

27 January 2023

[REDACTED]
Kia ora [REDACTED],

Ngaio connection

Thank you for your request made under the Local Government Official Information and Meetings Act 1987 (the Act), received on 14 December 2022. You requested the following information:

- I request information on the Ngaio connection. In particular I request all draft versions of the Ngaio Parking Management Plan Traffic Resolution and TR180 22 Ngaio Transitional Cycleway.
- I also request all correspondence between the council officers and the contractor engineers in the preparation of these drafts.

Wellington City Council has granted your request for information. Below are the documents that fall in scope of your request, all have been released in full and any redactions made are for privacy reasons under section 7(2)(a) of the Act.

Item	Document name/description
1.	Email 18 October 2022 Draft Ngaio Parking Management Plan (traffic resolution issue)
1a.	Attachment to above email
2.	Email 13 December 2022 Draft Ngaio PMP - Cameron Street updates
2a.	Attachment to above email
3.	Email 10 October 2022 Ngaio Traffic Resolution deliverables
3a.	Attachment one SOQ
3b.	Attachment two Step Change comments
3c.	Attachment three comment tracker
3d.	Attachment four
4.	Email 29 November 2022 - RE: Another Ngaio design question
5.	Email 14 December 2022 - RE: Another small change to the Ngaio Connections design
6.	Email 21 October 2022 - RE: Changes for Ngaio TR
6a	Attachment one
6b	Attachment two

7.	Email 18 October 2022 RE: Draft Ngaio Parking Management Plan (traffic resolution issue)
8.	Email 28 October 2022 RE: Final drawings for Ngaio - URGENT
8a.	Attachment one
9.	Email 10 November 2022 Ngaio Connection - Kenya St questions, and draft observations
10.	Email 27 October 2022 RE: Ngaio crossing stats
11.	Email 14 December 2022 RE: Ngaio Cycleway approved by council today
12.	Email 25 October 2022 RE: Ngaio Gorge Road ped crossing issue – URGENT
13.	Email 25 October 2022 RE: Ngaio Gorge Road ped crossing issue
14.	Email 14 October 2022 RE: PMP and minor changes for Ngaio
15.	90 to 100 design changes
16.	Email 12 October 2022 RE: Specific sightline issue for Kaiwharawhara Road pedestrian crossing
17.	Email 17 October 2022 RE: Street mix images
17a.	Attachment one: Cameron Street
18.	Email 29 November 2022 RE: Submission requiring your response

Right of review

If you are not satisfied with the Council’s response, you may request the Office of the Ombudsman to investigate the Council’s decision. Further information is available on the Ombudsman website, www.ombudsman.parliament.nz.

Please note, we may proactively release our response to your request with your personal information removed.

Thank you again for your request, if you have any questions, please feel free to contact me.

Kind regards

Claudia Holgate
Senior Advisor, Official Information

From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ann-Marie Head](#); [Ryan Dunn](#)
Subject: Draft Ngaio Parking Management Plan (traffic resolution issue)
Date: Tuesday, 18 October 2022 11:34:32 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[ATT00001.jpg](#)
[DRAFT Ngaio Parking Management Plan - Traffic Resolution issue.pdf](#)

Hi Jonathan

Please find attached the draft Parking Management Plan for the Ngaio Transitional Cycleway traffic resolution.

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

Tonkin + Taylor - *Exceptional thinking together*

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WCC Transitional Cycleways Parking Management Plan

Ngaio

17 October 2022



**Absolutely Positively
Wellington City Council**

Me Heke Ki Pōneke

Version	Date	Description	Author	Approver
1	29/07/2022	Draft for client review	J Morse	A Head
2	17/10/2022	Draft issue for traffic resolution	J Morse	A Head

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1. Introduction

The purpose of this report is to assess and analyse the parking demand and behaviour in Ngaio as part of the Ngaio Transitional Cycleway Project.

The Transitional Cycleways Programme, led by Wellington City Council (WCC) and alongside Lets Get Wellington Moving (LGWM), will take a new approach to community engagement and installation to help increase the pace of change. By using lower-cost materials that can be adjusted once they are in place, the city can install an interim bike network and gain feedback in real time. This will also inform future permanent changes while gaining benefits earlier.

For the project this route has been split into five distinct sections: Kaiwharawhara Road (Ngaio Gorge Road to Hutt Road), Cameron St (Kaiwharawhara Road to the Bridle Track), Ngaio Gorge Road (Perth Street to Kaiwharawhara Road), Kenya Street (Crofton Road to Ngaio Gorge Road) and Crofton Road (Ottawa Road to Kenya Street). These sections are shown in Figure 1. Parking demand on the adjacent streets to the transitional cycleway corridor are also considered in this report. For each section, the report examines current parking demand and drivers of parking demand on the street.

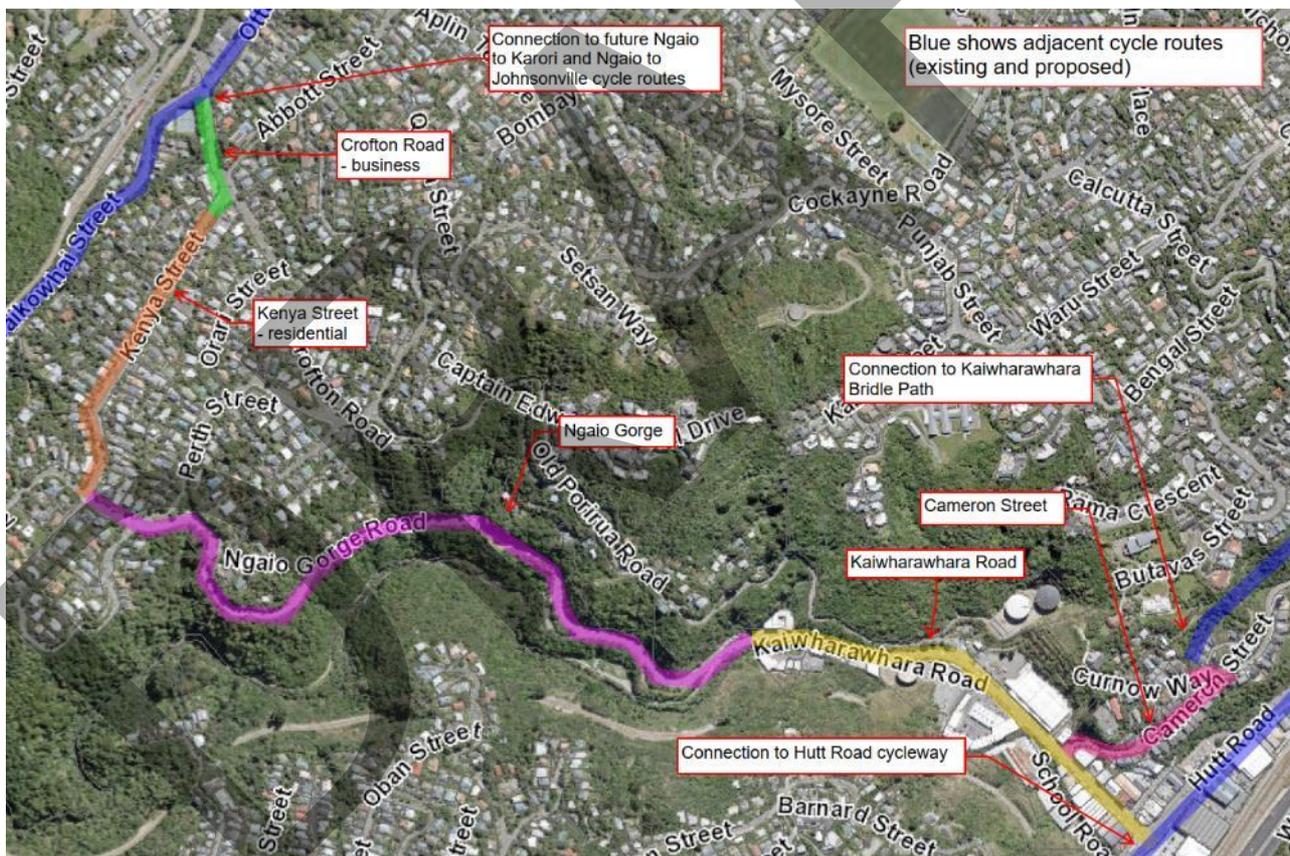


Figure 1 Extent of transitional cycleway and sections

For this report the parking inventory quoted is the number of parking spaces available, typically, on-street. Where spaces are unmarked, this is determined based on the kerb length, and assuming one space for every 6m (rounded down).

