limit, proper signage regarding the road as a shared zone, speed bumps, etc could all improve the experience for cyclists without removing spaces.

Feedback**159**

114181665691

NAME: Laura	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Overall, I am supportive of the changes (particularly when it comes to improving cycling safety), but I do have concerns about a few of them. The general feeling I have here is that the needs of residents are not being prioritised; they seem to suit those who don't live in the Valley, when the infrastructure is not in place (currently) to support residents to adapt to and enjoy the improvements, also. I feel the council has misunderstood the demographics of this and the surrounding suburbs and what certain members of our community need in order to continue to live here. Buses, though affected by pandemic issues, are still not running often enough to encourage people to use them. People on electric scooters tend to be discourteous when using footpaths. Charging carparks for EVs and carparking for the shared vehicles of the CityHop and Mevo companies is not guaranteed. It is too hard for us to be expected to make the shift so suddenly when there's little to help us do so.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Yes. The addition of only one mobility park on Epuni St is insufficient for the current needs of residents, shoppers, and visitors to the valley. There are many residents in this part of the valley who are of an age where access to mobility parking is crucial; this is expected to increase as part of our ageing population. In addition, there are residents of the council flats at the south of Epuni

St and the home for people with physical disabilities (run by the Argus Trust at the rear of the Aro Valley shopping village) who are likely to need this kind of parking. Therefore, there should be at least two mobility parks on Epuni St, one at the Aro-Epuni intersection, and one closer to the council housing. On the gradual changes to parking, this appears not to take the needs of certain members of Aro Valley residents into consideration. In particular, those with young children and those who are elderly, and those who are disabled. My household, for example, is multi-generational and shares ownership and use of one car with relatives further up the valley in Highbury; we do not have a garage or driveway, relying on the on-street parking spaces during the weekends when it is our turn to use it. We take turns using this car for large-scale shopping, school drop-offs and pick-ups, removals, etc. Our location is actually depicted in the artist's rendering of Aro St on this page. I believe little consideration has been made of people who are less likely and less able to shift their modes of transport, especially when they are already doing the best they can in terms of sustainable practices. This forces us to consider parking on one of the few side streets in the valley, which will surely become more in demand given the extent of these changes. The council needs to look more closely at the demographics of the Valley (and the projected populations) as it risks alienating and eventually excluding certain members of society from living there. This would be a death-blow for the area as diversity is one of its main attractions. In sum, more compassionate measures to accommodate the previously mentioned residents is required.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

So, what exactly are sharrows? Please ensure that council proposals are made using accessible language with key terms explained, otherwise it excludes those who are not generally 'in the know' and that would be most of us. Will broken yellow lines be sufficient for improving safety? Would these sections not have the green bicycle lanes?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?
People walking on the paths through Aro Park and Little Palmer Street?
People riding bikes along this route?
People driving vehicles on the surrounding streets?
People with disabilities or accessibility issues?

Positive

Negative

Neutral

Neutral

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

In principle, I support the idea. However, there have been many times when I have almost been knocked over by someone on an electric scooter and a bike in the Aro Park to Abel-Smith St area (I find that cyclists are far more considerate; they tend to lower their speed in this area, probably because the physical boundaries demands it, rather than the presence of pedestrians). I also have concerns for the plants and trees in the Abel-Smith/Palmer St lane and feel strongly that every effort should be made to protect and incorporate them into the final design.

What do you think of the timing of the proposed approach?

Why do you think that?

Please see my previous comments on the mobility park on Epuni St, the removal of carparks on the south side of Aro St, and the widening of the Palmer St lane. Where will these four speed humps be positioned? And I'm surprised that the 30kmh zone is not being extended in the westwards direction; most of the speeding I've witnessed occurs from vehicles coming downhill. There should be at least one car-sharing parking space at each end of Aro St. While the timing of the stages is roughly okay, why not break it down further into four or five phases?

Feedback**160**

114163151971

NAME: Liam O'Connor	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

Speed bumps are pointless because everyone just speeds up and the slows down. In fact, it causes chaos because there's sudden speed slowdown which causes congestion. This happens regularly, especially in Karori and down Tory St.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Raroa Rd isn't nearly wide enough. Removing the parking and the yellow lines on corners is a smart idea, because it's causing issues with just traffic alone. But there's not nearly enough room for a dedicated cycleway unless you remove the footpath.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I'd rather you didn't make them at all.

Feedback**161**

114164336876

NAME: Tim	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Stongly oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly oppose****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very negative
People riding bikes along this route?	Very negative
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Don't know
Why do you think that?

Feedback**162**

114174936767

NAME: Louis	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Make the bike lane go through the shopping area. Where are bikes meant to go when passing through Aro St? The road is for cars and the path is for bikes

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Make sure there's a retaining wall near the new Raroa stop. There are dozens of slips along that stretch every year

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
We needed to do all of this 2 decades ago. We need to do it all asap

Feedback**163**

114161301207

NAME: Tristan Campbell	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Road too narrow with cars parked on each side. Removing parking makes it better for cars and buses

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Extend through the village so don't have to mix with traffic.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Great change. Parked cars currently make this road difficult for cars and buses.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**164**

114175582594

NAME: Megan Chrystal	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I support the slower speed and speed bumps to make the road safer for pedestrians and cyclists. However, getting rid of that many car parks on Aro St seems like a disaster. People who don't have garages (i.e. people who may not be able to afford a place with a garage) will be forced to 1) park further away from their homes (a problem for the disabled) 2) park on streets that other residents live on and need access to and 3) pay more for residents parking- coupon exemption is cheaper but it seems these will be replaced by residents only. Of course the people who can afford garages/other parking options will be fine. It's only the less wealthy who will be negatively impacted by this.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback**165**

114181120001

NAME: Maree	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

On road parking for residents has not been given proper consideration. Parking in Aro Street and Raroa Road is already often at capacity and limiting parking to one side of the street will mean many residents who have no off street parking with their properties will no longer be able to park within easy access of their homes. Reducing parking for residents will create more stress on neighbouring streets which are also often at capacity with either residents or commuter parking. Local residents seem to be being disadvantaged in favour of those who wish to bike into the city but choose to live further away and where they likely have easy access to on road parking if needed and even off street parking. While those of us in this area may be close enough to walk to the CBD, and many of us do, we still need access to cars for children's sports, work outside the CBD, shopping etc. I am also concerned for the local businesses. Many people currently drive to the cafes and without being able to park in close proximity (while there will be a limited number of 20 minute parks this isn't sufficient time for lunch or a catch up over coffee) this clientele will likely choose to go elsewhere.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Taking away parking on one side of Aro Steet will be at the detriment to local businesses. Using the parking up Aro Street is a necessity when meeting visitors to the area at the local cafes. While

I can walk the people I'm meeting usually need to drive to the area. The 20 minute parking spaces are not long enough for lunch or even a coffee and catch up. Taking away parking on Aro Steet will mean we will look for other areas where there is easier parking available, as without being able to park in the coupon parking zones this will leave nowhere at all to park within close proximity,

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This will leave many residents without any access to on road parking. The figures used showing parking capacity are not at all what is experienced when residing in this area and wanting to park close to your house. We have been advised we could park on Plunket Street or Mt. Pleasant Road as alternative options if the limited spaces on the downhill section have no spaces. Getting to these streets is a 6 minute walk from where we live, and also these streets are usually at capacity so these are not viable options. Other streets around us are also usually at capacity. There are many flats in this area with multiple tenants many of whom have cars, and the area is also used for commuters to the city. This can make parking tricky as it is. I've just been to the supermarket and have been able to park reasonably close to my house (not as close as I'd like) on the left hand side of the road at the end of my access path. The closest park on the downhill side of the road when I parked was another 100 metres up the road and across from our access path. This makes sending the children out to get the bags a much more dangerous trip having to cross a busy road while laden with shopping bags. Also if there are deliveries (and the driver is having to leave the van) where could they park in an already congested area if parking on one side of the road is removed?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

This needs to be carefully planned and considered before rushing into it. We only found out about these proposed changes from a friend. We live on Raroa Road and it seems as if the intention is to try and push this through without people being aware of what is being proposed. I find it disappointing that there have been no mail drops in the areas impacted by these changes (the streets directly effected and the surrounding streets as there will be a flow on to these areas in parking is lost). It would also have been appropriate to send letters or emails to the property owners of properties in Aro Street and Raroa Road.

Feedback**166**

114181281666

NAME: Maria Cassidy	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Even when upgraded I don't believe that the bike route will be well utilised due to its topography and the fact there still wont be much separation between cyclists and vehicles. However, it will have a substantive impact on those that live in the Aro Valley community. The lack of car parking will make it difficult to transport kids to sports and after school activities (because it wont be feasible to have a car) and there will be nowhere for tradepeople to park when doing work.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

see attached document

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose****Do you have any comments to make about the proposed design?**

see attached document

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

I support increasing the quality of the path through aro park as it generally in very bad condition during winter and often floods making it not useable

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I don't agree with the changes - if they are to be made more time is required to identify parking options for those in the community (we are a community not just a thoroughfare)

Submission%20on%20Aro%20Valley%20route%20proposals%20-%20Maria%20Cassidy.pdf

I strongly oppose this proposal.

I do not believe that the proposed bike route will be utilised to any great extent and consider that the proposed reduction in parking along this route will have detrimental impacts on the Aro Valley community.

Utilisation of the bike route.

I believe that there will be limited use of the Aro Valley bike route (particularly the uphill route) and that those that do use it will be limited to a small number of 'hardy' cyclists. This assertion is based on my use of this route as a cyclist in the past and the 90% audit report that your organisation commissioned as part of development of this route (*An independent safety and accessibility assessment of the proposed changes* - dated October 2022).

The 90% audit report states that:

The proposed treatment for the Aro Valley route, whilst being an improvement on the existing situations, is only expected to attract cyclists of the "strong and fearless" or "enthused and confident" categories.

This is considered acceptable given the difficulty of providing a temporary treatment on a route with challenging space availability and topography, but a permanent solution in future should aim to provide more separation from motor traffic to attract a wider cycling audience.

I don't consider that any permanent solution can effectively address the space availability and topography challenges that would be necessary to make the route more accessible to a wider cycling audience. Obviously nothing can be done about the topography – it is simply a steep and twisty route!!! In terms of ensuring adequate space between bikes and vehicles, even the audit report notes that achieving adequate separation between vehicles and bikes is marginal in many places along the route.

I consider that the current road layout is adequate for the hardy cyclists that currently utilise it. While the audit report stated that 33% of serious injuries involved cyclists, it is important to note the low number of accidents that resulted in cyclist injuries. As stated in the audit report, over the last 5 years, only 3 of the 72 accidents on this route have resulted in injuries to cyclist – one injury was minor and two were serious. It would be interesting to know if the number of cyclist related accidents is higher for this route compared to a similar length of road in another part of Wellington city.

Alternatives:

- Instead of installing bike lanes on uphill routes, encourage, promote or even incentivise the use of the bike carriers on the front of public buses e.g. for the first x period of time those commuting with a bike go free on public transport. This approach would still achieve a key aim of the proposal – to encourage people away from commuting by car.
- Consider developing the bike lane on a route that utilises existing public spaces as much as possible. For example, a route that takes in the pedestrian bridge across Bolton Cemetery, and traverses the botanical gardens. This would be a more direct route between Karori and the CBD and addresses the issue that cyclists on the Aro Valley route end up on Willis Street where no formal bike route is proposed.

Detrimental impacts on the Aro Valley community

The removal of 70 car parks along the south side of Aro St and the removal of many car parks on Raroa Road will have a detrimental impact on our community.

The limited number, and location, of proposed replacement parks would effectively mean it would no longer be viable for many in the community to own a car. Even now finding a car park within 1-2 blocks of home is difficult. Removing 70 car parks would make it practically impossible to find a park that is within a reasonable walking distance of home.

While discouraging car ownership for those living on the outskirts of the CBD may be an intended consequence of the proposal, this will have a substantive impact on the community in terms of accessing activities and amenities. The removal of these car parks will also make it even more difficult to access service providers who need parking in close proximity to our houses to do their job.

Accessing activities and amenities

We are a household of four. We do not use our car to commute to work or school – we either walk or occasionally take the bus. However, we do use our car in the following ways:

- after school and in the weekend to transport our two children to extra curricula activities, including sports (games and practice), gym, dancing and swimming lessons.
- occasionally to visit friends across Wellington, do chores eg trips to the tip /purchase items for the house / grocery shopping (online isn't always available), or go for family outings e.g. trip to beach on a sunny day!

I accept that there are other ways to travel to the activities and locations above, including car share, uber or bus.

However, even just focusing on transporting our children to extra curricula activities, and ignoring that we use a car for other purposes, it would be prohibitively expensive to use these alternative means of transport on a regular basis. With the exception of perhaps swimming lessons, it is not realistic to bus to many of these activities because either no bus route exists (because the activity occurs on a Saturday) or multiple bus trips would be required each way.

I estimate that it would cost in the range of \$6.5K to \$8.5 K annually to transport our children to these activities using public or shared transport, which is approximately a third to double more than the entire annual running costs of our car. Below I have set out the input values I used to estimate these costs.

- Annual running costs of our car

Item	Cost
Insurance	\$790
Registration	\$102
Petrol (annual estimate based on last two months actual costs)	\$1573
Maintenance (based on actual costs incurred)	\$1305
Parking Permit	\$195
Total	\$3,971

- Estimated cost of using shared transport (eg car share, uber)

Activity (and hours required for each activity- this impacts the cost of the car share as these activities fall outside the home zone for car sharing)	No. of weeks doing activity annually	Cost per week			Total (range)
		Car share [#]	Uber ^{#1}	Bus	
Summer sport					
- Competition (takes 5 hrs)	28	\$85	\$42		\$1,176 to \$2,380
- Practice (takes 5 hrs)	24	\$85	\$42		\$1,008 to \$2,040
Winter sport					
- Competition (3 games ~5 hrs)	17	\$85	\$84		\$1,428 to \$1,445
Activity 1 (~1.5 hours)	40	\$25	\$42		\$1,000 to \$1,680
Activity 2 (~2 hours)	40	\$34	\$26		\$1,040 to \$1,360
Swimming (~2 hours)	40			\$6	\$240
Total					\$6,572 to \$8,465

- costs based on Mevo's hourly / day rates - assuming minimal traffic

#1 – based on quotes from Uber's website - assuming minimal traffic

Accessing service providers

As noted above, the removal of the car parks will make it even more difficult to access service providers, such as builders, plumbers, electricians. To provide these services they need to be able to park in close proximity to the houses they are working on.

I expect that the number of tradespeople working in the community will only increase over time as:

- property owners (both owner occupiers or landlords) work to improve the quality of the houses in the community to meet healthy home standards; and
- new high density housing standards allow increased infill housing within the community.

It is unrealistic to expect a community to be able improve the quality of their homes, and build additional infill housing, while at the same time provide nowhere for the necessary tradespeople to park while they are doing their job.

Feedback**167**

114164163875

NAME: Magnus McCarthy	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**168**

114181626738

NAME: Mark Wilson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

An Aro Valley cycle route is an impractical and unsafe option compared with other options, such as Glenmore Street (with the addition of a "no overtaking cyclists in Karori tunnel" policy). The overall plan proposed by WCC bundles some positive changes for Aro Valley (like improved pedestrian crossings) together with some negative ones (such as even less parking for residents, tradespeople, and visitors to the shopping center). The net effect seems like it will be very negative for residents. Further - even with the addition of the cycle lane - the route up Raroa Rd, through Raroa Cres, and onto Chaytor Street will continue to be too difficult and unsafe for the majority of commuter cyclists. This likely negates the return on investment of this option, compared with safer, more practical options.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

There is insufficient space for a cycle lane. It is an impractical route choice, compared with other options, like Glenmore Street, or a shared-use footpath (e.g. for cyclists and scooters on one half, and pedestrians on the other). The benefits are too few to justify the costs to local residents and business operators in Aro Valley. There is already insufficient parking in the area - especially given the number of houses in the area without off-street parking. Parking is required for residents, delivery drivers, tradespeople and visitors to the area. Local businesses are already finding it

difficult due to the pandemic and post-pandemic behavior changes. Where will their customers park? It is already difficult to find tradespeople who will work in the area with the current level of parking. How are people expected to maintain their properties without vehicle parking near their gates?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Raroa Road and Raroa Crescent are steep, winding roads. They both have many blind corners and areas where there's insufficient space for two vehicles to pass each other safely. Long vehicles like busses and trucks have to cut corners to avoid head-on collisions with downhill traffic. Green paint on the road will not deter this. Norway Street is a safer alternative to the lower part of Raroa Road - utilizing the zig zag pedestrian path up to the intersection of Plunket Street and Raroa Road. However, this still leaves the dangerously narrow and winding Raroa Crescent (above the Karori tunnel) and the terrifying intersection of Raroa Crescent with Chaytor Street. Overall, this is not a suitable route for uphill or downhill cycling. Experienced cyclists, and those with abnormally fast reflexes might be okay. For everyone else, it's just a matter of time before their first accident. Encouraging commuter cyclists to use this route is reckless. All cyclists should be strongly encouraged to use safer routes. Investing in a route that many commuters will STILL feel unsafe on (or find too steep) is not an effective use of ratepayer's funds. Especially when Glenmore street is significantly wider and less steep. The Glenmore street route lacks only the addition of a "no overtaking cyclists in Karori tunnel" policy and signage" to make it very safe by comparison to any of the Aro Valley route options.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

It seems like this is something that needs to be done anyway. A lot of people live in Aro Valley, and cyclists and people on scooters already use this route. A wider footpath - with a separate cyclist/scooter lane seems like a smart choice. This route also needs better lighting at night. Monitored security cameras could also help.

What do you think of the timing of the proposed approach?**Don't know****Why do you think that?**

The timing isn't the issue here. The list of changes WCC are proposing to make includes both positive and negative items. Delaying the negative changes (such as the cycle lane) for as long as possible would obviously be better. Bringing forwards the positive changes (such as safety enhancements to the pedestrian crossing, and a reduced speed limit) would be prudent. You should also consider extending the 30km zone up through the unsafe, narrow part of Aro Valley, up to the entrance to Pollhill reserve.

Feedback**169**

114162754600

NAME: Matthew Brown	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**170**

114171675690

NAME: Matt Stevens	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Removing all the car parking on Aro Street will have a massive negative impact on local residents. Cycleways already built are not used. Brooklyn Hill is the best local example. The proposed cycleway on Aro St and Raroa Rd will not replace 10000 car movements a day with 10000 bike movements.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

I am a resident of Aro Street, Removing all the car parking on Aro Street will have a massive negative impact on the local residents and I am completely opposed to it. The proposal treats the Aro community as merely a thoroughfare to Karori. The document delivered to Aro residents even states "it will free up space for people who need to drive". Similar cycleways nearby are mostly unused. Brooklyn Hill is the best example. The document implies everyone will be walking, cycling or travelling by bus with a few left driving, but the reality is the number of car movements will remain unchanged. 10000 car movements will not be replaced by 10000 bike movements every day. The raised pedestrian crossing and speed humps are a good idea and can be implemented independently of a cycleway.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Raroa Rd is very narrow with insufficient width for a cycleway and the roadway. Plus the removal of all car parks uphill will have a negative impact for the local residents.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Little Palmer Street is quite narrow. The proposed picture in the survey has the path width about the same as it is now. I have never experienced any issues biking in these areas.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

The tone of the plan, ie: "help residents to prepare and adapt to the change in parking availability", has the undercurrent meaning of "residents have this time to get rid of their cars". This treats the residents as second class citizens who cannot own a car unless they have off street parking.

Feedback**171**

114177995510

NAME: Matt Shipman	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Currently this area is terrible to ride, the level of service is just not okay. Drivers are impatient and unaware of the need for cyclists to ride away from the doors of parked cars ("a door and a bit more"). Cyclists are riding uphill, and the speed differential is significant between the different modes of transport - so separation is a very good solution. It will help the flow of vehicular traffic also (which also helps with emissions reduction) - and we will see that is it the parked cars, not the cyclists, that are "holding people up". Once more people can see the safety and ease aspects of the route I would expect a big uptake in active modes to and from one of the most populous suburbs in the country. Strongly support this project with the timelines sped up for Aro shops to Waimapihi Reserve.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

There is a parking issue on Aro Street currently, and it seems that many houses have multiple cars parked on street. The parking on the left side heading up makes it very intimidating to ride a bike - you have to ride wide of the car doors but the speed differential sees drivers become frustrated and make poor passing decisions. The proposed design removes this issue. Heading downhill cyclists and drivers will all need to be educated about where the best cycling line is - this is taking the lane, staying away from car doors, and using gravity to assist you in bridging the speed

differential. I strongly believe that Aro Village to Waimapihi Reserve is the most intimidating (and ridden by more) and would like to see that at Stage 1. Parking-wise, I think people need to take advantage of living in the city, and use active modes. This means those who don't have the luxury to engage in active modes can still find a park. More disability parking and car share - but it will need to be monitored and ticketed. The current car share outside my place has private vehicles parked in it almost every night.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Bit less pressure on parking here in general, so moving parks to one side makes sense. Again, education of both drivers and cyclist/scooter riders on how to ride in the downhill lane. Taking the lane for cyclists and drivers not trying to overtake for a perceived inconvenience (cyclist can keep up with vehicular traffic flow when they have the gravity and the skill/confidence to do so. Drivers need to know that the safe, recommended and taught place for cyclists to ride downhill is in the centre of the downhill lane. Drivers should never be "sharing" the lane with the rider, if they really need to pass they must use the opposite lane (if clear and safe to do so). We will see how the bus stops and cycle lanes work together, this might need a tweak depending on user experience.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

As you note, this is an established and safe, if not "official route". It take riders and scooter riders away from multilane roads (Willis) and the areas of worst congestion (and heightened risk). The widening should see an even better version of the current sharing of space here, which in my experience (walking and cycling) is usually very good. People are getting to destinations, using active modes, alongside eachother - it is a very good example of acitve modes being more convenient and direct than driving. It makes driving a single occupancy vehicle around the area become something you do by "need", not by "choice" and helps us lower carbon footprints

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly**Why do you think that?**

The area between Aro shops and Waimapihi reserve is the worst bit for riders and scooter riders. It is intimidating and puts off those less brave. If you need to be brave to access the other parts of the project further upstream, then we are not going to see as many people start using it, until much later. I strongly suggest that section goes to Stage 1. The earlier we get more people using active modes, the sooner we see the benefits for congestion, health, wellbeing, and the environment

Feedback**172**

114178350773

NAME: Max Fuhrer	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Council planned this uphill bike lane with downhill data!! This should be a red flag for everyone. Road sharing will help to slow traffic. Footpath sharing helps to slow bikers - re Aro park and Palmer lane Education will help safety more than bike lanes. Having bikers swing out around buses is dangerous, better to have them on the road. Anyone who designed this should be banned from road safety. Where do residence park at night when there are no bikes. Where do residence park in the middle of winter when it rains for weeks and bike traffic is reduced to close to zero. Slow the traffic and have road sharing. These changes will not make it safer, which makes some of the next questions which you cannot comment on loaded and dishonest.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

This will have a major impact on all parking in Aro Valley which is all ready in high demand. Bikes can share the road, they how done so successfully till now. A bike lane will allow cars to go faster, sharing helps the traffic slow down. removing parks 24/7 for afternoon uphill traffic is stupidity. Having no parks on stormy winter days when bike traffic is close to zero is fool hardy. At the time of making the plan ONLY downhill traffic was recorded apart for two weeks of uphill. This demonstrates negligence and the intention to push an idea with out due diligence.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is more dangerous than having parks, bike will swing into the traffic. Council is putting bikers at risk.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Very negative

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This highlights the fact that bikers do not keep the rules. Either leave it as it is or put turn styles to stop them using this route. Lanes will just make bikers go faster, it is safer if they share, actually share. Spend more time educating bikers and drivers to share the road

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Do not make the changes 21 days is way to little time for honest consultation

Feedback**173**

114180544600

NAME: M Dommett	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

This will unduly disadvantage locals, encourage people not to shop in Aro Valley all so rich white people on sunny days can cycle to and from their rich neighbourhoods where they can park their car easily. It will do little to encourage additional poor cyclists as the topography and climate is not conducive to cycling. You will make the rich's life more privileged at the expense of those less well off.

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

As we have seen already by removing car parking on roads and adding cycle lanes, this only increases pressure on alternate parking locations, which upsets locals and deters shoppers from coming to the area. The cycle ways only benefit those rich enough to own an electric bicycle and they are not well used on windy or wet days. These cycle paths benefit statistically only the richer, white members of society, allowing them to cycle from their neighbourhood (where they can park their cars) through areas now made devoid of local parking spaces.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose**Do you have any comments to make about the proposed design?**

As detailed in previous comment - this proposal disadvantages locals to allow those rich enough and in rich neighbourhoods to cycle with less cars in their way. It has nothing to do with empowering the locals or those of lower socioeconomic status.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose**Do you have any comments to make about the proposed design?**

If speed on the cycle way is not reduced/regulated it will allow fast moving cycles to threaten the safety of pedestrians. The path, if widened should have built in calming features placed across the width of the path, so they cannot be circumvented.

What do you think of the timing of the proposed approach?**Make the proposed changes more slowly****Why do you think that?**

I would have selected - don't enact the changes at all, as this whole plan smacks of entitled rich people getting a clear way to go from rich neighbourhoods to their work on sunny, non-windy days. Is LGWM only going to focus on rich white people? Currently that seems to be the plan.

Feedback**174**

114160817954

NAME: Me	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

Points to make

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Care at pinch points	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**175**

114161535534

NAME: Mel Foot	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
So nice being able to walk through the city on a path that isn't along a road	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The residents of this area have had private carpark paid for by the city for a long time, it is time to correct this. Roads are public spaces, not private property.

Feedback**176**

114180458125

NAME: Meredith	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

WCC has a duty of care to the residents for whom Aro St is not a thoroughfare. The road design penalises:

- families who need to take children to school and various activities,
- anyone who wants to use their own car do a weekend shop, transport sizeable items, shop in greater wellington, easily visit people or be visited by friends and family,
- people who much prefer to be familiar with the car they own,
- it is already hard to get trades people to carry out work because of the poor parking even when the homeowner buys the trade person a coupon for the day.

The WCC proposal supports a small minority in terms of age group and lifestyle. The proposal also runs counter to the Government's policy to move to an electric fleet that requires parking.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly oppose**

Do you have any comments to make about the proposed design?

Raroa Rd does not have great access. Most people need cars to live there. They need parks.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Don't know

Do you have any comments to make about the proposed design?

Could not locate the part of this plan that involves Willis St. Parking is a greater need for people with disabilities eg people in wheelchairs. A far more important issue is the need to install good street lighting throughout Aro Valley so people feel safe to walk at night.

What do you think of the timing of the proposed approach?**Why do you think that?**

We don't support the proposed changes in Aro St that favors cycleways at the expense of residents.

Feedback**177**

114174726281

NAME: Mike W	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

You say this will make it easier for pedestrians to get around but I only see negatives for pedestrians. If it has to happen that's fine but don't market it as benefiting pedestrians.

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

Just the speed humps - buses and trucks are very noisy as they accelerate after crossing a speed humps, and this is a main thoroughfare so there are many buses and trucks. From experiences this is a negative experience for pedestrians.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Whenever you remove on street parking spaces (or make lanes narrower) in residential areas we have more vans, trucks, utes and cars parked on the footpath (particularly tradies, who are there

for extended periods) . I have never been down Raroa Road without at least one vehicle parked on the footpath.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Don't know

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Do you have any comments to make about the proposed design?

Turning footpaths into shared paths is never good for pedestrians.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback**178**

114179675241

NAME: Mika Hervel	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I think, overall, creating infrastructure for people to travel in a low carbon way is critical to responding to the climate crisis and support council's action to do so.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I think that having a car share as an alternative for residents is a great idea. With that I think it might be helpful if there was some publicity given to the new car so residents know its there and available. I hope that the removal of the bus stop will not make bus use less appealing, and I would support ensuring that Aro Valley shops are still easily accessible by bus. I think the addition of a bike lane is an excellent idea and overall support the council moving towards a more climate-friendly city. One concern I do have about the bike lane is that it is just uphill. I have concerns about bikes coming downhill at speed alongside parked cars, where there is the opportunity for them to open the door and cause an accident. If something could be done to mitigate this that would be awesome.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support**Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:****People using Aro Park for recreation (e.g. picnicking)?****Positive****People walking on the paths through Aro Park and Little Palmer Street?****Positive****People riding bikes along this route?****Positive****People driving vehicles on the surrounding streets?****Positive****People with disabilities or accessibility issues?****Positive****Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?****Support****Do you have any comments to make about the proposed design?****What do you think of the timing of the proposed approach?****Make the proposed changes more quickly****Why do you think that?**

Council has declared a climate emergency, and action should be taken with all feasible and possible urgency to respond to this crisis. The sooner infrastructure is put in place for people to travel in a more climate-friendly way, the better.

Feedback**179**

114166591727

NAME: M Millen	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I do not agree to the the proposed changes.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

I DO NOT agree to the proposed change of putting a cycle way through Aro Valley or placing speed humps or bike parking in the shopping village or having car share on Epuni Street or parking changes on the south side of Aro Street

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

I DO NOT agree to any of the proposed changes

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Why do you think that?
I DO NOT agree to these proposed changes and the way this is being carried out by the Council. Even this survey is not providing better options to choose from

Feedback**180**

114167151822

NAME: Mark Johnston	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is a narrow and unpleasant route to ride. I support these changes to encourage more people to access the area by foot and on bikes.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
I have been guilty of riding my bike through here quite quickly - it would be good if you could limit the speed of people riding bikes here by design. Otherwise I think it is fantastic that you are formalising a common 'desire line' for people riding bikes from Karo Drive to Aro Valley	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
If they are notified now, you are giving people over a year to find a new plan for their parking or hopefully arrange to go car free. By adding car share, this is also helping towards that. The communication of these changes is now key - it will be very disappointing if people are able to make the "I wasn't consulted" claim. Big, visible on-street advertising is needed - and notifying landlords of properties as well as residents is important too, accounting for rental turnover over the next year.

Feedback**181**

114181683893

NAME: Martin	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Lots of mountain bikes using Aro to get to Waimapihi Reserve, parked vehicles on Raroa Road cause issues for traffic flow and road safety as well as block footpaths when they try not to stick out onto the road too much.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support**Do you have any comments to make about the proposed design?**

Fully support uphill bike lane. Support the raised crossing but this should include raising the other crossing at the shops too. Do not support the speed bumps or extended 30km speed extension. More work should be done on Ohiro road to make this a safe connection to Brooklyn for vehicles to avoid Willis and central. Currently the dangerous parking makes this an unsafe throughfare.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support**Do you have any comments to make about the proposed design?**

Generally support except for the unprotected bike lane between Holloway Rd and Broomhill in particular should be investigated to see if road reserve land on bank between could accommodate a protected path like it does with the walk path. No safe way citybound from Appleton Park to Moana road uphill mentioned. This should be investigated. Agree with parking removal to accommodate vehicles including busses to pass safely the whole duration of the road. Bus stop positions over cycleways and blocking roadways are a danger to both bikes and vehicles travelling opposite ways where cars leave centreline to pass parked bus and bikes get missed in bus driver view when driving off. Bike lanes should bypass bus stops and bus stops should have dedicated pull offs where possible or eventually as budget allows.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Get it all done at once, Plenty of side street parking for residents withing the plan as it will deal with those workers in town who currently drive to these side streets to avoid parking in the CBD. This should free up a lot of extra space for residents and users of Waimapihi.

Feedback**182**

114181663726

NAME: Anonymous	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

(1) 50KPH on the uphill side of this section, after passing thru the 30KPH business area, is too fast. Should be 40KPH max, or even 30KPH all the way up to Karori. Anyplace cyclists are protected only by paint & goodwill, especially on winding roads with blind curves, should be 30KPH. (2) Speed humps are an admission of design failure. Dangerous for cyclists & irritating for drivers. Wider cycle lanes w/physical separation would force cars to slow to 30KPH w/o having to put in speed humps.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

This is a good design, except for the failure to adjust down the speed limit. If bikes are separated from cars only by paint, the limit should be 40KPH max, or 30KPH for a much safer & quieter residential throughway.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

More cyclists means more picnickers & more park users in a good way. Pedestrians in parks are safer when more people are around; more bikes would mean more eyes on the street and more people to help out those in need. The disabled community. Cyclists taking this shortcut would mean fewer on the roadway with drivers, which is generally good for both where it's possible. People with accessibility issues are often the most dedicated users of bicycle infrastructure, and improving this space will improve their experience of their city.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Aotearo New Zealand has just promised at the COP27 climate summit to make fast and substantial changes to address the accelerating crisis of climate change. We must accelerate on all fronts the effort of our country to decarbonise.

Feedback**183**

114181070573

NAME: MVH	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

This is a disaster and must be stopped.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is going to seriously adversely affect the ability of residents (particularly tenants) to find parks on the street. This will create havoc and reduce property values. Where is the compensation for property owners?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

There are a lot of people in properties up Raroa Road who rely on the already very limited parking available here. Where are they going to park? Where is the compensation for the drop in property values? People in Karori just won't use this route – they will go through the tunnel and down

Tinakori Road, as that is much faster and closer to the CBD. This cycleway will be underutilised and a complete waste of money, also creating extreme difficulty for residents of Raroa Road.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This route already works well, and the changes are unnecessary and a waste of money.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Don't make them at all – this is completely unnecessary.

Feedback**184**

114160130606

NAME: Amanda Shima	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I'm so amazed by the brave cyclists who slog up Raroa Road. They deserve a safe route.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Overall, I am pleased about the changes. I think the parking changes might not quite fit how people use this area - I reckon 30 or 60 minute parks would work well for the nature of the area (ie picking up a video, dropping stuff to St Vinnies, grabbing a coffee at Aro Bakery)

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

This road is becoming so dangerous - these changes will really help improve the current conflict between cars and bikes. I'm unclear the nature of parking on the downhill parks? I'd suggest mainly residents-only

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

I'm just worried about the bit where this nice cycle path meets Garage Project Brewery. Needs careful management to avoid collisions, make sure everyone knows where to go.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I just witnessed a crash on Raroa Road. Maybe other changes can wait - but you need to act immediately to put yellow lines on the corners of the downhill side of Raroa Road. And from the other perspective - I also think there are a couple clusters of car parking on the uphill side of Raroa Road that are heavily used - residents there may need more time and support to adjust to any change.

Feedback**185**

114178553050

NAME: Thomas	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

That's a really silly question. There are so many different aspects to the proposal. I support some, strongly oppose others. I've also lived here long enough to know that this proposal is what's going to happen, and this "consultation" is a complete sham. You will only listen to the people who agree with you

Do you support the proposed speed changes on Aro Street?**Stongly oppose**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?**

30km/h is way too slow. I can't keep that limit on my bike in a headwind unless riding my brakes. It's not safe for cyclists or drivers. Newtown had it right with 40km/h. It works well in built up areas, and I would be happy with that compromise across the city except main arteries. Hard no to 30km/h

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

I support the uphill bike lane only if it is level with road so cyclists can get around slow riders. A raised path is too dangerous and expensive

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

You're proposing to make it living he'll spread out over 2-3 years, which in reality will be 4-5. Please just get it done and over with. 30km/h is way too slow, for bikes too. 40km/h is a much better compromise

Feedback**186**

114172493957

NAME: Nadine Dodge	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I support them, but they are not ambitious enough. Do more to make the town centre pedestrian friendly, and get on with it all faster.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

-This is a missed opportunity to make improvements to the town centre. Make in-lane bus stops and widen the footpath. This will a) allow for better waiting areas for bus passengers, b) allow for more outdoor dining and improve the attractiveness of the shops and c) just generally improve the attractiveness of the area. -As a resident, I oppose Residents only parking on Holloway Road - coupon would be a much better option and would allow my visitors and other people to visit the area. -If you are widening the path through the park to make it the main route, there needs to be a way for cyclists and pedestrians to cross/turn right at this point. Could you move the pedestrian crossing to this point instead? If not, you need to improve the ability of cyclists to use the existing zebra crossing by the park (take down the gate). - Implement everything faster! Taking your time isn't going to make it any easier/less painful to take out parking.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Neutral

Do you have any comments to make about the proposed design?