1.1. Wellington Parking Policy 2020

Wellington City Council adopted an updated Parking Policy in August 2020. The parking policy sets the objectives and principles for the management of Council-controlled on-street and off-street parking, and how parking supports achieving the vision for Wellington.

The Council’s vision for Wellington is built around people and communities. The future city will be a place where people and goods can easily move to and through the city, based on a transport system that can accommodate moving more people using fewer vehicles. The city has also set a goal to be a zero-carbon capital by 2050 and transport will play a key role in achieving this goal.

The policy acknowledges that Wellington needs a more efficient transport system that makes better use of limited road space. This means moving more people using fewer vehicles; using public transport more; more people walking and on bikes, and fewer people driving and parking in busy areas. Achieving this will mean removing some on-street parking spaces on key transport routes, reallocating on-street road space to support active and public transport, and re-prioritising the remaining on-street space.

The policy establishes a parking space hierarchy for different parts of the city to ensure that limited parking supply is prioritised appropriately. The parking space hierarchy describes which types of parking have the highest and lowest priorities in different areas. It also sets out the priority level for each type of parking space, rather than the number of spaces. The hierarchy for outer residential areas applies to the Ngaio Transitional Cycleway project and is shown in Table 1.

Table 1 Parking space hierarchy for Outer Residential Areas

Priority	Outer Residential Areas
Highest priority	Safe and efficient movement of people and goods
High priority	Bus stops Urban design features Residents
Medium priority	Car share Mobility Electric-vehicle charging Coach and bus (Short Stay)
Low priority	Short-stay parks (car & motorcycle) Loading zones
Lower priority	Bicycle/micromobility Small passenger service vehicle (SPSV)/taxi stands Commuter (car & motorcycle) Coach and bus (long stay)
Lowest priority	Long stay parking of private non-motorised vehicles

This report considers the impact of the proposed cycleway upgrades on the number of car parks available and the ability of users to access local destinations using these car parks, both before and after mitigation.

A six-point scale is used to assess the level of impact, as outlined in Table 2. This table is used to assess the ability of displaced users to find a similar parking space within a certain walking timeframe. This scale of impact considers the occupancy of the alternative parking spaces.

Table 2 Level of impact scales for parking removal

Level of Impact	Definition
Very High	Removal of parking spaces has a very high impact on the ability of users to find a parking space in the vicinity of their destination. Alternative parking spaces of the same type are not available within walking distance.
High	Removal of parking spaces has a high impact on the ability of users to find a parking space in the vicinity of their destination. Alternative parking spaces of the same type are available within a 10-minute walking distance.
Moderate	Removal of parking spaces has a moderate impact on the ability of users to find a parking space in the vicinity of their destination. Alternative parking spaces of the same type are available within a 5-minute walking distance.
Low	Removal of parking spaces has a low impact on the ability of users to find a parking space in the vicinity of their destination. Alternative parking spaces of the same type are available within a 3-minute walking distance.
Very low	Removal of parking spaces has a very low impact on the ability of users to find a parking space in the vicinity of their destination. Alternative parking spaces of the same type are available within a 1-minute walking distance.
None or N/A	No impact on the ability of users to park and access local destinations or not applicable because this type of parking is not present.

1.2. Occupancy Threshold

The figure of 85% represents a parking occupancy above which traffic circulation will be high as motorists search for an available car park and may not be able to find an available car park space. Non-compliant parking may also be widespread and illegal parking can be common in situations where high occupancy is occurring. The occupancy of 85% is discussed in “Parking Pricing Implementation Guidelines” (T. Litman, Victoria Transport Policy Institute, 2010). The occupancy of 85% is considered to be a threshold value in the Wellington City Council Parking Policy (2020). It is further noted that if peak parking occupancies are well below this threshold there is inefficient use of the road space allocated exclusively for parking.

1.3. Parking survey methodology

Parking surveys were undertaken from 9am to 5pm on Thursday 12th May and Saturday 14th May 2022. A survey was undertaken on two days to understand parking behaviour during both a weekday (predominantly to understand commuter parking behaviour) and a weekend (where resident, shopping and recreational parking behaviour is more prevalent). The weather on the Thursday survey was fine, however, there were showers during the Saturday survey. This weather is not expected to have significant implications on the results. This survey does not measure any differences in behaviour during different seasons, however, it is considered to provide an accurate picture of typical parking demand and characteristics in Ngaio. In addition, a weekday and weekend overnight snapshot survey was undertaken to assess the level of occupancy in the area after working hours.

This survey involved assessing both the occupancy and duration of stay of all vehicles parked within the study area (See Figure 2). The duration of stay and occupancy was recorded by recording the first four characters of the license plates of vehicles parked on-street or within Council controlled parking areas each hour during the survey. This involved recording vehicles on both the roads where the cycleway is proposed, and the surrounding streets to attain an overall picture of parking behaviour. Duration of stay has been assessed on all streets in the study area. This allows for any changes to on-street parking availability as a result of the transitional cycleway to be managed effectively.

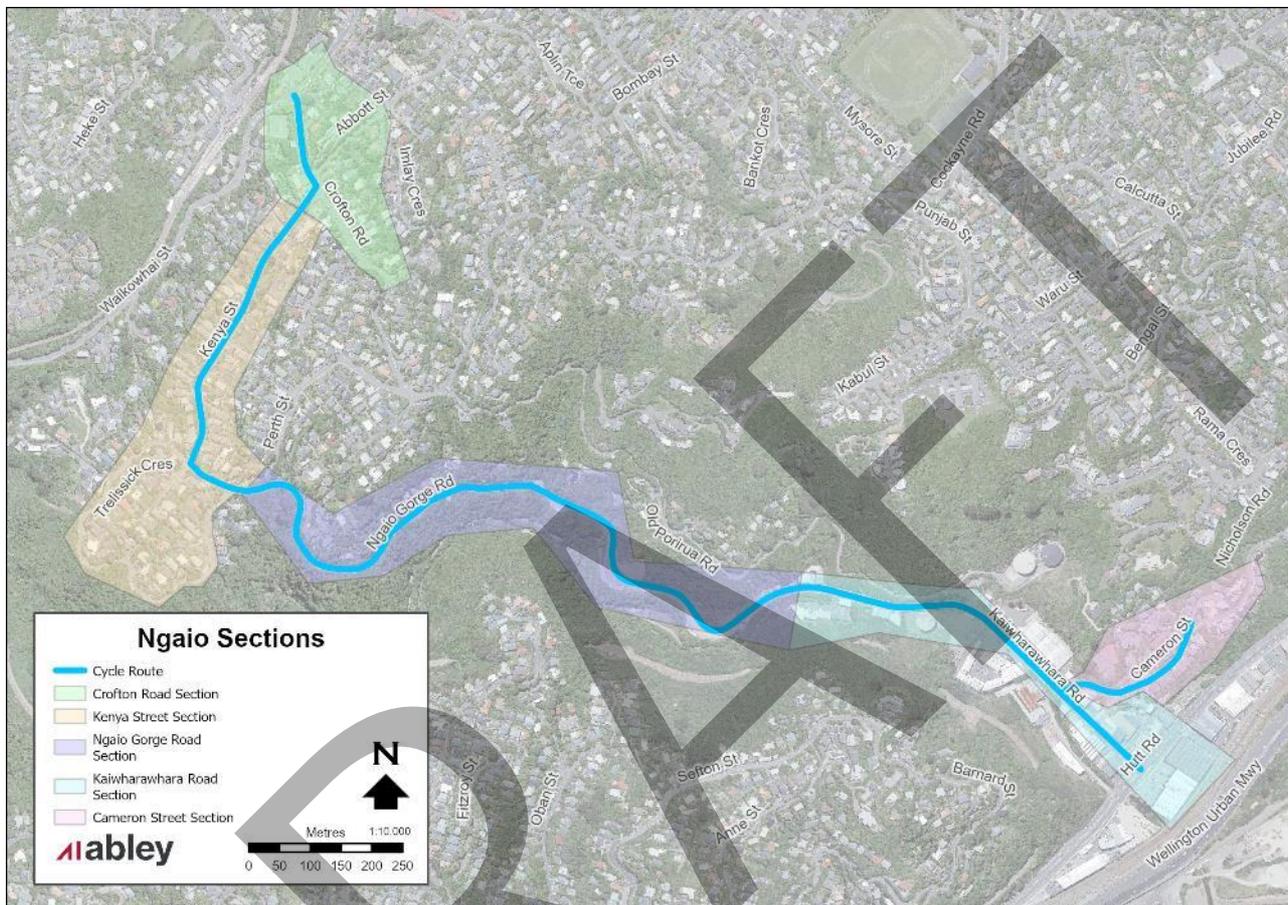


Figure 2 Ngaio study area

1.4. Impacts of Covid-19

During the time of the survey occurring, New Zealand was in the orange traffic light setting of the COVID-19 protection framework. The largest impact this setting has on the survey results is that people are encouraged to work from home where possible. Therefore, it is expected that there is less commuter demand into the city, and the demand for parking by residents may be higher than anticipated as there is more working from home. This effect is difficult to quantify, so the data obtained has not been modified to account for this.

2. Kaiwharawhara Road

2.1. About the area

The Kaiwharawhara Road section is a primarily commercial section at the southern end of the proposed cycleway corridor. This section connects the bottom of the Ngaio Gorge to Hutt Road. This area has multiple cafes, retail businesses, commercial/industrial businesses, a courier post depot, and other various commercial activities. At the northern end of this section, roadworks were occurring on the Ngaio Gorge during the parking surveys (major works relating to the large slip at the lower end of the gorge). These roadworks did not affect the parking supply in this area. This section is shown as Figure 3.

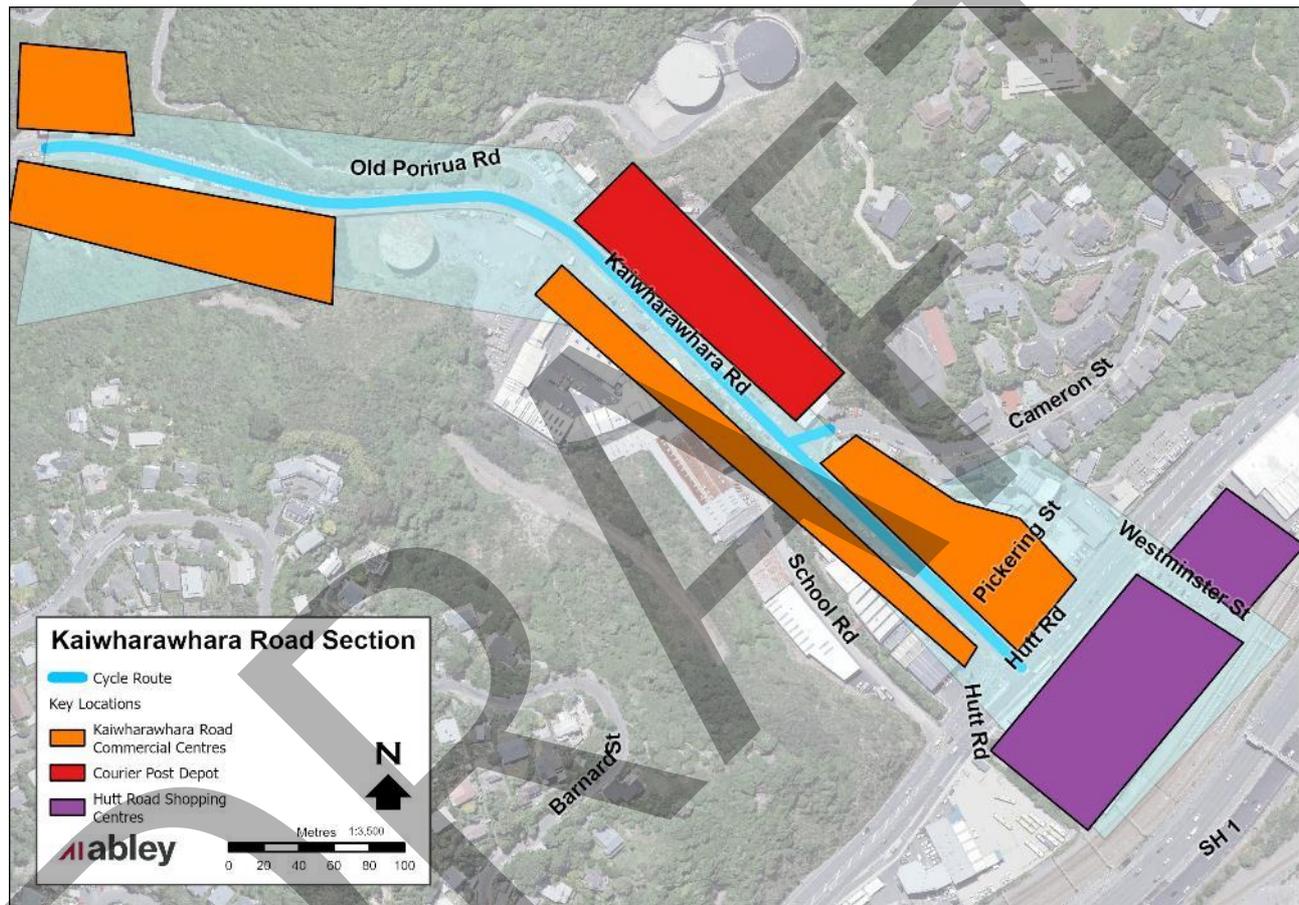


Figure 3 Kaiwharawhara Road parking study area

2.2. Current parking and usage

The parking in this section is a mixture of both unrestricted and time restricted parking (See Appendix A). The inventory and type of parking in this section is shown in Table 3. The parking in this section is all on-street, however, given the low volumes and no exit nature of Westminster Street, it functions as a pseudo-off-street parking facility. There is one mobility park and one loading zone on Westminster Street. The eight (8) P60 parking spaces on Cameron Street are considered to be part of this section for the analysis. This is because it is expected that most people who use these parks are accessing the businesses on Kaiwharawhara Road. The parking spaces on the north-eastern side of Kaiwharawhara Road are not available when the bus lane is operating between 7-9am on weekdays. The parking spaces on School Road were not surveyed.