-This seems like a lot of pain for minimal gain. I would prefer that you concentrate on getting Aro Street right over doing this section, as Aro Street has much more potential users.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

If you are upgrading the path through Aro Park, you need to make it easier for pedestrians and cyclists to turn right/cross on Aro Street where the path ends - move the zebra crossing to this point. Alternatively, make improvements to the path that leads to the existing Zebra crossing.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The proposed slow approach isn't going to make anything easier. In fact, if anything, it will make things harder. Why? Research shows that adding residents parking increases car ownership. Therefore, your approach will increase car ownership in the short term, making it even harder to make the changes in 2024/2025.

Feedback**187**

114173919023

NAME: Natalie Whiye	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

As per previous comments, it's significant disruption to benefit a very small number of cyclists, whilst disadvantaging the community, local business and homeowners in the area. I'm unsure where the council seeks support for this ahead of proposal, but it's the first I've heard of it and seems like it's relatively far along, and many of us strongly oppose this.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Whilst it's admiral to be looking at improving the routes for cyclists, the fact remains that public transport and the densification of the area does not support alternatives. Parking for residents is already terrible, the routes targeted are terrible terrain for cyclists (majority steeply uphill), and parking is already stretched for residents, with spill over to surrounding streets. Removing more parks will hurt local businesses, and make it even harder for residents to park, let alone visit our wonderful community. Support in the voices of those outside the area should be leveraged carefully, not least as it's many of these who tie up residents parking to make use of the already strong cycling facilities in the area.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

I have no issue with changes to the bud route, but adding an uphill cycle lane is ridiculous, as few cyclists regularly use this uphill, and will only further hurt residents trying to park.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

I'm unsure why the impact of cyclists is included as to the best of my knowledge cyclists are not meant to be on the foot path. It seems we need to make a decision as to where they actually want to be!

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I don't think they should be implemented at all, and the proposed changes will do nothing but cause significant disruption for the benefit of the very few.

Feedback**188**

114181684932

NAME: Natalie Wilson	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

I strongly support changing the speed limit to 30kmp/h and adding in raised pedestrian crossings & speed bumps, people regularly drive too fast down Aro St so I think this would help. I prefer the idea of raised concrete speed bumps to the black (plastic?) types as those can be noisy. Bike parking by the shops would be great, riders stopping at Aro Cafe regularly lock their bikes to the poles outside so I'm sure they would welcome a dedicated parking area. I do think we should be promoting the use of bikes where possible, however I strongly disagree with removing the resident/coupon parking on Aro Street/Raroa Road. Some residents eg tradies have to use their van/car for work so can't just get rid of it. Likewise others with mobility issues who can't get where they need to by bike, walking or bus need their cars. I work in the CBD so walk/bus to work everyday, however I need my car on an evening and weekend. There are no direct buses to the main supermarkets, so the options for residents would be shopping at New World Metro, which is more expensive, or 2 busses, bus and walk, or taxi. I have a reactive dog that I regularly take in the car and to visit friends, she can't go on a bus, and in case of a vet emergency there's no guarantee a share van would be available, or even pet friendly, I know some CityHops are but not all. Taking

the car parks away will mean even the basics such as supermarket/Bunnings trips become far more difficult for some and impossible for others. I'd also just like to add that buses are regularly cancelled and the push to get everyone on bikes doesn't work for Wellington, eg I would love to ride a bike however it's too wet and windy a lot of the time to make people want to do this, not to mention the hills. I'm lucky to live on the flat but pushing people who live uphill to get rid of their cars and cycle everywhere is unrealistic and ableist.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

As previously mentioned I disagree with getting rid of the resident parking

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

Feedback**189**

114161376073

NAME: Bill Nelson	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Looking at the image provided, would love a raised cycle path, or some other kind of kerb/barrier to stop car/road debris on the cycle lane. Too many stones/glass etc and cyclists won't use it and go on the road, or the footpath, which will be annoying for everybody. Otherwise it looks good.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Sounds great as it is already used by cyclists, including myself, so anything to make it safer sounds good. Just wondering if additionally could have clear signage about slowing down (speed limits?) for cyclists in some areas, like bottlenecks and 90 degree blind corners around hall/pre-school and Little Palmer St. Otherwise, I love it.	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**190**

114174352120

NAME: Nicole	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

My partner and I do not own a car for climate change/sustainability reasons. Neither do we think we need one. Aro Valley is so close to anywhere in the CBD, it's easy to walk/cycle. When we want to leave town, we get a car-share car. It would be great if more people lived this way to reduce congestion, pollution and make the city a more pleasant way to live in.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Fully support every single proposed change. Currently vehicles are speeding through the area (despite the 30 km/h zone), and then stuck behind me on my bike. I tend to cycle too close to the door zone of the parked cars to try and give cars room to overtake. Feeling very much pressured on the uphill on Aro St. The raised pedestrian crossing is a good idea - it will slow down the cars and make it more visible - the current crossing is dangerous.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support**

Do you have any comments to make about the proposed design?

Cycling up Raroa Rd is unpleasant but doesn't feel inherently dangerous to me.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Getting around the corners to/from Little Palmer Street is dangerous as there is a lot of foot traffic, no visibility and obstacles. One way of making it better would be to lower the curb on both sides (would require removal of a car park each) so bikes can go straight onto the road instead of going around a corner.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I do think there will be a lot of opposition to the removal of car parks and people will need to get used to the situation and start to re-think how they use transport. This will take time.

Feedback**191**

114171756257

NAME: Nick	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The proposed changes will make this route much safer for people traveling by foot, bike, car, and bus. I travel this route often and visit family in Aro Valley - mostly by car, sometimes by foot, and occasionally by bike. I'll be more likely to go by bike when all the changes are implemented (as it's currently a treacherous route to cycle from Northland/Karori).

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

These changes are positive and long overdue. The pedestrian crossing by Aro park is very unsafe currently, as there aren't good sight lines for vehicle drivers, and people often travel too fast through the area. The raised pedestrian crossing and removal of car parking will make it much safer for people traveling by foot. The proposed changes will also make this part of the city much safer for people traveling by bike. I regularly travel this route on my way to/from Northland (sometimes by bike, mostly by car) and it can be a very scary road to ride currently (and I'm an experienced cyclist) due to the narrow road, risked ob being 'doored', uphill gradient that slows cyclists, and pressure from people in cars to ride fast or get out of the way. My main comment is that three years is too long to wait for parking changes to make cycling safe - it would be much more preferable to make these changes over a shorter timeframe. Also, there's a big lack of secure bike parking in the neighbourhood. I often don't feel safe leaving my bike locked to a

signpost (as it's an area known for lots of thefts), which dissuades me from biking there. There needs to be secure biking up and down Aro St, not just by the shops so that residents, and people visiting residents, can rest assured that their bike will be safe to leave on the street.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

I regularly travel this route by car, and I thoroughly support this to make travel much safer for people to travel by car, bike, and bus. I'll also be a lot more likely to bike this route if it's safer (it can be a terrifying route to cycle at the moment). These changes clearly aren't ideal from a cycling perspective - obviously a proper dedicated bike lane is what's needed longer-term, but I accept that these are transitional measures that will make a positive difference until more major road changes can be considered in the future.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**192**

114160293309

NAME: Nicky	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

The previous question should say "To what extent do you support or oppose the proposed changes..." The way it's written implies a yes or no response is needed. Also it doesn't allow someone to express that they like some parts of the proposal but hate others, so it has forced me to lean towards "strongly oppose". The speed bumps on Aro Street, the raised pedestrian crossings, and extended 30km speed limit are great initiatives. Removing parking spaces for a cycle lane is a slap in the face for residents who are older, disabled, or have young kids to transport around without getting battered by the Wellington wind and rain. Electric vehicles are the future, they're clean and green and practical, where will we park them. Not everyone is young and able bodied!

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is ludicrous! An uphill cycle-lane, are you kidding me? Unless everyone is able bodied, doesn't mind getting wind swept and saturated on their journey, and can afford an electric bike this will only benefit a very small minority of Wellingtonians. What an utter waste of money and huge inconvenience for those of us who actually live in the area.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Don't know

People walking on the paths through Aro Park and Little Palmer Street?

Don't know

People riding bikes along this route?

Don't know

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Again, these questions are biased towards the positive, please be more objective when designing future questionnaires. The images shown are also misleading and not to scale...will the trees and gardens either side of the Palmer St path need to go in order to widen the path? This whole section of the proposal seems unnecessary and a waste of money, cyclists and pedestrians already share this route quite happily - I walk this way frequently, and have done for years, never had a problem. The biggest problem in the park is the unruly drunk/drugged folk that hang out on the tables and benches, sort them out please.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

The previous question assumes I want you to make ALL those changes in the first place, I don't. I like some of the ideas and don't like others. The biased nature of the questions will ensure you collect biased results, and there's something quite corrupt about that form of consultation. I'm only 37% of the way through and am already considering whether I spend anymore of my precious time responding when the questions imply you only want feedback in favour of the proposal and it's all a foregone conclusion.

Feedback**193**

114181591888

NAME: Nicole Duckworth	SUBURB: Owhiro Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?**

The removal of parking will critically affect non-residents ability to visit the area. Many non-residents visit Aro St to socialise, shop & study at the School of Philosophy. Older people can find it difficult to catch busses let alone get on a bike.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Neutral****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
With this improved bike access via Little Palmer & Aro Park, it should be possible to leave the parking spaces in place between Willis St & Aro St shopping area. This would be a nice compromise to balance the needs of carparking & bike lanes.	

What do you think of the timing of the proposed approach?
Don't know
Why do you think that?

Feedback**194**

114176749903

NAME: Nigel Charman	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

While broadly supportive of the changes, I have some concerns re parking. Specifically, I feel strongly that Durham Crescent and lower Mortimer Terrace should be eligible for residents parking. The proposed parking zones on p60 of TR179-22 and lack of mention of Durham Crescent and Mortimer Terrace show that these streets have been overlooked. By a queer twist of fate and street naming, Durham St is eligible right to the top of the street, nearly 600m away from Aro St. However Durham Crescent and lower Mortimer Terrace are less than 200m away from Aro St but are neither eligible for residents parking nor in a coupon parking zone. As per the attached map which overlays the residents eligible parking map on the coupon zone parking map, we can clearly see that Durham Crescent and Lower Mortimer Terrace are the only places with free parking (non coupon and non-residents parking eligible) within 500m of Aro St. It appears that the decision around residents parking is based purely on street name and not on geography. It is already difficult to get a park anywhere on Durham Crescent and will become nigh-on impossible when Aro St residents and visitors are looking for the closest free place to park.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I'm a bit concerned that the planter boxes shown on the Ohiro Road crossing will make it trickier for pedestrians to cross, and potentially force pedestrians to cross the wider bit of the road

(rather than walking up hill to cross). It's a very wide intersection to cross. I'd like to see a permanent narrowing of this crossing.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

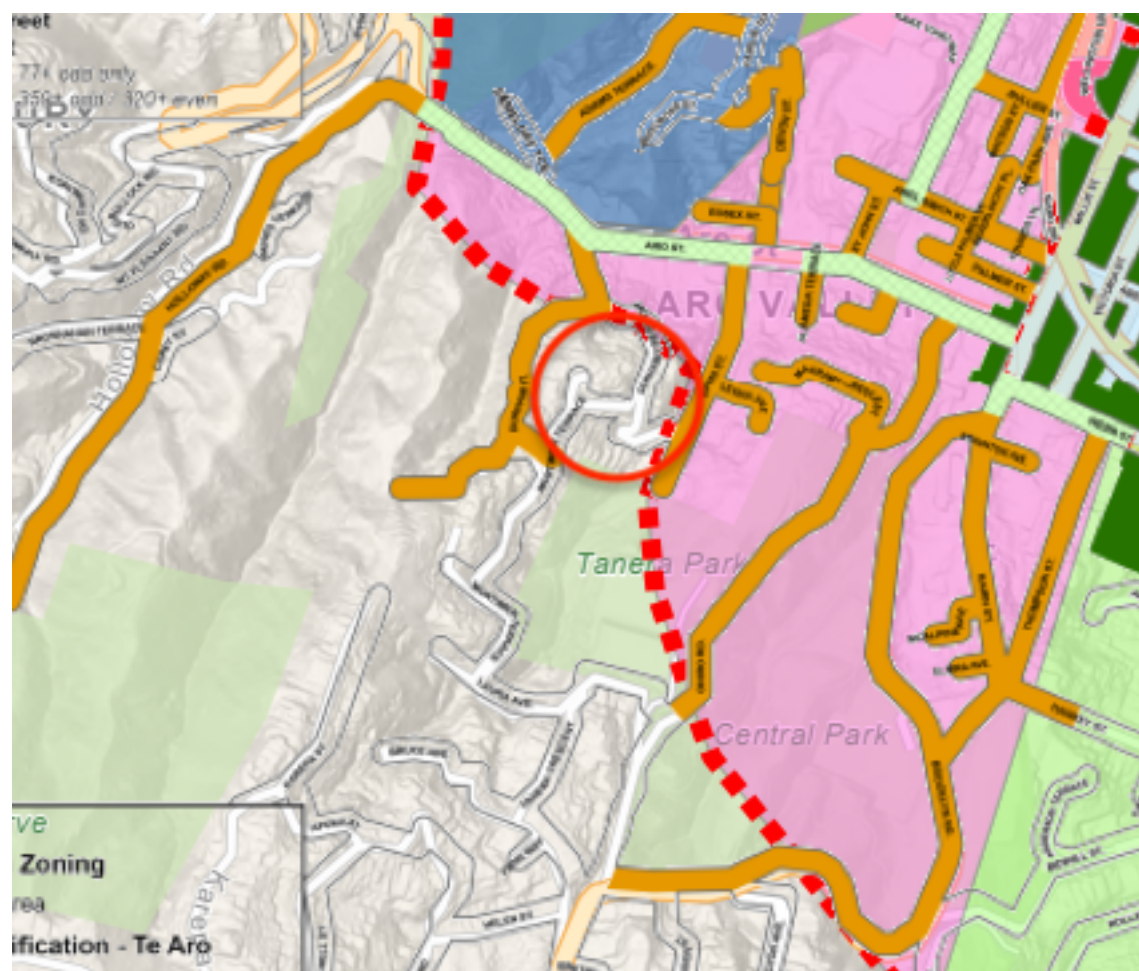
I'm concerned that there will be potential for accidents both around the choke point between Palmer St and the basketball court and especially at the right hand bends at each end of Little Palmer St. The line of sight at these bends is very poor, I've already seen an accident between a scooter and a pedestrian at the junction of Little Palmer and Abel Smith St. I'd like to see measures in place to slow bikes and scooters down at these points (eg stop signs for cyclists at either end of Little Palmer St)?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

coupon_plus_residents_eligible_map.png



Feedback**195**

114161537587

NAME: Nigel Ramsay	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I think having just one uphill bike lane is smart. Bikes and cars travel at a similar speed when riding downhill, so is much safer. Long term though, I'd like my kids to be able to ride that route, and mixing young children with cars, trucks, etc is not smart.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I ride this route each week, but try to avoid Raroa Road because of safety concerns. Instead, I have to ride via Norway St - but this is very steep.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I once fell off my bike because my wheel slipped on the wooden divider between the path and the grass. A wider path should mean that I do not have to ride on the grass to avoid walkers.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Obviously as a rider, I would prefer these changes to be delivered ASAP - because it is currently unsafe. But, I realise that not everyone sees it that way.

Feedback**196**

114160289511

NAME: Nikola Andic	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

The proposals are a great way to encourage mode shift and make cycling safer. Although I think there need to be a few additional changes to parking on side streets to mitigate impacts on affected residents. I live on Aro Street and have a child with a physical disability, so am dependent on having a car. Small changes to parking on Adams Terrace could mitigate the impact on us in a big way. I.e. make all on street parking residents/not coupon parking; limit availability to 1 space per house; and finally allowing Aro Street addresses to obtain some of these residents parks. You could further limit access by only providing residents parking to houses without off street parking. These changes would make much better use of space on Adams Terrace, while still limiting the total number of cars and encouraging mode shift. I think it will also increase support for the changes more broadly, as people will see them as less threatening in the short term. Note this could also be replicated on other side streets in the valley. Alternatively I request disabled parking be added to Adams Terrace.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I live at [redacted] Aro Street and have a child with a physical disability, so I am dependent on having a car. But I think this can be accommodated alongside your proposals. I support the addition of a bike lane on the Southern side of Aro Street, but impacts on Aro Street residents like me could be

mitigated better, by making complimentary changes to parking on Adams Terrace. I.e. make all on street parking residents/not coupon parking; limit availability to 1 space per house; and finally allowing Aro Street addresses to obtain some of these residents parks. You could further limit access by only providing residents parking to houses without off street parking. These changes would make much better use of ample space on Adams Terrace, while still limiting the total number of cars and encouraging mode shift. I think it will also increase support for the changes more broadly, as people will see them as less threatening in the short term. Note this could also be replicated on other side streets in the valley. Alternatively I request disabled parking be added to Adams Terrace.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I support the changes, although they should be accompanied by some provision of parking for those who are dependent on it (i.e. people with disabilities and elderly with no off street parking). I think it can be done relatively easily, please see my comments on Aro Street changes (where I live) for more detail.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I walk this route daily, and this is a great idea! The cyclists are very courteous, but it can be hard sharing the narrow path on Little Palmer (e.g. often have to step into the bushes and mud to get out of someone's way, and you do run into the odd dickhead in a hurry). I think widening this path is a great idea and will make it a lot more usable for all. Ka pai.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think the timing is about right, especially as I would like to see them accompanied by changes to parking on Adams Terrace - which I suspect will take time. For reference, I have suggested

impacts on Aro Street residents like me could be mitigated better (I have a child with a physical disability so need a car), by making complimentary changes to parking on Adams Terrace. I.e. make all on street parking residents/not coupon parking; limit availability to 1 space per house; and finally allowing Aro Street addresses to obtain some of these residents parks. You could further limit access by only providing residents parking to houses without off street parking. These changes would make much better use of ample space on Adams Terrace, while still limiting the total number of cars and encouraging mode shift. I think it will also increase support for the changes more broadly, as people will see them as less threatening in the short term. Note this could also be replicated on other side streets in the valley. Alternatively I request disabled parking be added to Adams Terrace.

Feedback**197**

114169275276

NAME: Nick	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

The concept is sound but as a daily cyclist and driver through Aro St I worry that there it won't achieve the desired effect. Bus drivers are terrible at staying in their lane and when I drive I am often forced to pull over into the uphill car parking space to enable the bus to pass. I am concerned that this means uphill cars/buses/trucks will encroach into the uphill bike lane when a bus is coming downhill.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

In principle this is all good, but there is a major problem when bus stops are planned in uphill bike lanes. The theory would be that bikes then leave the bike path into traffic to pass the bus, or have to stop in an uphill until the bus moves again. Both are problematic.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**198**

114181510247

NAME: joby	SUBURB: Wellington Central	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Green wash. Prevents better cheaper options like bus clearways. Favours a tiny number of able bodied males at vast expense and to the detriment of the rest of the community

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very negative
People walking on the paths through Aro Park and Little Palmer Street?	Very negative
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly oppose	
Do you have any comments to make about the proposed design?	
cyclists are making safe walking spaces unsafe	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
They should not be made. Its green wash and destroys the ability to make better green choices like bus clearways that benefit far more people at lower cost.

Feedback**199**

114175903626

NAME: O Peacock	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Residents parking will be a serious issue if this change is put through. It would require residents parking to be put in place on Durham St, Durham Cres to ensure residents are able to park so they can access their workplaces and family commitments.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	
People walking on the paths through Aro Park and Little Palmer Street?	
People riding bikes along this route?	
People driving vehicles on the surrounding streets?	Negative
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Oppose	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Why do you think that?

Feedback**200**

114180755960

NAME: Olivia	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**201**

114178498715

NAME: Mike Reid	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I would like the 30km speed limit extended from the Aro shopping centre to the end of Raroa Rd as part of the first stage.

Feedback**202**

114171756400

NAME: Pam Ramsay	SUBURB: Northland	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
I would like to make it more quickly, but realise it will be an adjustment for residents who are used to being able to park on the street.

Feedback**203**

114165503223

NAME: Patrick	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

Ideally some sort of safe thoroughfare will be implemented in the shopping area

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
Some sort of markings should be used to clearly show its a shared path	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I would swap the timeline around, do the valley area first which is more narrow and dangerous to ride on as a biker

Feedback**204**

114168628666

NAME: Paul Schrader	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**205**

114181246329

NAME: Penny	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Don't know

Do you have any comments to make about the proposed design?

I fully support the cycle way up Aro St but am concerned about the loss of parking, as a user of the facilities in the area. It would definitely put me off using the shops and cafes there with no parking available - and hence would have a huge impact on those businesses. But also concerned for residents on the street - the carparks are at capacity most of the time and I cannot see where you would expect people to park.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

As a cyclist I feel you have ignored the option of utilising the footpath as a dual use cycle and walking path. It is already quite wide and many cyclists already use it. It would by far be the safest

way up the hill. The road is very narrow in parts and even removing the parking does not make the cyclists safe from the buses particularly on the corners of which there are many. As a resident I oppose the removal of parking as and question your methodology from which you have based your decisions - spaces on Raroa Rd are frequently full to capacity. Your data suggests otherwise however you have taken a survey once at 9am - most drivers have left the area by then - you need to do a much more thorough survey to be able to make any conclusions about the current parking numbers. It appears no one has actually looked at the street apart from on a map or photo - there are many corners that it is not possible to park on which you have included as possible parks - ridiculous - no one can park safely in these areas and they should not be marked as current parking spots. there are many other flaws in your data - you have marked houses on the road as having off street parking when they don't, and have not taken into account that many of the properties are rentals - hence there are many cars that belong to those properties. Surveys need to be done after hours and also during university terms so you can see just how full the street is of cars. What will happen is that by removing parks you are simply transferring the problem to the surrounding streets which are already full to capacity - Harold Street and Zetland Street for example. We frequently have difficulties getting parks as it is on Zetland street and this will just make it worse.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Negative

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I feel that this project is being rushed through without proper consultation with residents and is based on flawed data which does not give an accurate view of the current parking situation.

Feedback**206**

114170611324

NAME: Peter shaw	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I regularly both drive and cycle this route, but see dangerous situations to the far less protected cyclists by aggressive drivers or simply people misjudging distances on the narrow streets. I would bike more if safer.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
While I would love for it to be quicker, there needs to be time for residents to adjust

Feedback**207**

114179345977

NAME: Phaedra Brice-Chen	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Not enough parks for paying residents as it is.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

It's hard enough to find a paid coupon park as a resigned in the area, as it is now. This will cause so much more difficulty. Also people speed up the Aro street! Please focus on the bumps or speed cameras. There's been more than a few accidents or high near misses with bikes and cars. This is going to be another high distraction coming in and out of side streets. Make the street more visible. Not the roads. Bike are fine sharing. It's a small street. Not enough room for a bike lane. And NOW not enough parking for residence as it is.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

It's a very windy road and cars always over take the medium lane as it is.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

Bikes have been speeding though the park. Too many near misses with hitting people and also children. As the preschool and Te Aro school are around.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Feedback**208**

114162737056

NAME: pippi	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**209**

114168581947

NAME: Michael	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

The street I live on is too steep, narrow and windy for bicycles, they are a major hazard for cars, and my elderly mother needs a motor vehicle to have any independence or ability to go and do her weekly shopping. Aro Valley needs better public transport not support for bicycles

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback**210**

114178629278

NAME: Ron Goudswaard	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I.E. FIND/ BUILD AN ALTERNATIVE ROUTE FOR BIKE LANES, WELL AWAY FROM MAIN VEHICLE HIGHWAYS ! The whole cycle lane project seems to be being driven by a certain faction of the cycling community that want to be able to cycle faster and more easily (rather than safer) so that they do not have to give way to everything. I.e. pedestrians have right of way on the footpaths and cars, trucks, buses etc have right of way on the roads and cyclists are expected to give way to all of them. N.B. It is easier for a vehicle to slow down for a cyclist struggling to ride up hill than it is for a person trying to load / unload furniture, rubbish, garden/renovation supplies from their vehicle parked 50+m away, (up/down hill?). Incidentally, in answer to the questions below, I live in the area, I work and manage my own business in the area, and when not working (carrying tools and building supplies, i.e my private vehicle is also my commercial vehicle) I like to walk in the area, N.B. walking is just as fast for me as using a bus.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

1. Oppose. A separate bike lane uphill allows cars to travel faster uphill, but provides no improved safety for bikes traveling downhill and if the bike lane encourages more bike users the risk of serious accident down hill increases. I recently followed a 12-14 year old racing downhill on his bike. What happens when a bus, rubbish skip, recycling truck, trades person's van, taxi, furniture

removal truck, Countdown food truck, ambulance, resident's car and trailer etc blocks the cycle lane? Whenever there is a major building project as in Willis St and Vivian Streets at present, the city council appears to be obliged to provide permits to block off one or more lanes for as long as it is required to do the work. Presumably in the event of a blocked cycle lane the 80% more cyclists will have no option but to join the vehicles in the vehicle lane? 2 & 3. Support. I have no problem with extending the speed limit area - people drive to the conditions. And presumably any speed humps & raised crossing will be bus friendly for the increased bus service that is needed to combat global warming. 4 Oppose. Epuni St. has no spare car parking area, in fact residents have been threatened with parking tickets for parking in front of their own garage driveway, 5. The proposed bike park/disability park says it all, ie the bike park is to be located directly outside the shops and the disability park is to be located on the far side of Epuni St. so that a disabled person has first to cross a road to reach any of the shops! Why do the shared parks need to be near the shops? Anybody needing to drive somewhere will not be going to the shop just around the corner. The shared parks are better set near the 80 parks that the cycling fraternity want removed, e.g. in the parking space near the Garage Project. 6.Oppose I feel it is very short sighted of any local body to start reducing vehicle access for the largest and fastest growing demographic of the New Zealand population. Not all pensioners can walk long distances easily and fewer still will use skateboards, bikes or scooters and they will still need easy vehicle access to their homes, shops, medical appointments etc. Are such pensioners meant to park their car up some side street that already struggles to provide enough parking for the residents of that street? Or do they have two years to sell their house and move elsewhere? N.B. Are the Uber drivers, ambulances and taxis meant to stop in the bike lane or the vehicle lane when picking up or dropping off passengers? At the moment they are able to use the nearest available car park. 7. I am all for specialised bike paths for bikes (even shared pedestrian/bike paths), but ideally on shared paths all bikes are fitted with bells and all cyclists are encouraged to use them to alert pedestrians of their approach (works well in Amsterdam) and bicycles should not assume automatic right of way, e.g. a parent pushing a pram and with toddlers should not be expected to suddenly move out of the way of a speeding cyclist. In Amsterdam where cars trams and pedestrians all mix together they all go slow. Here cars, cyclists, scooters, skateboarders all seem to travel as fast as they can get away with.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

1. Oppose. A separate bike lane uphill allows cars to travel faster uphill, but provides no improved safety for bikes traveling downhill and if the bike lane encourages more bike users the risk of serious accident down hill increases. I recently followed a 12-14 year old racing downhill on his bike. What happens when a bus, rubbish skip, recycling truck, trades person's van, taxi, Countdown food delivery truck, furniture removal truck etc blocks the cycle lane? 2 & 4. Support. I would assume the suggested bus stop locations are based on bus driver and passenger recommendation; if not there is probably something wrong with it. 3. Oppose. I doubt many pensioners choose to live on Raroa Rd. because very few properties appear to have easy access to vehicles (unlike Aro St.) so that in theory young people that do live there can carry their groceries etc from a car parked some distance away, but I do wonder how many of the people proposing these changes would want to carry their own weekly groceries, gardening /renovation supplies from some distance away, up hill to their home? Or are the residents meant to have all their supplies delivered, and where is the delivery person/food truck (or car and trailer being loaded with rubbish/gardening waste to go to the local tip) meant to park? We have 80 steps uphill from our letterbox to the front door and 4 out of 5 courier deliveries result in a card in our letterbox

stating no one was home (when we most definitely have been home) and we have to go to the depot to pick up the package of printer ink, small book or whatever (i.e lightweight) ourselves (which means 2 trips adding to global warming/traffic congestion instead of one).

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

1 & 2. I doubt it is the pedestrians demanding that bikes do not use the footpaths, just that cyclist go slower, treat pedestrians with respect and give way to pedestrians. A parent pushing a pram with toddlers in tow should not have to suddenly make room for a bicycle. Some cyclists do not want to go slow (walking speed) and do not want to give way to pedestrians; i.e. it appears to be a certain section of the cycling community demanding all the cycle lanes so that they can go faster without having to give way to everything. The fact that the same people are happy to speed downhill, not wear bike helmets, jump lights at intersections etc suggests that increased safety is a lesser consideration for them. The last two questions depends on what changes are made to the surrounding streets to get cyclists off the vehicle roads. Adding cyclists to vehicle roads slows traffic and increases risk to cyclists. Making new specialist cycling tracks away from main vehicle roads reduces risk and increases safety. Instead of the busy Aro St./ Raroa Rd. vehicle highway to/from Wilton, Northland and Karori why not make use of the much quieter route up Salamanca Rd on the edge of the Botanic Gardens to the university, along the back of the university to Grove Rd, Kellburn Rd., St Michaels Cres. and along the back of the Kelburn shops?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Two years is not very long for an elderly, or handicapped person to sell their house and buy a new home at a decent market rate because they no longer have access to a car in front of their home, i.e. the plan is to remove 80 car parks and replace them with ... nothing? Not even a series of handy pedestrian crossings to safely reach the parking spaces on the other side of the road. (Incidentally I wonder what one of the cyclists demanding exclusive cycle lanes would think of having to stop on Raroa Rd. for pedestrians on a pedestrian crossing?) I.E. FIND/ BUILD AN ALTERNATIVE ROUTE FOR BIKE LANES, WELL AWAY FROM MAIN VEHICLE HIGHWAYS !

Feedback**211**

114172955711

NAME: Pip	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

People in Aro Valley tend to walk as we're close enough to town. Removing parking would hinder people from further afield coming to visit the shops

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Oppose****Do you have any comments to make about the proposed design?**

Aro valley people are walkers as were close to town. Cars are for specific purposes like getting to work. The money would be far better spent on water assets in the area

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Negative
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
This bit makes sense and should be the first priority	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
Ditch the bike lanes removing carpark and prioritise the safety things like slowing down traffic and raised pedestrian crossings. Spend the rest of the money on water assets in the area (that are just going to continually dig up your new seal anyway!)

Feedback**212**

114161632212

NAME: Peter Steven	SUBURB: Karori	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

At the moment, biking up Aro Street and Raroa Road is stressful because the amount of parked cars force you to hold up traffic, causing some drivers to do dangerous overtakes. The current situation is bad for both drivers and cyclists. While I think it's a shame that the bike lane won't run through the shopping area, I think that overall this plan is a practical compromise.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

How are the speed bumps going to affect the buses? Can they be designed in a way that lets the bus drive straight over them? Also, what about moving the crossing at Aro Park to the Western end of the park, and adding a green stripe to it? This would make it easier for cyclists to get across the road to the cycleway and for pedestrians to easily get to the shops. I think it's a shame that the bike lane will be compromised by car parking in the shopping area.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Honestly this design seems less than ideal, but I guess we have to make do with what we've got. It's better than nothing.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

This is long overdue, these paths are tiny!

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I would love for it to happen quicker but I think this is a cunning approach that will ease the transition. There are obviously some smart people working on this project.

Feedback**213**

114165541662

NAME: Alan Platt	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

The whole point of Aro Valley's character is the people, not cars. These proposed changes really reflect that.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

As a daily commuter on this route I strongly support the proposal. It is one part of my ride where I am very aware of the danger of vehicles passing too close, there are some parts where a cyclist simply cannot make themselves safe. This would become a key route into the Western suburbs from the CBD and I believe encourage cyclists to use this lane rather than hold up cars elsewhere.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Change is hard, but the reality will prove itself a positive change. Doing it sooner gives people time to get over it rather than dwelling on the negative anticipation.

Feedback**214**

114160163180

NAME: Patrick McKenna	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

my only hesitation is that the separated (ie enclosed) cycle lane appears to be very narrow so may cause problems when blocked by rubble (it must be swept regularly) and faster cyclists need to overtake

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

it is vital that cycle lanes are of adequate width and are swept regularly to ensure they remain passable. enforcement of no-parking will be required at least initially

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

safety can't wait

Feedback**215**

114160173875

NAME: Liam Prince	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It provides for much safer cycling uphill with a separated lane, and slightly safer downhill riding by creating more space for vehicles.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
I would like to see the uphill cycle lane on Aro Street completed in 2023, or at least the majority of it. I understand the need to reduce impact on residents due to removal of parking spaces, but I am concerned only partially completing the bicycle lane could be hazardous. As a compromise, the section between Durham Street and Holloway Road could be completed in 2023, and the section from Durham Street to the shops completed in a later stage. However, this latter section is the most disruptive to traffic and ideally would be completed sooner.

Feedback**216**

114160192988

NAME: Patrick Lam	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

This is a step in the right direction, but doesn't go far enough or fast enough. Generally we need more segregated infrastructure. Sharrows basically don't count.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Segregated bike lane, great. Sharrows don't really work. Shopping area has more cars that are parking and entering the roadway, though at least usually they're doing it slowly. Support better bike parking. The Aro Valley shopping village has a lot of potential for being a place for people rather than cars. There is a reasonable number of cafes and shops there and even some grocery options, and people live nearby. Better infrastructure will help it live up to its potential.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Ambivalent about bike shoulders. I don't think nervous riders will feel safe with them. I understand that the geography makes things tricky with respect to the total amount of road space. Definitely strongly support removing on-street parking.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The climate emergency is now.

Feedback**217**

114164198935

NAME: Peter Ramage	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

The cycleway as designed is inconsistent with WCC's 2020 parking policy, that says the safe movement of people and goods is more important than car parking, and the Paneke Poneke network principles, including that "The network needs to be safe and to feel safe for everyone who wants to get around on a bike or scooter"

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

sharrows suck: they actively make roads less safe for cyclists. I can't believe you still think they're an appropriate intervention, let alone as a substitute for proper protected cycling infrastructure WCC's parking policy identifies that the highest priority of road use in the city fringe and inner suburbs is "Safe and efficient movement of people and goods" not providing a protected cycle lane to retain parking is inconsistent with this policy. for suburban centers, such as Aro village, "Safe and efficient movement of people and goods" is also the highest priority. Moreover, the policy identifies all parking that is not "short stay" as lower priority. This means the Council should continue any

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

sharrows suck: they actively make roads less safe for cyclists. please stop installing them. This route falls well short of the Paneke Poneke design principle that cycleways "be safe and to feel safe for everyone who wants to get around on a bike or scooter, for short or long trips."

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Feedback**218**

114169091561

NAME: Rachel Bisset	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I think this is going to negatively affect the people of Aro Valley who cannot simply swap their vehicle for a bike. Parents, those with disability equipment, tradespeople who require work vehicles. For the sake of a bike lane that goes up a road too steep for most to use.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is a street where most people have no off street car parking and you're removing all the parks on one side when half the street has yellow lines in the other side, not everyone can ditch their car for a bike, eg tradespeople, the disabled, people with children. What do they do? What do visitors do? What do we do if we need a builder or a plumber but there is nowhere within 150m that they can park?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This is a very steep hill, very unlike the Newtown or Thorndon cycle lanes, only those with a very high physical fitness will be able to cycle them. It's not going to work for the general public.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Don't know

People driving vehicles on the surrounding streets?

Very negative

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

The footpaths are already wide enough to get to where we need to go. I walk everywhere because I am very "green" generally. I can easily give up my car but my husband is a plumber and tradespeople will always need a vehicle with equipment and tools and we have no options. We have no idea what we will do. It took all we had to buy this property - house prices are too high for this generation so luxuries like offstreet garages are not available to all.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

You're asking people to make huge sacrifices. People with children, disabilities, trade peoples etc - you're leaving them with no choice but to leave Aro Valley and sell their homes. Do you feel that less than a year to do this is adequate? You are giving up your inclusive tick in order to gain the "sustainable" tick. There are ways to do both but you're doing it too quickly with no regard for the people who live here.