Table 3 Parking inventory in the Kaiwharawhara section (including number of spaces on Kaiwharawhara Road)

Restriction	Overall section inventory	Kaiwharawhara Road inventory (towards Ngaio)	Kaiwharawhara Road inventory (towards City)*
Unrestricted	125	43	74
P60	31	0	0
P30	19	3	16
Mobility	1	0	0
Loading	1	0	0
Total	177	46	90

*Note this inventory is the parking spaces available when the bus lane is not operating

The parking occupancy on both days of this survey is shown as Figure 4. The occupancy of the overall area and the parking just on Kaiwharawhara road is shown. This occupancy has been compared to the occupancy threshold of 85%. The parking occupancy in this section varies significantly between the two survey days. The parking occupancy on the Thursday peaks at 85% on Kaiwharawhara Road. On the Saturday the peak on Kaiwharawhara Road is 45%. This is expected given the high level of business activity that occurs along Kaiwharawhara Road. During the Thursday survey, the occupancy of this section overall was above 70% for most of the day (10am-2pm). This is a high occupancy and is driven by visitors/staff of the businesses in this area.

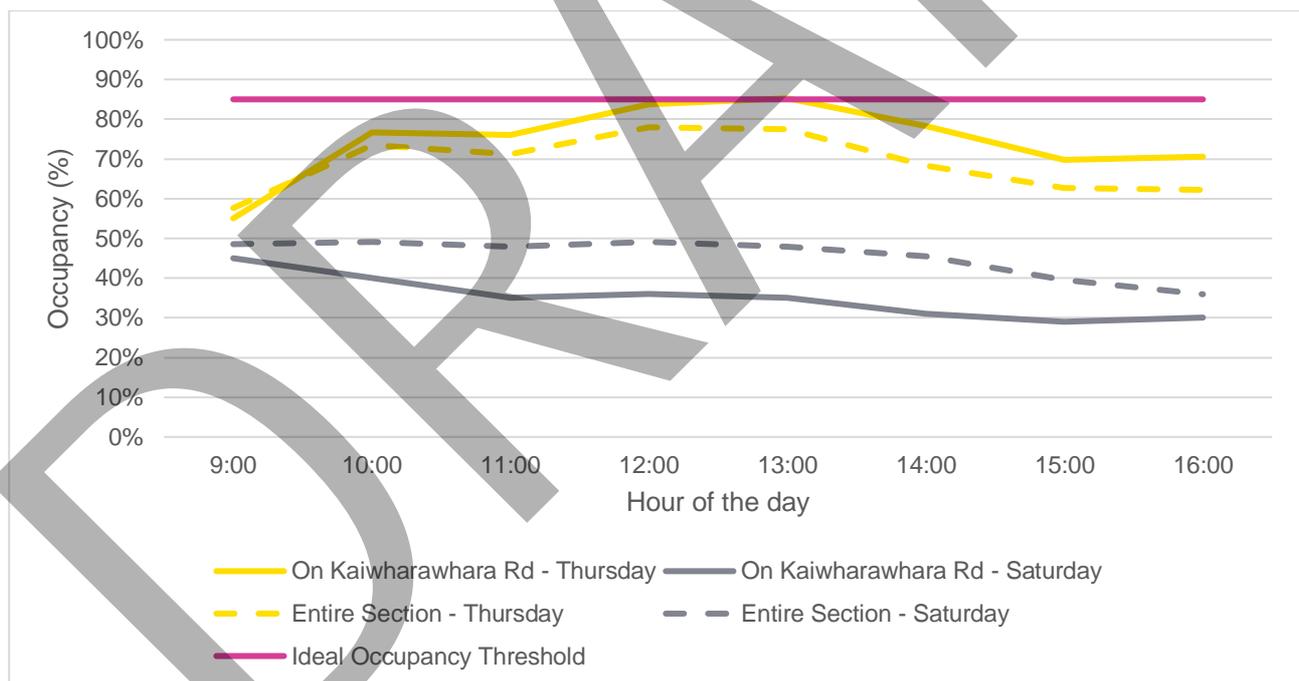


Figure 4 Kaiwharawhara Road parking occupancy on both survey days compared to the occupancy threshold of 85%.

Figure 5 shows the parking occupancy for the different restriction types in this section. The time restricted parking is typically less occupied than the unrestricted parking on the Thursday, however, on the Saturday, the time restricted parking is significantly more occupied.

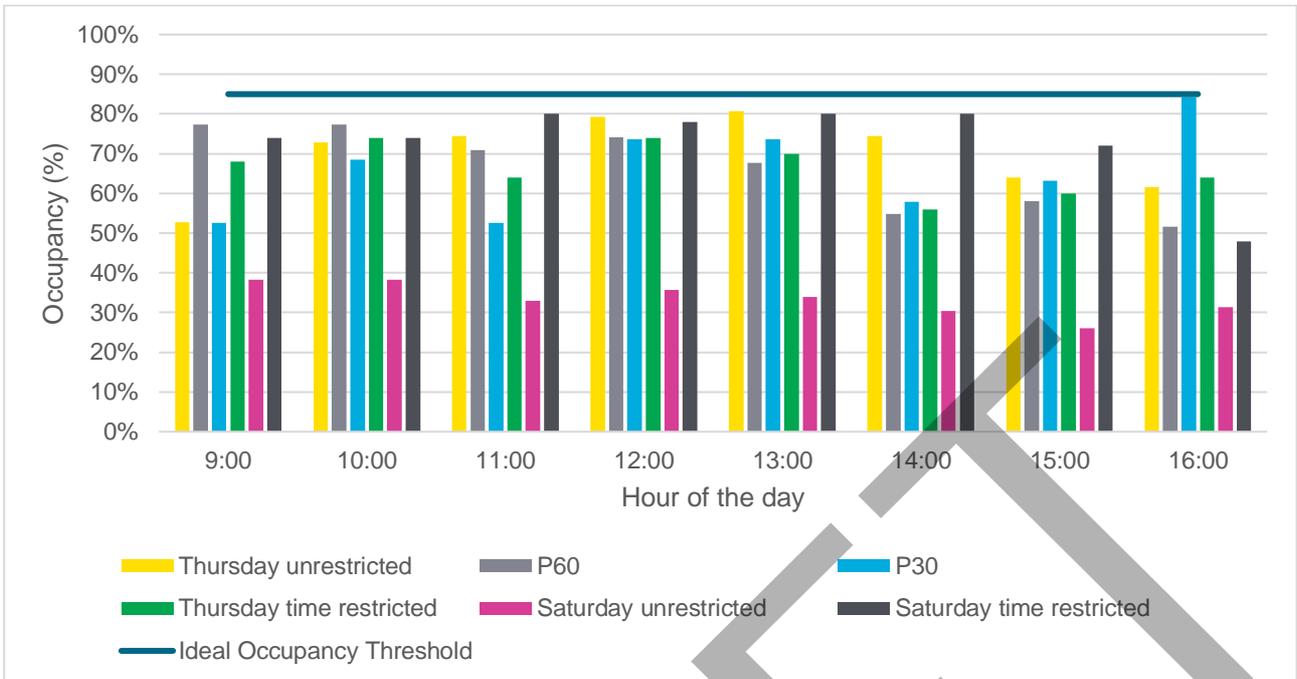


Figure 5 Comparison of parking occupancy between unrestricted and time restricted parking spaces

Figure 6 and Figure 7 show the parking occupancy per street in this section. This data shows that there is uniform parking demand on the Thursday, as nearly all the section is over 80%. This is not the case on Saturday, as there are concentrated areas of high parking demand on Pickering Street and Westminster Street, however, the rest of the section is relatively unoccupied. This demand is expected on Westminster Street given the proximity of the local businesses (Animates, cafes, etc.).