Feedback**219**

114180567453

NAME: Rachel Griffiths	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

The Aro St section of this scheme provides an opportunity for all commuters to change the idea of road use and "ownership" - encouraging shared space with the reduction of speed to 30km. Installing physical barriers may decrease the traffic flow when buses and trucks are trying to pass. With the removal of parking on one side of the street, the other side will most likely be completely full. This may result in opportunities to pull over being lost, creating bottlenecks. Removing extensive parking used by residents on Aro St will create more stress on the neighbouring streets, some of which already suffer from unmanaged commuter parking.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

re: car sharing. Why is the limited to CityHop and not available for Mevo cars. re: separated bike lane. The physical separation for the bike lane is going to make the street less safe for traffic. Currently, vehicles pull into the gaps in parked cars (driveways) to allow other buses or trucks to get past. This is a daily occurrence and not just during peak hour traffic - this will impact on traffic flow and make any cyclists on the downhill section less safe. The illustration you have used is misleading as the number of cars that are parked by residents exceeds 30 cars most days. There is limited parking on surrounding streets already so suggesting that people will be able to park there is naïve. To help the current surrounding street neighbourhoods the 24hr parking solution from

Ngaio should be investigated. This will impact on Holloway Rd, Entrance and Norway Sts not just Ohiro Rd. The other issue created is making any trade parking even more difficult than it already is, creating an additional barrier for a trade person to bring materials to site or even being able to come to visit to carry out minor repairs. Raising the pedestrian crossing may encourage a reduction of speed but relocating the crossing may make better sense - to the Garage Project entrance to the park. If removing the bus stop by the current crossing location is actioned then this crossing will be used less as people may be more likely to get off the bus at the shops instead of Ohiro Rd. Extending the 30km speed limit from Willis St to Holloway Rd would allow traffic to move at a similar speed to most cyclists and scooters, encouraging a shared road approach instead of a us vs them approach. This is currently the case in the evening commute for cyclists so formalising the shared road may help with driver mentality. Speed bumps will increase the traffic noise for those on the Aro St section so should be installed close enough to discourage acceleration between. To reiterate - my biggest concerns are around the physical barrier for the up hill cycle way creating more hazards and hindering traffic flow and removal of so many car parks without any actual place for the current residents to relocate their cars to - even with a two year staged approach.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

re: cycle lane: Painting a cycle lane on the uphill section is not going to make this road any safer. It is currently a narrow road with buses and trucks trying to pass each other and many cars parked to near corners or on both sides of the street. re: yellow lines I support putting in yellow lines on corners to remind people of sensible parking but would encourage this to be extended further where there is a blind corner coming up ie near 125 Raroa Rd bustop. re: new Bus stop. This should have been put in to start with as there is no where for people to disembark the bus. re: removing all uphill parking This makes sense on paper but where are the current cars going to be relocated to. If they move to the other side of the street around 125 Raroa Rd, will this cause a potential issue with people crossing the road with limited visibility. I believe this requires more real time investigation and review of where and when the cars are parked.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support**Do you have any comments to make about the proposed design?**

This work should have been completed previously when raised. With the relocation of the Community Hall removing the pinch point and blind spot, this route will be more favourable for shared cycling and walking.

What do you think of the timing of the proposed approach?**The timing of the stages is about right****Why do you think that?**

in addition to earlier comments. Agree with: marking parking spots at the shops I am concerned: by the additional cost for residents who will need to apply for resident parking permits.

Feedback**220**

114181286515

NAME: R Hurst	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Neutral**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly oppose****Do you have any comments to make about the proposed design?**

I own a three flat property up the Harrold St 'path' (actually an overly steep and poorly maintained paper road). My concerns are as follows: Debris is constantly falling from the area onto the road where you are Proposing a bike lane and bus stop . The residents up this path already have a very steep ascent from the path which can be quite treacherous in wet weather due to the gradient and leaf debris. To step out, with no proper formed or cleared pathway into this proposed bike path and a bus stop at the bottom would be incredibly dangerous. (Harrold St

zigzag is not as pictured here in your drawing). The parked cars there currently are the only protection a pedestrian has coming off of the steep zig zag pathway. I would suggest one of your engineers or designers take a walk up this pathway which has several residences on it. There is no landing there for these residents to safely stop and cross the road let alone leave rubbish bags and recycling. Rock falls are very common all along the side of this hill, including along Harrold St zigzag and this will not stop until the whole bank right up Raroa Rd is retained in some way. To put a cycle lane along here, an already tight road where buses can barely fit or cars pass is an incredibly hazardous decision. Bikes will be swerving to avoid clay rock falls all the way, the parked cars currently hide this. Please look at a better pathway to make Harrold St safer, better gradient, better lighting and safer landing rather than passing more bikes up a narrow, windy road plagued by rock falls. In your proposal if there are only parks left on the down side of Raroa road this will create an additional hazard where cars will be performing u turns to park on the downside as the predominant route from town for residents is up Raroa Rd. There is no visibility on Raroa Rd for this to be safe. Being a steep and windy road, removed from the main centre of the Aro Shops there are many young professional couples flatting up here who do own a car and they require parking as close to their houses so are parked on Raroa Rd. Where will these residents move cars too? It will create problems in other areas in the Aro Valley/Highbury area which is already over parked. It is important to have access to supermarkets and mobility when not in close proximity to resources. Despite the wish for a carless city, this area is not easily accessible due to the hills and most will have a vehicle. A bus will not get you out of town in the weekends. Harrold St itself is a small narrow street which is heavily congested with parked cars already, this is not an option for any house on the Harrold St zig zag. Access to most of these is best off Raroa Rd. As a landlord we will have trouble letting this 3 flat property in the future if the proposed changes go ahead. Very few houses along this upper side of Raroa Rd have drive on or garages. This will be of great concern for the value of their properties if there is no parking in the vicinity. Cars are still an essential part of Wellingtonians mobility. A few charging parks would be of more benefit to this community on a steep and windy road than removing essential parking spaces.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback**221**

114181277706

NAME: Ralph McCubbin Howell	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I have lived in various places within or close to Aro Valley for over a decade. I have a close connection to the area as a place to live and work, as well as a thoroughfare to other suburbs. I am a pedestrian, cyclist, and car driver, and I strongly support these overall proposed changes. There is much I love about living where I do, but I've long found it bewildering that I've felt safer cycling through the middle of London and New York than I do in my own neighbourhood of Wellington - this is something I'm even more aware of now I'm often moving about the city with my toddler in tow. Well designed, fully separated cycle lanes make things so much better for car drivers as well as for cyclists (in the inner city I'm more far more likely to use and spend money at businesses if I can get to them safely by bike). Having owned a car when I lived just off Aro Street for a number of years, I am aware that parking space is limited, however I believe Aro Street itself is too narrow to safely accommodate on street parking. Amidst the climate crisis we need to rebalance street space to encourage travel by public transport, walking, and cycling. I strongly support this plan as it will make our streets safer, more attractive, and liveable for users of all modes of transport. As someone who drives in the area, I support the changes to parking, which are aligned with WCC's parking policy to prioritise main streets for moving people - not parking - and note the Council will manage parking on side streets to mitigate the impacts. For the route to be successful, it is essential that the changes are clear to everyone. Painting the whole bike lane green will help make clear what's what, and reduce illegal parking. It's confusing and frustrating when drivers see people not using what appears to be a completed bike lane. It's important that these bike lanes be available and enforced as soon as possible once street space is reallocated. These proposed changes are promising. I look forward to seeing their benefits for a neighbourhood dear to my heart being realised in the near future.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:****People using Aro Park for recreation (e.g. picnicking)?**

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?**Strongly support****Do you have any comments to make about the proposed design?**

I'm particularly pleased to see the proposed changes to Little Palmer Street, which has significant cycling and pedestrian traffic, including parents and kids travelling to and from Aro Preschool and primary school. The current arrangement means you're running a gauntlet whether walking, cycling, or pushing a push-chair. Widening and redesignating this path will make official a well established cycling route for those travelling between Aro Valley and the Terrace (<https://cycwell.wordpress.com/2017/10/19/laneways-wellingtons-secret-bike-network/>). This is a logical route as the one way road system means that without this, to get from Abel Smith to Palmer Street cyclists need to cross the bypass, merge with traffic on Victoria Street, follow this around Webb Street, and then cross three lanes of traffic to get into Palmer). Removing parking spaces and adding curb ramps at either end of Little Palmer Street will greatly improve sightlines for cyclists and pedestrians on the path as they head out to cross the road (many continue across to Inverlochy Place), and allow those entering the path to make way for those on it, and widening this thoroughfare will make it easier to pass in both directions. I would love to see work continued along the informal route north to make it safer for both cyclists and pedestrians, e.g. addressing the blind corner at the end of Oak Park Avenue, and adding curb ramp on the north

side of Ghuznee opposite Buller Streer to allow a largely off road connection all the way to MacDonald Crescent.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I'd support doing the work in one stage, rather than spread out over three stages to deliver the benefits sooner, especially given the transitional approach.

Feedback**222**

114161721539

NAME: John Wilson	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

Do you have any comments to make about the proposed design?

As a person who services the properties I the area what thought has been taken to trades working and finding parks as parks are difficult to come by now.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Again related to trade parking

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Negative
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**223**

114181502410

NAME: Rebecca Day	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

As we regularly cycle with our young son we support the changes which provide a safer route for those that wish to engage in active travel. For me, improved safety for all road users should be the top priority.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?**

We cycle this route regularly with our child on the back. I would strongly prefer a separated cycle lane although I acknowledge this is a challenging road to do this on. We turn off raroa road on mt pleasant road. The footpath between Holloway and Mt pleasant in the uphill direction is rarely used as there are alternative steps for most pedestrians. I strongly believe that the best solution

up to the Mt pleasant road turn off would be to put in a shared uphill cycleway and footpath where the current footpath is. I also occasionally use that path with my young son in a stroller and really wouldn't mind sharing it with uphill cyclists. I rarely see any other pedestrians using it as everyone uses the steps. A painted 'cycle shoulder' on the road really does hardly anything to help cyclists. Many use the pavement currently and the painted cycleway on the road on this section would 'force' then onto a more dangerous route on the road and frustrate more drivers. It would be a waste of the underutilized footpath space.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I use this route and fully support improvements

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

We need safe cycle routes for my family as soon as possible, however I'm willing to accept slower implementation of that means the changes are better accepted by all residents.

Feedback**224**

114161567693

NAME: Jo	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Could go further by having two way protected bike lanes but this is an awesome start!

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Great start, further reduction of parking on downhill section would be awesome

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	
Needs communication around culture change - speed of bikes, shared path but pedestrian right of way etc. is there a possibility to use different substrates eg brick/tile to define the different transport modes?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
Gets people out on alternative modes of transport and gives people time to figure out if they still need to find alternative car parking. Allows time for new rental tenants to know that change is coming and limited parking is available

Feedback**225**

114181681495

NAME: Rex Collett	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

This is one section that I think works as one who has to drive through here at peak times almost every day--but only in so far as it gives free flow and safe cycling for this small section. The problem with all of these is that Wellingtons topography and "infrastructure history" leads to multiple pinch points which in their own way create an additional hazard and this will be much worse even if you get 10% increase in cyclists.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?**

No provision for motorcycle parking and this is increasingly the preferred and efficient mode of transport especially for younger people. Small business struggles already. Where is provision for short stops for pickups from Arobake, Arovideo etc. As a nearly 80 yr old there is no way I can expect to ride a bike up Ohiro Rd then Todman st after picking up a few things from my favourite small businesses!! And given the decline in public transport I don't expect me to want to spend half a day catching two busses to do the same!

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose**Do you have any comments to make about the proposed design?**

Residential density is increasing up Raroa rd, especially around 150 Raroa. They need somewhere to park as do visitors. There is no getting away from car ownership in NZ even if we get people using bikes, walking, PT for short commutes.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?**What do you think of the timing of the proposed approach?****Make the proposed changes more slowly****Why do you think that?**

I don't think the whole thing has been thought through from all perspectives. Its an ageist approach and privileges the fit, mobile and the well off with the focus on cycling, those able to take advantage of ride share/Mevo etc.

Feedback**226**

114181461621

NAME: Rhys Hayward	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

A city connection from Victoria St to Aro valley needs to be included in this package. It is extremely dangerous for cyclists travelling South-bound along Victoria st who must cross 3-4 lanes of traffic to get into correct lane for turning into Aro valley rd.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

1. The planter box by 72 Aro st is dangerous for downhill bikers. During peak hour traffic bikes can pass cars along the lower section of Aro st however this planter box compresses bikes and cars into a dangerous squeeze. 2. Should convert footpath below Aro park to dual-use cycling/walking path. Passengers of cars often get out as cars wait in traffic along lower Aro st and the opening of car doors is a major hazard (it has happened to me three times).

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

1. There are landslips and debris from the hill that often cover the road and force cyclists to move across into traffic. These slopes (near 94 Raroa Rd) need to be addressed. 2. The intersection of Raroa Cres off Chaytor St heading into city is way too tight for cyclists - needs widening, and something to slow approaching cars.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

The route is not particularly obvious so would need very good signage. 2.5m is too narrow for bi-directional cycling and walking.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Because this is a core cycling route to the Western suburbs and proper infrastructure will encourage mode-shift. The existing roading configuration is dangerous for cyclists.

Feedback**227**

114167639961

NAME: Rich H	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

This route is well used by people on bikes and other non-car transport options so sharing out the road space better makes a lot of sense. It will also make it easier for people in cars to pass people on bikes so everyone traveling there wins. It will be important for residents to still have access to car parking spaces along the route so a balance needs to be found.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Improving part of the route will make it better people who currently ride but still leaves challenging sections for those people who aren't currently confident enough to ride.

Feedback**228**

114161510929

NAME: Richard Dobson	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

I agree to any project that gets rid of on-street parking for cars on streets used for bus routes. The vast majority of Wellington streets are TOO NARROW for cars parked on both sides, to then run a bus route through them and also expect cyclists to be safe when riding these streets. These narrow streets with parking on both sides (e.g. Raroa Road) force law-abiding drivers to cross the centreline in many places, whether passing cyclists or JUST trying to navigate stretches or corners. Get rid of on-street car parking on both sides of the road!

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Neutral****Do you have any comments to make about the proposed design?**

"Some car parks removed on Aro Street and Raroa Road at very narrow places where it's currently hard for buses to get through"- GET RID of ALL onstreet parking along this section of Raroa Road. IT IS UNSAFE! There are 4 blind corners in this section that force the driver to cross the centreline into oncoming traffic- DANGEROUS!

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

Negative

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

(via Aro Park and Little Palmer Street) This is a pointless project- other than 'greenwashing' this section of Te Aro. The users are minimal and the current infrastructure is adequate for these users

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Get rid of ALL on-street parking as quickly as possible

Feedback**229**

114161108278

NAME: Rory	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Because it will improve my journey to and from Karori whether i cycle or ride my bike

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Fully separated bike lane would be better, but this is an excellent first step

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Removing the on-street parking on this road is absolutely essential to the improvement of bus services to and from Karori so I think Council could have done a better job explaining that the parking removal is not just being proposed for the benefit of cyclists but also for the thousands of bus users in Karori

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

We need a joined up network in order to encourage mode shift. If cyclists can feel confident in making a safe journey from door to door then they are more likely to get out of their car and onto a bike

Feedback**230**

114174961701

NAME: Rosalie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I do not support the removal of the car parks on the uphill side of Aro Street and Raroa Road. This proposal, will make it easier for those that live in the outer suburbs to travel through Aro Valley without being hindered by cyclists, or for cyclists in good weather to use it to travel to the city. But it comes at a great cost to the community of Aro Valley. The side streets do not have enough parking. Without car parks in Aro street where are all the service vehicle supposed to stop. Or maybe the paramedics will stop the ambulance in Epuni street and run a few hundred or so yards with a stretcher. If you need a tradesmen to work on a property do they park their trucks hundreds of yards away and carry the tools wood etc. But then if there is restricted parking in the side streets they could not even park there. It does not serve the elderly, the disabled or those will children well either. The community has not been considered at all with this idealistic plan. There are other options. With a 30 kilometre speed limit and humps, cars, bikes and scooters can share the road. This would be an inconvenience to those using it only as a thoroughfare to the city but it must be workable for all. Pedestrians have to deal with these 2 wheeled vehicles on the footpaths and going through Aro Park which can be very dangerous for the elderly, children and the disabled. Another option would be to take the bike lane up past Victoria university - Kelburn Parade into Upland road. Far better gradient for cycling and far less car parks would be lost. A third option would be to invest in the public transport system. It is woefully inadequate and getting worse. If it was vastly improved those that travel from the outer suburbs would be more likely to use it. This would cut down far more emissions than removing car parks in a Aro Street and Raroa Road. If the rates money is going to be spent on changes like this proposal then I think a lot more consultation is needed - to get it right for everybody with respect to the environment.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

The only part I agree with is dropping the speed limit to 30 ks for the whole Willis to Holloway Road and putting in speed humps to ensure the speed limit works. I also agree with raising the pedestrian crossing by Aro Park. Motorists and cyclists have expressed concern about its lack of visibility for years. I oppose taking away any car parks as this will impact far too much on the community. Aro street is not only an arterial route for cars buses and bikes. It also serves the local community. Putting a new disability park in Epuni Street will not serve a person that lives in upper Aro Street at all. They need to be able to park as close as they can to their home. I would also like to think that tradesmen will be able to park near to where they are working. Without car parks this will make it impossible for them to service the houses in the community. Our transport system is woefully inadequate. This needs vast improvements before people can be expected to use it as an alternative to cars. I think Aro Street should be a shared space for cars bikes scooters etc. With the speed dropped and humps on the road it should be able to be achieved. Otherwise has any thought been put into a bike lane going from Willis street, up the Terrace and past the university through Kelburn. The gradient is much more conducive to bike riding and there would be far less car parks that would need to be removed.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Raroa Road is a steep narrow winding road. The average cyclist will not use that route as an alternative to car or bus. Taking away the car parks for a few elite cyclists is not serving the community. Once again it could be a shared space.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Negative

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

As scary as cyclists say it is dealing with cars on the road - for pedestrians having to deal with scooters cycles and skateboards it is even scarier. I don't agree with widening to path so it's a free for all. I think there should be a separate path for cyclists scooters and skateboards so pedestrians can relax whilst walking through the park.

What do you think of the timing of the proposed approach?**Why do you think that?**

I don't agree with the changes apart from speed limits, speed humps etc.

Feedback**231**

114181162070

NAME: Michelle Rush	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Aro Valley is one of Wellington's 'special places' and being able to safely access it by bike or as a pedestrian will enhance its attractiveness as a place to live in, work in, and visit. It's also a good alternative route to Karori as it's picturesque and less busy than the main Karori road: with the significant traffic issues facing that suburb, having a safe cycling route to and from town via Aro Valley has many benefits.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Making this route safer for cyclists and pedestrians will enhance this unique part of Wellington. I would like to see care taken with the design of speed humps however: some are dangerous for cyclists, esp road cyclists with narrow tyre cycles - the humps should be designed so that cyclists can avoid them, or in a way that they don't create a hazard for cyclists.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

This design will make me feel more confident about cycling this route: currently I am worried when riding it because of the tightness of the route; the blind corners and the drivers that cut into the corners. Having a marked cycle route up hill, and removing parking, will help improve its safety.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

As this is a busy and tight area, I recommend you consider different pavement applications where the path can be significantly widened to separate cyclists from walkers; another option, e.g. where it is too narrow to do this, is to use signage signalling that pedestrians take priority and that cyclists need to go slow - and if there are options to change the path routes, consider keeping them well away from 'ball kicking' zones and where kids are wanting to run freely

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

It is important to give time for residents to adjust - e.g. getting Mevo or CityHop a happening thing to replace a group of residents' cars, or getting through consenting/building to put in an off-street park. It also allows time for the WCC parking policy 'parking plan' option to be carried out - which may well be necessary to help implement what is required here.

Feedback**232**

114174257610

NAME: Sean Kells	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

The current flow of traffic through Aro Valley is quite concerned with getting home at a decent pace. Works like this that will help provide a separation of concerns between riders and drivers will go a long way in fostering a safe riding environment. New riders already have a lot to contend with when it comes to AV, weather and gradient being two strong ones. Anything to help reduce other traffic from their list of concerns seems like a good investment to me.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?**

For the longest time travelling on Raroa road has felt terrifyingly crowded due to the over abundance of parked cars, chances are even with the uphill parking being removed, it will still feel cramped, but this is a good step in the right direction

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Chances are it will take around this long for works to be done on each stage, and it will give folks a chance to adapt and make plans for the future of Aro valley

Feedback**233**

114168802587

NAME: Samuel irwin	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Diversifying traffic to encourage more cycling and walking is essential for city health, action of climate change, and a reduction of overall traffic/congestion

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Hopefully the bike lanes can be protected safely against cars

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	
We could always be doing more disabled people's accessibility.	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**234**

114165156596

NAME: Sarah Bennett	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I've lived in the valley (or rather Highbury) for more than twenty years, and ridden a bicycle in Wellington since 1988. Progress on making our city bike-friendly have been slow, albeit they have picked some pace in the last few years. We need to go further, faster, with more courage. I am a veteran New Zealand travel writer and sustainable tourism advocate with a keen interest in cycle tourism. I'm also a board director of New Zealand Cycle Trail Inc. This has given me considerable insight into what makes for good cycling, both for communities and visitors. Towns and cities all around the motu are trying, to various degrees, to become more cycle-friendly, in many ways and with varying levels of success. For Wellington, this will always be hard due to our topography. But I'd say that our willingness quotient is high. We should throw everything we can at it, listen to the detractors but push on regardless. This is about kids biking to school, and people like me doing pretty much every they need to do around town by bike or foot. I do that now, anyway, but I often feel vulnerable. Lots of close shaves. As it is, I ride a MTB with knobbly tyres and do a huge amount of mounting the kerb and riding on the footpath to avoid danger. How people negotiate Wellington on a commuter bike I don't know!

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 km/h limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. The Aro St shopping area is well over-due for improvements that make it easy and appealing for cyclists to visit and park.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

I support installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent. I support removing all on-street parking space on the uphill side.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I ride this exact route several times a week. Pedestrians are incredibly kind and patient, but I don't enjoy weaving around them. This looks an easy fix and a total no-brainer! Of course, we'll need a better crossing on to Aro Street from near Garage Project?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

If it were possible, I think we should push ahead. I feel like I'm risking my life every day I cycle Aro Street, particularly up-hill. We should also seize this opportunity when so many more people are getting bikes (esp. ebikes) to encourage a major mode shift.

Feedback**235**

114174215783

NAME: Sarah	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

No Consultation with residents. Very difficult for Holloway road residents and parking. Bike parks should be on Devon St and Epuni. No need for them to be right outside shops. Cyclists are fit and able after all. Smaller buses on route. No to painted cycle lane on Raroa road, yes to parking on downhillside. no to separate cycle lane on Aro St. Bikes can go 30kms so will meet the speed limit. Also, what thought has been given to rubbish day and buses not being able to pass rubbish trucks? Increased emissions due to buses, trucks and cars waiting to pass. What about construction. This is an area the council has identified for massive housing at great heights - where will the contrators park? Or will they get permission to block cycle lane (imagine the outrage from the cycle lobby).

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

Road is too narrow. Smaller buses are needed. Spaces for construction vehicles. Proper consultation with residents who are already quite green. ANother mad ideological dream with no basis in reality

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Yes move parking to down hill side on Raora road only. Not the flat bits along Plunket St to Raroa Cres, Smaller buses or no buses on Raroa Road. Do not install a painted cycle lane yes to 125 Raroa Road bus stop.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Negative

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

Stop bikes going through Aro Park, they can go onto the road and join the cycle lane as it will be so 'safe'.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Install cycle counters on all ends of each section so you can properly monitor the numbers. Most have ebikes and can go at 30kms so can use road if speed limit reduced. Businesses need parks. I cycle and drive through. If driving, I go elsewhere if there isn't a park as I go by.

Feedback**236**

114172555203

NAME: S	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I regularly cycle from the CBD or newtown up Raroa to Plunket Street. I currently use a pushbike but will soon have a family so looking to use an E-bike. These plans make me feel a lot more comfortable about using a bike than switching to a car (which i do use occasionally I partner uses regularly for work. It also encourages me to get kids cycling early.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I do wonder where the parked cars will go. There really is limited space especially with the steepness of the terrain it's ot easy to build your own driveway. I'm not opposed to some parking

being kept or a communal park somewhere, tagging onto Zealandia carpark for instance. Sadly the large green next to it is very rarely used and concentrating parked cars may solve some issues.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I regularly cycle through that pathway but always feel a bit guilty as it's a bit snug if there's pedestrians. Widening it and making it a shared path would be useful. I rarely see people picnicking there but a bike lane could encourage it. Wider lanes would also be useful for Argo house across the road as I know they have some accessible bikes.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Better to not rush and get it right. As far as cycling goes in Wellington, Aro Valley is pretty good - wide roads and a village feel where cars tend to respect cyclists, especially with mountain bikers. Better to disrupt once and have it right. It's worth keeping coupon parking to encourage some travel. People do have a genuine use for cars and that shouldn't be ignored.

Feedback**237**

114178439450

NAME: Sarah Jane Parton	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

I am largely in support and think this needs to happen, that it's an excellent idea and makes sense for our community in terms of its geography and our largely progressive ideological leanings (leanings which mean that, as a community, we understand why we need to be making any changes we can to reduce carbon emissions) but I also think it's going to take genuine community engagement and some creative solutions to get residents and businesses, particularly those on Aro Street, on board and feeling good about the changes. The reality is we live in a car-centric context and a cycle lane plus a car share park or two isn't going to change that to a degree that makes this plan workable for those businesses and residents. Unless more solutions are introduced, including huge improvements to public transport that make using it more accessible and convenient than driving, the main concern I'm hearing from many of those businesses and residents is that they fear they are going to be inconvenienced to an extreme and unmanageable degree. Anecdotal evidence suggests many residents who own cars are relatively infrequent drivers with no off-street parking on their properties and they just need their vehicles to be somewhere safe near their homes. Perhaps one solution could be to build and maintain a secure vehicle storage space for residents somewhere nearby, like in upper Epuni St. People are also worried about accessing their homes when they need to transport items and people that cannot travel long distances on foot (i.e. small children, disabled folks, the elderly), so some allowance for people to unload vehicles for brief periods ought to be factored in. I'd also like to propose that residential parking permits be made free (with a limit of one per household), or at the very least that fines are waived when proof of residence is produced, as there is plenty of research in criminal law that supports the premise fines are fundamentally classist, operating to punish poor people and potentially needlessly engage them with the justice system, while they have been shown to have little impact on the behaviours of the rich. Here in Aro the properties we live in may be worth a lot but the 2018 census indicated our household incomes are way below the national median and well over 60% of us actually rent those valuable properties, rather than

owning them. We're not rich people. I'd also like to see the 30km/h area extended to all of the side streets and the section of Aro Street from the shops through to Raroa Road. Nobody should be driving along Devon St at 50km/h, or Norway, or Holloway, or Adams, or Durham, or anywhere in the Valley (or Highbury for that matter). It's not safe. Finally, folks from local businesses are worried they will lose a percentage of the customers they currently attract, that is, those who are driving through the valley and park outside to make purchases. There is a fear those people won't be able to find parks so won't bother. These concerns need to be acknowledged and worked through so that the impact is mitigated. Let's make this happen, together, in a way that works for everyone.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I am largely in support and think this needs to happen, but I also think it's going to take genuine community engagement and some creative solutions to get the people who live on Aro Street on board. The reality is that we live in a car-centric context and a cycle lane plus a car share park isn't going to change that without some other significant changes happening at the same time. Unless more solutions are introduced, including huge improvements to public transport that make using it more accessible and convenient than driving, the main concern I'm hearing from many of those residents is that they are going to be inconvenienced to an extreme degree. Anecdotal evidence suggests many are relatively infrequent drivers with no off-street parking on their properties who just need their vehicles to be somewhere safe near their homes. Perhaps one solution could be to build and maintain a secure vehicle storage space for residents somewhere nearby, like in upper Epuni St. People are also worried about accessing their homes when they need to transport items and people that cannot travel long distances on foot, so maybe there needs to be some allowance for people to unload vehicles for brief periods.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**238**

114176639595

NAME: Sarah	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Neutral****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?

Feedback**239**

114165201007

NAME: Sasha Vlassoff	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

For the most part I strongly support these changes and it is understandable given the width of the road and number of businesses in the area that only an uphill cycleway is proposed. I strongly disagree with the decision to not continue the cycleway through the shopping area – this forces bikes to merge back into traffic for a short distance to preserve a very small number of car parks. Preserve parking on the north side but give the south side to cyclists travelling uphill. The requirement to merge with traffic will put people off using the bike lane.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

The uphill cycleway on Raroa Road is a good compromise option – this is a narrow road with limited options so prioritising support for uphill cycling is a good focus. It would be good to see more green hatching in the narrow cycle shoulder sections as a reminder to vehicles that they can expect to see bikes here, and also more hatching in areas where vehicles will often be crossing over the cycleway to park like outside 172 and 138. It seems unnecessary to have a 14-metre shared pathway for pedestrians and cyclists at the Holloway Road intersection – if the pedestrian path could be redirected adjacent to the cycleway they would not have to overlap and it would remove potential conflict between pedestrians and cyclists. I support the proposed cycleway for the Plunket Street to Moana Road section. Parking opposite 12/14 Raroa Crescent should be removed regardless of whether there is a cycleway on that side of the road – when there are currently cars parked on both sides of the road this severely restricts traffic flow and I expect the same problem would occur with the cycleway taking up road space on one side. Retaining parking on other parts of the northern lane should be less problematic. It is understandable that options are very limited from Moana Road to Chaytor Street given the width of the road but it is disappointing that there are few improvements suggested. Even a narrow cycle shoulder on the uphill climb from Chaytor Street to Moana Road would be welcome as this is currently a dangerous stretch of road to cycle. This is the only uphill stretch of the Aro Valley Connection in the city direction – cyclists will be slower moving through here and need more protection than in other areas where they are going downhill. Signage or road markings should also be added to stop cars passing on the corner below 21 and 14 Raroa Crescent as vehicles undertake dangerous overtaking manoeuvres here trying to pass cyclists.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

These are good, common-sense changes that will make cyclists safer and more confident when using this back route and give more space to pedestrians. I support establishing Little Palmer Street as a shared pathway as it will make the area more accessible. It would be good to clarify expectations for cyclists moving between Abel Smith Street and Oak Park Ave – it looks like there is a temporary kerb ramp proposed on the north side of Abel Smith Street but no indication of additional signage to confirm that this section is a shared pathway and it is acceptable for cyclists to move on to the footpath. Cyclists travelling south from Oak Park Ave to Abel Smith Street should not have to wait for the crossing to get to the other side of the road – most traffic moving through this intersection is travelling on other roads.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Overall, I strongly support the proposed changes. I disagree with the phased rollout – these are transitional changes and we need them in place as soon as possible to test their effectiveness. A partial cycleway will get less usage and risks creating the perception that there is less demand from cyclists than there could otherwise be. In particular, the proposal to wait until 2025 for Stage 3 seems difficult to justify. If Raroa Road residents can be expected to adapt to changes in 2023 then I see no reason Aro Street residents and businesses cannot do the same – they just have more lobbying power to slow the pace of change. It is also important to acknowledge that the chosen option for Aro Street is already a compromise – the option that scored most highly was a cycleway in both directions. A phased approach may have been appropriate for complete removal of parking but the decision to pursue an uphill bike lane only should mean the changes can be implemented more quickly as the impact on parking is less significant.

Feedback**240**

114174896014

NAME: Sarah Martin	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

Overall I support changes that increase people's ability to walk, bike and use public transport safety but do have some concerns about the impact of accessibility and utility of people living in Aro Valley.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

I like the idea but worry as a resident in the area worry that it'll make some aspects of life more difficult - eg popping into the dairy/fish and chips shop in my car on the way home from things. I'm also worried about the effect on local businesses

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Neutral****Do you have any comments to make about the proposed design?**

How many people are going to be able to ride up Raroa Rd? I'm not sure if the benefit to people living in Aro Valley.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Good idea - hopefully make it safer for pedestrians walking through there. Currently it can be a bit of a hazard - risk getting knocked over by cyclists and scooters

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**241**

114177322781

NAME: Shaun Cornelius	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

pedestrian, cycle and scooter use is increasing rapidly and these changes are necessary to improve user safety

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

I like the stage approach very much. The stage on focus on reducing traffic speeds and improving pedestrian safety are great. A lot of children walk / cycle through the village area to go to Te Aro school and the changes will make it much safer. I cycle this route most days and the uphill section up to Waimapihi gully entrance is particularly dangerous so putting a cycle lane in this section in stage one is great.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I ride this route daily and it is very congested. The proposed changes will be a big improvement for all users.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

I'm a cyclist and pedestrian so would love to see these changes happen faster. I appreciate however that car users and some businesses may need to longer to adapt.

Feedback**242**

114160239168

NAME: John Scrivener	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I don't think it is going to solve any of the issues it states it will resolve. Just make the area worse to live in at great expense. 100% renewable energy and EVs are a more practical solution to climate change rather than making it harder to own a car and making cycle lanes. There is not a city in the world which had car dependency and has subsequently removed it, I don't see Wellington being any different.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

If you were to have a cycle lane, it would see more use if it was on the downhill side. Hardly anyone will cycle up it.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

I don't think safety is the main reason no one cycles up Raroa road, it's because it is too steep for 98% of the population to even consider.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

I'm worried that it'll just become a cycle way, rather than an actual shared pathway. Like the shared path from the end of Oriental parade around the bays. It's a worse walking experience there now because cyclists just speed along with no consideration for pedestrians. I would even consider adding speed bumps in the park to stop people cycling fast.

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

I live on Holloway Road, and agree with the changes in part but believe the P120 parks should be reduced to 30 minutes. The only reason to have P120 is to cater for recreational mountain bikers who drive giant petrol guzzling cars to Aro Valley and ruin the local reserve by treating it as a rich, white male playground. If the plan is to reduce carbon emissions, then stop leisure drivers coming to the area and focus on residents and commuters. They downhill brigade can use the new cycle lanes to get here, and if they find it too far they should go elsewhere closer and release less carbon.

Feedback**243**

114160148164

NAME: Shannen Petersen	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

I am generally in support of these changes, I think they're important but also worry that removing so many car parks will cause mobility issues and issues of accessibility for people who can't use buses or walk places for whatever reason

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

Again, removing too many car parks will cause problems for people who need to drive for mobility reasons/have children/public transport is not an option

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**244**

114164435084

NAME: Sheila Hart	SUBURB: Southgate	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

To improve the road for all users and encourage more active modes of transport e.g walking, biking. It will help make the street more liveable by slowing traffic down.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

It will be great to see Aro shop area more friendly for people walking and riding bikes. Currently, it is very narrow for riding. Also the pavements are narrow and always very busy, especially around the cafes. I think it should be rolled out quicker than the 2-3 year plan, in keeping with the transitional plan for new infrastructure. We need to create mode shift as a matter of priority.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

An uphill bike lane will be awesome! So I can keep out of the way of the cars, and have less close passes by those in a hurry to nip past me.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

And a little tidy-up in the area would be great too!

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The bike lane should be implemented in full ASAP!

Feedback**245**

114163098768

NAME: Sinny West	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Positive
People walking on the paths through Aro Park and Little Palmer Street?	Positive
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
It's a dangerous area for cyclists, so the priority should move from parked cars to cyclists and pedestrians as soon as possible

Feedback**246**

114181264035

NAME: Sven Ericksen	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Neutral

Do you have any comments to make about the proposed design?

The option of a shared pedestrian and uphill cycle lane between Holloway Rd and Plunket St should have been explored.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

A shared pedestrian and uphill cycle lane should be considered.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**247**

114181501175

NAME: Steve Naismith	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

We own a 2 flat rental property on Aro st. If the suggested removal of the existing parking options and subsequent addition of restricted parking goes ahead, we would like to know where the residential tenants are going to be able to park .The removal of the existing car parks are going cause a huge issue for the residential tenants as the number of parking options would be significantly reduced. It's hard enough now for them to find parks let alone in the future if existing parking options are removed. Where are trade's people supposed to park when working inside houses? Where do delivery vehicles and couriers park whist delivering goods? These are things that need to be considered and before taking away car parking.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Where are residents supposed to park their cars ?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

I strongly oppose all of these changes

Feedback**248**

114181629128

NAME: Sonya Hogan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

Great amount of thinking and planning, well done. A few additional things for consideration: - make all residential car parks residential 24/7 some are currently only mon-fri limited hours - move the conversion of coupon parks to residential parks on ohiro road to phase 1 and extend this to all of ohiro road up to maarama cres this is due to the higher density of apartments and rental housing in this area without off-street parking - provide sites for electric car and bike charge near ohiro road willis st end of aro st - due to the high accident and dangerous driving on lower ohiro road where people are turning from aro street due to it being used as a shortcut to brooklyn to avoid the one way system on willis street - speed humps on ohiro road between aro st and maarama cres - and/or one way only down ohiro from maarama cres to aro st in the direction away from brooklyn to stop it being used as a shortcut. Kia ora Thanks for the chance to comment on the proposed changes. There wasn't really a general comments/question space on the feedback process (which I have just finished) so wanted to send a quick email to ask whether there had been any consideration in having carless days in the central city and surrounding suburbs as a way to help people get more used to using public transport/bikes etc., say first

Sunday of every month? That way people who are nervous about biking around lots of cars can give the route a go and get more used to it, people get used to navigating the city out of a car and there could be events on the streets in town with just the main roads being used for buses. Just a thought :-)

Ngā mihi mahana Sonya

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

Please make dedicated lanes for bikes and slow bike zones where pedestrians are sharing the space. Also pedestrians should have right of way in pedestrian areas.

What do you think of the timing of the proposed approach?

Why do you think that?

Would prefer - change 20 coupon car parks to residents parking on Ohiro Road - to be in stage one (see earlier comments) Would also want to see the frequency and punctuality of public transport in the area increased especially over the weekends.

Feedback**249**

114165572971

NAME: Stefanie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I strongly support the overall changes including speed bumps and reducing the speed limit on Aro St. We have many residents, cyclists, pedestrians, pets and children in the area, and a lower speed will be safer for us all. My 5 year old wanted to add that he thinks maybe our cat wouldn't have got run over if people drove slower which I think is probably a fair enough point.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

I support actions the Council can take to make the city more suited to walking, biking and public transport. I strongly support most of the initiatives proposed. The one that is the least attractive for me is the removal of car parks, as a resident and someone who parks their car on the street, but we will find a way to make it work for the greater good, as I can't see how we can make the city more inclusive for all without moving cars elsewhere.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

I strongly support this initiative and don't think it will really affect people driving on the surrounding streets, as Palmer St is a dead end and not that much traffic goes through that section of Abel Smith St.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

A balanced approach gives residents enough time to figure out where we're going to put our cars while still making some positive and tangible change

Feedback**250**

114173702711

NAME: Steven Firth	SUBURB: Aro Valley	ON BEHALF OF: Scipio Firth Trust	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly oppose****Do you have any comments to make about the proposed design?**

10th November 2022 Wellington City Council Re Aro Street Cycle lane. As a resident of Aro Street I object to the council's planned cycle way. The cycle lane takes away parking for residents who need access to their properties. There has been no provision for additional residents parking in adjoining streets, (Epuni and Adams). This action will prevent owners of homes in the historic Te Aro precinct, enjoying their property, where houses were historically built without drive on access. The action of a cycle lane will also devalue the existing property of many residents who have required the benefits of inner city living. The proposed cycleway plan has been done without consultation with the residents. I see no alternative but to fight the council's cycleway change through the courts. As there are a number of disgruntled homeowners, we plan to take immediate action unless we can see some form of council consultation.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

10th November 2022 Wellington City Council Re Aro Street Cycle lane. As a resident of Aro Street I object to the council's planned cycle way. The cycle lane takes away parking for residents who need access to their properties. There has been no provision for additional residents parking in adjoining streets, (Epuni and Adams). This action will prevent owners of homes in the historic Te Aro precinct, enjoying their property, where houses were historically built without drive on access. The action of a cycle lane will also devalue the existing property of many residents who have required the benefits of inner city living. The proposed cycleway plan has been done without consultation with the residents. I see no alternative but to fight the council's cycleway change through the courts. As there are a number of disgruntled homeowners, we plan to take immediate action unless we can see some form of council consultation.

Feedback**251**

114181501755

NAME: Stuart Cunningham	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**252**

114167156621

NAME: Suraya Sidhu Singh	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

All road users should have safe streets. Aro Valley and connected suburbs are very attractive and vibrant but unfortunately growing numbers and size of cars has made them unsafe for pedestrians and cyclists. These changes will make it safer for people to walk, cycle, scoot, skateboard and use public transport, which will mean more people choose those transport modes over driving -- benefitting all, including drivers. I support reduced on-street parking and the 30 kmh limit, which will also improve business in the area. I think the work should be done in one stage not spread out over three stages to make sure people can experience the full benefits right away and because creating "bitsy", partially finished infrastructure can be counterproductive to community buy-in.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

People walking on the paths through Aro Park and Little Palmer Street?

People riding bikes along this route?

People driving vehicles on the surrounding streets?

People with disabilities or accessibility issues?

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

Feedback**253**

114181541206

NAME: Susan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly oppose

Why do you think that?

Negative effects on residents, especially the elderly and those with disabilities, greatly outweigh the benefits. Consultation on this project seems to be seriously flawed. In the interests of natural justice, you need to give greater weight to views of the local residents, whose daily life will be significantly affected by the proposal.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Negative effects on residents outweigh benefits for people passing through

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Negative effects on residents significantly outweigh benefits for people passing through

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Why do you think that?

Rethink the whole project! Not enough consultation with the community.

Feedback**254**

114162851150

NAME: Taison	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

I support the changes that are geographically centred around the shops, e.g. speed bumps, raised crossing, protected cycle paths, and improving the path through the park and beyond. There needs to be significant work done on the uphill connection as the current proposal is woeful and will not achieve the desired goal of more people cycling. Lanes and shoulders are virtue signalling investments and do not improve cycling safety. If safety is not improved, the status quo will be maintained, and our emissions will not reduce.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Good to see traffic calming in conjunction with speed limit signs. I would also like to say that through the shopping area specifically, where the cycle traffic merges with the car traffic, to reduce the speed limit even more and add appropriate traffic calming to assist that. Don't encourage people to drive 30 through the shops with bikes merging, people crossing roads, cars trying to pull out of Devon st - just slow it all down a bit more just for the shopping precinct. 30km for the rest of it is good, and in fact, I'd even like to see the 30km speed limit extended to the beginning of Holloway rd because of Waimapihi reserve mountain bike area (lots of bikers exiting and entering around this area), and the fact that the road is still really tight with lots of parked cars. Again, traffic calming needs to be added to enable this, not just a 30 sign.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

I cannot understand how anyone can think bike "lanes" and "shoulders" are going to make it safer for people riding bikes uphill. This will change nothing. It will not make it safer, it will not get more people cycling. This is an utterly ridiculous proposal. I understand that road space is limited, I get that, but be bold. Find a better solution. It definitely exists, but it's being kicked down the road because no one wants to upset car drivers. While I clearly disagree with the design, it could be kept with some added improvements. Firstly, if you've got room for a cycle "lane", make it protected. Use those temporary yellow post things or something, I don't like them, but I understand the need to use them as a temporary measure. Secondly, at the "shoulders", add some sort of calming infrastructure and signage on both the uphill and downhill sides, so that drivers have to slow down. Slowing both sides down calms everyone's interaction with the shoulder and allows the uphill car traffic to check for cyclists, give way to them if they are close by, move into the shoulder, and continue on their way. Improving bus access is good though.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Yea love this. I've used the route a number of times and it's not too bad as is because people waling and people biking are considerate, but making it wider is good too. I would say just adding a lick of paint to indicate a cycling lane is a good idea too, just so that when it's busy people know where they have and don't have right of way. I think the impact on the street is negligible, as is the impact on people using the park to picnic - there's heaps of space. I would also add, just directing cyclists using some bike lane paint in the vicinity of the basketball court is a good idea too. I have seen a few near misses there because people get distracted by rogue balls and move in the way of cyclists. Just having the paint makes people aware that there could be a bike at any second.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

I think that's reasonable. My instinct is to say "more quickly" but I mean Rome wasn't built in a day. Just do it right.

Feedback**255**

114181203699

NAME: Lisa Thompson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Oppose****Do you have any comments to make about the proposed design?**

The proposed changes are not sympathetic to the needs of the people who actually live, own businesses and work in Aro Valley.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Neutral****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very negative
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Neutral
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Very negative
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly oppose	
Do you have any comments to make about the proposed design?	
There are already significant issues with bike users traveling very quickly through the community centre precinct. I think there needs to be more consultation about whose benefit widening this park is actually for. Aro Valleyites fought very hard for the park decades ago and I do not think diminishing it further will be popular. Local people use the park to recreate. They don't mind sharing but they do not it turned into an expressway for cyclists.	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
Many people who live in Aro Valley already walk to work, bike or use public transport to get to work. These changes are designed to benefit those passing through Aro Valley, not the actual residents who live here. Aro Street is very narrow already and I believe there are better streets that would be safer and offer greater flexibility for the introduction of bike lanes. I am hugely surprised that the statistics for the use of bikes on Aro Street for October 2022 is only 143 people per day and that the measure is in the direction going into the city. This is of course the downhill direction and easier for bikes to keep up with cars etc. The bike lane is proposed on the uphill side - what are the counters for that side?? I am able to walk to my current job and do most days. However, in my previous employment, I required a car to visit clients and suppliers. I am concerned that by removing such a large tranche of carparks in Aro Valley will mean an entire sector of the population will not be able to live here and have no access to a vehicle when they need it. Ride share does not work for all people. I'm talking about those whose employment requires them to travel, tradespeople, people who are caregivers for elderly and dependents, grandparents who pick up and drop off grandkids so that the parents can work. There is also a group who do not ride bikes for various reasons. This includes families with young children or those who rely on a vehicle including the elderly and those with medical or physical conditions.

Feedback**256**

114169432410

NAME: Emma Mayo	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
<p>Prioritizing making it safer and more accessible for bikers is more important than people's ability to adjust to parking changes. People are adaptable, they'll quickly make do with what's available, the slow approach won't make much difference really whilst having negative safety impacts.</p> <p>Currently I use a car rather than bike on this route due to concerns about safety.</p>

Feedback**257**

114181254596

NAME: Emily	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Not enough parking for residents, let alone those visiting the area

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

There is already fights for parking including a lot of illegal parking happening. This will just escalate. The hills around aro are unsuited to bike riding with extreme incline and camber. Residents will not be able to have friends and family visit. And cyclists using the aro park track mostly arrive on the back of wait for it CARS

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

There is already not enough parking. The incline and camber make the hills unsuited to bike riding. Having bus stops in the bike lane is a recipe for disaster and cyclists vs bus altercation.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Negative

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

They are already wide I don't see this making any difference

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Don't make the changes they are not useful and will just continue get wellington moving's great work in making the city an unpleasant place to live, visit or shop. There is already major parking issues and with unreliable slow expensive public transport that is not an effective option. I visit my sister in aro and we use the local walking tracks and businesses. I will not be able to do that anymore if i cannot park in the area. It would be 1.5 - 2 hours on public transport to visit vs a 20min drive. I cannot afford to park in the city and walk out to visit her as i am young and not earning the living wage. As many visiting friends or family in the area are. We will have to meet up outside wellington city as i will not be able to manage any more. Possibly pick her up & head out to Porirua where shopping is easier and parking free.

Feedback**258**

114178452116

NAME: Sarah Spencer	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Do you have any comments to make about the proposed design?

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	
People walking on the paths through Aro Park and Little Palmer Street?	
People riding bikes along this route?	
People driving vehicles on the surrounding streets?	
People with disabilities or accessibility issues?	
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Why do you think that?

Sarah%20Spencer%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support☐ Support☐ Neutral☐ Oppose☒ Strongly oppose☐ Don't know

Do you have any comments to make about the proposed design?

I strongly oppose most of the proposal, except for the points I've highlighted which are community oriented and would be helpful.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The main area of the proposed design we are concerned about is the introduction of a cycle lane going up Raroa Road to eventually finish at Chaytor Street. We drive on this road every day. Currently the situation with cyclists sharing that road with cars is exactly what it needs to be for a narrow winding road in a hilly city. When a car encounters a cyclist they wait until there is a wider part of the road to go around them, safely. The cyclists are very good at pulling in to areas on the road to allow cars to pass. The behaviour of cyclists and cars combining seems to be one of patience and understanding, as it doesn't take long at all for there to be a moment where a car can safely pass. Drivers are showing patience, and cyclists are aware of the traffic around them and move aside as soon as is practicable. It all flows reasonably well. However the introduction of a cycle lane changes everything. There is no room for cars to safely pass, and there will be huge traffic blockages that build up to a gridlock at peak times. The buses are now going to have even less room to navigate around the corners. Frustration from all road users is going to reach boiling point with this new proposal. We feel really strongly that this proposal is only going to increase the chances of accidents, which could have serious consequences.

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. *picnicking*)?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☒ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive ☐ Positive ☒ Neutral ☒ Negative ☐ Very negative ☐ Don't know

People riding bikes along this route?

☐ Very positive ☐ Positive ☒ Neutral ☒ Negative ☐ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive ☐ Positive ☒ Neutral ☒ Negative ☐ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive ☐ Positive ☒ Neutral ☒ Negative ☐ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Again, this area is fine as it is. We have lived and walked/biked through this area a lot. It just isn't necessary. This seems wasteful to spend this sort of money when we have pipes and infrastructure which need urgent attention.

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☐ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

I have no view on the timing of the proposed changes as I don't agree they should go ahead at any time.

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

This is not a safe proposal.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

I'm not concerned about this, and would not be opposed to it. I have not encountered any issue with the current speed limit, but can see it would be more consistent if the speed limit was kept at 30kmh along that stretch of road.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☒ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☒ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback**259**

114164362770

NAME: Thomas Adams	SUBURB: Northland	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I cycle that route a lot, and it will make it a lot safer

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Neutral
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?
It's a good compromise, and gives people time to adapt

Feedback**260**

114162663384

NAME: Tom O'Flaherty	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

On the whole, these changes should lead to a suburb that feels more like a suburb and less like an arterial route for motorvehicles to get to Karori.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly support

Do you have any comments to make about the proposed design?

These will make for very good changes. This is an intimidating and dangerous section of road for a cyclist, which is avoided by most except the most confident of cyclists. Cars should not be allowed to be haphazardly parked over an arterial route, they should be limited to one side of the road and preferably removed altogether, it is a horrifically wasteful use of public space. The other thing on this route is the huge amount of speeding and aggressive driving from people driving very wide and large vehicles, the lane will need to be protected to stop cars from encroaching into it. And speed limits will need to be properly enforced. Too many people drive this stretch of road as fast as they possibly can which makes it so dangerous for vulnerable road users.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

Remove as many car parks as possible as fast as possible. Install speed cameras to keep car speeds down. Stop people from blocking the kerb for walkers.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Yep, great idea.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Yeah it should happen faster than this. If it happens at this pace, there is a good chance we will lose a lot of the changes. Rip the bandaid off. Remove the carparking. Do not replace it with other carparking, there are far too many cars in Aro Valley.

Feedback**261**

114163043332

NAME: Craig Anderson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

I would also like to see some enforcement of 30kmh limits if speed humps are delayed or reduced. It is widely ignored at the moment.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Negative
People riding bikes along this route?	Positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Don't know
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
The timing of the stages is about right
Why do you think that?

Feedback**262**

114164407974

NAME: Tim	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Very positive
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Very positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?
The sooner the better

Feedback**263**

114176226582

NAME: Tim	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

The proposals are a mix of good and bad. I'm also concerned that your consultation process is inherently flawed. It is designed as a push poll. This undermines the consultation process and reinforces the problem that citizens have no say over hie things are managed in the area where they live.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Buses are a liability on Aro Street.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Oppose

Do you have any comments to make about the proposed design?

Buses should not be on Raroa Road.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very negative

People walking on the paths through Aro Park and Little Palmer Street?

Positive

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

The path through the park should be altered so there is an uninterrupted grass area.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Feedback**264**

114181685067

NAME: Tish	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I support these changes and I look forward to being able to safely ride my bike in this area. My biggest concern is where the cars currently parked on Raroa Road will park after these changes have been implemented. Please make Norna Crescent and Highbury Road for residents only and increase dotted yellow lines on both.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Support****Do you have any comments to make about the proposed design?**

Concerned about where the cars currently parked on Raroa Road will move to. Norna Crescent and Highbury Road are already full - and dangerous with the bus and current road narrowing due to parking on Norna Cres. Can we make Highbury Rd and Norna Cres Residents only please.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Concern where the path will pop out on Aro St - near Garage Project? Will there be suitable crossing for bikes heading from Aro Park onto Aro St?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Add Resident parking and/or Coupon parking to Highbury Rd and Norna Cres.

Feedback**265**

114175842104

NAME: Tegan van der Peet	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

That the bike lane is SO important! I recently biked down this route and one thing that I found really unfriendly for biking was how on aro st parts of the road are not flat across the road, like the left part of the road slopes down reasonably steeply. Because of this I rode in the middle of the road to keep safe, which meant cars couldn't overtake me. To make our roads bike friendly (especially for amateur cyclists like myself) we need to make sure our roads are smoothed out. I also want to stress how important this is for me from an accessibility/disability equity view - I experience chronic brain fatigue after a traumatic brain injury and taking the bus into town is SO exhausting for my brain. Since I've started biking I experience far less brain fog and arrive at work and other places feeling refreshed rather than exhausted. I can't drive (nor would I want to because I care about the climate and peoples health) so having safe ways to cycle is essential to me.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

I think a cycleway going downhill should be added on parts where road is wide enough - particularly round the bend(s) coming into Aro st from raroa road. I found it scary cycling this way with the traffic and lack of visibility with the corners, so having a cycleway would make me feel much safer! Also think removing parking space on the uphill is a really smart move that will help traffic flow a lot! A lot of stopping and starting can occur on this road due to parked cars on both sides, so having them only on one side will give better visibility and flow for traffic.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

The concept of people needing TWO years to adapt to removal of parking... I think that is ridiculous. People are constantly adapting to change and certainly don't need that long to adapt. I think 6 months is a good time for adaption, or a year at most. In two years many people will have moved to and from the area, people will have adapted to other things. I think doing it sooner is better for the safety of pedestrians, cyclists, and drivers. It will have positive impacts for emissions reductions and public health. It will also take less construction time and thus be less disturbing for the community, less noise pollution. In te aro park changes were quick in removing parking and putting in the pathway and it allowed for a much smoother and less disruptive transition. Cyclists have also had to wait so long for cycleways, please make this safe travel for us finally!!

Feedback**266**

114171436800

NAME: Tom Brodie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

too much traffic control poor outcomes for perdestrians unnecessary speed control changes too far for cyclist that dont actullay use the road that must or as much as claimed . what will happen on a wet rainy windy work day - people, take the cars

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

aro street will be too narrow , you have not addressed the width of footpaths and seem only to consider cyclists . more thought design into pedestrians is required as more people walk than the over inflated number of cyclists people still need and requiure cars trucks and busses need to be able to access this road maybe look at the bus stops so thatthey are qucibk pick ups and drop off do we actaluut need bas parking for such a short time maybe delete bthe bus parking so passengers get on and off while bus is on the road , the disruption will be no more than what ou are propoing . From my research and reading into this elaborate and clearly very expensive plan the aro valley changes to roading parking cycle lanes and vehicle speed controls are effectivly for 100 users . 112 per day on week days and 93 per day on weekends . this is a complete waste of money and will inconvenience and annoy thousands daily , trucks, buses cars will all be effective . i see very few cyclists on the roads and see no reason not to just tweek or slightly

widen whats in place now widen the footpath , no speed calming measures , share the existing paths . this will become another Island Bay disaster Tom Brodie Ratepayer

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

one cycle lane only shared with pedestrians will be better

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Very positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

good

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

dont raise pedestrian crossing works fine now just more unnecessary controls . no speed cushions unnecessary as it works fine now . how is the parking for residents controlled in holloway road . i have 2 houses all with parking and 4 new ones to be built all with [parking i encourage parking and provide it but may require additional residents parking how is this to be managed one per dwelling or a lottery

Feedback**267**

114180844819

NAME: Thomas bisset	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

i need the ability to park outside my house and you are taking that away from me. there will be no climate positive change of any kind but the cost to the people that live on aro street will be huge. why are the residents of aro street expected to bear the brunt of this?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

I bought a house on aro street 18 months ago. i work as a trades person, I require a van to carry my tools for my job and lively hood. if you take away my ability to park my vehicle on the street, i dont know what i am going to do. i have a wife and daughter to support. creating a cycle way up aro street and toward raroa road, this will only be used by a handful of people as the street is incredibly steep. you will take away my parking so a few people can cycle this route for a hobby? this will hugely impact my life and i cant afford to sell my house and move. i got lucky with interest rates and put everything i have into this house. the interest rates have double so there is no way i can afford a house somewhere else in wellington. the real problem here is the wellington public transport is rubbish. buses are cancelled, due to no drivers. sort the public transport out, thats what wellingtonians want. stop straw manning what we want, we dont want cycle ways at the sacrafice of regular transportation. look at island bay, that cycle way never has bikes on it. your proposal will push families, the elderly, people with disability, out of aro Valley.

this is not inclusive at all and is just not ok. how will tradespeople be able to do any work on aro street? what are the required numbers of cyclists to justify this cycle way? where am i meant to park my vehicle so i can do my job and also keep an eye on my vehicle, which contains 10 thousand dollars worth of tools in an area which has a high crime rate in regards to vehicles being stolen, particularly trade vehicles? this decision seems to have no positive for the people of Wellington, only the few that go for a ride once a week. im absolutely disappointed and im seeing a huge amount of rate payers money being wasted to achieve nothing in relation to climate change.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

its a very steep hill, only a few people will ever use this cycleway

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Very positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

these changes will actually have a net positige for EVERYONE, not just a select few

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

removing any carparks to create a cycle way is a terrible idea. i need to oark outside my house to keeo my tools safe that i require to do my job so i csn oay my mortgage and feed my family

Feedback**268**

114161072343

NAME: Tom Dench	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

It is a step in the right direction for improving mobility in the area. Aro Valley is where I live, and I personally walk around the area a lot. Anything that positively impacts my ability to walk or otherwise move around without a personal vehicle is greatly welcomed. My one criticism is of the proposed cycleway design being based on green paint on the existing roadway. I would prefer a raised path at the same height as the existing footpath. I feel this would be appropriate for much of the uphill cycle section as speeds would not be as high as a flat section of cycleway.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

My preference on the bike lane would be to have it raised at the same height as the footpath, essentially widening the footpath and having it become a shared path.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Support

Do you have any comments to make about the proposed design?

Again, my preference here would be to raise the bike lane and have it become a shared path. Personal stake here - I am a resident of 146 Raroa, and it's difficult to cross the road to reach the many houses by the Zetland Street stairs where the road bends to provide two blind corners. If I could cross over to a safe shared path further uphill on Raroa road, it would improve my access significantly. Raised paths for bikes are also much safer to ride on.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

Great call. These streets can be very busy with walkers and bikers.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

Seems to be about the right amount of time for people who need to own a vehicle to move.

Feedback**269**

114179673339

NAME: Louis White	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

This entire project is shocking. Clearly designed by people with a anti-car agenda.

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

Shocking, on the cheap, nasty

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

This street isn't wide enough to support vehicles this large. It shouldn't have busses going up it AT ALL

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?	Very negative
People walking on the paths through Aro Park and Little Palmer Street?	Very negative
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Neutral
People with disabilities or accessibility issues?	Neutral
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Neutral	
Do you have any comments to make about the proposed design?	
This is fine	

What do you think of the timing of the proposed approach?
Make the proposed changes more slowly
Why do you think that?
Clearly designed by people who are specifically anti-car.

Feedback**270**

114169080483

NAME: Bill Viggers	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This is a narrow but highly used route. I'm scared driving it uphill every time I need to follow a cyclist.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Support

Do you have any comments to make about the proposed design?

Keep the bus stop. It is handy having lots of bus stops going up Aro Valley. I just keep walking knowing they are so close I can easily take a bus. While talking about pedestrian crossings, having one at the bottom of Norway street would also be really good, as that is also a major walking route. Also why only raise one crossing, not both of them? The one in the village gets a lot of use, and the poles on the north side can sometimes hide people who want to cross it. Speed humps (if installed) should only be in the 30kmh areas. It is unclear from the description where they are intended to be located. I am somewhat against adding speed humps to an arterial route such as this. It reduces their value in other streets as a reminder that such areas are not really intended as through roads. P20 parks in the village should be P10, which is plenty of time to get in, buy something and leave again.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

Do you have any comments to make about the proposed design?

An extra (uphill) bus stop on the big corner before Norma would be good. There is no safe crossing from the existing bus stop near Cluny ave. for people heading down into Kelburn. Or you could add a pedestrian crossing there. That would be good too.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

Do you have any comments to make about the proposed design?

I worry that making this a shared area for bikes and pedestrians may put pedestrians at risk from bikers going too fast for the area. I'd support having a green bike-line on a widened path.

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Not sure why you would do the coupon parking (which doesn't really impact locals) AFTER removing locals parking. Likewise change other coupon parks to resident parks before removing other resident parks. Think about the people who are living here first.

Feedback**271**

114160167770

NAME: Vivian Stephens	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Aro Valley feels very dangerous as a pedestrian and cyclist. Vehicles often make dangerous u-turns and overtake buses stopped at bus stops without clear view of the road ahead, especially at the pedestrian crossing at Aro shops, and the bus stop opposite Durham Street.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?**

Extending the 30km/h zone to the bottom of Raroa road at Holloway road would improve safety. Do not support the use of speed bumps.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Strongly support****Do you have any comments to make about the proposed design?**

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Support

Do you have any comments to make about the proposed design?

If the little Palmer Street path is to be used as a cyclepath it needs to be appropriately signposted as it is currently quite dangerous for pedestrians

What do you think of the timing of the proposed approach?

Make the proposed changes more quickly

Why do you think that?

Feedback**272**

114181483910

NAME: Marcail Parkinson	SUBURB: Thorndon	ON BEHALF OF: VUWSA	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

These changes will make it safer and easier to travel sustainably around the city for those who are able to do so, and will also make travel by car quicker and reduce congestion for those who are unable to access other transport options. Many of our students will now have additional safe and affordable ways to travel around the city.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Support****Do you have any comments to make about the proposed design?**

Adding a connecting bike lane from the Universities Kelburn campus would enable students living in Aro Valley to safely travel between uni and home via bike.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?**Support****Do you have any comments to make about the proposed design?**

Adding additional pathways between this area and the university will enable students to travel more safely.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Positive

People walking on the paths through Aro Park and Little Palmer Street?

Very positive

People riding bikes along this route?

Very positive

People driving vehicles on the surrounding streets?

Positive

People with disabilities or accessibility issues?

Very positive

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly support

Do you have any comments to make about the proposed design?

These paths need to be well lit to ensure additional safety and accessibility at night.

What do you think of the timing of the proposed approach?

The timing of the stages is about right

Why do you think that?

These changes are all great and if possible to do them slightly quicker that would be good however the proposed timing is adequate.

Feedback**273**

114179744059

NAME: Kelsey Lafferty	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

You say you want to open our streets up for people of different ages and abilities yet your proposal significantly impacts the elderly and disabled. Removing car parking on a street that already has only one side of parks, means that you are essentially making it impossible for anyone living on this street to have a car. I understand the need to address climate change, but what you propose will have very little impact yet a massive disruption to the every day lives of the community. Cycling is an unrealistic approach to the issue. Wellington is hilly, windy and rainy so opting for cycling as your only form of transport (assuming you are even fit enough to opt for this) is impossible. You would still need a car, and now residents are basically unable to have one. Aside from able bodied single households who may opt to cycle (when weather permits) there are families with babies and children who need a car to get around. There are also tradesmen, who need a van to transport their tools. What are these people supposed to do? Unless your aim is to drive people OUT of the community I do not understand in the slightest how this proposal is of any use whatsoever. Even if people are driven to move out of the area because of this, their ability to sell their houses will be greatly affected by the fact that you essentially cannot own a car on this street. This doesn't just affect the residents. It will become almost impossible for visitors. Including tradesmen if you need work done. Where will they park? I STRONGLY oppose this ridiculous proposal that seems to be about ticking a green box to feel good about yourselves without actually doing anything useful. The cost and impact versus what it aims to achieve seem very very out of touch. Look into better public transport before you start limiting people's options.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

You say you want to open our streets up for people of different ages and abilities yet your proposal significantly impacts the elderly and disabled. Removing car parking on a street that already has only one side of parks, means that you are essentially making it impossible for anyone living on this street to have a car. I understand the need to address climate change, but what you propose will have very little impact yet a massive disruption to the every day lives of the community. Cycling is an unrealistic approach to the issue. Wellington is hilly, windy and rainy so opting for cycling as your only form of transport (assuming you are even fit enough to opt for this) is impossible. You would still need a car, and now residents are basically unable to have one. Aside from able bodied single households who may opt to cycle (when weather permits) there are families with babies and children who need a car to get around. There are also tradesmen, who need a van to transport their tools. What are these people supposed to do? Unless your aim is to drive people OUT of the community I do not understand in the slightest how this proposal is of any use whatsoever. Even if people are driven to move out of the area because of this, their ability to sell their houses will be greatly affected by the fact that you essentially cannot own a car on this street. This doesn't just affect the residents. It will become almost impossible for visitors. Including tradesmen if you need work done. Where will they park? I STRONGLY oppose this ridiculous proposal that seems to be about ticking a green box to feel good about yourselves without actually doing anything useful. The cost and impact versus what it aims to achieve seem very very out of touch. Look into better public transport before you start limiting people's options.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

You say you want to open our streets up for people of different ages and abilities yet your proposal significantly impacts the elderly and disabled. Removing car parking on a street that already has only one side of parks, means that you are essentially making it impossible for anyone living on this street to have a car. I understand the need to address climate change, but what you propose will have very little impact yet a massive disruption to the every day lives of the community. Cycling is an unrealistic approach to the issue. Wellington is hilly, windy and rainy so opting for cycling as your only form of transport (assuming you are even fit enough to opt for this) is impossible. You would still need a car, and now residents are basically unable to have one. Aside from able bodied single households who may opt to cycle (when weather permits) there are families with babies and children who need a car to get around. There are also tradesmen, who need a van to transport their tools. What are these people supposed to do? Unless your aim is to drive people OUT of the community I do not understand in the slightest how this proposal is of any use whatsoever. Even if people are driven to move out of the area because of this, their ability to sell their houses will be greatly affected by the fact that you essentially cannot own a car on this street. This doesn't just affect the residents. It will become almost impossible for visitors. Including tradesmen if you need work done. Where will they park? I STRONGLY oppose this ridiculous proposal that seems to be about ticking a green box to feel good about yourselves without actually doing anything useful. The cost and impact versus what it aims to achieve seem very very out of touch. Look into better public transport before you start limiting people's options.

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Neutral

People walking on the paths through Aro Park and Little Palmer Street?

Neutral

People riding bikes along this route?

Neutral

People driving vehicles on the surrounding streets?

Neutral

People with disabilities or accessibility issues?

Neutral

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Neutral

Do you have any comments to make about the proposed design?

What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

Why do you think that?

Because I disagree with the changes so I think you should allow more time to Come to Your senses

Feedback**274**

114181688015

NAME: Zachary	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I support some parts of the proposal, but the removal of parking on the South side of Aro Street appears to have very little consideration on how it would impact residents who have mobility issues, students that move in and out of the area on a frequent basis, and over supply of resident/coupon parking permits (if the council wishes to reduce the amount of cars, they need to limit the amount they issue, more so for property's that have off street parking available).

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Strongly oppose

Do you have any comments to make about the proposed design?

It appears not enough consideration has gone into the impact on different households. Ie, anyone with mobility issues along the Wills/Aro and Aro/Holloway Road ends both sides of the street will not have any parking access. We also have a lot of student flats in Aro valley, i am unsure where moving trucks will be able to park, because of supply and demand off parking would mean no one will want to give up parking unless they had to. I dont think the Council should give any household resident parking if they have off street parking available. Limit each household to 1 permit her address to encourage less vehicles.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Don't know

Do you have any comments to make about the proposed design?

Aro Street to Willis Street link: What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

Negative

People walking on the paths through Aro Park and Little Palmer Street?

Very negative

People riding bikes along this route?

Don't know

People driving vehicles on the surrounding streets?

Don't know

People with disabilities or accessibility issues?

Negative

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Strongly oppose

Do you have any comments to make about the proposed design?

Bikes should not be on the footpath. I have nearly been taken out on the Little Palmer Street path by bikes due to the blind corner at both ends (widening the path would not solve this issue). I would advocate for bikes to be barred from using the Little Palmer Street path.

What do you think of the timing of the proposed approach?

Don't know

Why do you think that?

Feedback**275**

114170505930

NAME: Ben	SUBURB: Wellington Central	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Neutral**

Decision made by Waka Kotahi

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?**Strongly support****Do you have any comments to make about the proposed design?****Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?****Strongly support****Do you have any comments to make about the proposed design?****Aro Street to Willis Street link: What do you think the impact of widening the path would be for:**

People using Aro Park for recreation (e.g. picnicking)?	Neutral
People walking on the paths through Aro Park and Little Palmer Street?	Very positive
People riding bikes along this route?	Very positive
People driving vehicles on the surrounding streets?	Positive
People with disabilities or accessibility issues?	Very positive
Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?	
Strongly support	
Do you have any comments to make about the proposed design?	

What do you think of the timing of the proposed approach?
Make the proposed changes more quickly
Why do you think that?