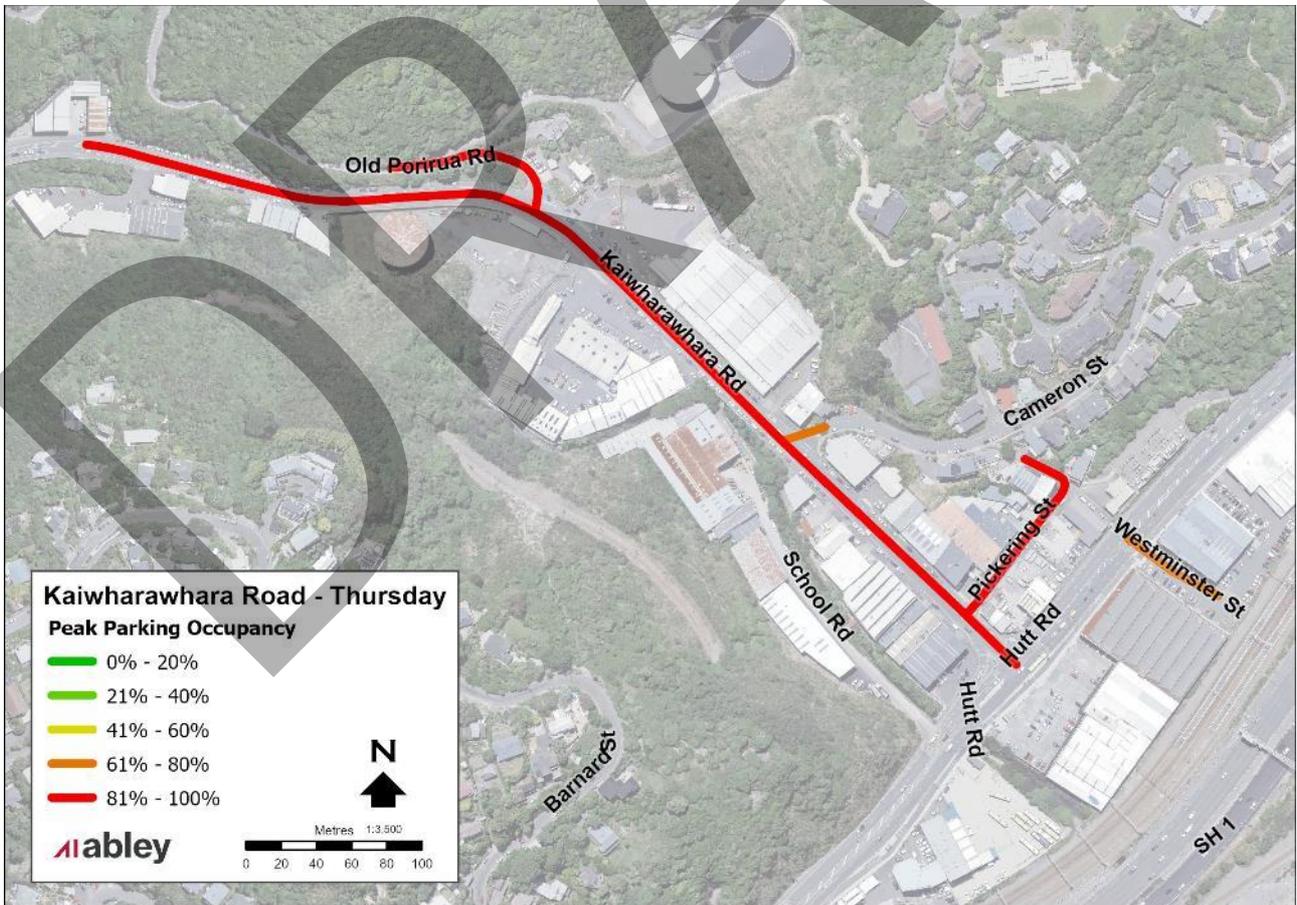


Figure 6 Kaiwharawhara Road section Thursday peak parking occupancy per street

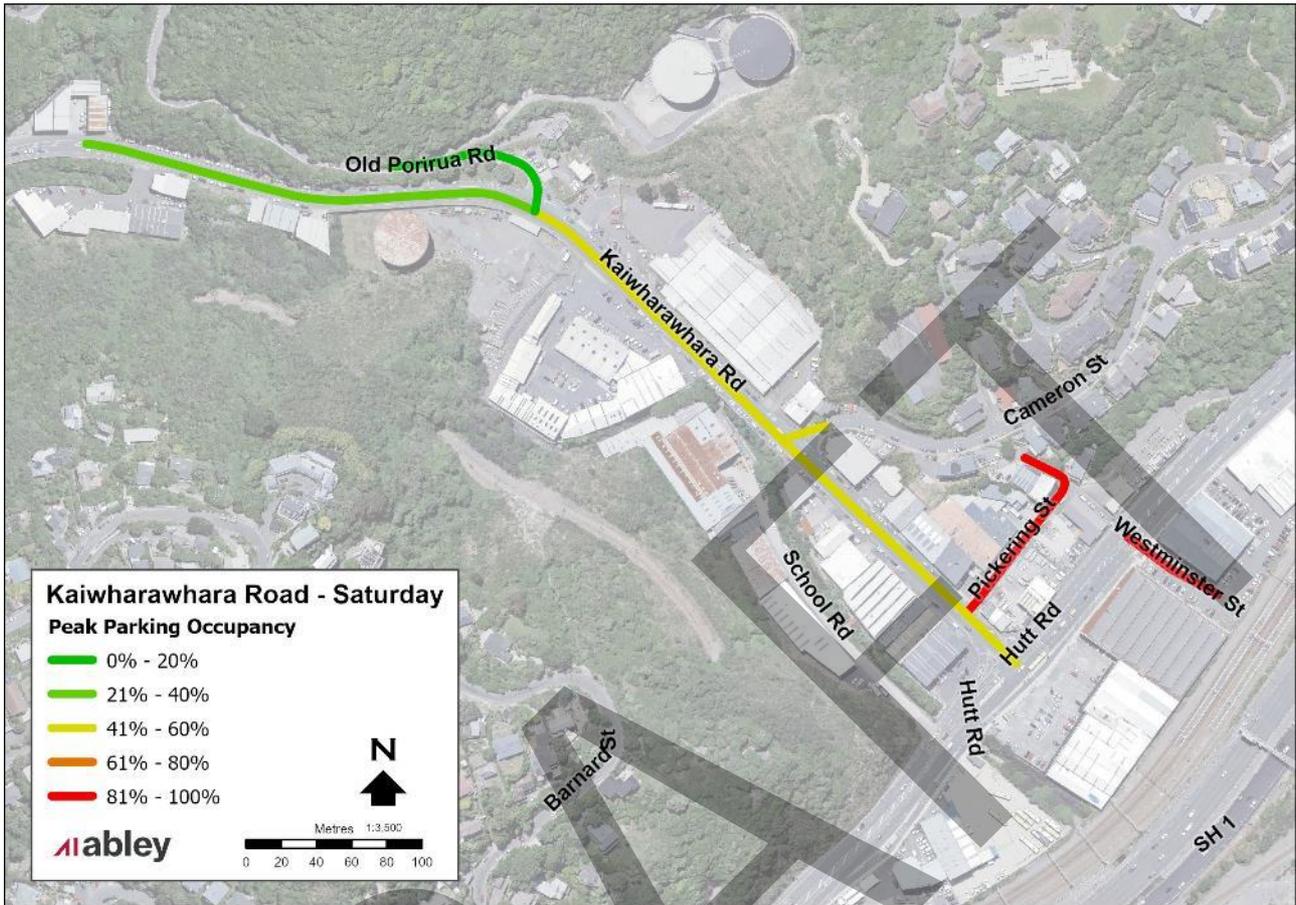


Figure 7 Kaiwharawhara Road section Saturday peak parking occupancy per street

2.2.1. Duration of stay

The duration of stay data can be assessed in this section to get a picture of the different user types of the parking. Figure 8 shows the duration of stay data on both the Thursday and the Saturday. The parking behaviour changes significantly on the two days. On the Saturday survey, the majority of users of the unrestricted parking were short stay users (45% <1 hour compared to 11% 4+hour). On the Thursday, 18% of users were short stay compared to 27% long stay. This shows there is a significant portion of users parking in this area for the entire day as part of their commute. It is not clear whether these users are working in the Kaiwharawhara businesses or commuting further into Wellington CBD (this parking is free compared to parking on Hutt Road which is paid).