Feedback**276**

114165214585

NAME: t peters	SUBURB:	ON BEHALF OF: delivery drivers	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

How to kill a village in one misguided swoop

Do you support the proposed speed changes on Aro Street?**Oppose**

Decision made by Waka Kotahi

Feedback**277**

114173539633

NAME: Nathan Hall	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**278**

114181454624

NAME: Andrew Sussman	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Safer, future focused commuting. Saving lives and the environment. Riding aro is quite scary at the moment

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**279**

114179203843

NAME: Abir Rubel	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

It is not quite efficient to use that road for cycle lane, private vehicles, bus and also parked cars on the street as the Aro Valley road is narrow winding and uphill. It will be only creating congestion. There could be other alternative routes for cycle lane which should be spacious and flat so that everything moves smooth and fast.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**280**

114178646362

NAME: Alan Heays	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Biking and e-biking is an attractive option for me, but a little hairy right now, particularly going up hill. The proposed changes would help with that.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**281**

114171182804

NAME: Andrew Williams	SUBURB: Southgate	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

A cycle route on the uphill side will make it much safer for cyclists and much less frustrating for motorists

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**282**

114164418015

NAME: Alecia	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I strongly support the Aro Valley connection route - Wellingtonians need better low carbon options for getting around, and making cycling safer is a key part of this. Cycling on this route is currently very dangerous due to the narrow and steep nature, so specific bike lanes are a very good idea.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**283**

114160217721

NAME: Alessandro Dal Sasso	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I believe the proposal makes bike commuting safer for everyone.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**284**

114164042031

NAME: Alex Saunders	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**285**

114169120598

NAME: Alex Gray	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I think it's unnecessary and removing car parking in this area is unwise

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**286**

114167166345

NAME: Keys Family	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Because there is a serious lack of parking currently and reducing parking is not a good answer.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback

287

114160497568

NAME: Alex Brocklehurst	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I support better options for walking, cycling and public transport, and wish to discourage private car usage

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**288**

114160159014

NAME: Alex Kinzett	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**289**

114178552560

NAME: Alice Coppard	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**290**

114169231487

NAME: Alison	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

At present we run the gauntlet of parked cars, buses, and cyclists using Raroa Rd - it feels unsafe even in a car - so anything to reduce the obstacles on this course will be welcome.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback

291

114170642294

NAME: Alistair	SUBURB: Wilton	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

As a regular user of the road (bus, bike and car), I really do not like the current layout. It suits no-one!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**292**

114181087637

NAME: Alley	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

The removal on parking along Aro Street will affect the entire business community here. There is already extremely limited parking and by taking that away you will discourage people from the area. Cafes, breweries, retail that are all in the area will be affected. The community will not be what it is, it will die.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**293**

114172976216

NAME: Andrea Knox	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I cycle through Aro Valley regularly (and drive occasionally) and it's super-dangerous on a bike and tricky to navigate in a car. These changes will go a long way towards fixing that. I hope they can be made before a cyclist dies.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**294**

114171605816

NAME: Andre Geldenhuis	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

It looks great, a partially separated cycle lane will make people on bikes feel substantially safer and will help increase biking rates over time.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**295**

114169110446

NAME: Emma Barnes	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I just want it to be safer for bikes, scooters and pedestrians. Lower speed is great! It looks like a reasonable way to manage it all.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**296**

114160265117

NAME: Gus	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I've been knocked off my bike before, which meant I stopped commuting by bike

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**297**

114181651368

NAME: Anke	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is a crucial route for people travelling from Wellington's largest suburb, Karori, into the city, including hundreds of young people on their way to College. If this route can be made safer for cyclists, we could reduce the number of cars pouring through the qAro Street community and into the city. Ebikes solve the hill issue. Due to current safety concerns along this route, my son cycles to College for training early on Sunday mornings, but not during busy peak traffic times. I would love this to change, for everyone involved. My good friend, a nurse, cycles to work at hospital daily, but is shaken due to her husband being injured by a truck on this route. Enabling people to choose active transport will be good for the whole of Wellington, as every car that doesn't travel to the city during peak times improves congestion, air quality and climate, and makes travel safer for other modes. It may take a little time for people to adjust, e.g. have the confidence to start cycling in Wellington, but we need to create the conditions to enable this change first. Thank you for this important mahi.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**298**

114168712483

NAME: Anne Jacobsen	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I think that these changes would open up affordable and safe opportunities of travel for many people leading us towards a more sustainable future.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**299**

114179248664

NAME: Alice Donnell	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

These roads are incredibly narrower as they are and there is little parking available. Those of us who are elderly can only travel by the infrequent bus service with its high steps or by motor vehicle and we will no longer be able to visit the Aro Street bakeries . as you are reducing the already scaqre parking. We will have to take an alternative route into town and pity on those who already live in Aro street and Raroa road,

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback

300

114164025018

NAME: Anne Heins	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I bike along here and it is uncomfortable and nerve-wracking - and I am an experienced cyclist!!
We need to make this a safe and comfortable route for EVERYONE, not just the 'hardcore' among us

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**301**

114179735363

NAME: Ant Burt	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

As a cyclist separate spaces, not just paint marking, make a big difference to our safety. A lot of motorists are impatient with cyclists and will not willing give that space putting cyclists at risk and cutting us off

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**302**

114173811063

NAME: Jane	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

stupid idea - all the people in the comments for the proposal don't live in Aro Valley.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**303**

114171158870

NAME: A Stillaman	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I'm not against cycle lanes or some of these ideas but question why this is being installed along major thoroughfare routes where use of the road is already under intense pressure, instead of encouraging alternative routes to and from the city. I'm not going to cycle most of the time as it's frequently too windy, or just cold and wet. You get saturated and dirty when cycling! These particular roads are narrow and winding and busy, and trying to turn them from a two lane road into a three lane road will always be problematic. The impact of loss of parking cannot be underestimated or brushed over for residents. There are loads of pedestrian access paths in this area, that should be better maintained and made safer as alternative routes. It's a more long-term vision for the city, but I'm more in favor of looking into access ways for cycling and walking that don't just use the same major roads, and the value that would bring for providing low-cost and low-carbon alternatives to move around (which appears to be the overarching objective). At this specific moment though, I would rather our infrastructure and water was invested in as priority. Plus we should be preparing the city now for charging electric vehicles, e-bikes etc. Maybe incentivizing/helping residents to install solar panels, or proving free charging stations, that sort of thing... Electricity is limited and very expensive and our use of it seems set to increase. Thank you for providing opportunity to respond.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

304

114181700328

NAME: A. G.	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

If it makes it easier for ppl to move around without cars, removes current ind car parking and congestion, then it's a great idea.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**305**

114161610928

NAME: Brad	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The changes are a better use for this road. It is poorly suited to the current set up of on street parking with dual lane of traffic. Bike lanes provide safer options.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**306**

114181460094

NAME: Ben and Charlotte Darlow	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

arovalleyconnections_Darlow_221120.docx

SUBMISSION FROM BEN AND CHARLOTTE DARLOW OF 26 PALMER ST IN RELATION TO THE ARO VALLEY CONNECTION PROPOSAL

ARO VALLEY CONNECTION

1. Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

Oppose

The route that goes across the Park, through the community centre, down Palmer St, across the footpath, along little Palmer St, across Abel Smith St footpath then the street, down Inverloch Place, round the side of an apartment building through a narrow alley, and then along Oak Park Avenue is tortuous, has multiple bottlenecks, poor visibility at merge points, dangerous road crossings (when merging onto Abel Smith St travelling towards the city or merging onto Aro St when travelling toward Raroa Road) with poor visibility for riders and drivers, and puts riders and walkers into direct conflict. Most riders following this route are doing so to get somewhere rather than to enjoy the ride so the speeds at which they need to travel are not amenable to shared cycle and walkways. The plan seems to suggest that cyclists will travel up and down Abel Smith St and take the path west of SH1 between Abel Smith St and Oak Park Ave. Observation of current cyclist behaviour clearly shows this route (currently available) will not be followed. It is too slow, particularly with a need to wait for the lights when travelling north to south.

A much more sensible option would be to continue the east to west sharrows to the bottom of Aro St, then construct a cycle lane along Willis St that joins with the existing path that runs west of the State Highway 1 on ramp and joins with Oak Park Ave. This is a much more efficient route to cycle and (with appropriate design, cyclist protection, and cycle priority over road traffic turning left into Palmer and Abel Smith Streets) would be quicker and easier than the route proposed (and so would be used). Such a route would also connect much more effectively with the Karo Drive cycle way, improving connection with existing and well used infrastructure.

The west to east section of Aro Street already has a no stopping zone between the crossing at the eastern end of Aro Park and Willis St, meaning there is reduced risk to cyclists from parked cars. The footpath is very wide at this point, so could be narrowed to allow a separated bike path. A free left turn (that does not require giving way to car traffic on Willis St but does require giving way to the pedestrian crossing) would be cheap and easy to install, as would a cycle way along Willis St. This would require the removal of relatively few car clearway parks (that frequently disrupt rush hour traffic when people do not respect the parking times) and a slight narrowing of the existing (moderately wide) footpath. The 10 minute parks outside 292 Willis were formerly required for dairy customers, but this has since shut down (the 60 minute parks at the bottom of Palmer St could be converted to 10 minute parks). Ideally, the ability for cars to turn from Karo drive onto Abel Smith St would be blocked (instead they could go around the Victoria Street and Webb Street routes already used by those driving to Aro or Palmer Streets). This would enable the cyclist (and pedestrian) route across the bottom of Abel Smith St to be green most of the time, improving route efficiency and cyclists' motivation to use it.

Many cyclists coming from town to the Valley already come up Victoria St, but have difficulty efficiently getting to Aro St and use a combination of riding on the footpath on the eastern side of Willis St to the crossing, or riding up the footpath on the western side of Willis St to Palmer St, or going up Abel Smith St and through Little Palmer. All of these routes bring them into conflict with pedestrians, diminishing safety for both groups. A north to south cycleway on the eastern side of Willis St between Abel Smith St and the Aro St pedestrian crossing would solve this problem and also provide an efficient route for those travelling north to south down the pathway from Oak Park Avenue.

If this solution were instituted, the path across Aro Park, the route between the community centre and the preschool, the footpath on Palmer St, the path down Little Palmer St, and the path between Inverloch Pl and Oak Park Avenue could be pedestrian only, with instructions for cyclists to dismount and speed limits for e-scooters. This would make the route substantially safer for pedestrians, particularly children and those with reduced mobility. This is a route with heavy foot traffic during busy transport periods, and a large number of children, commuting both to Aro School and to the Aro Valley Pre-School.

We also oppose the installation of speed bumps on Aro Street. Experience as cyclists demonstrates that these have a concertina effect on cars and causes unpredictable rates and timing of braking. A consistent speed of 30km an hour on the downhill lane will be much safer and more efficient than constant braking and acceleration.

CHANGES PROPOSED FOR THE RAROA ROAD SECTION

2. Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

Strongly support

This route is currently very unsafe for drivers and cyclists. These changes appear to strike a pragmatic balance between improvement and feasibility.

CHANGES PROPOSED FOR ARO PARK AND LITTLE PALMER STREET

3. What do you think the impact of widening the path would be for:
 - a. People using Aro Park for recreation (e.g. picnicking)? **Very negative**
 - b. People walking on the paths through Aro Park and Little Palmer Street? **Very negative**
 - c. People riding bikes along this route? **Neutral**
 - d. People driving vehicles on the surrounding streets? **Negative**
 - e. People with disabilities or accessibility issues? **Very negative**
4. Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

Oppose

We have lived in ■ Palmer St (which borders Little Palmer St) for 14 years. Our back access gate opens directly onto Little Palmer Street. We have used the route on a daily basis for cycling, walking, and for our children (including when they were toddlers and pre-schoolers). We will be directly affected by these proposed changes and are also likely have more understanding of the issues and opportunities than most others. As a directly affected party, due to our immediate proximity to Little Palmer Street, we are surprised and disappointed that no one who is involved in proposing these changes has made any attempt to contact us and discuss the proposal, including the direct impact on our property. They appear to have given very little thought to the complexities of this route and current problems that will not be addressed by these changes (and may be aggravated).

We absolutely agree that Little Palmer St needs to be improved and upgraded. The surface is poor, representing a tripping hazard, and the sealed path is too narrow even for passing pedestrians with child buggies. As mentioned above, we do not think that the proposed cycle path route is close to the best solution. It is the current defacto best path for cyclists because the route down Aro St and along Willis St to the city is currently inefficient and unsafe for cyclists. It would be much better, safer, and likely cheaper, to improve this connection rather than funnel more cyclists through the park, past the community centre and through Little Palmer St. This route has numerous conflict points and we have seen numerous collisions and near misses, including with small children. Our sense is that these dangers will be aggravated rather than improved by the proposed plan.

We hope that the Council will do further work on the proposal to deconflict the routes and maximise transport efficiency. However, if the Council decide to proceed with this proposal, then from our perspective, there are important considerations that need to be addressed:

- 1) The garden space on the eastern side of Little Palmer St needs to be maintained. At times when this has been unplanted, graffiti, vandalism, and accidental collisions with our house have substantially increased.
- 2) There needs to be safe exit from the gates to 26 Palmer St and 127A Abel Smith St into Little Palmer St.
- 3) The run off from the path into 26 Palmer St and 127 Abel Smith St needs to be improved not aggravated.
- 4) The gardens on both sides of the sealed path need to be lowered to enable these to absorb run off from the lane and stop pushing up against the weather boards of 26 Palmer St.
- 5) The edges of the path need clear hard margins to stop creep of the gardens into the path or people moving along the lane running into the garden.
- 6) The access to water and sewer pipes that run under the garden on the eastern side of Little Palmer St needs to be maintained (these old pipes have required frequent work, including digging and resurfacing of the laneway).
- 7) The two large pine needle trees at the north end of the garden on the eastern side of Little Palmer St need to be removed or they will continue to disrupt the surface, invade the pipes, and limit the width.
- 8) Mirrors need to be installed at both ends to allow visibility around the corners.

9) Speed limit signs need to be installed at both ends.

10) Cameras need to be installed to identify and ticket motorised scooters and motor bikes that illegally use the path.

11) Proper safety lighting needs to be installed along the laneway.

12) A pedestrian crossing needs to be installed between the northern end of Little Palmer St and 148 Abel Smith St so that children can cross safely on their way to Aro School and so there is improved driver attention to cyclists crossing Abel Smith St here.

13) Individual parking spaces need to be marked outside 26 Palmer St with yellow cross hatching outside the garage to prevent people squeezing more than two cars in and blocking the garage.

We note that further consultation with the community on the impacts for Aro Park may be required, as undertaking work to alter the pathway through Aro Park is also in conflict with the community aspiration to restore the Waimapihi Stream as it flows through the park.

STAGED APPROACH FOR ARO VALLEY CONNECTION

5. What do you think of the timing of the proposed approach?

Make the proposed changes more slowly

We are very supportive of cycling and walking as healthier and more sustainable means of transportation and exercise. We also believe that storing private vehicles on public roadways is a very poor use of valuable public infrastructure. However, we recognise that our views on this point may differ from many of those within our community and that many people rely in kerbside parking to be able to participate in their day-to-day lives, particularly those who have limited mobility.

We agree that car sharing rather than private car ownership will be the way of the future, but this transition will take some time. Irrespective of this, one more car share space on Epuni St is unlikely to be sufficient for everyone who is losing parking outside their homes.

We recognise the need to bring the community with us on this journey and are concerned that this proposal risks dividing our currently strong but diverse community in the way it did Island Bay, pitting cyclists against drivers and residents. We would rather have a poor transport solution than a divided community and long-running litigation.

In our view, more time and methods for consultation with the community and impacted parties is needed. The Aro Valley is a community that needs a long time to come to consensus on change, but it is much more efficient to spend time achieving this than pushing forward with divisive changes and short time frames.

6. Do you support the proposed speed changes on the Aro Valley Connection route?

Strongly support

The current approach of people slowing through the shops, then accelerating toward the Ohiro Road intersection and the Willis St intersection is unsafe and inefficient.

YOUR RELATIONSHIP WITH THE AREA

7. What is your main relationship to the area?

We live in the area

8. How do you normally travel along the Aro Valley route? Please tick one:

Bicycle

CITY GOALS AND NETWORK QUESTIONS

9. How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use

Very important

WOULD YOU LIKE TO SPEAK TO COUNCILLORS IN SUPPORT OF YOUR SUBMISSION?

Yes – ☐

Providing feedback as an individual

OUR DETAILS

Names: **Ben and Charlotte Darlow**

Email:

Suburb: **Aro Valley**

We would like to receive email updates about this project

☐

Male and female

We do not live with a disability and identify as New Zealand European.

Feedback**307**

114174805013

NAME: Ben Evans	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Not allowing parking on Aro Street will mean that those of us that actually live in the area will struggle to have trades people deliveries or any access via vehicle to our homes extremely diminished so that people in Karori with their large houses and off street parking are less inconvenienced while riding through our suburb.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**308**

114181474637

NAME: Benjamin Swale	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Although I mainly drive through this area and visit Waimapihi Reserve, I strongly support making this area safer for cyclists. In fact, cycle lanes would actually make me as a car driver more relaxed through this winding bit of road too. One day when there's linked up network from home in Karori that makes it safe enough, I hope to be able to cycle over to here too. Great work WCC!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**309**

114172626995

NAME: Elliot Blyth	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

It's great that the area will be safer to access by bike. Some areas seem slightly questionable for cyclist safety still, after proposed changes. It's also critical to massively change the intersections leading into and out of Aro Valley as that is currently very scary to bike through! Doesn't matter how good Aro is if you can't safely get to it.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**310**

114162970380

NAME: Robert Clegg	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Neutral

Why do you think that?

Only concern is removal of resident carparks in Raroa Road. Has any consideration been given to goods delivery in those affected areas? Have you allowed for say, spaced out loading zones. E.g. What happens with the next Covid lock down and people are relying on grocery deliveries?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**311**

114161500734

NAME: Luca	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

It looks good. This corridor does have major constraints in regards to width. It would definitely be easiest to have a shared path through Aro Park compared to running it through the rest of Aro Street. Based on my observations, people do travel at the 30km/h design speed. But more traffic calming measures would be welcome as more people travel through here by active transport.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**312**

114166370037

NAME: Bronwen Wall	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I ride all over Wellington, and my interaction with motorised transport can be challenging.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**313**

114169040015

NAME: Paul Bruce	SUBURB: Brooklyn	ON BEHALF OF: Our Climate Declaration	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

These changes to make cycling safer and more attractive are vital for us to live within our carbon budget and improve the well being of everybody. Vehículo drivers have to accept slower speeds and more restricted space, but there are advantages to that too, with fewer accidents, less pollution, and less vehicles on the road as cycling becomes more attractive

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**314**

114169158265

NAME: Buda Szerelem-Tolnay	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

e are going to lose parks down one whole side when we already have yellow lines on the other side, in a neighborhood where most houses have no off street parking. There will be nowhere to park our own cars, nowhere for visitors to our homes to park, nowhere for service people or builders to park if you have a leaking pipe or work needing doing.

Do you support the proposed speed changes on Aro Street?**Oppose**

Decision made by Waka Kotahi

Feedback**315**

114169285533

NAME: Cindy Jemmett	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**316**

114181661382

NAME: Andrew Calder	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I have had one accident and several near misses on or around Aro St and Raroa road. It is too narrow and busy a road for safe riding particularly at busy times and with larger buses now more frequent on the route. I know the removal of parking will be upsetting but I think the issues will only get worse with the volume of traffic trying to use Aro St as a route to and from Karori etc into town. I think a lower speed limit for the whole of Aro valley all the way to Raroa road would be appropriate.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**317**

114178599177

NAME: Cam Macduff	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I think investment in bike lanes is folly given the need for better public transport. Sort public transport first. Having better public transport will remove more cars and the busyness of the city. It'll reduce emissions assuming you go electric or hybrid - that'll also make it quieter. Then you have a better idea of if you even need to 'rebalance' the roads or if investment in bike lanes is needed across major suburbs.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**318**

114165541797

NAME: Cameron Trauger	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Living on Roroa Crescent, I feel that this area is hostile for pedestrians and dangerous for cyclists - I strongly support interventions that reduce cars flying around blind corners, and allow people to safely walk and cycle.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**319**

114161527976

NAME: Carl Blackmun	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

320

114181551810

NAME: Laurence Diack	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I cycle this route on occasion

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback

321

114168738813

NAME: Catherine Hay	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**322**

114181555581

NAME: Cathy Dean	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

I cannot believe that there are enough cycle users in our hilly city to support such drastic changes on roads that are already too narrow and windy.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**323**

114180780079

NAME: Cathy	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**324**

114160367049

NAME: Chelsea Grant	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**325**

114181559110

NAME: Claire	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I support sustainable transport options, but this project has not been thought through properly. It needs more thorough study and consultation, especially with Aro Valley residents.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback

326

114161196081

NAME: Chris Pearce	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I strongly support because the proposed changes improve walking and cycling but also recognise that this is still an important and busy arterial route.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**327**

114160311319

NAME: Chris	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I'm an experienced cyclist (10,000kms+ per year) who regularly uses that route. I'm in favour of uphill bike lanes and downhill sharrows. I prefer this type of design - minimal separation, green lanes and plastic bollards. I've had no problems up there though it is quite narrow in parts and these bottlenecks are what cause most of the frustration for all road users.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**328**

114181671240

NAME: Chris Ford	SUBURB:	ON BEHALF OF: Disabled Persons Assembly NZ	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

DPAroValleyConnectionDraftSubmissionv1.docx

Disabled Persons Assembly nz



November 2022

To Wellington City Council

Please find attached DPA's submission on the Aro Valley Connection

Disabled Persons Assembly NZ

Contact:

Chris Ford

Regional Policy Advisor

[Redacted]

[Redacted]

Introducing Disabled Persons Assembly

We work on systemic change for the equity of disabled people

Disabled Persons Assembly NZ (DPA) is a not-for-profit pan-impairment Disabled People's Organisation run by and for disabled people.

Since our formation in 1983, DPA has brought disabled people together and shaped our collective input in a way that drives system level change.

We recognise:

- Māori as Tangata Whenua and [Te Tiriti o Waitangi](#) as the founding document of Aotearoa New Zealand;
- disabled people as experts on their own lives;
- the [Social Model of Disability](#) as the guiding principle for interpreting disability and impairment;
- the [United Nations Convention on the Rights of Persons with Disabilities](#) as the basis for disabled people's relationship with the State;
- the [New Zealand Disability Strategy](#) as Government agencies' guide on disability issues; and
- the [Enabling Good Lives Principles](#) and [Whāia Te Ao Mārama: Māori Disability Action Plan](#) as avenues to disabled people gaining greater choice and control over their lives and supports.

We drive systemic change through:

Leadership: reflecting the collective voice of disabled people, locally, nationally and internationally.

Information and advice: informing and advising on policies impacting on the lives of disabled people.

Advocacy: supporting disabled people to have a voice, including a collective voice, in society.

Monitoring: monitoring and giving feedback on existing laws, policies and practices about and relevant to disabled people.

The submission

DPA is providing this submission for the benefit of the Wellington City Council in its consideration of the Aro Valley Connection Project.

Firstly, DPA is pleased to see the following proposals:

- New street layout with a separated uphill bike lane on Aro Street and Raroa Road
- Extended 30km/h zone on Aro Street through to Willis Street
- New raised pedestrian crossing by Aro Park and four speed humps introduced on Aro Street
- New car share parking spaces on Epuni Street
- New mobility car park on Epuni Street
- Changes to parking on Aro Street, Raroa Road and some side streets.

Secondly, DPA recognises the lengths that Council have gone to in order that the project is well consulted upon. We were pleased to be approached regarding this and would welcome the opportunity to participate in offering feedback on future projects. Our brief submission makes some recommendations around changes that could be made to make the Aro Connection more accessible, inclusive and safer for everyone, including disabled people.

Little Palmer Street Path Widening and Wayfinding Signage

DPA does not support the concept of establishing a shared pathway for both cyclists and pedestrians. Instead, we support the creation of more separated cycle ways, such as that proposed on Aro Street and Raroa Road. This would enable pedestrians travelling either, for example, via foot or on a mobility device such as a wheelchair to safely navigate within the space without fear of collision.

Around the issue of wayfinding signage, DPA recommends that it be developed in a way where print, height and colour contrast are fully considered, especially for blind

and low vision users. We would like to encourage the development of signage in accessible formats as well (i.e., in New Zealand Sign Language, Easy Read, Te Reo and ethnic languages) which can be done via the use of electronic apps where people can access this information via a QR code. This would help make the signage clearer, accessible and more visible for everyone.

DPA's recommendations

- **Recommendation 1:** DPA does not support the concept of establishing a shared pathway for both cyclists and pedestrians. Instead, we support the creation of more separated cycle ways, such as that proposed on Aro Street and Raroa Road.
- **Recommendation 2:** That wayfinding signage be developed in a way where print, height and colour contrast are fully considered, especially for blind and low vision users. We would like to see the development of signage in accessible formats as well (i.e., in New Zealand Sign Language, Easy Read,) which can be done via the use of electronic apps where people can access this information via a QR code.

Feedback**329**

114166750773

NAME: Chris Rosser	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

The changes overall look good for cycling, but it may not solve the problem of dangerous overtakes on Aro Valley uphill.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**330**

114174603854

NAME: Christine Hewitt	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Don't know**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**331**

114171231042

NAME: Nat	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Better safe and separated cycle ways

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**332**

114163969309

NAME: Christian Hoerning	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The current lack of cycling infrastructure not only in this part of our city is a real barrier to more people choosing active, healthy and environmentally transport. High time for this to change! It will help Wellington to become a safer, healthier and more fun place to live and work.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**333**

114160218177

NAME: Christina Mitchell	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Why do you think that?

The plan is daft because the council is going about the whole problem back to front. Taking away parking is not going to encourage people to get rid of their cars. It's only going to cause more frustration and distress. WCC should start with providing a great public transport system.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**334**

114181242022

NAME: Mark Coburn	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

335

114175901377

NAME: Craig Starnes	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I pedal up Aro St most evenings and it's a safety challenge due to narrowness - mainly to opposing traffic from vehicles overtaking me.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**336**

114164529096

NAME: Jarrod Crossland	SUBURB: Maupuia	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

The staged approach is too long and exposes vulnerable Road users (pedestrians and cyclists) to dangerous conditions. The full plan should be implemented first, and tweaked as we learn and adapt

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**337**

114168709715

NAME: Diego Villalobos	SUBURB: Owhiro Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Neutral**

Decision made by Waka Kotahi

Feedback

338

114164065192

NAME: Dan	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**339**

114181480472

NAME: Daniel	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**340**

114164300829

NAME: Danyon Graham	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**341**

114181591282

NAME: David	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The proposal fails to address impacts on Aro valley residents for the benefit of very few cyclists. Surely a greater weighting should be given to those who are affected the most and not to those who use Aro Street as a rat run. How will deliveries be made? Where will tradespeople park when working on projects or doing maintenance? Where will skips be placed? A better option would be to make the Southern footpath a shared cycle way and keep the parking. Where will the cars that currently use the South side of Aro Street park? Your parking survey is a very short ill-considered piece of work. It needs to be far more detailed in its scope and extent. How will the elderly and the disabled get to and from their houses? Putting a Disabled park in the middle of the village wont help those who need access to the end of the street.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**342**

114164207099

NAME: David	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

In a city that is failing to offer basic public transport, the least that can be done is to provide a safe space for people who choose to ride a bike as their form of private transport.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**343**

114171330439

NAME: Debbie Stowe-Hunt	SUBURB: Crofton Downs	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

344

114182428156

NAME: Denise Duke	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Denise%20Duke%20Submission.docx

To whom it may concern

I am exceedingly alarmed at the proposal to put a bike lane up Aro Street removing a large number of car parks.

I feel this ignores the needs of the properties that line both sides of the street.

Most of these properties do not have off street parking. Therefore they will have huge issues accessing their places particularly if they are elderly, have mobility issues or young children.

Other vehicle needs such as ambulances and trades people will be similarly disadvantaged.

Was this thought though?

I am a cyclist myself but feel outer suburbs - Kelburn , Karori etc could be better served by a cycle lane down Kelburn Parade past the University where the impact on local residents wouldn't be so significant.

Yours sincerely

Denise Duke

Feedback**345**

114161503358

NAME: Denise Stephens	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The changes will improve pedestrian safety by providing space for cyclists and scooters on the road. With the increasing numbers cycling and scootering, this is useful and welcome.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**346**

114170392738

NAME: D. Pedlow	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**347**

114164335173

NAME: Digby	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

348

114181458963

NAME: Philip Dinniss	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

PJ%20Dinniss%20Aro-Valley-Connection%20Submission%20form.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Aro Street and Raroa Road are a critical link between Highbury, Kelburn and Karori and the motorway and southern end of the city and the airport. Unfortunately neither Aro Street nor Raroa Road have the road space to accommodate a third mode of transport; both are too narrow and the proposal has no regard for the needs of existing residents along the route and others who will use it.

Existing property owners on both streets will see values depreciate when coming to sell properties as on-road resident or visitor parking will be almost impossible making the area impractical and unattractive for young families and difficult for older residents.

The route is the main access for dump trucks hauling from some western suburbs to the landfill in Happy Valley. The road is so narrow that in places buses travelling in either direction have to "stop and fill" to pass.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Cycle usage on the Raroa Road part of the route will have minimal impact on emissions in the city. The exposed nature and gradient of Raroa Road is such that it is doubtful many more cyclists than the handful who at present use the road will be attracted even by a cycle lane.

The audit report of the design implies meeting the bare minimum of lane dimensions; cyclists need to recognise the risks they will be exposing themselves to.

Having removed all the parking on the uphill lane, by the time all the corners are yellow lined both sides there will be scant parking on the downhill lane too. Many houses on the downhill side have off street parking but several on the uphill side have none. The proposal will make it almost impossible for people to have friends or families to their homes as they won't be able to park their electric cars.

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive☐ Positive☐ Neutral☐ Negative☐ Very negative☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive☐ Positive☒ Neutral☒ Negative☐ Very negative☐ Don't know

People riding bikes along this route?

☐ Very positive☒ Positive☐ Neutral☐ Negative☐ Very negative☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive☐ Positive☒ Neutral☒ Negative☐ Very negative☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive☐ Positive☒ Neutral☒ Negative☐ Very negative☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support☐ Support☒ Neutral☐ Oppose☐ Strongly oppose☐ Don't know

Do you have any comments to make about the proposed design?

There is an programming eror in the multi choice lines above: ticking neutral also automatically ticks negative and vice versa..

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

This is a project that looks good on paper but seems to ignore the reality of the topography and the climate. It appear to be based on the "build it and they will come" mentality. As a grandparent I would discourage teenagers using the Raroa Road route, let alone primary aged children. Nor would people in the older age brackets use it. Having discussed it with several friends who are keen cyclists with electric bikes, they agree it is just not a suitable road for cycling, cycle lane or not.

Aro Valley may benefits from the proposed changes.

If it proceeds and cyclists do not use it, it will be a huge waste of the city's scarce financial resources.

Removing parking in Raroa Rd will exacerbate already crowded parking in surrounding streets - Norna Cres, Mertoun Terrace, Highbury Road, Zetland Street, most of which are accessible by steps from Raroa Rd.

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

The proposal has no regard for the interests of the residents and property owners along the route who are not cyclists. There is no cost benefit analysis that looks at the entire equation; the Council's Transition Cycling group sponsoring the proposal quotes Waka Kotahi guidelines which show positive benefits based on statistics for cyclists but is silent on the effects on property values and dislocation for non-cycling residents.

Raroa Road is inherently dangerous for cyclists. Installing a dedicated uphill cycle lane which is intended to attract more cyclists will in fact expose more cyclists to accident risks. In places the lane widths are the bare minimum and when heavy trucks and buses are passing or traversing the sharp corners, regardless of road markings they will at times encroach on cycle lanes.

WCC data show that in the month of March there were approximately 200,000 motor vehicle movements and October data shows 4075 cycle movements (in Aro Street; no separate data is available for Raroa Road), ie cycle movements represent 0.02% of the total. There are no quoted measurable empirical benefits for the environment.

Building the proposed cycleway will not reduce vehicle traffic, that may only happen with improved reliability and accessibility to public transport; unlikely in the short to medium term, and the progressive transition of the private vehicle fleet to electric power.

The project cost of \$1.3 million may deliver benefits to the handful of hardy cyclists that take on the Raroa Road challenge, while creating significant disruption for many of the residents that call the road home.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

It is already a relatively low speed area with the Owhiro Road intersection and a pedestrian crossing in the short stretch of road.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☒ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☒ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number: [REDACTED]

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback**349**

114179509343

NAME: Tim Watson	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Feedback**350**

114178524076

NAME: Sarah	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Climate change is the biggest issue facing us, so we need to do whatever we can to promote better ways of travelling. Residents opposed to losing car parks need to realise that they are not entitled to park private vehicles on public roads. They have purchased/rented a home, not road space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

351

114176168784

NAME: Dan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Parking is already an issue in Aro Valley. No need to make it worse for a short term gain.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback

352

114161179635

NAME: Eamonn Marra	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It will make cycling and walking safer, public transport quicker. Reductions over time to carparks can be appeased by car sharing options in the area.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**353**

114161307308

NAME: Nell	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

We need to move away from our reliance on cars. Pedestrian and cycle friendly cities are better for everyone.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**354**

114174842070

NAME: Ella	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Totally impractical proposal, and nothing short of cruel for older/disabled residents. We already have yellow lines on one side of Aro St, and few residents have off- street parking. Leaving aside parking for residents, where will delivery/ taxi/ tradespeople/ emergency services stop?

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback**355**

114160286034

NAME: Emma	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Raroa road is very dangerous to drive, walk and bike on right now - these changes will really help. The bike parking in Aro valley village is also awesome - so many mountain bikers have to leave their bikes stacked outside the cafe right now.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**356**

114173697115

NAME: Ella Borrie	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I strongly support the redesigned streets, parking changes and new speed limits. For me, this connection would open access to Aro Valley, Karori and Zealandia on my bike. I would support the work happening at the same time, not transitionally, to deliver the benefits sooner.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**357**

114179632857

NAME: Ellen	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I think it will be really awesome! I walk through the area and it can be really dangerous crossing Aro street as drivers don't often stop at the crosswalks, even though kids use them frequently. The speed bumps and 30kph zone will be great to improve people's experience here. I have often wanted to start riding a bike into work, but I don't feel comfortable cycling through the area. With these changes, I could finally do that and get to work more easily. My one concern is with the bike shoulders - if cars are able to move into these lanes, then that sounds dangerous for people on bikes. The car share park is a fantastic idea as well! I have also looked in to one of these services so I could get rid of my car, however the nearest car share park for me is in Newtown, which isn't easy to walk or bus from. I think you might find you need to add more car share spaces after a little while because lots of people are likely to find this a more convenient and cheaper way of getting around.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**358**

114178464975

NAME: Emma	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Much safer for cyclists who use the route, encourages use of public transport

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

359

114161385896

NAME: Anon	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

360

114175439022

NAME: Emma Reid	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Biased and manipulative information to justify a bike lane removing the human rights, safety & servicing of the Aro Valley residents. A missed opportunity for better and more attractive public transport options for residents in the outer suburbs.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

361

114181455526

NAME: Esther Albon	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Esther%20Albon%20Submission.pdf

Esther Albon

[REDACTED]

Aro Valley

Wellington 6021

Submission Proposed Cycleway – Raroa Road, Aro Street

To whom it may concern,

I am writing to express my concern over the proposed cycleway down the Aro Street path. Although I am happy with the cycleway down Aro Street I am concerned with a few issues surrounding this.

As a resident of Aro Valley with a property that backs onto Holloway Road, I am concerned parking allocation of Holloway road. There are residents on Aro Street, who are losing their ability to have or park a car, Holloway Road should be resident parking only with a small allocation for P120 at the entrance. As you are aware there are a number of tracks in the area that are used by cyclists. By moving P120 parking past number 50 this is primarily going to be used by cyclists using the tracks. As far as I am concerned, they can bike to use the tracks. I know this because I live here and see the road several times a day. I know who uses these parks and who is likely to use them in the future, this should not be at the expense of the residents.

My second issue is Raroa Road, as a daily user of this road (I take my kids to school at Kelburn). This is in my opinion one of the most dangerous roads in Wellington. Yesterday as I was collecting my son from school, I witnessed a bus coming downhill in the middle of the road, with the white line running up the middle of the bus. I witness something like this almost daily due to the tight and windy conditions of the roads. The cycleways are going to encourage more bikes on the road, downhill as well as uphill, there will be a fatality if this road is not thought out properly.

My third issue is with the increase in bike traffic they will no doubt be moving onto the Willis Street and the Aro Street intersection. This is a hugely busy road and again with the increase in bike traffic, what safety initiatives are in place at this intersection?