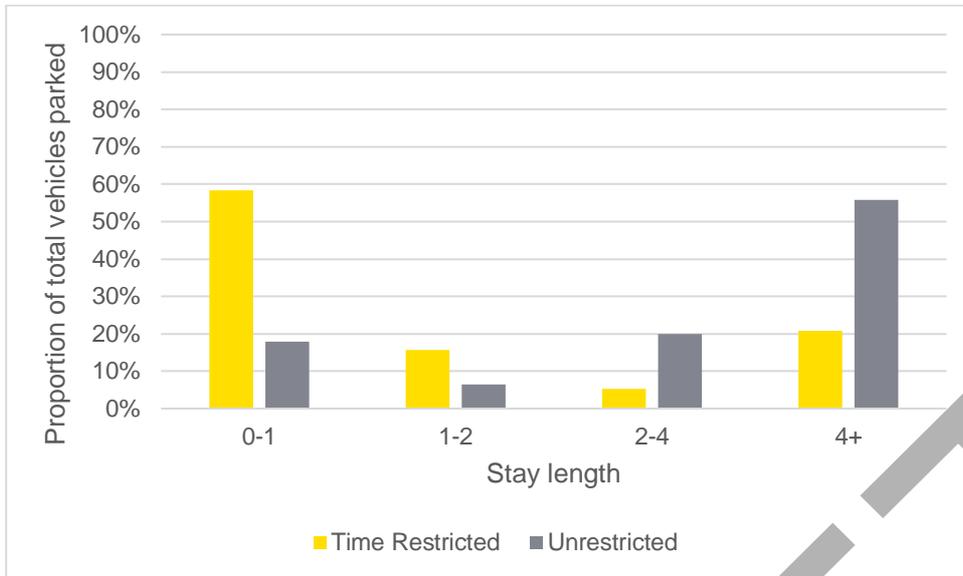


Figure 8 Duration of stay comparison for time restricted and unrestricted parking spaces

This data can be used to assess the extent to which drivers are non-compliant with parking restrictions in this area. Given the duration of stay data is taken every hour, the exact extent of the restriction compliance cannot be assessed (as the restrictions in this section are P10 and P30). It has been assumed that everyone who stays for longer than one hour is non-compliant.

Table 4 shows the level of non-compliance for the different parking restrictions. This data shows a relatively high amount of non-compliance with the parking restrictions.

Table 4 Non-compliance rates for the time-restricted parks on both survey days

Restriction	Thursday	Saturday
P60	41%	20%
P30	42%	32%

2.2.2. Overnight parking

The overnight parking shows a similar behaviour to the weekend occupancy (see Figure 9). There is relatively low occupancy throughout the area, which is driven by the high proportion of commercial land use in this area, rather than residential. One thing which was noticed was there was a significant number of courier post vehicles parked on the south side of Kaiwharawhara Road (opposite courier post). These vehicles were not observed during the 9am survey, which implies they are purely overnight parkers, resulting from staff. There is a bus lane which operates from 7am-9am during the weekdays. During the overnight weekday survey, there were no vehicles present in this bus lane. However, by the 10am survey period, the lane was significantly filled. This implies that people are parking in this section to commute, either to Kaiwharawhara Road or Wellington CBD. This is evidenced by most parkers in this section being long-stay users (46% stay for 4+ hours).

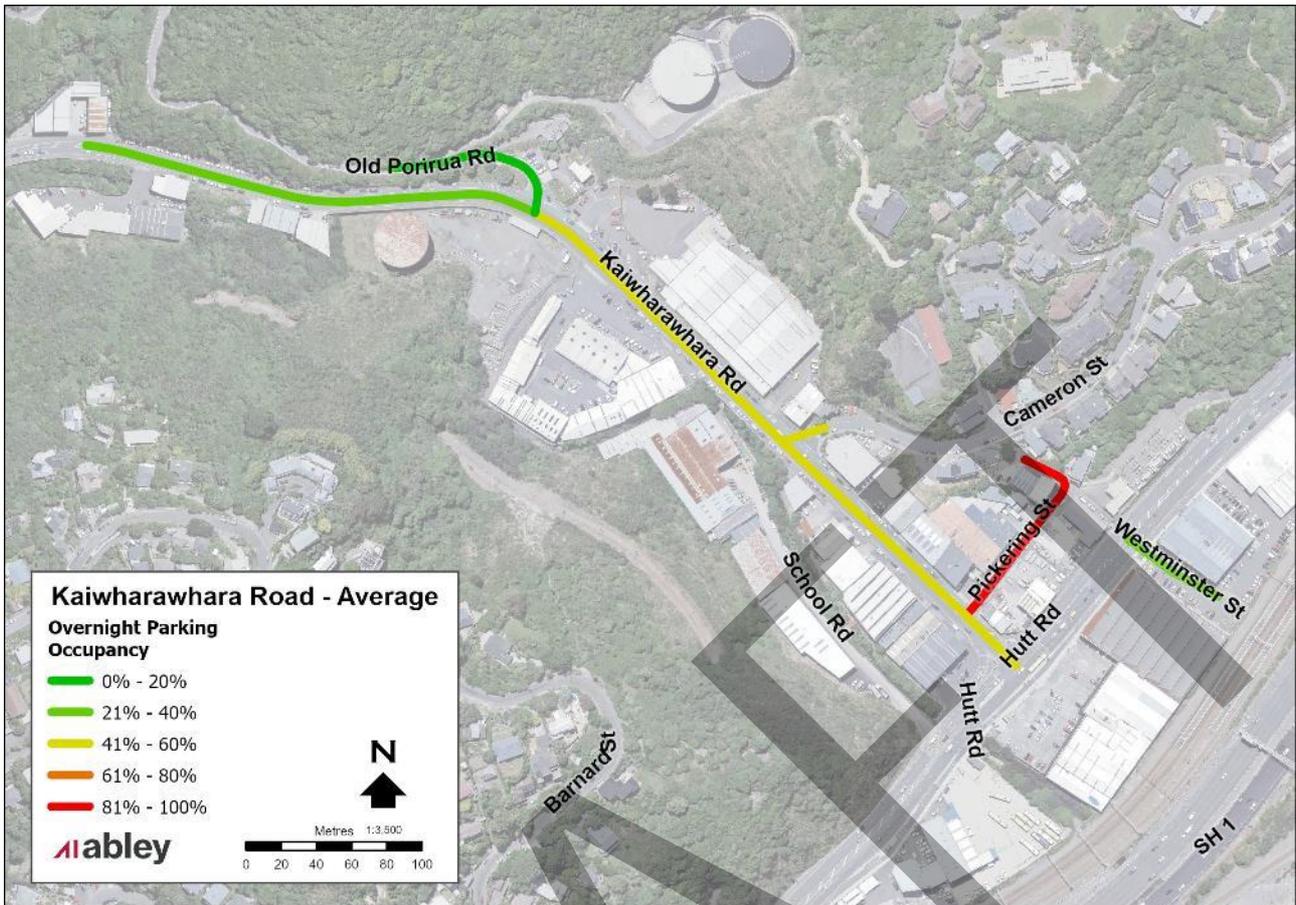


Figure 9 Kaiwharawhara Road overnight average occupancy from the two survey days

2.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed cycle facility in this section is to be implemented in phases. The impacts on parking are outlined below.