My fourth and final issue is today I was driving down Vivian Street and witnessed a bike rider, a lady on a E-bike weaving through heavy traffic without a helmet. This too is a daily occurrence. What mandatory safety checks are in place for bike riders? There are no mandates for high vis, WOFS or something similar for bikes or accountability courses for bike riders who break the law. There are no safety campaigns or education for bike road rules in place. In my opinion, the majority of these daily bike riders come across as very entitled to "their ownership" of the road but do not want to address the issues of the bike riders that don't follow the safety guidelines. I propose some type of registration and checks as the bikes are vehicles entering the roads with a high number now being electric bikes and therefore should be treated as an electric vehicles with the same type of checks. It may even go some

way to covering some of the cost of building the cycleways, without weighing on the ratepayer, especially those who are losing their parking as a result.
Your sincerely,

Esther Albon

Feedback**362**

114174153096

NAME: Peter	SUBURB: Churton Park	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

363

114168774755

NAME: Phillip Bolton	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Waste of resources and money

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**364**

114165617047

NAME: Felix Mussell	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

365

114181686606

NAME: Finn	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Having a easier/safer route to get to school, would definitely make biking a far more popular mode of transport around school. So far I only use this route to e-bike to my training at school.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**366**

114178493964

NAME: Fiona McCarthy	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Neutral**

Decision made by Waka Kotahi

Feedback**367**

114164309469

NAME: Fiona	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Sustainable modes of transport are good for people's wallets, good for people's health, and good for the environment (we are facing a climate emergency). Many residents do not feel safe taking up cycling as a form of transport until they are given safe bike lanes. With Wellington's population growing, car driving is not feasible as the default transport method of the future. It makes sense to act now and prepare Wellington's streets for future growth.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**368**

114171340487

NAME: Frances Ross	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Triumph of ideology over commonsense. I understand the need to reduce emissions but the Aro Valley route couldn't be less suitable for a cycle way.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**369**

114181058263

NAME: Georgia	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

The parking is already restricted in the area, ride share parking is all well and good but when it comes to getting up for work at 3am and walking up the road in the dark trying to find a car to use or a park as a female I would feel unsafe especially as I either leave in the dark or return in the dark for work walking across the road to my car at 3am in the morning is scary enough along with if this is in the weekends the drunk people you pass you never know how someone's going to react

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**370**

114164408566

NAME: George	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The current route from Karori to Willis st (and back) via this route is treacherous. Raroa road is especially tight, far too many cars parked which impact free flow of traffic, leading to too many cars making high risk overtakes of people biking. In fact, from Karori to Willis st there really is nowhere appropriate for a car to overtake a bike in need. The road in Aro valley is also quite bumpy on the downhill, which is a little dodgy with kids on the back of your bike. Biking uphill would benefit from the bike lane, great idea. Such an exciting proposal, can't wait to see it in action. Bring it on!

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**371**

114170122119

NAME: Genie	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**372**

114170573701

NAME: Jo	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This will make it a lot safer for those who cycle and walk in the area, and hopefully will encourage more people to use active ways to get around.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**373**

114160271842

NAME: G B	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Parking space is already over constrained on Aro Street. The proposed changes further reduce parking. Before parking is reduced major investment is needed in public transportation to make routes run more regularly, more reliably (less skipped services, better on time performance), better public transportation interconnections between suburbs. Creating bicycle lanes is not enough to support people reducing car ownership.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**374**

114164396118

NAME: Grant Buchan	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**375**

114167191982

NAME: Hannah hopper	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**376**

114174173866

NAME: Hayley	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Having lived on Aro St, I think this is ridiculous. It is already near impossible to find a park near your own home and for tradespeople it is disastrous. Cyclists come en masse on weekends and take up every park available, meaning residents can't leave their houses by vehicle for fear of not being able to park anywhere within Aro Valley upon their return. Nobody bikes up Raroa road anyway, it is steep and difficult - save a few elite cyclists and ebikers. this proposal is clearly in favour of the cycle elite at the detriment of residents - particularly the disabled and tradespeople

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**377**

114162989521

NAME: Adam Hayman	SUBURB: Melrose	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I would prefer cycle lanes on both sides.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**378**

114181702876

NAME: Heather Mackay	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

It is not only about how people get from A to B but about the people that live in Aro Street and environs. Must suit all people. Please protect the planted areas on Aro Street and the trees. Also where is a long term parking solution if you need to maintain a car for occasional use to go out of town? That would be very useful for residents with no garages. Very keen to slow the traffic in Aro Street but other changes need to be monitored for usefulness... not just for those travelling through but for everyone. We are not just a transit route.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

379

114165672641

NAME: Helen Jurie	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Aro Street too narrow and Raroa Road too steep and curving with many “blind” spots

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**380**

114170548454

NAME: Hilary	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**381**

114178569959

NAME: Holly Foran	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Pleased with proposals to make cycling safer and more attractive, and ease bus journeys. Strongly support removing car parks to do so. It will be essential to ensure the bus network is working well so that residents can get on with fewer cars

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

382

114181689653

NAME: Suzanne Holmes	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

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**Aro Valley Connection Project
Wellington City Council**

18 November 2022

[REDACTED]
Aro Valley
Wellington 6021

Dear Jonathan, and City Design Team,

I watched with great interest, your recent web seminar on the cycle way proposed for Aro Valley. With population growth, the use of Aro Valley as an arterial route does prove to be a dilemma. I guess it would be out of the question to divert commuters away from such a tight passage, that is obviously not practical to widen or give preference to cyclists over residents? Please hear me out.

Within your school of thought, I agree heartily with the idea of further slowing 'through' traffic. At the very least, create some appropriate safety for residents of Aro Valley, using speed humps in the village area. Other presented cycle way plans do concern me. Judging by your final poll the night of the seminar, these concerns are shared by a significant percentage of the Aro Valley community. Your web seminar presentation team seemed surprised by those concluding poll results. Therefore, it is fair to say the research of our Aro Valley community is incomplete.

One major concern arose when you casually mentioned, Jonathan, there would be loss of 70 residential car parks, of which the displaced people could park on a side street. I live in a flatting situation. We do not have any off-street parking, as is with most of Aro Valley. In my three years residing here, I have often needed to park on a side street a distance from our home. I count myself lucky to get a park, even then. I hope you will understand from this, there obviously is already barely any parking on the side streets. Your proposal sells with the positive words "opening our street for people of all ages and abilities". If I am feeling slighted here, I wonder about the elderly and disabled who need safe access as close as possible to their property? You are good people whom we voted to represent us, please fully put your feet in the shoes of the Aro Valley residents as well as on the pedals.

Aro Valley has become my home and provides a peaceful haven after many stressful day within an extremely short staffed health profession. While I often walk half an hour to the hospital and partly bus to some other required work locations, other mornings there is a sudden emergency call asking me to immediately travel further afield to such a distance as Johnsonville. My workplace requires personal car use. There is no time for public transport or cycling on such occasion. A car-share scenario is not an option for our household. "Connect the places where people want to go" does not ring true here.

I'm interested to know if you are you proposing that myself, as merely a tenant, be the one to move house? As a result, will you take care enough to find me a space in a Council flat, and displace another person to do so? Because that is all I would be able to afford on my present income, which is supplemented by growing vegetables on this land where I reside. These are all important questions that come to mind, and have brought me anxiety the past week since hearing your proposal.

I worked for the Far North Environment Centre for two years, over a decade ago. My role was educating the community about lowering their ecological footprint, before recycling even became fashionable. I fully understand the sudden push you are making here, for Zero Carbon emissions. However, I am curious whether your design team and council members are walking the talk here too. Would you be brave enough to sell your only car, or do you also need it for emergency and holidays?

Thank you for considering our feedback on your proposal. I look forward to hearing your response.

Yours sincerely,

Suzanne Holmes

Feedback**383**

114165611060

NAME: Max Bloomfield	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I commute by bike down and up Aro Street, often with my children on the bike with me. It often feels unsafe coming up the hill, where traffic will often come past us at speed and very close. A dedicated uphill bike lane would be great for keeping my children and me safe.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

384

114161366804

NAME: Hugh Chesterman	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The proposed changes will dramatically improve the safety of biking and walking in the area.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**385**

114176787788

NAME: Louise Lin	SUBURB: Thorndon	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

More cycle lanes means more safety for us and more environment friendly options for travel

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

386

114182481793

NAME: Alex Dyer	SUBURB:	ON BEHALF OF: Cycle Wellington	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

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cyclewellington.org.nz
info@cyclewellington.org.nz
Twitter: @CycleWgtn
Facebook: groups/cyclewellington

Aro Raroa Transitional Design

Cycle Wellington submission

We would like to make an oral submission. Please contact info@cyclewellington.org.nz

Key points of our submission

- We strongly support this project
- We look forward to giving more feedback when these designs are in place
- Deliver the full transitional design in one 'stage'
- The designs will likely underperform at attracting more people to ride for everyday journeys
- Downhill shared traffic spaces need more work to protect riders from car door zones
- Specific issues with these designs

We strongly support this plan

Cycle Wellington strongly supports this project.

Everyone in our community deserves safe and attractive streets. Bike lanes are climate action.

We are grateful for the effort of the Council to accelerate work on this infrastructure.

We strongly support the repurposing of street space as much as possible as infrastructure to enable the public to choose to travel by public transport and active transport.

We support the changes to on-street parking and all speed reductions to 30k/ph.

We look forward to giving more feedback when these designs are in place

We recognise that these designs are working within a 'transitional' scope, with limitations and constraints to enable fast turnaround in physical implementation.

We look forward to the opportunity for Cycle Wellington and the people of Wellington to feedback more insights about the route once transitional designs are in place and how further changes might better support safe and comfortable cycling. We look forward to the insights and concerns from that feedback being embraced and addressed in the future 'transformational' change project for this route when appropriate.

Deliver the full transitional design in one 'stage'

We support doing the work in one stage, rather than spread out over three stages. A single transitional stage that creates a reprioritized street space design in a fast and low-cost turn-around is consistent with a transitional approach. It would deliver the benefits sooner.

Breaking this already staged project into even more stages will prolong the changes and may leave more people unconvinced of the success of the project. Aro Valley is not unique when it comes to designing street space.

The designs will likely underperform at attracting more people to ride for everyday journeys

A top level principle for the Bike Network plan is 'Opening up our streets for people of all ages and abilities'. The safety report says:

"The proposed treatment for the Aro Valley route, whilst being an improvement on the existing situation, is only expected to attract cyclists of the "strong and fearless" or "enthused and confident" categories (according to the Geller classification). This was detailed in the 30% audit and confirmed by the designer and client. This is considered acceptable given the difficulty of providing a temporary treatment on a route with challenging space availability and topography, but a permanent solution in the future should aim to provide more separation from motor traffic to attract a wider cycling audience."

Cycle Wellington considers the incrementalism demonstrated by this project to be not in spirit with the hoped-for tactical urbanism methodology envisaged for the Bike Network Plan roll out. This project seems to be more like 'faster traditional methodology'.

We understand council officers still do not have access to means of delivering change like is potentially coming from the Reshaping Streets regulatory package recently consulted on.

Instead of slightly tweaking things a little now and a little later, we would expect to see a fast implementation of street changes that transform the use of the road to be heavily in favour of active transport and public transport users. The future transformational treatments should only be significant in terms of engineering effort - not in how different the road space allocation is.

We agree that confident and enthused cyclists who already use this route, such as mountain bikers and road cyclists, will have a safer journey due to these designs. We consider it is vital to ensure that other rider demographics deserve a safe comfortable journey too.

Downhill shared traffic spaces need more work to protect riders from car door zones

We do not believe sharrows make a significant improvement in the higher speed sections of this plan.

In several places, downhill riders will pass parked vehicles. Rider speeds will be high – due to the hill, but also among less experienced riders who fear the response of drivers they ‘hold up’. The safety of these riders depends on them staying 1.5m or more out from parked vehicles, to avoid any suddenly opening doors. Looking into the backs of vehicles to spot drivers is impossible at speed, or with commercial vehicles or those with tinted windows.

Riders (especially those less confident) will feel pressure to ride closer to parked vehicles, to allow traffic to pass. This will place them in the dangerous ‘door zone’ at high speed. The need for assertive road positioning as a cyclist – in the face of daunting traffic volumes, extremely large and heavy vehicles, and highly variable and often aggressive motorist behaviour – makes cycling less intuitive and attractive for many.

The worst part of a ‘dooring’ incident is often not impact with the door but falling into the path of a following vehicle. That is why we prefer parking retention (where necessary) on the uphill side of the road than the downhill side. This reduces the speed difference, helping drivers spot approaching riders and giving riders more time to react and avoid a crash.

Where there’s an uphill bike lane, placing the parking between the bike lane and the traffic lane helps further:

- low car occupancy means dooring incidents from passengers are less frequent
- any fall will be away from following traffic, not towards it.

Specific issues with these designs

The widening of the shared paths through Aro Park and between Palmer and Abel Smith Streets is welcome, but is an underwhelming development if the main road routes of this project remain unimproved for longer, as the staged approach intends.

The treatment of the intersection of Aro with Owhiro should align with plans being finalised as a part of the Brooklyn transformation project. This intersection should have a raised pedestrian crossing and parallel cycle lane.

About Cycle Wellington

Cycle Wellington is a voluntary, not-for-profit organisation aimed at improving conditions for existing cyclists and encouraging more people to bike more often. We advocate for cyclists who use their bikes for recreation and transport. Since 1994, we've worked constructively with local and central government, NZTA, businesses, and the community on a wide variety of cycle projects. We represent around 5,000 members and supporters.

Nā mātou noa, nā Cycle Wellington

21 November 2022

Feedback**387**

114170619829

NAME: Richele McKenzie	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**388**

114161080876

NAME: Helen Trustrum	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

One mobility car park in Epuni Street is hardly going to be enough for those with mobility issues. Parking is an issue now for these people or those with young children so where are they going to shop now? Aro Street is going to be so very narrow for driving along if these changes are made. The pedestrian crossings need to be better defined - pedestrians just walk or bike out without looking or slowing down now. The speed limit needs to be extended to bikes also.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**389**

114181682684

NAME: Irene Papp	SUBURB:	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

As an elderly person, biking is not an option for me or my peers. The proposal is not family friendly. Transporting children by bike is not an option for most. Public transport needs to be fixed first.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback

390

114175652217

NAME: Isaac Spedding	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

In general I think it will lower noise levels and make people feel safer. I think it enhances access to the mountain biking in Aro Valley as well.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**391**

114165589337

NAME: James E	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is such an important street to have good cycling infrastructure on. It's always a source of anxiety for me when riding to and from work in the city. Making these changes will make it easier for many people to start riding bikes around Wellington, and push some to reconsider their primary mode of transport at a time where change is needed.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**392**

114160196104

NAME: Joseph Fletcher	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I think that we should abolish all on street parking in the Valley. Cars are dumb and the people who drive them suck.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

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Feedback**393**

114181200200

NAME: Jane Campbell	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

The road is too narrow, it does not allow people to park cars, which they all ready have and can not dispose of, the rod is too steep and bikes travel too slowly

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Feedback**394**

114170823553

NAME: Jacquie Bown	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This reads as a well considered plan incorporating local stakeholder feedback. The proposals are sensible, low in detrimental impact (ie motor vehicle road users, residents, businesses) and high impact for transport options that are more conscious of climate and wellbeing.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**395**

114181653620

NAME: Jake	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

It's an important cycling route and a separated bike lane will make the uphill route much more user friendly. I think the changes for parking are well-considered given the issues in the area and the need for residents to transition away from private car transport

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**396**

114162751831

NAME: Jan Pearson	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Current parking on both sides of Raroa is dangerous and obstructive for cyclists buses and emergency vehicles. Restricting parking to one side and putting in a cycling lane is a great solution

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**397**

114162363193

NAME: Helen Taylor	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Lack of parking will kill the businesses, scooting or biking this route is restricted to few able, fit commuters, who are far outnumbered by others using this route & parking at all times of the day.

Do you support the proposed speed changes on Aro Street?**Oppose**

Decision made by Waka Kotahi

Feedback

398

114183843131

NAME: Janette Sherwood	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Janette%20Aro-Valley-Connection-paper-consultation-form%20(1).pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☒ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

I support most of these ideas:

- Reducing the speed limit
- Introductions of car share
- Removal of one of the bus stops
- Bike parking in the Village outside the fish and chip shop
- Raising the pedestrian crossing outside Aro Park - so long as drainage issues are sorted - as that area also floods with debris
- The change of Coupon Parking to Residential and adding some P120

I am concerned about the number of coupon parks being taken away. I think changing more of them to P120 would be a far better idea. That gives people longer to shop, have a beer and/or a meal, and go for a bike ride. P20 is not long enough to do any of these things. P20 outside the shops is fine for short trips. But those wanting to stay longer and can't always use other forms of transport need to be able to park close by.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

<input type="checkbox"/> Strongly support	<input type="checkbox"/> Support	<input type="checkbox"/> Neutral	<input type="checkbox"/> Oppose	<input type="checkbox"/> Strongly oppose	<input checked="" type="checkbox"/> Don't know
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Do you have any comments to make about the proposed design?

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive☐ Positive☐ Neutral☐ Negative☐ Very negative☒ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive☒ Positive☐ Neutral☐ Negative☐ Very negative☐ Don't know

People riding bikes along this route?

☒ Very positive☐ Positive☐ Neutral☐ Negative☐ Very negative☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive☐ Positive☐ Neutral☐ Negative☐ Very negative☒ Don't know

People with disabilities or accessibility issues?

☐ Very positive☒ Positive☐ Neutral☐ Negative☐ Very negative☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support☐ Support☒ Neutral☐ Oppose☐ Strongly oppose☐ Don't know

Do you have any comments to make about the proposed design?

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☒ The timing of the stages is about right

☐ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☒ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

People speed through the Aro St Village, even in the current 30km/h zone. 50km/hr is not necessary in these areas. The street is narrow, with busy traffic, including turning into other streets and driveways, along with buses, regular emergency vehicles, cyclists and pedestrians. 50km/hr is not safe with all this activity.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☒ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☒ Commercial vehicle (*e.g. van or truck*)
- ☒ Bicycle
- ☒ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☒ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☒ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation **Verdant Design Ltd**

Name of school

Feedback

399

114181454609

NAME: Jinnie Potter	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Jinnie%20Potter%20Submission.docx

The 25 Highbury route on Raroa Rd is already a nightmare. Accidents HAVE happened. 2 buses meeting head-on on those tight corners is a nightmare. Queues of cars trying to squeeze through a narrow windy street with cars parked (very little off-street parking for houses on land-slipping rock-crumbly hillsides) is TROUBLE. Bicycles added into that mix is Accident101. Many people with disabilities are on extremely restricted incomes. Mobility drivers need their cars to remain independent and more Mobility parking is needed, free and closer to commonly used venues-locations.

Feedback

400

114179428694

NAME: Jasper Kueppers	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I support car ownership & car dependency being discouraged in Wellington, and alternative transport options being prioritized.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

401

114181652770

NAME: Jacob Munz	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
--	------------------------------

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

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Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

RECEIVED
Wellington City Council
Arapaki
Service Centre

18 NOV 2022

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohio Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

As a lifelong Te Aro/Brooklyn resident and keen cyclist I believe this to be completely misguided. It is one thing to put a cycleway up the Brooklyn hill - but this will destroy a community. Much like the block on Riddiford St outside the hospital it will turn Aro Valley into a wasteland and the route WILL NOT HARDLY BE USED as biking up Raroa Rd. is a stupid way to get to Karori.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

POSSIBLY THE SILLIEST IDEA EVER
Raroa Rd. is totally unsuited to
bicycle commuting. Way too steep and windy.
You will be ruining the liveability of
an entire neighbourhood of ratepayers
for the pursuit of a crazy ideal.

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:					
People using Aro Park for recreation (e.g. picnicking)?					
<input type="checkbox"/> Very positive	<input type="checkbox"/> Positive	<input type="checkbox"/> Neutral	<input type="checkbox"/> Negative	<input checked="" type="checkbox"/> Very negative	<input type="checkbox"/> Don't know
People walking on the paths through Aro Park and Little Palmer Street?					
<input type="checkbox"/> Very positive	<input type="checkbox"/> Positive	<input type="checkbox"/> Neutral	<input type="checkbox"/> Negative	<input checked="" type="checkbox"/> Very negative	<input type="checkbox"/> Don't know
People riding bikes along this route?					
<input type="checkbox"/> Very positive	<input checked="" type="checkbox"/> Positive	<input type="checkbox"/> Neutral	<input type="checkbox"/> Negative	<input type="checkbox"/> Very negative	<input type="checkbox"/> Don't know
People driving vehicles on the surrounding streets?					
<input type="checkbox"/> Very positive	<input type="checkbox"/> Positive	<input type="checkbox"/> Neutral	<input checked="" type="checkbox"/> Negative	<input type="checkbox"/> Very negative	<input type="checkbox"/> Don't know
People with disabilities or accessibility issues?					
<input type="checkbox"/> Very positive	<input type="checkbox"/> Positive	<input type="checkbox"/> Neutral	<input type="checkbox"/> Negative	<input checked="" type="checkbox"/> Very negative	<input type="checkbox"/> Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?					
<input type="checkbox"/> Strongly support	<input type="checkbox"/> Support	<input type="checkbox"/> Neutral	<input type="checkbox"/> Oppose	<input checked="" type="checkbox"/> Strongly oppose	<input type="checkbox"/> Don't know

Do you have any comments to make about the proposed design?					
<p>Attempting to direct fast-moving wheeled vehicles through the bottleneck of the child-care centre/community hall/end of Palmer St. will cause severe injuries. For sure. I live in Palmer St. and cycle this route. It requires great care to avoid collision. Pedestrians will be in danger.</p>					

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

*This whole idea is obviously being driven by ideology rather than practicality or concern for residents.
Perhaps more time may result in some much-needed sensible reflection and rejection of the most stupid aspects.*

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

Go on. Destroy an entire community's liveability to provide a ridiculous commuting route for wealthy e-bike owners. It could be time to pack up and leave. Karori should be accessed by Glenmore St. and Kelburn.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

No need really. Traffic down end of Aro St. is not travelling very fast anyway. Will make no difference.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☒ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☒ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

NOT COMPARED TO KEEPING BUSINESS + LIFE IN OUR CITY.

- ☐ Very important
- ☐ Important
- ☐ Moderate importance
- ☒ Low importance
- ☐ Not important
- ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No
 - ☐ Yes
- If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback**402**

114178663208

NAME: Jed Soane	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Support**

Decision made by Waka Kotahi

Feedback**403**

114174317590

NAME: Jeet	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

It's a narrow, road with a bus route. What a dumb idea. Where do the residence park?

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback

404

114180580082

NAME: Jeff Galt	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Getting through Aro Valley is a bit of a nightmare...it's too vehicle focused and needs more options for other transport modes (including freeing up of bus space and routes).

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**405**

114181615844

NAME: Jessica Matcham	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

I think it's a good start and I like that the cycle lanes are protected

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**406**

114166784759

NAME: Jess	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

407

114161222805

NAME: Jez Weston	SUBURB: Brooklyn	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

408

114164286565

NAME:	SUBURB:	ON BEHALF OF:	ORAL
Jill Ford	Newtown	An individual	No

General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

We deserve safe streets for all ages and abilities and to facilitate better transport choices. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. The current plan through Aro st really doesnt make much difference in this staged approach, in fact its bitsy and messy. As a cyclists I will be in and out of traffic, and it isnt any safer for pedestrians or improve things for buses. So I support doing the work in one stage, rather than spread out over three stages. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space. Its very close to the CBD so is perfect for having better safer transport options with less parking on what is a major arterial route.

Do you support the proposed speed changes on Aro Street?

Stongly support

Decision made by Waka Kotahi

Feedback**409**

114169101346

NAME: Jamie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Stongly oppose**

Decision made by Waka Kotahi

Feedback**410**

114167309098

NAME: Joanna	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This stretch of road is dangerous to navigate as a cyclist. This is an important connection within the overall Wellington city cycling network.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**411**

114181254792

NAME: Joanne Davidson	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

In general I support this - I drive through here quite regularly and find it very narrow and risky in spots particularly coming around blind corners where there is a car parked on the side just after the corner, or it is effectively one-way due to cars parked on the road (Raroa mainly). It can be hard to get past cyclists - who are often going much slower - when going uphill. I would also like to have the option of biking myself but due to the lack of space and having to share with both cars and buses it feels very unsafe

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**412**

114174179011

NAME: Joanne Lentfer	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

One size does not fit all when installing cycleways. Aro is a unique suburb. I live on the street and can count on the fingers of one hand the number of cyclists that use Aro Street in a one-hour time frame. Aro is not a middle-upper class suburb with lots of cycling civil servants, it is made up of young families and student accommodation. Come on WCC, listen to your ratepayers and residents and come up with a solution that works for Aro, which by the way is already a very "green" suburb.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**413**

114171704134

NAME: Joe Ballard	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is a crucial route for many people - so we need it to work for all sorts of people and their ways of getting around, not just those in cars. The plan will deliver a better, safer experience for everyone.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**414**

114181294097

NAME: Johnny Haylock	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I think that any cycleways added to roads around Wellington are a good thing. I support the approach the council is taking by acting quickly and getting infrastructure on to the ground on the cycleway stuff. It might not be perfect but we simply cannot continue to add roads for yet more cars around the place.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

415

114169309886

NAME: Glenn Jones	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Pedestrian and cycle safety, making Wellington a friendlier city to live in.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**416**

114160363060

NAME: Jono Wood	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

417

114164039012

NAME: Josh	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**418**

114161544043

NAME: Judith	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**419**

114181652987

NAME: Judith	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

I do not support removal of 70 car parks, will have huge impacts on those who use those parks. I am lucky to have off street parking but people park in it illegally all the time which means that I am not able to. Also our street is full of trades and delivery people where will they park? I support cycling but feel that Aro Street is too narrow for two lanes of cars let alone including a lane for cyclists.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**420**

114181514349

NAME: Julian Silver	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I support encouraging more use of non-car options. However there is inadequate consideration given to impact on disabled or those with limited mobility. There is a net reduction in overnight parks and those that are added are quite dispersed. There is no attempt to actually apply a prioritisation framework for residents parking, eg priority for those with disabled parking permits, no off-street parking etc. Need basic info eg how many people in the valley have disabled permits? One park is likely to be inadequate and, coupled with reduction in overnight parks, will encourage some perverse behaviour.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

421

114161134366

NAME: Justin	SUBURB: Melrose	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

Why not go through Kelburn? A more direct route to the CBD and it would also accommodate anyone cycling to the University.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**422**

114160358600

NAME: Matthew Eden	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Removing the car park's as a traders man in Wellington. Finding a park around all the cycle ways add's time and money for my clients,also Wellington has old houses that have on parks off the road network, where are we going to park all the cars Disabled people are not able to cycle,we need to stop and think about all the community old and young people Regards Matthew Ps I'm an Ebiker

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Feedback**423**

114178247485

NAME: Kahikatea morgan	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Aro st will not, and does not currently, have the appropriate space to support the car centric lifestyle of many that live there. A radical change is needed to ensure the people that live there are able to move safely through the area.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**424**

114179445260

NAME: Kahu	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Stongly oppose**

Decision made by Waka Kotahi

Feedback**425**

114164325376

NAME: Karien Mallee	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**426**

114178401880

NAME: K Maclaren	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

We already have so many cyclists using this as an access, and the area is amazing, but the people pushing for this all seem to be not actually living in this suburb. I think we have been very lucky that more deaths have not occurred on this stretch, because the speed some drivers go is madness. I think pedestrian crossings should be raised, and the speed limit lowered. Even an additional crossing higher up the street could be beneficial to all the families & students that already share this space, and whom have largely been forgotten about. Sometimes the cyclists act like they own the road, and I see high levels of frustration every day. I don't think there is an easy fit answer, but the area is highly occupied, popular & beautiful, but it just can't handle a cycle lane because when it was built, there was never this need or consideration. Build lanes into roads being planned ahead, not try & implement them into areas that weren't built or can manage these cycle lanes. Removing the carpark for access for cyclists seems very narrow minded when there is a bigger community that relies on these for their access, education, work & have visitors. By handicapping 90% for the benefit of the 10% is not sustainable, warranted or wanted.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**427**

114180237133

NAME: Kate	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Neutral**Why do you think that?**

While I support safer cycle ways I am concerned that it will be very narrow for bus traffic to flow without crossing the median line. It's already tight in much of the proposed route and we often have to pull over to let the other direction to pass by. The removal of parking is also likely to lead to more illegal parking on side road footpaths and blocking pedestrians.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**428**

114164228581

NAME: Katherine Blow	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

We need cycleways and pedestrian crossings!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**429**

114161484999

NAME: Kathryn Kelly	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I am a regular biker on Raroa Rd and Aro St. An uphill cycle lane would mean one is not holding up traffic and one is much safer. I seldom stop when driving through Aro St because there is usually no free car park. To have 6 dedicated bike parks would encourage me to stop and visit shops or the cafe while biking through.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**430**

114173824568

NAME: Katie Carey	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

Less parking for residents. Parking is already chaotic in the area. Most cyclists in the area going mountain biking drive to the reserves anyway. It's all up hill so difficult to cycle. It's completely impractical for those living in the area. I also don't want my rates money going towards something that will lower the value of my house

Do you support the proposed speed changes on Aro Street?**Stongly oppose**

Decision made by Waka Kotahi

Feedback

431

114162992941

NAME: Kenneth Dixon	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

432

114181445220

NAME: Khiry Hewitt	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It is extremely dangerous in Aro valley as there are currently no protections for cyclists in the area.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

433

114164280860

NAME: Kieran Lee	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I cycle through this area and for safety reasons I strongly support segregated cycle ways and reduce speeds. I would prefer this to be completed sooner rather than in stages

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

434

114172822926

NAME: Kiri	SUBURB: Miramar	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Wellington's streets need to be safer for bikes and pedestrians. I encourage the Council to be even more ambitious and work more quickly to achieve this.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**435**

114180616488

NAME: Kirsten Malpas	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Aro St so narrow and busy, nice to calm and reduce traffic to make street more friendly for the whole community. Good back road bike access up the hill too.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**436**

114169042051

NAME: Michael Flyger	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This is already a popular route for both commuter and recreational cyclists - the proposed changes make sense and will improve safety and accessibility.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**437**

114170236171

NAME: K McCaskill	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Aro is such a beautiful thriving community, and I'd like to see it become safer for kids and just people in general. It's a main thorough fair for commuters who live nearby who are traveling to the city, so it's a great candidate for separated cycle lanes. I tend to stop in at shops often on a whim, especially when I have time to look at the shop front, which I don't and can't do in a car. Slower speeds and more foot and bike traffic should improve business in the area. I love Aro, I think this is a great idea.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

438

114166708605

NAME: Lachlan Ridley	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I'd love to bike in Wellington but the narrow roads always put me off. The proposed changes would make me feel much safer.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**439**

114172768760

NAME: Lachie Philipson	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

440

114162433749

NAME: Lauren	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Creating safe ways for Wellingtonians to cycle and use other low-emission transport options is key to combating climate change and improving quality of life for everyone!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**441**

114161170531

NAME: Lachlan Patterson	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I cycle this entire route semi-frequently, and these changes will make it so much more comfortable and safer. Right now, it feels very dangerous riding around the windy bends of Raroa Road, particularly given their poor visibility. These proposed changes will make a significant difference to the safety of the route.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

442

114161068997

NAME: leslie alldridge	SUBURB: Newlands	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Two years to paint the road green feels like a very slow moving project :(

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**443**

114164259726

NAME: Liam O'Connor	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

444

114175895358

NAME: Lindsay	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I think 4 speed bumps through Aro as well as a raised pedestrian crossing is too much. 2speed bumps and the crossing would slow traffic down but still keep it moving. Through Aro is a high traffic area.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

445

114162770262

NAME: Trapped in garage	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I like cycling, but where will the cars go? People are already park in front of my garage non-stop. Opportunities for replacement parking should be examined.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

446

114161351026

NAME: Xianglin Deng	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Any effort to separate cyclist from motorist is good. My only concern is that the street is already narrow... please make sure after marking the bike lane, there's enough room for two way car traffic.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

OK.png



Feedback

447

114181601924

NAME: Lisa Nickson	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Lisa%20Nickson%20Submission%20on%20proposed%20design%20and%20traffic%20changes.docx

Submission on proposed design and traffic changes (traffic resolutions) TR179-22

Thank you for the opportunity to make a submission on the proposals related to Aro Valley.

In summary:

- I support most of the measures
- I believe more is needed to better support pedestrians
- I do not support the proposed removal of 70 car parks

I support most of the measures proposed

I support measures to make walking, cycling, public transport, and driving safer and to help address climate change.

In particular, I support the following proposed design and traffic changes:

- 30km/h zone extension
- installation of speed humps
- additional car share parking
- additional mobility parking
- improved bike parking
- wider paths
- clear car park marking

Reduced vehicle speeds and the installation of speed humps will go a long way toward addressing safety risks posed to pedestrians and cyclists, while the additional parking will support modal shift and improve accessibility.

More needs to be done to support pedestrians

Although I support wider paths, I am concerned that this measure will not address the risks caused to pedestrians using Aro Park (including small children attending the local day care centre) and the Palmer Street laneway by bike and scooter-riders travelling through the park at speed, including around corners, and getting too close to people. As one of many people in this area for whom walking is a primary means of getting to, from, and around town, I have been startled and nearly clipped on numerous occasions and would like the Council to explore other options to address this and better support those of us who get around in this lowest of low-carbon ways. The proposals do not, in my view, go far enough in this respect.

I oppose changes to carparking on Aro Street

I do not support the removal of 70 car parks from Aro Street.

If I understand the proposal correctly, local people currently using those car parks will need to compete for 20 resident parking permits on Ohiro Road, switch to ride share services and compete for one of two ride-share car parks on Epuni Street, or try to find a park on Durham Street or Holloway Road.

This will be completely inadequate and have a disproportionate impact on the lives of the people who live and work on Aro Street, potentially forcing them out of their homes and to leave the area. This includes people who:

- need vehicles for work

- rely on their customers and clients from outside the area having parking to access their businesses
- engage the services of tradespeople, gardeners, cleaners, and utility providers
- are moving into or out of the area – barely a week goes by without a furniture removal van or truck parking on the south side of Aro Street
- have mobility issues meaning they need their vehicles to be as close as possible to their homes
- need support from health and community agencies, and their families
- use taxis and app-based passenger services

It will also undermine the connections people have with their out-of-town friends and family who will be discouraged from visiting.

One of the impacts I find most concerning is the personal safety risk and anxiety posed to women and vulnerable people who will no longer be able to park near their homes. Much of Ohiro Road for instance runs between Central and Tanera Parks, is poorly lit, and little used by pedestrians.

It also seems inevitable that people will face an increased risk of having their vehicles being broken into, stolen and damaged, as has already been the experience in Durham Street and Holloway Road.

The impacts of the proposals are not only disproportionate, they are also inequitable when compared to the installation of cycleways in other parts of the city, where car-parking has either been retained or there has been less need or demand for on-road residential parking anyway.

It also does not seem credible that this proposal will lead to an uptick in people cycling up Raroa Road to Karori and Highbury etc. As a former cyclist myself, I know how challenging that would pose the ordinary commuter, even without the wind, rain, and cold we experience most of the year.

Of course, this will be less of a problem for those living in the leafy suburbs to our west who are able to afford e-bikes on top of their private vehicles.

In summary, I don't think it can be said that this proposal will achieve the stated aims of 'making it safer and easier for everyone'.

Feedback**448**

114181705123

NAME: Lisa Mutch	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

My submission is attached as a pdf

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Submission%20on%20proposed%20removal%20of%20Aro%20Street%20parking%20and%20cycleway%20%20L.%20Mutch.pdf

Submission on proposed removal of Aro Street parking and cycleway

Lisa Mutch, resident of Aro Street, mobility permit holder.

I oppose the removal of parking on Aro Street. I do not believe that the increase in cycle use will outweigh the negative impacts on residents. In particular, most residents need and want to reduce our private car trips, but still have various reasons to use cars at some times (beyond the very short-term availability of car share businesses). In order to do this, we need somewhere to leave our cars, given that half of us have no off-street parking. There is virtually no private parking we can pay for, and we bought houses here on the basis that we could pay for residents parking, and have done so for almost twenty years in my case.

If the proposed changes are made, the residents parking scheme should be reviewed and priority given to mobility, EVs, and those without offstreet parking. This is consistent with your Parking Policy for new residents schemes

If you go ahead with removing all the overnight carparks on Aro Street and Raroa Road, please review the residents parking scheme and introduce a new one using the priorities for new residents parking schemes contained in the council's parking policy.

With up to 141 cars losing overnight/ long term parks there will clearly be massive pressure on the remaining parks on side streets, which are heavily used already. You already admit the scheme is over allocated. Removing such a large number of parks will tip all those side streets into the high pressure situation which is undesirable. Converting 88 existing unrestricted (in Holloway Road) and coupon carparks (in Ohiro Road and Holloway Road) to residents will reduce the number of people who can park there, but obviously will not be sufficient, as I have argued that residents already use those parks.

Perhaps the intention is to make parking so difficult that many of us will give up our cars. In this case be honest about it and change the residents parking allocation to reflect the council's priorities. Use the hierarchy already in your parking policy for new residents parking schemes i.e. allocate residents parking only to reflect available number of parks, in the order 1. Mobility permit holders 2. Electric vehicles 3. Those in older homes with no offstreet parking.

If you do not allocate residents permits by priority, there is no guarantee that those without off-street parking are the ones who will get the available residents parks

The briefings provided by council staff suggested that those of us without offstreet parking will park in side streets. But if you do not prioritise residents permits there is no guarantee that will happen. There will be massive competition for overnight parks. The twenty parks which I have been told I should use in Ohiro Road are already mostly full according to the parking counts you did. In any of the side streets off Aro the residents parks might be filled with people from flats who already have multiple permits, or people with one offstreet park who keep two or more cars. Why should I have to compete with them, never knowing when I come home if I will get a park? To match your own parking policy principles you should review the Aro Valley scheme.

The impact section of the proposal document underestimates the impact of the changes on residents.

The impact section of the proposals document states that 141 overnight parks are going to be removed. The impact section of the document is misleading when it lists "addition of residents parks". There will be no added overnight parks, rather you are converting them from coupon and unrestricted to residents parks. It is the case that residents already use those carparks.

Residents' top priority is overnight parks. We use residents, coupon and unrestricted parks as available.

Changing a coupon park to a residents park does not help the parking pressure when a resident already was using that park overnight.

Residents require carparks which they can leave their cars in overnight. We are rightly being encouraged to use our cars much less. In order to do so we need to leave our cars overnight and during the days.

We do not discriminate whether these are residents, coupon or unrestricted, we park in whichever legal park we can find. The "Impact" section of the proposal is misleading as it separates out residents parks and states that 88 of these are "additional" residential parks. There are no additional parks, they are simply converting them from coupon and unrestricted *which are already used by residents*. Where I live in Aro Street Residential East the parking management plan admits that parking is already at optimal levels in lower Aro Street and Ohiro Road.

The competition for the overnight parking is going to become much more intense, and Aro St residential East is already at maximum capacity, especially at night

The net parking impact of the cycleway is that 141 overnight parks are going to be removed from Aro Street and Raroa Road

The documents state that the Aro Valley residents parking scheme is over allocated already

Most of your parking management plan is not accurately reporting the parking pressure for residents biggest need - overnight parking

The vast majority of your parking count analysis was focused on daytime parking and found that usage was acceptable 8-6pm. Only one weeknight and one weekday count was undertaken, the time and day of which was not reported. It seems that the night count was not late enough to capture all those who had taken their cars out for the evening as they state for my area that "Aro Street is only around the 85% ideal occupancy threshold on weekday night. This is likely driven by more residents going out in the night on weekends". This suggests that they were not counting late enough eg after midnight to count all residents who will come home and look for an overnight park. If the count was done before midnight, it was not an accurate capture of the number of overnight parks. It is extremely difficult in the weekend and at night to find parks already, as residents parking requirements only apply weekdays 8-6pm.

I already will miss evening events rather than take my car because I know it's unlikely I would find a park anywhere near my house when I return. And that is before you remove all the parking on Aro St.

Accessibility for residents with reduced mobility

I would rather not share my personal details, but as this is very concerning and will make a huge difference to my life, I will share that I have an incurable disease, and currently have a mobility permit as well as pay for residents parking permit.

The alternative residents parking is already full and difficult to access

When the lower Aro Street parking is removed, we are expected to park in Ohiro Road, which is a steep hill, where cars have to queue and cross the centre line merely to go up and down the hill. Parking here is difficult and walking up it is steep and difficult for those with mobility problems. Much of that parking will already be taken by residents -who else? This is obviously much less convenient than parking on Aro St, despite it being listed as within a couple of minutes walk and therefore convenient.

The impact section of the proposal document incorrectly states that the impact of the changes on disabled residents will be positive. It will be negative because like other residents we currently use the 141 overnight parks which will be removed

The addition of a single mobility park in Epuni Street does not result in a net positive for those of us who are residents. We currently use residents/coupon/unrestricted parks in Aro Valley like other residents. Therefore we are facing the loss of 141 overnight parks with the conversion of one park to mobility. There is no reporting of how many mobility permit holders there are in the valley, but CCS have said I am not the only one, Therefore a number of us, as well as visitors, will be competing for that park. When someone gets it, they may well stay there for an extended number of days until they absolutely have to use their car again,

Parking is already a considerable problem for home care workers, district nurses and other short term workers and visitors, and these changes will make it much more difficult

I have in recent years had need of home care (provided by Access Community Health and Nurse Maude in Wellington on behalf of the government health system previously the DHB. I was assessed as requiring 90 minutes assistance in the morning and 30 minutes in the afternoon. I also had a period when district nurses had to visit every three days for about 30 minutes. Hospice nurses and doctors visit for up to an hour. In addition obviously when I am less well friends from across the region will visit when they can.

Carers are already in real shortage, and are not paid for travel time only for the time they are in our houses (measured by their employer through gps on their phone app). Partly due to the shortages, many carers are not only working in their locality but regularly are given clients from Miramar to Johnsonville. Swapping to e bikes will be very unlikely given the older demographic of most of that workforce, but also because they are not paid for the time they spend travelling between clients. Anyone in the valley at any time could end up needing this care as I did, especially given our ageing population.

In the current parking system when I had carers, there was often no coupon space (ie free parking for 2 hours) near my house. One of the regular carers would park in p 20 and either try

and cut short her time with me, or take the risk of getting a ticket, which she regularly did, as they was less costly for her than driving around trying to find a park further away. One of the district nurses who were coming for a time every three days, refused to come to me on a weekend day because the parking was so bad and she couldn't afford the time spent driving around looking for a park. We had to manage my change of medical device ourselves on weekends. Again, this is with the current parking available on Aro St, most of which are going to be removed.

Safe spaces for cars to pick up and drop off residents including low mobility residents will not exist

If I don't use my car for shorter trips, I will use taxis or Uber, but as there will be cycleway I don't know where I will embark/disembark near my house safely. It is often difficult already to unload shopping when the parks immediately outside our house are full.

Feedback**449**

114162604183

NAME: Louis Schmitt	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

This road is the connection to the Happy Valley landfill for Karori and Northland. It is the parking for all the flats on the road, there have been other cycle lanes built where local businesses have gone bankrupt, thus Council backed the idiotic pedestrian crossing on the road to the Airport, when will common sense prevail?

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback

450

114172952935

NAME: Lucy Weston-Taylor	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Wellington needs a joined up network of cycleways to enable more people to cycle safely. Cycling is fun, quick, climate friendly and reduces traffic congestion. Investing in cycleways and public transport is investing in our future.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

451

114179777460

NAME: Luke Holden	SUBURB: Te Aro	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Feedback**452**

114160212412

NAME: Maddalena Dal Sasso	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Uphill bike traffic on Raroa Road is and will get increasingly more heavy. It is important to adapt and allow for those biking to have a safer commute. The increased bike parking, addition of bike lanes, and lowered speed limit will not only allow regular bikers a safer commute, but also those who come to enjoy Waimapihi Reserve from all around Wellington and overseas tourists.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**453**

114170061402

NAME: Simon	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

A safe secure cycle route through that area is a necessity

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**454**

114181507779

NAME: Marcia Abell	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Street parking in Aro Valley is already under extreme pressure. Many houses have no drive-on and multi-adult homes may well have more than 1 vehicle. Public transport is unreliable and too slow, especially for those who need to move between study and part-time work outside of the CBD. I provide end-of-tenancy cleaning and maintenance services for several Aro Valley properties and parking availability is crucial. I can't use public transport to take my cleaning, gardening, plastering and painting equipment, nor can I carry it long distances. Lack of parking already makes this problematic; removing large numbers of parks will exacerbate this. I also use the area recreationally, walking my dog in the reserves, having a coffee or meal at a cafe or checking out the latest offering from Garage Project. I can't transport a 20kg dog on the bus/train - it is also a 20min walk from my home to the nearest public transport. This use of the area requires parking - if I can't park, I will go elsewhere and my support will go to a business in Lower Hutt or Porirua. It seems that services for those who live, work in and use the Aro Valley are being reduced in favour of those who transit through it. The local businesses need the support of those who spend time in the area and that requires parking. The devastating effects of cycleway failure in Newtown and Island Bay on local businesses leave me with little confidence that Aro Valley will not suffer a similar fate.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**455**

114160165694

NAME: Mark Ramsden	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I am a long-term commuting cyclist and also regularly ride from home in Khandallah to Aro Valley to ride the Waimaphihi MTB tracks, often finishing at an Aro St business for a debrief (that may or may not be the Garage Project taproom). These change would make biking through the area much safer and would connect Aro Valley with other parts of the city

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**456**

114161563237

NAME: mary	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

457

114161569018

NAME: Mat	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

458

114181450529

NAME: Matt	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**459**

114169876323

NAME: Matthew	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Living in highbury I welcome aro st proposed bike lane, I cycle every week and always tell me family I live them in case I don't make it back from dangerous car and other drivers who can't seem to wait to pass

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**460**

114177304062

NAME: Max	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Increasing safety and decreasing emissions from transport through the area are massively important. Providing more transport options and improving the viability of some options (eg biking, especially with the uphill bike lane) are great ways to get there.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

461

114181456680

NAME: Max Dickens	SUBURB:	ON BEHALF OF: Bus and Coach Association	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Bus%20and%20Coach%20-%20Aro-Valley-Connection-paper-consultation-form.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The proposal does not go far enough. WCC should also:

- Remove southbound car parks. These roads are extremely narrow, and storage of personal property impedes a main thoroughfare. This area is already a difficult area to navigate by bus. Parked cars are a safety hazard as they block visibility.
- Remove more/all of the car parks around the shopping area. There is more than adequate foot-traffic to maintain the businesses on this road. Keeping the on-street parking renders the other changes pointless, and this will force buses and cyclists to continue navigating cars on a narrow road.
- Remove northbound parking to Holloway Road as part of the process. There is no reasonable justification that Stage 3 should not be Stage 1.

We also have some concerns about the raised tables. Although we support slowing traffic in principle, they can damage buses and make riding PT more uncomfortable.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?					
<input checked="" type="checkbox"/> Strongly support	<input type="checkbox"/> Support	<input type="checkbox"/> Neutral	<input type="checkbox"/> Oppose	<input type="checkbox"/> Strongly oppose	<input type="checkbox"/> Don't know

Do you have any comments to make about the proposed design?
<p>Holloway Road should be fully under Te Aro parking zone. This should help balance out local concerns about reduced parking. It will also discourage commuters parking close to town and walking in, rather than taking the bus from further out.</p>

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. *picnicking*)?

☒ Very positive ☐ Positive ☒ Neutral ☒ Negative ☐ Very negative ☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☒ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People riding bikes along this route?

☒ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☒ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☒ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The Bus and Coach Association support separated infrastructure for active transport modes. This is for two key reasons:

- Cyclists can hold up buses when the two share lanes.
- PT users will often complete the first and last miles using active modes.

We also recommend delineating part of the path for cycling, and part for pedestrians. This should also be done in areas like the waterfront, as their speed differential causes conflicts.

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?			
<input checked="" type="checkbox"/> Make the proposed changes more quickly	<input type="checkbox"/> The timing of the stages is about right	<input type="checkbox"/> Make the proposed changes more slowly	<input type="checkbox"/> Don't know

Why do you think that?
<p>All three stages should be implemented immediately. This is for two reasons:</p> <ul style="list-style-type: none">- Most residents will never approve of the proposed changes. Therefore staggering the changes will do nothing but slow progress and increase opportunities for legal challenges.- Residents already live very close to the CBD, and this is a key thoroughfare.

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

It is positive that WCC are reprioritising our urban space away from the private car. This is particularly true in areas such as Aro Valley, which were designed before the invention of space-inefficient private cars.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☒ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

We support moves to increase pedestrian safety. Large sections of Wellington are still very difficult to traverse by foot due to wide roads and high vehicle speeds. This has a flow on effect to reducing PT uptake.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (*e.g. to see friends or businesses*)
- ☐ I do recreational activities in the area (*e.g. running, walking etc.*)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☒ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☐ Commercial vehicle (*e.g. van or truck*)
- ☐ Bicycle
- ☐ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☒ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number:

Are you providing feedback as ☐ An individual ☒ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation Bus and Coach Association

Name of school

Feedback

462

114167912473

NAME: Finn	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**463**

114161046928

NAME: Dan Barratt	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

I'm a cyclist living in Highbury, so would benefit from being able to cycle through Aro Valley. I'm particularly supportive of cycle parking being made available.

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback**464**

114165555766

NAME: Matthew Bartlett	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

I'm enthusiastic about all efforts to make Wellington roads nicer for cyclists, because encouraging cycling seems key to a climate-appropriate transport future. I'd love to see child-friendly cycle paths in as many places as possible.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

465

114174308904

NAME: Michael Riemann	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on? **Aro Valley Connection**

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I support the idea of speed limit and speed bumps, but don't agree to the idea of a bike lane that would put pressure on everyone's resident parking. Aro is one of the worst places to find a car park and the bike lane would reduce this immensely. I cycle a lot myself and yes fundamental changes are required to make the city more bike friendly. Perhaps build a carpark for residents close by and guarantee free parking for the and monitoring so no cars get stolen or damaged would be an alternative solution.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**466**

114174150845

NAME: Michael Ingham	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The proposed stage 1 & 2 changes will only sporadically benefit the less than 5% of the population who are fit enough or can afford an e-bike. The benefit to those <5% will be also be weather dependent. The community who live and work here will be impacted by the reduced parking and the inherent effect of reducing visitors and the related social contact and financial spend within the local commercial area. The impact on the existing community who need vehicles to go to work, and the general community who would be isolated as a result of reduced visits/social contact from the deleted parking. The public transport system has been unreliable and infrequent since local authority meddling in the last few years - which won't offset the proposal which does not improve connectivity to the >95% who live here. This survey is also flawed in that it doesn't allow you to indicate the many ways people who live here already travel happily along Aro Valley -therefore filtering the results and skewing the outcome of the survey. The local authority have respectfully repeatedly meddled and stuffed up with introducing cycle lanes, new bus hubs and routes, and now a pedestrian crossing on Cobham Drive. Please focus on delivering consistent core transport infrastructure, not acting on the whim of a Sunday rider lobby group who'll only use this lane through our suburb on a fine day. In conclusion the proposal does not make it easier for everyone to move around in our growing city does not help connect the places where people want to go, reduces - doesn't open up our streets for people of all ages and abilities, and increases our emissions from road transport as the demand is still there and was long as the population increases.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

467

114161362581

NAME: Mike	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Cycling now up Aro Street is dangerous because it's uphill and cyclist are therefore very slow, this can frustrate drivers who may overtake inappropriately and risk hitting the cyclist.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

468

114180820680

NAME: James	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

This will encourage more cycling, which will be better for everyone.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**469**

114160969567

NAME: Matt McKillop	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I recently cycled to Karori through Aro Valley and found the experience to have a lot of dangerous features. The uphill cycleway and traffic calming in the village will do a lot to make the area safer for cyclists.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

470

114175910860

NAME: Mike	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

The road is far too narrow at present, as a cyclist I am often holding up a number of cars because there is no room to pass.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

471

114181686172

NAME: Natasha	SUBURB: Northland	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Happy to see more access for cyclists and pedestrians. Would love to see more bus routes between the city and Northland

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**472**

114164345947

NAME: Neil	SUBURB: Berhampore	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Provides a route between city and Karori

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

473

114160137714

NAME: Nick Rinehart	SUBURB: Oriental Bay	ON BEHALF OF: An individual	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**474**

114160169087

NAME: Nicole Gaston	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Looking forward to better cycling & walking infrastructure!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**475**

114181699545

NAME: Nico	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Because I want to be able to bike to Wellington Collage safely.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

476

114181560478

NAME: Kathy and Nigel Hughes	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Kathy%20and%20Nigel%20Hughes%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

- ☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

IF THE CYCLEWAY GOES AHEAD AS PLANNED, THERE WILL BE VERY FEW CAR PARKS IN ARO ST. VEHICLES WILL NOT BE ABLE TO STOP WITH YELLOW LINES ON MUCH OF THE OTHER SIDE OF THE STREET. YOU SEEM UNWILLING OR INCAPABLE OF SAYING HOW ACCESS TO PROPERTIES ~~WILL~~ WILL WORK SAFELY FOR TRADES PEOPLE, COURIERS, ELDERLY, DISABLED, PEOPLE MOVING IN OR OUT OF A PROPERTY, CARE GIVERS ETC. SIDE STREETS ARE ALREADY FULL OF VEHICLES! AS WE LIVE IN ONE WE KNOW

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☒ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

THIS ROUTE WILL BE MAINLY USED BY
EXPERIENCED CYCLISTS.
IT DOES NOT JUSTIFY PEOPLE LIVING ON
THE HILL LOSING THEIR PARKING OR
SAFE ACCESS TO THEIR PROPERTIES.

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People riding bikes along this route?

☒ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

YOU USE THE WORD FOOTPATH - THAT IS WHAT IT SHOULD BE.
CYCLISTS & PEOPLE ON SCOOTERS CAN EASILY WALK THROUGH THIS AREA.
THIS SHOULD BE A SAFE COMMUNITY SPACE FREE FROM THE RISK OF INJURY FROM BIKES OR SCOOTERS.

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

THIS WILL GIVE PEOPLE TIME TO MOVE IF THEY FEEL UNABLE TO STAY IN THE AREA AND WE LOSE DIVERSITY WHICH WOULD BE SAD.

Do you support the overall proposed changes to the Aro Valley Connection route?
These include traffic resolution TR179-22.

- ☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

BECAUSE RESIDENTS WELL BEING AND SAFETY ARE NOT BEING CONSIDERED.

MANY OF US ALREADY WALK AND/OR USE PUBLIC TRANSPORT AND USE OUR VEHICLES RESPONSIBLY. BECAUSE SOME OF US DO NOT HAVE OFF STREET PARKING. WE ARE AN EASY TARGET FOR THE CYCLE LOBBY MANTRA "PRIVATE PROPERTY ON PUBLIC ROADS"

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

- ☐ Strongly support ☒ Support ☐ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

THE 30 km/h SHOULD APPLY TO THE WHOLE STREET.

THERE IS NO PEDESTRIAN CROSSING BETWEEN THE SHOPS AND HOLLOWAY ROAD AND THERE SHOULD BE. AS THERE WILL BE VERY LITTLE PARKING IN THIS AREA AND THEREFORE MORE PEOPLE PARKING NEEDING TO PARK ELSEWHERE & MORE PEOPLE WILL BE CROSSING ARO ST

Your relationship with the area

What is your main relationship to the area? Please tick one:

- ☒ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? Please tick one:

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☐ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☒ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important
- ☐ Important
- ☐ Moderate importance
- ☐ Low importance
- ☐ Not important
- ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

477

114181463644

NAME: Dale Mctavish	SUBURB:	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Dale%20McTavish%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

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We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

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Changes proposed for the Aro Street section

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- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Does the 30 km speed apply to bicycles? Should not apply to cars.
ARO St. is far too narrow to sustain these changes.
There should be no changes to paths through Aro Park & Little Palmer St. No loss of parks for cars - parking is already dire.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?					
<input type="checkbox"/> Strongly support	<input type="checkbox"/> Support	<input type="checkbox"/> Neutral	<input type="checkbox"/> Oppose	<input checked="" type="checkbox"/> Strongly oppose	<input type="checkbox"/> Don't know

Do you have any comments to make about the proposed design?
NONSENSE

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People riding bikes along this route?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

RUBBISH

Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
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- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
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Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☐ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

Don't make any changes. WELLINGTON IS BEING STRANGLED.

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

NOT NECESSARY.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Why do you think that?

THERE IS ALREADY CONGESTION IN THIS AREA + FURTHER.
30 km WILL ONLY ADD TO THE PROBLEM - GRID IRON.
ALL OF ARO SHOPPING AREA WILL BE NEGATIVELY
AFFECTED.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☒ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☐ I travel through the area
- ☐ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☒ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

478

114165037625

NAME: Nick Russ	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**479**

114181637301

NAME: Lynley	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

It can be difficult to find parking in the Aro Valley area as it is. Removing coupon and unrestricted car parks will reduce the accessibility of the Valley for many people who need to use a private car to visit the area.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**480**

114166733056

NAME: J	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I'm very supportive of the plans generally and greatly appreciate the thought and work that has been put into this from everyone so far. I would love to cycle more - I used to be a daily cyclist where I lived previously but it's not yet safe enough here. That said, I am also a regular walker in the neighbourhood and there are several key places this plan does NOT "make it safer and easier for more people to walk" because of proposed shared paths with increasing numbers of bicycles where bicycles could be more effectively accommodated separately. 1. I highly recommend keeping bicycles OFF of Little Palmer St and the path through Aro Park, both should move bicycles to the streets instead where they can be less interrupted by pedestrians and tight turns, not to mention little kids crossing at the playgrounds and preschool – where the plan is conspicuously 'silent' on how bicycles are meant to connect from Palmer to Aro Street. Instead create a contra-flow bicycle path on the east side of Willis connecting Hwy1 to the existing crossing at Aro Street. If you don't then bicycles seeking to avoid the playground conflict will continue to circulate on the Willis footpath. 2. Please take a bit of extra effort to keep pedestrians fully separate at Memorial Park where it meets Holloway. The design currently bottlenecks bicycles and pedestrians here creating unnecessary conflict. There is room to make a separate pedestrian path to the crossing at Holloway. 3. Please create an official pedestrian crossing on Raroa at either Holloway or Entrance Street. Right now it is difficult to safely cross for those of us who are not able to navigate the very unusual ramp/stairs on the north side at 202 Aro St. Right now we have to cross at the pedestrian crossing at 96 Aro St then take our chances to get across once we're past 202 Aro Street. 4. Please consider removing the difficult ramp/stairs at 202 Aro St which would also have the benefit of not putting pedestrians within reach of the drooping cable/power lines. 5. Is the raised speed hump at 144A Aro Street meant to be a pedestrian crossing? If so, please mark and add appropriate signage. If not, I don't think it's necessary or beneficial to have two so close together. I would prefer to have just one at 144A Aro St which IS a pedestrian crossing and not have one also at 116 Aro St which already is an effective chicane. 6. Lastly, please create an official

pedestrian crossing on Raroa uphill from Cluny around #22 Raroa so we older and slower pedestrians with babies/groceries in all weather (dark, rain, etc) don't have to take risks with fast traffic to get across especially now that parking is removed from the south side.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**481**

114168723480

NAME: Gabriel Pollard	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

482

114171192185

NAME: Andrew Maloney	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

An uphill cycle lane will be a great asset to the neighbourhood and encourage cycle commuting

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback

483

114174830648

NAME: Odessa Owens	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It's great to see cycling being made safer on the valley!

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**484**

114160965713

NAME: Ollie	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**485**

114169028512

NAME: Olly	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Cycling up/down Raroa Road is extremely dangerous due to the lack of space and respect for cyclists on the road. This change will allow for more accessible cycling into Karori, Highbury, Wilton, and Northland.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**486**

114162699997

NAME: Owen Matthews	SUBURB: Vogeltown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Roads should be primarily for transport including all users, parking should not be prioritized.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

487

114174611142

NAME: Phil Kendon	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

safer for bikes but be careful there is enough space for buses in both directions above the hairpin bends if part of the road is taken out for a bike lane

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

488

114179574588

NAME: Ruth Palmer	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Cycling safety on a busy city route will be improved. Vehicles and cyclists will be separated and traffic flow improved.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**489**

114180407021

NAME: Patricia	SUBURB: Highbury	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Support**Why do you think that?**

Good ideas to start on this project However very unfortunate timing with even more bus routes removed. Paying drivers better should be a high overall priority for LGWM Also where are the people who live on Raroa Rd supposed to park their cars? Can we possibly do what they do in Europe and share footpaths with pedestrians?

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**490**

114174720112

NAME: Paul Gray	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**491**

114181452839

NAME: Paul Jones	SUBURB: Mount Victoria	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback

492

114181795933

NAME: Philippa Curthbert	SUBURB:	ON BEHALF OF: NZAA	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Attachment%202022-11-15%20Raroa%20Road%20issues.pdf

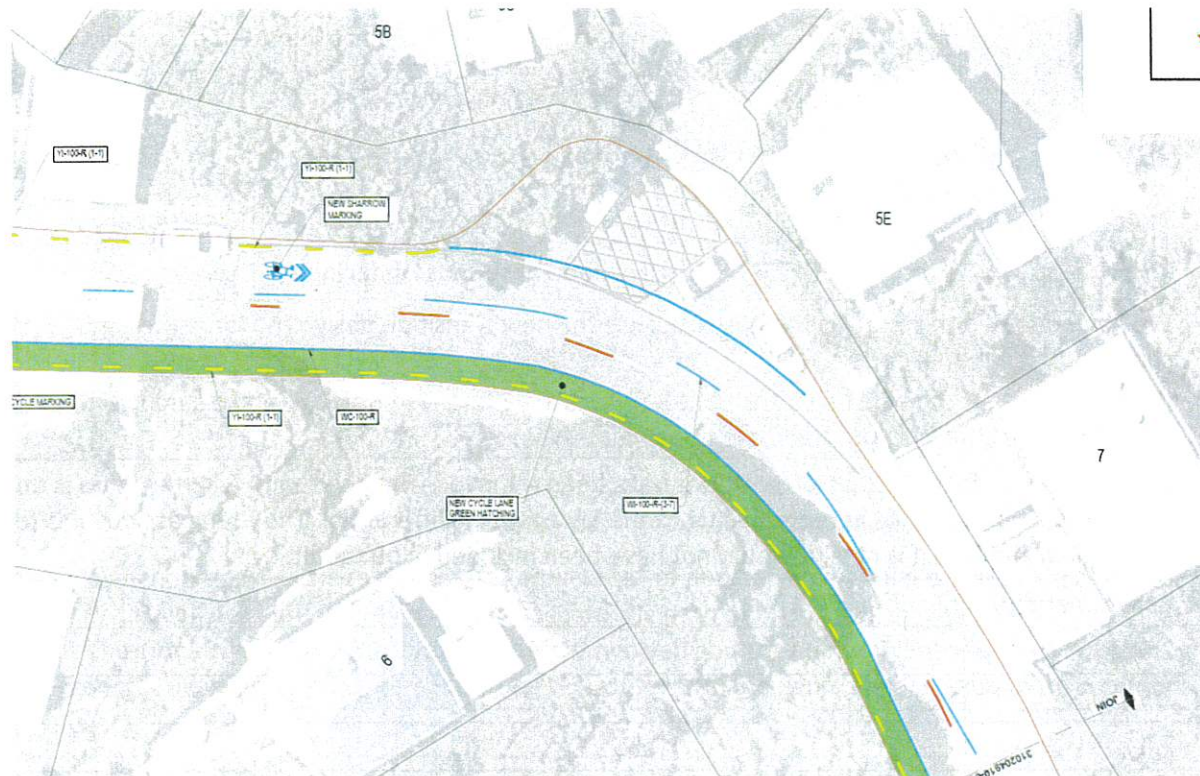


Figure 2.4-1: Cycle lane on inside of curve, Raroa Road

Locations of particular concern include the curves in the vicinity of:

- 150 Raroa Road
- 146 Raora Road
- 82 Raroa Road
- 44 Raroa Road
- 2 Cluny Avenue
- 6 Raroa Road (see Figure 2.4-1)
- 1 Moana Road

Motorists cutting the corner into the cycle lane will be a frequent occurrence, and while most motorists will adjust their course if a cyclist is present, crashes may still result occasionally. Such crashes are likely to result in serious injury to the cyclist.

This issue was raised in the 30% CASA, with the recommendation to install either flexiposts or audio-tactile profiled (ATP) line-marking leading up to and around a cycle lane on the inside of a curve. The designer noted that buses would need to encroach on the cycle lane around the corners. The decision from the safety engineer and client was to install ATP, but these have not been indicated on the plans.

Recommendations

2.4.1	Install audio-tactile profiled (ATP) line-marking leading up to and around a cycle lane on the inside of a curve.
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Responses

Designer	ATP to be added to the drawing package at locations identified above.
Safety Engineer	Agree with CAT and Designer.
Client	Agree.

Wellington

Google

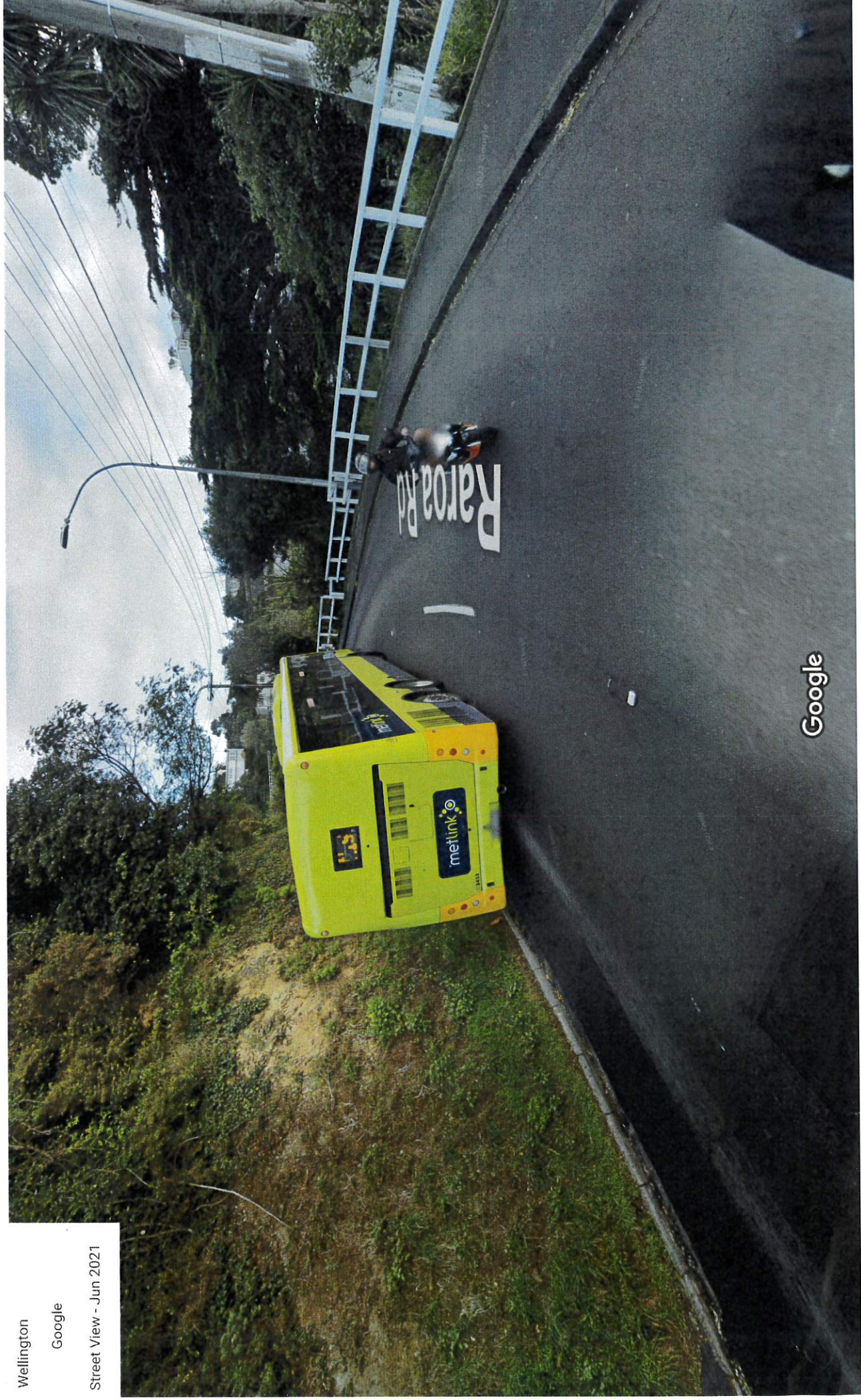
Street View - Jun 2021



Wellington

Google

Street View - Jun 2021



Feedback

493

114174939719

NAME: Peter de Boer	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

It's a narrow road, with parked cars, making passing cyclists safely almost impossible

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

494

114181537673

NAME: Peter Taylor	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Makes it safer to ride through the area

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

495

114160187641

NAME: Pam Fuller	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

496

114174308034

NAME: Raj PARBHU	SUBURB: Northland	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Not enough room for both cars and public transportation. It'll make it more dangerous! Public transportation is unreliable.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Feedback**497**

114178529136

NAME: Richard Taylor	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support

Why do you think that?

Support cycleways, not 100% certain this is the best route given the extreme narrowness of the road already.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**498**

114166747630

NAME: Todd	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Stop taking away carparks in favour of cyclists in a hilly city. It is utterly ridiculous

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**499**

114169329158

NAME: Rhedyn Law	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Providing travel choice is important to support climate action and to improve the livability of our city. Safe, connected infrastructure is essential for cycling and other active transport modes to be a legitimate travel choice for all ages.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

500

114163129264

NAME: Richard Mansfield	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**501**

114161554630

NAME: Richard McNamara	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

Important to improve things for bus users (higher frequency would be great too!). Good to have additional stop at bottom of Harrold St path, but to make it more usable that path needs to be better maintained as it's often slippery and dangerous (improved lighting would help too)

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**502**

114161217300

NAME: Jim	SUBURB: Johnsonville	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

only 1% of traffic through Aro valley is cycle traffic, i.e. 100 cars use the road for every bike and cyclists are in a MASSIVE MINORITY. Stop mucking around with our roads - we don't want them to become cycleways.

Do you support the proposed speed changes on Aro Street?

Stongly oppose

Decision made by Waka Kotahi

Feedback**503**

114167851453

NAME: Geoff Ridley	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

It is time that our streets be redesigned to make it easier and safer for people to walk, ride, or use public transport. I strongly support changes to on-street parking, and use of 30 kmh limits to make this happen. I think that this should be done in one stage and not three stages as it is important that we reap the benefits as soon as possible. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**504**

114164330728

NAME: Rachel Ridley	SUBURB: Northland	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**505**

114160133458

NAME: Robb	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

Honestly I just can't see how a cycle lane can go up Raroa Rd, in places it's so narrow oncoming vehicles barely have enough room. Let alone buses and heavy delivery vehicles. In some cases, council is making owning or renting space in Aro Valley untenable, especially if people that live in the area, need a vehicle to get to work, move young children around or have mobility issues that make walking, or riding very difficult. Also council doesn't control the bus service, how can you improve that (you can't).

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback

506

114162846693

NAME: Rod Page	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Council has ruined Island Bay, a prime suburb. Please do not ruin the Aro Valley.

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**507**

114174258027

NAME: Roni Alder	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Yes I think we need to consider the safety of all people who travel through that area, not just cars and parking for residents. I also think it will mean more people start biking and that will be good for climate change.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**508**

114181620697

NAME: Divya Nathu	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

Considerably restricting and removing parking options on Aro Street and Ohiro Valley will make it very difficult for Wellingtonians from further afield to come to Aro Valley.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback

509

114160388978

NAME: Simon	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

An uphill separated bike lane is sorely needed on this route.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**510**

114160285751

NAME: Ross Carter	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Good balance between needs of those who drive ride and walk Aro St - I do all those things.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

511

114176177289

NAME: Roz Scott	SUBURB: Aro Valley	ON BEHALF OF: Tickadeeboo Insideout Design LTD and all other businesses owners in the neighbourhood	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Aro Valley is a little community and has been like this for years all the business will die as you will not be able to go to a cafe, pick up supplies get your local fish n chips or video your killing peoples lively hood and the community of this special area ! No no no

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback**512**

114161157719

NAME: Roz	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Lots of cyclists use this route – either for commuting during the week or for mountain biking at the weekend – with blind corners and a steady incline, it's important that cyclists feel safe and protected going up hill at a speed that doesn't aggravate drivers. a separate cycle lane will protect all road users.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback**513**

114178479363

NAME: Ruby	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

I think that increasing the accessibility to cycling in this area is a really awesome project

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**514**

114172756775

NAME: Susannah	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**515**

114160142149

NAME: Sam gilkison	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

There is a shortage of car parks at the moment this will make it so much worse and kill the busnuises in the aro Valley village. Narrowing the lanes will be unsafe for buses as this is a bus route!