Stage 1 – completion in 2024

The transitional cycleway project will maintain the existing peak hour bus lane with off-street parking on the east side of Kaiwharawhara Road. On the west side, the transitional project will initially include a combination of peak hour clearway with parking permitted off-peak (from #1 to #25 and #53 to #57 Kaiwharawhara Road) and a separated cycleway along the remainder of this section. Overall, there are 46 existing parking spaces on the west side of Kaiwharawhara Road. The following details Stage 1 for the west side:

- From #1 to #25 Kaiwharawhara Road, there will be a clearway from 4-7pm. This means during this three-hour period, parking will not be permitted in this location. This equates to a removal of nine unrestricted spaces and three P30 spaces between 4-7pm. When the clearway is not active (i.e. 7pm to 4pm) the remaining spaces will be P30 at all times.
- From #27 to #53 Kaiwharawhara Road, the facility will be a separated/buffered cycleway. This results in the removal of all the parking on the west side of Kaiwharawhara Road in this section (at all times). This amounts to a removal of 23 parking spaces.
- From #53 to #57 Kaiwharawhara Road, the cycle facility will be a clearway from 4pm-7pm. During this three-hour period, parking or loading will not be permitted in this location. Three of the spaces in this section will be removed. Four of the spaces will be converted into a Loading Zone P10 from 7pm to 4pm and clearway from 4-7pm. Four of the spaces will be converted to P30 from 7-4pm and clearway from 4-7pm.

The P60 angle parking at the bottom of Cameron Street, which has been assessed as part of the Kaiwharawhara Road section (see Section 2.2), will be changed from four angle spaces to two parallel spaces. This results in a loss of two spaces at all times.

One unrestricted parking space will be removed due to the installation of no stopping lines at the bottom of Old Porirua Road.

Stage 2 – completion in 2024

Stage 2 involves extending the clearway times on the west side of Kaiwharawhara Road from 4-7pm in Stage 1 to 2-9pm in 2024. The quantity of parking available outside of this period will remain the same as Stage 1.

Stage 3 – completion in 2025

The second stage of this cycleway is to construct a separated cycleway in the uphill direction along the entire length of Kaiwharawhara Road. This results in the removal of all parking spaces on the west side (48 spaces on Kaiwharawhara Road). When the removals on Cameron Street are also considered, this is a removal of 53 spaces.

Parking inventory through the stages

The parking inventory in this section throughout the changes is shown in Table 5. This shows the existing inventory, Stage 1&2, and Stage 3. There is a total removal of 33 spaces in Stages 1 and 2. There is a total removal of 51 spaces in Stage 3. There is one existing loading zone, and one loading zone being added. These are not included in Table 5.

Table 5 Parking inventory in the Kaiwharawhara Road section before and after the cycleway project

Restriction	Existing inventory	Stage 1&2 inventory	Stage 3 inventory
Unrestricted	125	79	79
P60	31	31*	31*
P30	19	32 (16 are subject to the clearway)	14
Mobility	1	1	1
Total	176	143	125

*Two P60 spaces removed and two added

Anticipated parking occupancy after Stage 3

At the end of Stage 3, there will be 51 parking spaces permanently removed. This is approximately 29% of the entire section inventory and 37% of the inventory on Kaiwharawhara Road. When only the unrestricted spaces are considered, this is a removal of 37% of the unrestricted spaces in the entire section and 37% of the unrestricted spaces on Kaiwharawhara Road. For the time restricted spaces, this is a 10% decrease in the entire section and 26% on Kaiwharawhara Road.

Table 6 shows the peak occupancy of the unrestricted and P30 parking on Kaiwharawhara Road from the surveys as well as the expected peak occupancy after the parking is removed.

Table 6 Occupancy on Kaiwharawhara Road from survey and the expected occupancy after parking removal

Day	Restriction	Peak from survey	Expected peak after parking removals
Thursday	P30	84%	100%**
	Unrestricted	80%	100%**
Saturday	P30	74%*	95%
	Unrestricted	40%*	63%

*It should be noted that on the Saturday the two peak values were the first recordings of the day

** This is the maximum capacity achievable; the actual demand is higher than this

During weekdays when the clearway is operating for cyclists, it is expected that there will be insufficient parking supply on Kaiwharawhara Road to achieve the desired occupancy threshold of 85%. On Saturdays, the P30 parking is likely to be over capacity and the unrestricted parking is likely to have sufficient capacity.

During the weekday, the unrestricted parking removed is typically used by commuters (See Section 2.2.1). This parking is classed as lower priority according to the Wellington Parking Policy 2020 (See Table 1). The occupancy of the Kaiwharawhara Road section is relatively high (approximately 81% throughout the day on the Thursday). After the removal of parking spaces, there are approximately 22 unrestricted parking users who will need to find an alternative car park to reduce the occupancy to approximately 85%. The other unrestricted parking in the Kaiwharawhara Road section is already over the 85% occupancy threshold. Therefore, it is expected that the vehicles who have been displaced will transfer to the Cameron Street section, as the occupancy on Cameron Street and the surrounding streets is lower (approximately 65% throughout the day on Cameron Street). These vehicles are expected to first park on Cameron Street, and then the surrounding streets as the occupancy on Cameron Street reaches the 85% occupancy threshold. The analysis for Cameron Street, considering the proposed changes as part of the cycleway project, is shown in Section 3.3. This details the expected change in parking occupancy due to parking removals in the Cameron Street section, and the excess vehicles from the Kaiwharawhara Road section. Table 7 shows the expected peak occupancy of the unrestricted spaces in both the Kaiwharawhara Road section and the Cameron Street section after these removals on the Thursday, Table 8 shows the same for Saturday. The occupancy in Cameron Street is not the overall peak, but the occupancy that corresponds to the Kaiwharawhara Road peak. The analysis for the Cameron Street sections is provided in Section 3.3.

In the weekend, the occupancy of the unrestricted spaces on Kaiwharawhara Road are not expected to exceed the 85% occupancy threshold, however, the time restricted parking is. There are adequate unrestricted spaces to accommodate the short-term users who have been displaced. Therefore, no users are expected to transfer into the Cameron Street section on weekends. This change is reflected in Table 8.

Table 7 Thursday peak occupancy and expected occupancy of the unrestricted spaces after removals in the Kaiwharawhara Road and Cameron Street sections due to the cycleway

Section	Current occupancy (Kaiwharawhara Road peak)	Expected peak occupancy (Kaiwharawhara Road peak)
Kaiwharawhara Road	81%	85%*
Cameron Street	39%	74%

*This is capped at the occupancy threshold. The demand exceeds 100%.

Table 8 Saturday peak occupancy and expected occupancy of the unrestricted spaces after removals in the Kaiwharawhara Road and Cameron Street sections due to the cycleway

Section	Current occupancy (Kaiwharawhara Road peak)	Expected peak occupancy (Kaiwharawhara Road peak)
Kaiwharawhara Road	38%	63%
Cameron Street	27%	27%

The parking changes through the stages are shown in Figure 10, Figure 11, and Figure 12. These figures show all spaces which are being removed/ modified as a result of the cycleway project. Figure 10 shows the existing restriction of these spaces, Figure 11 shows the restrictions in Stages 1&2, and Figure 12 shows the restrictions in Stage 3. Where parking has been removed all together for the improvements, it has been removed from the maps.