Do you support the proposed speed changes on Aro Street?**Stongly oppose**

Decision made by Waka Kotahi

Feedback**516**

114181201050

NAME: Samuel Kempthorne	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

great proposed changes, I travel through Aro Valley every day and think this will be a real improvement. The project should be completed in 2023, in one stage only. please don't drag this process out for three years, the true benefit will only exist once all three stages are complete.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**517**

114174250734

NAME: Sara Bishop	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Aro Street needs cars, not a cycleway for the most confident cyclists in neighbouring suburbs. This plan will be very disruptive for the many people on and around Aro St who for better or worse own cars and need a place to park them. I'm all for the having less cars, but the infrastructure needs to be in place first, and an advanced level cycleway is not inclusive infrastructure.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**518**

114164307799

NAME: stuart gardyne	SUBURB: Roseneath	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**519**

114167112427

NAME: Simon O'Rorke	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Production for profit has led to the persistent increase in the economic drivers of climate change. Reliance on the atomised action of individuals has failed to stem this trend. Yet this is essentially what WCC proposes: to “enable more people to get around in low-carbon ways”. Effective action on climate change will require the bulk of the economy, currently dominated by large corporations, to be taken into public ownership. That will enable a democratic plan of production, for use and sustainability. An integrated system of public transport will play an essential role.

Do you support the proposed speed changes on Aro Street?**Stongly oppose**

Decision made by Waka Kotahi

Feedback

520

114161355205

NAME: Simon Planzer	SUBURB: Kilbirnie	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

521

114161074868

NAME: Siobhan Oldale	SUBURB: Miramar	ON BEHALF OF: Abel Odor	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

522

114175983030

NAME: Matt	SUBURB: Khandallah	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Oppose

Decision made by Waka Kotahi

Feedback**523**

114170414161

NAME: Carolyn Hagenson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

It's a very narrow road for a primary road as it is. Often buses have trouble getting through. Our son can be biking home at 29km/hr in the 30k zone and cars will pass him just because it's a bike. It will be so much safer with a dedicated bike lane with some separation from traffic. Busses will also be less squeezed .

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**524**

114179665758

NAME: Sophie Yeoman	SUBURB: Newtown	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**525**

114181690495

NAME: Stuart	SUBURB: Island Bay	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Support**Why do you think that?**

Aro Valley is highly utilised by riders - making it safer and easier to use will benefit riders and vehicle traffic

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**526**

114181629134

NAME: Sarah	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?****Do you support the proposed speed changes on Aro Street?****Strongly support**

Decision made by Waka Kotahi

Feedback**527**

114175860064

NAME: Stephanie	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

I don't believe you are taking residents and those who require vehicles into consideration. Those who live on Raroa Road and Aro Street, majority of which don't have off-street parking, are going to be left with no parking near their house due to the number of parks being removed in this process. Raroa Road itself mentions removal of carparks where 34-44 cars currently park on the uphill side - where are they meant to go? Downhill are also having their parks reduced so they won't be able to park there. The bus service is not reliable enough or accessible enough for some residents who require their vehicle - have you ever tried taking groceries and/or children on the bus?? If the proposal goes ahead there will be an added health and safety risk of all residents who live on the uphill side of the roads needing to cross the road more regularly to access vehicles or footpath on the other side of the road.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**528**

114170408149

NAME: Steve Bielby	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I'm a cyclist. Aro Valley's roads are increasingly unsafe and unpleasant for the community - Aro's roads were never designed for the current group of people who choose to drive at high speeds in huge numbers of huge vehicles. Am VERY keen that council takes steps like these (and preferably more - its not safe for me commuting by bike to work in Porirua) just so we can move safely again from A to B.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**529**

114162654664

NAME: Steve Mackle	SUBURB: Wadestown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

making the streets more user friendly to more diverse transport options will increase use of these forms of transport, reducing climate impacts

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**530**

114164230294

NAME: Steve	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**531**

114168974153

NAME: Steve	SUBURB: Hataitai	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

This makes it safer for cyclists and all road users. Many many mountain bikers cycle up the Valley to Polhill MTB and beyond it is currently hair-raising to be using the road with the many parked cars, buses and other vehicles.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**532**

114181701163

NAME: H Brunswick	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

Good initiative, wrong area for implementation. If you are trying to connect karori to the city why would you suggest Raroa road and Aro street - the most cramped and dodgiest route - instead of say Glenmore street or upland road and past the University? Those on upland road mostly have their own garaging so are less affected as well as having less people per square km, for example. Aro valley residents are already struggling with car parking. A lot of commuters travel to the area for the coupon and free parking on side streets, which you haven't considered at all. The proposal disadvantages Aro residents in favour of those who live in areas with plentiful street parking who want to cycle. Improve public transport firstly. Also there are sections of Raroa road which are only wide enough for two cars - that doesn't include a bike lane or any parked cars. Raroa road has exploded in the last year with cars parked, made even more obvious when cars were no longer allowed to park up on the footpath. It's surprising there hasn't been a serious accident yet. Consider the topography and the current situation, survey the actual residents of the areas you are proposing to impact and you will find that your plan is dangerous and pushing residents away from the area. Many of us do not have the ability to cycle or park far away from our houses - whether due to disability, elderly, or having young children.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**533**

114181246247

NAME: Howard	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

The proposed changes severely impact Aro Valley residents with all the benefit to commuters who pass through. The presenters of this proposed change lack any empathy for the impact on residents and one example is their refusal the answer how residents on Aro street will be serviced by trades people , deliveries, visitors etc with the flippant answer given is there are other houses in Wellington where you can't park outside. How will a scaffolder get their steel to a house for maintenance? It is utter hypocrisy to label short term parking at the beginning of Holloway Rd as being a short walk to the local cafes when as now the predominant use is cyclists who drive to Waimapihi reserve to use the cycle track. Aro Valley residents lose on street parking but it is retained for cyclist who prefer to drive.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**534**

114173606339

NAME: Tessa	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The changes will improve safety for everyone and make more transport modes viable. In my case I don't feel safe cycling on Raroa Road, so I look forward to being able to visit Aro shops more regularly using the bike route!

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**535**

114164126373

NAME: Lorraine Boennic	SUBURB: Seatoun	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**536**

114181455660

NAME: Helen Gear	SUBURB: Other	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

While I agree with allowing the use of more bikes and pedestrian use on Aro Street I consider there is also an ongoing need for some parking for private vehicles from outside the suburb. Travelling to businesses and educational institutions when the weather is cold, wet or windy and during the night will be significantly affected by the planned reduction in parking. I belong to the School of Practical Philosophy which offers courses and events both during the day and in the evening. A lack of public parking and poor public transport options will have a significant effect on the ability of students from other areas of Wellington to attend. This initiative, will overtime, reduce the ability for those who live outside the immediate suburb to access services in this area decreasing the ability of businesses and organisations dependent on a wider catchment for their survival. Those that provide services for locals will survive but others are likely to leave reducing the diversity currently available in the valley. While most activities can now be undertaken online there are some activities that are best undertaken physically. – Education, client consultations, personal services such a hair dressing are examples of these. Aro valley does have a public transport service. That service runs every 20 minutes at the busiest time of the day and only hourly during the evening. For people who want to visit Aro street from one of Wellingtons outer suburbs this will make the use of public transport in the evening almost impossible especially in the winter. If the Council wishes to proceed with this type of initiative it is important that a public transport system is developed which allows people (including young families and the elderly) in greater Wellington to visit suburbs which they cannot walk or bike to reliably. Note with New Zealand's ageing population the need for systems which support those with reduced physical ability will only increase (over the next 20 to 30 years) . I recognise that Wellington is a small city by International Standards and has a large catchment area. It will be a challenge to provide a comprehensive public transport system that provides easy / fast door to door options for all of its citizens . The Council must in its planning also recognise that cars are currently being developed and sold that have a smaller environmental impact and the economics of

transport are leading to more car pooling. A complete switch to public transport , walking and cycling will not support the needs of all citizens. I request that the Council take into account the ongoing needs for people to visit Aro Valley businesses and residents by either • changing the plan to reserve a reasonable number of parking spaces for non-residents to ensure that there is the ability for visitors to the suburb to access parking. Or, • prioritising the development of a central parking area in Wellington with on-demand public transport that provides quick and easy transport to areas such as Aro street where parking is almost non-existent. This would support greater city wide connectivity and a city that supports the growth of community and diversity for all Wellingtonians.

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

537

114160412069

NAME: Regan Sayer	SUBURB: Karori	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

A better bike connection between Aro Valley and Karori is fantastic, takes cars off of a narrow road and makes the area more liveable.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**538**

114164149207

NAME: Tom Turton	SUBURB: Karori	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

We deserve safe and attractive streets. I support plans to redesign street space to make it safer and easier for people to walk, ride, scooter, or use public transport. I support changes to on-street parking, and use of 30 kmh limits to make this happen. I support doing the work in one stage, rather than spread out over three stages. That's consistent with the transitional approach. It would deliver the benefits sooner. I don't agree that Aro Valley is unique when it comes to designing street space.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**539**

114174136052

NAME: Thomas	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

I have thought for a long time that we need speed bumps in Aro Valley - so that would be great. We also really need bike parking!! I really support the general project idea of getting people into public transport or walking/cycling, and moving away from private car ownership.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

540

114160129197

NAME: Tara	SUBURB: Aro Valley	ON BEHALF OF:	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback

541

114160281534

NAME: Trace	SUBURB: Highbury	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

The issue with the ability to bike through this area is actually the buses. To be successful the bus route needs to be changed. Its dangerous!

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback**542**

114181454319

NAME: Tristan	SUBURB: Mount Cook	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

These changes are needing to encourage cycling in Aro Valley and allow for safer journeys for all road users on this particularly narrow section of road.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**543**

114177385484

NAME: Trudy	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

I have been living in Aro Valley for 22 years and love it, apart from paying your yearly parking permit and there is still no guarantee you will get a park. Last year I could not park near my home in Devon street and had to park in Epuni Street and my car was stolen!! I am not against cyclists at all.. I love seeing cyclists in our community. What I am against is the loss of parks for the people living in Aro Valley and the stress that it will cause my community. I am for lower speed limits and courtesy towards cyclists, but am against a cycle way through Aro Valley.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**544**

114181220412

NAME: P van Dyk	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Don't know

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Oppose

Why do you think that?

I support more cycle ways in Wellington in places where they can be accommodated without seriously impacting residents. I'm worried that removing 70 car parks on Aro Street will adversely affect residents who cannot cycle or who need a car for work. It will also affect trades people visiting residents, as they will have nowhere to park. The cycle way is a huge change for Aro Valley and it's important to get the balance right between the needs of residents, some of whom experience vulnerability, and those of a few cyclists, many of whom are well-off and don't live in the area. It seems this plan hasn't got the balance right. I support making the whole of Aro Street and Raroa Road a 30 km/h zone, which would avoid the need to remove so many car parks.

Do you support the proposed speed changes on Aro Street?

Strongly support

Decision made by Waka Kotahi

Feedback**545**

114181482662

NAME: Chandra Perera	SUBURB: Other	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Not important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

As a frequent attendee of the School of Philosophy which pays increasingly high rates, I despair at the lack of parking for non residents. Classes are in the evenings. Many of us travel from Kapiti, Whitby, Porirua & the Hutt Valley. A lot of us are in our 60s & 70's. Some even 80's. So using public transport for classes between 7-10pm is very difficult even from Karori on public transport. As a 74 yr old who has paid rates in the Wellington region for 47 yrs, I am being squeezed out of pursuing our activities & interests more & more at this stage in our lives.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

546

114182592174

NAME: Vaughn Scott	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Vaughn%20Scott%20Submission.docx

Hello,

Very supportive of proposal, good luck with the project!!

I am a ratepayer and property owner in Epuni Street. I fully support the proposed parking changes and up-hill bike lane on Aro Street. Good to have raised crossing and reduced traffic speeds in the village.

There will be more parking demand on the side streets, often people park their cars in Epuni Street and take up more than one space. Can you consider marking out the parking bays and put the hockey sticks in (for allocated parking), and extend the coupon parking zone through to Saturday so can get some turnover please. Okay with where you are putting the car share parking on Epuni Street and good that you consider mobility park.

The uphill cycle lane will be good since drivers do get behind you on bike and causes friction.

Please put the other zebra crossing on Aro Street nearest to Devon Street on a table also.

I think you will be challenged with the parking loss but is necessary to give people option for bike usage, this is a really good first step in right direction so good luck with your project!

There is still missing connection from the Aro park towards city and crossing the motorway is still not great: (1) Why is the kerb ramp on Abel Smith Street noted as 'temporary'? Consider building out the kerb at that location to fit in a proper cycle ramp. (2) Consider an uphill cycle lane on Abel Smith Street too so as to connect Little Palmer St. You could probably do it without loss of parking – like what you tried in Island Bay, that would work here! Having only sharrows markings and a poor temp ramp is not great. This laneway 'Little Palmer St' is the safest route from city (going uphill) by bike so should be better quality.

I haven't had time to study the plans in too much detail and nearly forgot, I hope I not too late and that you find my comments constructive. Looking forward to seeing better streets and safer options to get around neighbourhood, particularly the journey into city by bike. Still a long way to go in getting options for travel.

Can you please acknowledge my email feedback for this project,

Nga mihi / thanks,

Vaughn

>>

Vaughn Scott Ph 0204 828 446

Feedback**547**

114172945028

NAME: V. Watson	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

The width of our streets and the topography make the wide, long buses a real danger. I feel sorry for the bus drivers on this route. Source more suitable buses for this route.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**548**

114179065553

NAME: Eve	SUBURB: Kelburn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Because this route benefits all people using the road, not just residents of aro valley, and there is no real downside, except a slight loss of parking. I walk or bike through aro valley five days a week, and am keen to see change to further insure that people are safe.

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**549**

114181197880

NAME: Warren Kaler	SUBURB: Ngaio	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Low importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Street based parking is important for the many properties in the Aro Valley heritage area with no drive on access. Many households consist of groups of adults that need vehicles to go between study and work in a timely manner. Using several busses to get to work, which may also be outside public transport hours, is not feasible. A resident's park per household does not fulfil this need. The current mix of residents and coupon parking needs to be retained. Try getting from university to a Hutt valley based part time job. Parking for tradie's service vehicles, maintaining properties, visitors, and users of the parks is already a problem. Those that find it difficult to walk, bike or bus will no longer patronise the businesses in the area or use the parks. This change may destroy many of these mixed households.

Do you support the proposed speed changes on Aro Street?**Support**

Decision made by Waka Kotahi

Feedback**550**

114172888018

NAME: Will Miller	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

Much safer for cyclists, which will make Wellington a more livable and climate friendly city. I live outside the area, and this route will make it easier for me to access businesses along the route like Kelburn shops. Currently it is tough to get there by bike/e-scooter so I don't go as often as I would like

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback**551**

114164321402

NAME: Will Thomson	SUBURB: Newtown	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Very important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Strongly support**Why do you think that?**

The city needs to future-proof itself

Do you support the proposed speed changes on Aro Street?**Strongly support**

Decision made by Waka Kotahi

Feedback

552

114182646513

NAME: Winston Cochrane	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Wiston%20Cochrane%20Submission.docx

I **oppose** the construction of cycle lanes in Raroa Road and in the Aro Valley on the grounds of community safety, usage and public interest.

Route Access and Topography

- Aro/Raroa Road and Glenmore/Bowen St are the only two major access routes to/from the very populous western suburbs of Karori, Northland, Kelburn, Highbury
-
- BUT
-
- Their topography is vastly different – Raroa Rd is a narrow, winding, hill route prone to land slips; the Aro Valley, the entry/exit point, is an important local community/commercial centre
- Both routes access the CBD and the motorway, but at opposite ends of the CBD, thus alleviating additional traffic usage through the city centre and extra pollution
- Aro/Raroa Rd provides essential and swift access to the public hospital and two private hospitals, the airport, two Wellington colleges, a large university campus, the Wellington tip, the Zoo, the Basin Reserve, Evans Bay indoor sports arena, many commercial enterprises, and the large suburbs of East and South Wellington

Usage

- There is frequent/heavy use of Aro/Raroa Road by a wide variety of vehicular traffic, public, commercial and private. This will continue. For many purposes, there is no alternative efficient route
- Intensive housing in the Aro Valley generates much activity and traffic
- Cyclists represent the tiniest fraction of road users, especially in bad or windy weather; topography alone will dictate that cycling will never be an option for the overwhelming majority of the population
- Vehicles, especially the city's unsuitably large buses for this route, already encounter road congestion, making navigation difficult
- Traffic often cannot flow smoothly; near misses, especially on tight bends, are uncomfortably common; there needs to be additional road planning/restrictions
- Increased parking restrictions in the CBD are ostensibly responsible for greater public parking along the route, on both sides. This situation will worsen
- WCC's granting of building permits along stretches of this sharply winding road is the clear cause of the increase in the number of parked cars; a major access road should not be turned into a suburban street

Safety

- Safe and swift access for ambulances and private vehicle access to hospitals is an essential planning priority, especially in Wellington's difficult topography. Minutes can save lives.
- Fire services must have similar ease of road use. Lives and property depend on it.
- Planning for earthquakes or other public disasters is also a prime communications/access consideration. Optimal access to large suburban

communities must be provided for emergency services. As described already, Raroa Road is narrow and carved out of a pulverised rock hillside on one side and a precipitous gully on the other. There is no possibility for expansion on either side. Therefore, road design to provide for emergency use is paramount

-It will ill serve the safety and health of city communities, as well as the safety of all road users of the Aro/Raroa route, if the difficult conditions and limitations described above should be made worse, or neglected in planning for emergencies, as well as the multiple public and private use made of this route.

Public Interest

-Public safety should be the overriding concern in this matter; that will be adversely affected to a considerable degree, if the major Aro Street/ Raroa Road route is further narrowed to provide for a cycle lane; normal vehicular navigation, but especially ambulance, fire service and public transport, should be facilitated, not hindered

-Wellington City's paramount priority should be to fix the pipe network; this is a matter of public health as well as an environmental concern.

Winston Cochrane



Feedback**553**

114162624258

NAME: Nat	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Important

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection**

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose**Why do you think that?**

Do you support the proposed speed changes on Aro Street?

Neutral

Decision made by Waka Kotahi

Feedback**554**

114160143629

NAME: Speedy	SUBURB: Aro Valley	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection**Aro Valley connection****Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Oppose**Why do you think that?**

Even with current settings there's barely enough space for two busses to cross each other. Busses aren't getting smaller but roads are as per the proposed plans.

Do you support the proposed speed changes on Aro Street?**Neutral**

Decision made by Waka Kotahi

Feedback

555

114173906380

NAME: Zoe	SUBURB: Brooklyn	ON BEHALF OF: An individual	ORAL No
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Moderate importance

Which route proposal/s would you like to have your say on?

Aro Valley Connection

Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?

These include traffic resolution TR173-22

Stongly oppose

Why do you think that?

There is not enough parking for all the people who live in the area as it is. Removing cars completely is not realistic & will take some time. How will elderly or disabled people be able to use this street?

Do you support the proposed speed changes on Aro Street?

Support

Decision made by Waka Kotahi

Feedback

556

114187833845

NAME: Thomas McGrath	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Thomas%20McGrath%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Epuni Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Providing a bike lane on Aro Street and bike parking (and restricting car parking in various places) gives advantage to a minority group (cyclists) at the expense of the majority (motorists, bus passengers etc). As a cyclist, I can say cycling is good exercise but (irrespective of whether the bike is an e-bike or not) is best done in dry weather on wide roads. It is relatively inefficient - and only moves 1-2 people at a time. Bus transport moves many more people at a time (and if car parks are being lost on Aro Str more thought to bus stops and bus lanes is needed).

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

As a cyclist, I often use Raroa Rd to get from Keron to the Polhill Gully Mountain bike area. In using Raroa Rd I note that very few others are biking on the road (because it's narrow and hilly). I notice that if people aren't walking on the Raroa Rd footpaths, people are driving on the road and from time to time the ^{scheduled high bus} service operates. Raroa Rd is narrow and the uphill lane for cyclists (proposed) won't be safe.

The disadvantages of cycling I referred to in my previous response — minority activity, weather dependent activity, etc. also apply here. If private car access on Raroa Rd is going to be restricted, more thought should be given to improving access for buses.

Changes proposed for Aro Park and Little Palmer Street

We know that people are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride through to the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People riding bikes along this route?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?



Staged approach for Aro Valley Connection

We're proposing to make changes along Aro Street using a staged approach over two years, to help residents in the area to adapt to the parking changes. The first stage addresses safety concerns and provides better transport options for people that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

- ☒ Make the proposed changes more quickly ☒ The timing of the stages is about right ☒ Make the proposed changes more slowly ☐ Don't know

Why do you think that?

I don't support any of the timing for implementation (as outlined above) because I don't support the installation of bike lanes on Aro Street and Raroa Rd. Given the plan is intended to reduce car use and provide better transport options by such measures as removing car parks, more thought should be given to improving bus transport in the area - bus stops, bus priority lanes - for example.

Do you support the overall proposed changes to the Aro Valley Connection route?

These include traffic resolution TR179-22.

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

I don't support it (for reasons outlined earlier) - and would add that if the intention of Council is to have private car use reduced in Aro Valley and on Raroa Rd, more thought should be given to improving bus access and availability because bus transport can enable large numbers of people to travel at one time - safely.

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support ☐ Support ☒ Neutral ☐ Oppose ☐ Strongly oppose ☐ Don't know

Why do you think that?

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☒ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☒ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☒ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- ☐ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☐ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback

557

114187829903

NAME: E Tillot	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

E%20Tillot%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

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The street changes planned for this route include:

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- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
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We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

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- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Epuni Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Epuni Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☒ Oppose ☐ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

The homeowners will lose many car parks. Very few off street parks. Many are elderly, with young children, disabled. Very hard to manage without parking nearby. Very close to city - meaning many non-residents block parks.

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support

☐ Support

☐ Neutral

☒ Oppose

☐ Strongly oppose

☐ Don't know

Do you have any comments to make about the proposed design?

loss of parking for
local residents.

Difficult access for mum
with infants, elderly,
injured & disabled.

It's fine for single young
men (who mostly bike)
& some young women
to use bike lanes but
they are not the
majority.

These homes will lose
value too.

Your relationship with the area

What is your **main** relationship to the area? **Please tick one:**

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☐ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☒ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? **Please tick one:**

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☐ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

- should have more urban - it needs to say this needs to be just one*
- ☐ Very important
 - ☐ Important
 - ☐ Moderate importance
 - ☒ Low importance
 - ☐ Not important
 - ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☒ No ☐ Yes If yes, please provide your contact number:

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

Feedback**558**

114187827222

NAME: Maysie Pyatt	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Why do you think that?

To whom it may concern, My partner and I are renters living on Holloway Road in Aro Valley. I am emailing regarding the alarming proposals to happen on Holloway Road. I completely disagree with the reduction of parking to create bike lanes and allow buses onto Holloway Road. As a car owner on this road, it can often be difficult already to find parking close to your house. There are a lot of people living on this road who require parking as well as cyclists who park near the trails. I have also noted that the residents on our road are not getting rid of their cars for climate change but replacing them with electric cars so parking is still needed. Holloway road is essentially a one-way road due to how slim it is. Despite this, drivers and cyclists are kind to each other and understand how tricky it can be to commute on this road. The addition of a cycle lane feels unnecessary due to this understanding as well as an inconvenience for people trying to move about the road while it would be getting built. Regarding the idea of allowing buses up Holloway road, this seems completely unrealistic and unnecessary. With our road being so thin, the idea of a bus trying to turn around seems impossible. I would not envy any bus driver who had to struggle with this road. We already have a bus stop at the end of our road and is a nice easy walk of at most ten minutes if you are walking from the other end of the road. Having a bus come down Holloway Road is again unnecessary as well as would disrupt the peaceful environment created. In saying all of these things, if the council is able to come up with a way for cars to pass each other easier on our road without reducing parking, I would be very interested in this. I am happy with making our road safer, but especially having buses on our road will not achieve this. I would like to say that I also do not agree with removing 70 residents and coupon car parks from Aro Street. Such as the parking on Holloway Road, it is almost always full of cars. I doubt that these cars will go away anytime soon and will most likely be replaced with electric cars. I think the smartest move for parking on both Aro Street and Holloway Road would be to keep the carparks but put in more electric car charging stations. Overall, I am happy for the streets and roads in Aro Valley to become safer, but reducing carparks will not help residents. For myself, I will not be getting rid of

my car as I love driving and especially with Wellington's weather, I do not find it practical to use public transport or walk/cycle most of the time. I do have plans to replace my car in the future with an electric car though. I would like this need for people moving from petrol/diesel cars to electric cars to be taken more seriously by the council as a whole and instead of reducing parking, introduce electric charging parks. I am aware that I have missed the feedback deadline. This is due to family illness and I have not had the time to respond until now. As the final decisions are not being made until mid-December, I am hoping this email will be added to the feedback or at least acknowledged. Kind regards and hoping for a better Wellington, Maysie Pyatt

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Feedback**559**

114187823286

NAME: Robert Fisher	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Why do you think that?

Design team Re your letter dated 31st October. Aro Street moves quite smoothly. People, cyclists and cars find their own way with few hold ups. Why try and fix something that works. Removing the remaining 70 parks on Aro Street is simply an insult to residents and will lower the value of properties. First home buyers....what does the mother of a toddler do in the pouring rain trying to get her child to preschool? What does a mother with baby or small child going to the supermarket do in the pouring rain...or to the doctor.....or the school of her choice? Forget the bike lane, forget the speed bumps, forget the speed change. They are irrelevant in Aro Street. Why proliferate the mess you are making of Wellington, particularly the removal of parks. Not everybody can ride a bike or scooter, the bulk of elderly people use a car, including those in Aro Street. Please see reality, our terrain is not Amsterdam! Would appreciate a reply. Owner 43 Aro Street Robert Fisher

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Feedback**560**

114187820710

NAME: Ben Young	SUBURB:	ON BEHALF OF:	ORAL
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection**Do you support the proposed changes to the Aro Valley connection route?**

These include traffic resolution TR173-22

Why do you think that?

To Whom it may concern , As a Aro Valley resident ,I am opposed to the cycle lane ,as it has not worked in Island Bay, with the fact that Island Bay had a wider road then Aro Street/ Valley. As you would know the current width of the road makes it difficult for the buses/larger vehicle's to navigate, with traffic having to give way to buses/larger vehicles approaching in the opposite direction, and being caught behind buses/larger vehicles going in the same direction . With the removal of 70 Resident street carparks on the southside, will this mean that the Residents Parks be reinstated on the north side, and buses will run more frequently and on time ?to accommodate for the extra 70 people needing to move around the wider Wellington district for work . As with the cycle Lane Unless the gutter side of the edge is levelled off , I don't see any cyclists using the 1/3 of the lane closest to the gutter, so that would waste precious roadway, on a already narrow street. If at a latter stage it is decided to level the cycle way then comes the problem of blocking of storm water drainage, which already exists. If this goes ahead I would want a letter that Guarantees that Council/Whaka Kotahi will be liable for any damages occurred this initiative. I thank you for your consideration in regards to this matter and await your response Regards Ben Young

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Feedback

561

114184540983

NAME: A Bramly	SUBURB:	ON BEHALF OF:	ORAL Yes
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General feedback

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport?

Which route proposal/s would you like to have your say on?	Aro Valley Connection
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Aro Valley connection

Do you support the proposed changes to the Aro Valley connection route?
These include traffic resolution TR173-22

Why do you think that?

Do you support the proposed speed changes on Aro Street?

Decision made by Waka Kotahi

Avryl%20Bramly%20Aro%20Submission.pdf

Have your say on pedestrian and bike improvements in Aro Valley

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

Wellingtonians have asked for better transport options, action on climate change and more accessible housing so Wellington City Council is getting on with it. As part of preparing our city for the future, we're planning interim improvements for people walking, riding bikes, and using buses from Karori to the city through Aro Valley.

We're seeking your feedback on these proposed street changes until 5pm Monday 21 November.

The proposal aims to make it safer and easier for more people to walk and ride and deliver more efficient and reliable bus trips. These changes will enable more people to get around in zero or low-carbon ways, support growing neighbourhoods, and free up space for people who really need to drive. If approved, the changes will be installed using materials that can be adapted as needed. We've developed the designs based on technical advice and consultation with the community.

Aro Valley Connection

The Aro Valley Connection project will make it safer and easier for more people to walk, bike, scoot and bus into the city from Karori, Highbury, Kelburn and Aro Valley.

The street changes planned for this route include:

- New street layout with a separated uphill bike lane on Aro Street, and uphill bike lanes and cycle shoulders on Raroa Road, with sharrows in the downhill direction
- Extended 30km/h zone further along Aro Street through to Willis Street
- New raised pedestrian crossing and new speed cushions on Aro Street to reduce speeds
- New car share parking spaces and new mobility car park on Eponi Street
- Better bike parking in the shopping village
- Changes to parking on Aro Street, Raroa Road, and some side streets
- Changes to the paths through Aro Park and along Little Palmer Street to create shared paths

We want your feedback to see if there are any improvements that can be made before we make these changes.

You can view the full project details for Aro Valley Connection at transportprojects.org.nz/arovalley

You'll be asked about the proposal as a whole, and you'll also have the opportunity to provide detailed comments if desired. We'll also ask general questions about these changes, and you can attach any relevant photos, sketches, or documents if you wish. Feel free to skip questions. You don't have to answer them all.

You can contact us at arovalleyconnections@wcc.govt.nz if you have any questions or you can visit a Aro Valley drop-in session if you need help filling out a submission.

Changes proposed for the Aro Street section

We are proposing changes for the road layout along Aro Street to include:

- New bike lane on the uphill side
- Raising the pedestrian crossing by Aro Park
- Extending the 30km/h speed zone down to Willis Street
- Better bike parking in the shopping village
- Removing one bus stop outside 47b Aro Street
- Introducing car share (one car, one van) and a mobility park on Eponi Street.
- Parking changes on the south side of Aro Street rolled out over two years.
- These changes will also require some parking changes for Aro Street, Ohiro Road, Eponi Street, and Holloway Road.

Do you support the proposed changes for this section of the route on Aro Street (between Willis Street and Holloway Road)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Changes proposed for the Raroa Road section

We are proposing changes for the road layout along Raroa Road to include:

- Installing uphill bike lanes and cycle shoulders on Raroa Road through to Raroa Crescent, with sharrows in the downhill direction
- Installing a new bus stop opposite 125 Raroa Road (at the bottom of the Harrold Streetpath)
- Removing all on-street parking space on the uphill side, and putting broken yellow lines on the corners in the downhill direction to improve safety and visibility
- Moving two bus stops between Cluny Avenue and Fairview Crescent 10-15 metres to make it easier to access buses.

Do you support proposed changes for this section of the route on Raroa Road (between Holloway Road and Chaytor Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Bus & other changes should not be included in cycleway projects.

Why do a few cyclists need multiple ways from home

Proposed for Aro Park and Little Palmer Street

People are currently using the footpath through Aro Park and along Little Palmer Street as a quiet route to ride into the city.

We are proposing to widen these footpaths from 1.5m to 2.5m and improve their condition to create proper shared paths that would be safer for people, both walking and cycling, to pass each other.

What do you think the impact of widening the path would be for:

People using Aro Park for recreation (e.g. picnicking)?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People walking on the paths through Aro Park and Little Palmer Street?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

People riding bikes along this route?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People driving vehicles on the surrounding streets?

☐ Very positive ☐ Positive ☒ Neutral ☐ Negative ☐ Very negative ☐ Don't know

People with disabilities or accessibility issues?

☐ Very positive ☐ Positive ☐ Neutral ☐ Negative ☒ Very negative ☐ Don't know

Do you support the proposed changes for the connection between Aro Street and Willis Street (via Aro Park and Little Palmer Street)?

☐ Strongly support ☐ Support ☐ Neutral ☐ Oppose ☒ Strongly oppose ☐ Don't know

Do you have any comments to make about the proposed design?

Cyclists and other small wheel electric vehicles already display massive amounts of entitled and unsafe behaviour around roadways and this only encourages them.

Plan for Aro Valley Connection

...g to make changes along Aro Street using a staged approach over two years, to help residents in the
not to the parking changes. The first stage addresses safety concerns and provides better transport options for
that can help reduce the number of cars in the area over time, before further changes are made.

Stage one - 2023

- Extend the existing 30km/h zone on Aro Street by 150m from the School of Philosophy to Willis Street
- Raise the pedestrian crossing on Aro Street by Aro Park
- Install four sets of speed cushions along Aro Street
- Add two new car share parking spaces on Epuni Street
- Add one new mobility car park on Epuni Street
- One car park in the shopping area will be changed to six bike parks
- Mark/paint car parks on Aro Street
- Install 100m of the uphill separated bike lane from 171-197 Aro Street
- Install the uphill bike lane along the top end of Aro Street from Holloway Road, on Raroa Road to Raroa Crescent
- On Raroa Road, remove all on-street parking on the uphill side and at corners on the downhill side
- Widen and improve the paths through Aro Park and Little Palmer Street
- Remove three coupon car parks on Aro Street
- Introduce 35 residents car parks and 33 residents and P120 parks on Holloway Road.

Stage two - 2024

- Install the remaining 400m of uphill separated bike lane from 117 Aro Street to Holloway Road
- Remove 48 coupon car parks on the south side of Aro Street.

Stage three - 2025

- Install the final section of bike lane on Aro Street, from Willis Street to the shops at Alameda Terrace
- Change 20 coupon car parks to residents parking on Ohiro Road
- Remove 18 residents' car parks and four coupon car parks on the south side of Aro Street.

What do you think of the timing of the proposed approach?

☐ Make the proposed changes more quickly

☐ The timing of the stages is about right

☒ Make the proposed changes more slowly

☐ Don't know

Why do you think that?

Don't make cycle changes at all. Private transport option costing far too much that crowds out better green transport projects. ~~Not a transport~~ Non exclusive non directly very few potential users

What do you think about the overall proposed changes to the Aro Valley Connection route?

Local traffic resolution TR179-22.

☐ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☒ Strongly oppose

☐ Don't know

Why do you think that?

as above

Do you support the proposed speed changes on the Aro Valley Connection route?

Extending the 30km/h zone in the Aro shopping area 150m through to Willis Street. This is a separate decision from the other proposed changes, and the final decision is made by Waka Kotahi NZ Transport Agency as per the Setting of Speed Limits Rule 2022. We're consulting on both the road and speed change decisions at the same time, as the changes would be delivered together to achieve the best outcome for the community.

☐ Strongly support

☐ Support

☐ Neutral

☐ Oppose

☒ Strongly oppose

☐ Don't know

Why do you think that?

as above
needs to be consulted apart from cycleways

Relationship with the area

What is your main relationship to the area? Please tick one:

- ☐ I live in the area
- ☐ I work in the area
- ☐ I own or manage a business in the area
- ☐ I go to school or education in area
- ☒ I visit the area (e.g. to see friends or businesses)
- ☐ I do recreational activities in the area (e.g. running, walking etc.)
- ☐ I drop my kids at childcare, school or education in the area
- ☒ I travel through the area
- ☒ I live in Wellington
- ☐ I don't have a relationship to the area

How do you normally travel along the Aro Valley route? Please tick one:

We understand you may use a number of different ways to travel around this area, we would like to know what mode you use most of the time.

- ☒ Car/Van *hybrid*
- ☐ Commercial vehicle (e.g. van or truck)
- ☐ Bicycle
- ☐ Walk/run
- ☒ Bus
- ☐ Motorcycle or motor scooter
- ☐ E-scooter, skateboard etc
- ☐ Wheelchair or mobility scooter

City goals and network questions

How important is it to rebalance our existing street space to make it safer and easier for people to walk, ride, scooter, or use public transport? *started question*

- ☐ Very important ☐ Important ☐ Moderate importance ☐ Low importance ☒ Not important ☐ Don't know

Would you like to speak to Councillors in support of your submission?

This usually involves a 5 min presentation in support of your submission to all Councillors or participation in a discussion forum with a small group of Councillors. We are planning the hearing or forum for 1 December, if you select 'yes', one of our team will be in touch to confirm.

- ☐ No ☒ Yes If yes, please provide your contact number:

[Redacted contact number]

Are you providing feedback as ☒ An individual ☐ On behalf of an organisation ☐ On behalf of a primary or secondary school

Name of organisation

Name of school

*Do not
publish this
privacy reason*