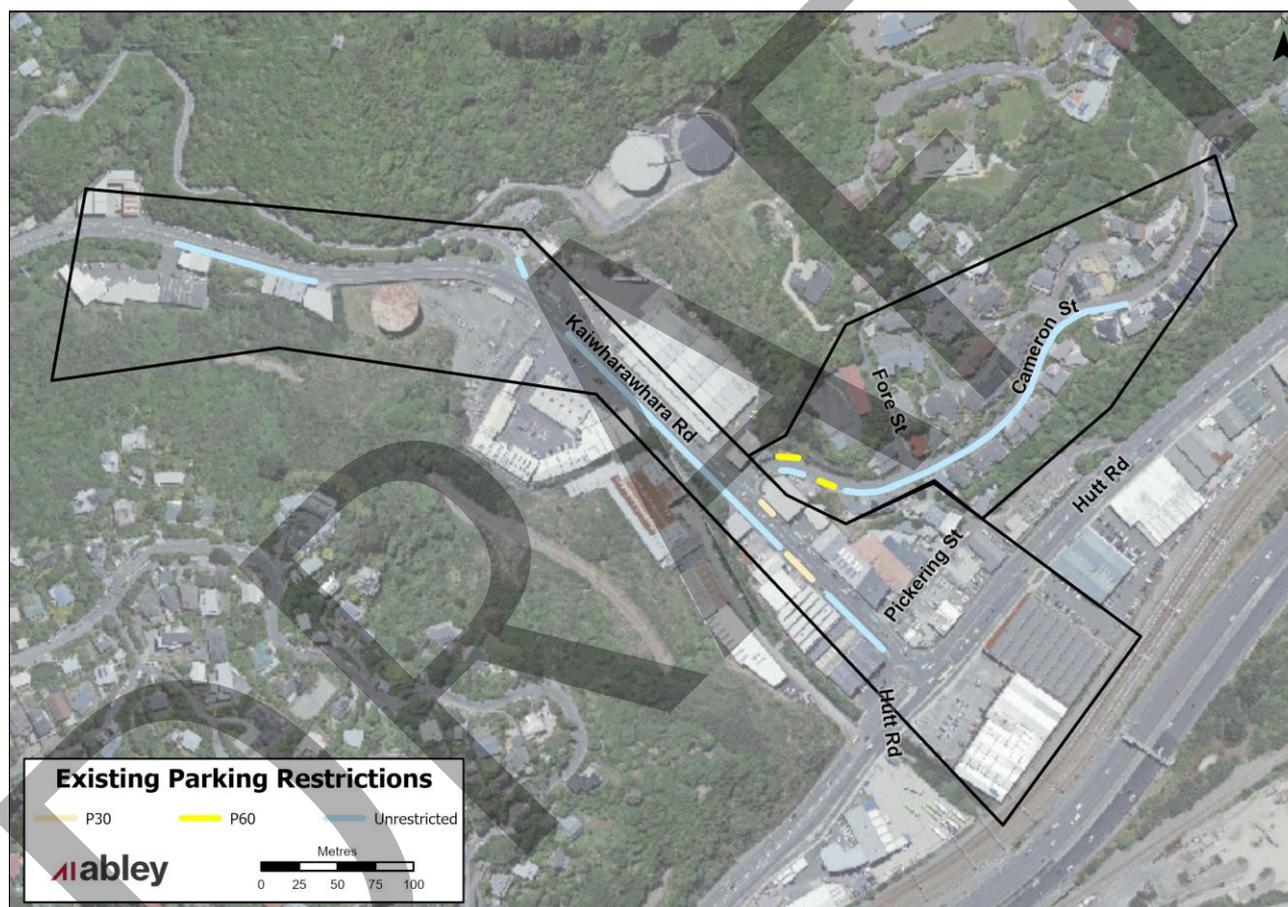


Figure 10 Existing parking restrictions of parking spaces affected by the cycleway project

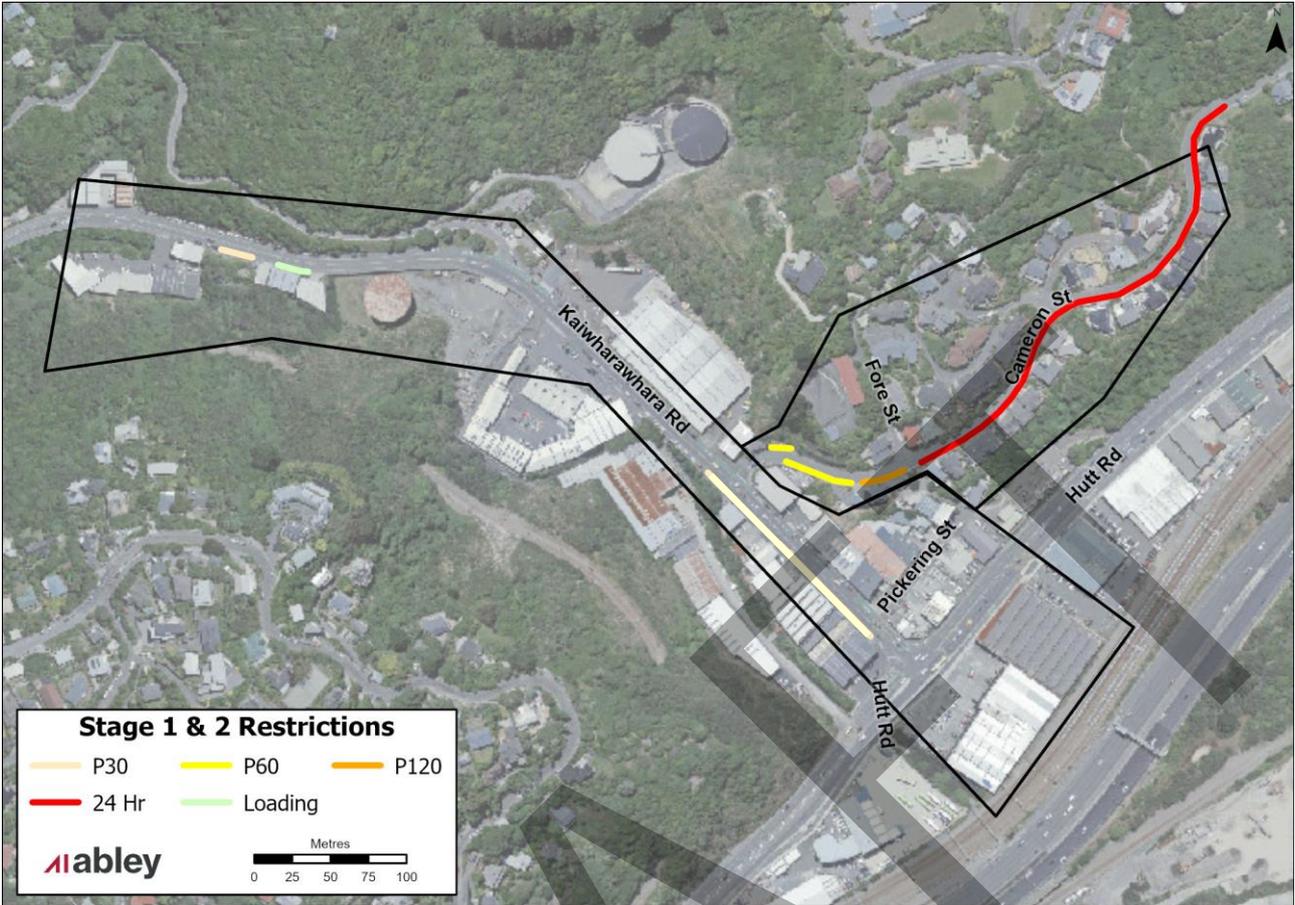


Figure 11 Stage 1 & 2 parking restrictions of parking spaces affected by the cycleway project (Note: Parking spaces and the loading zone on the west side of Kaiwharawhara Road will be subject to a clearway during the afternoon period)

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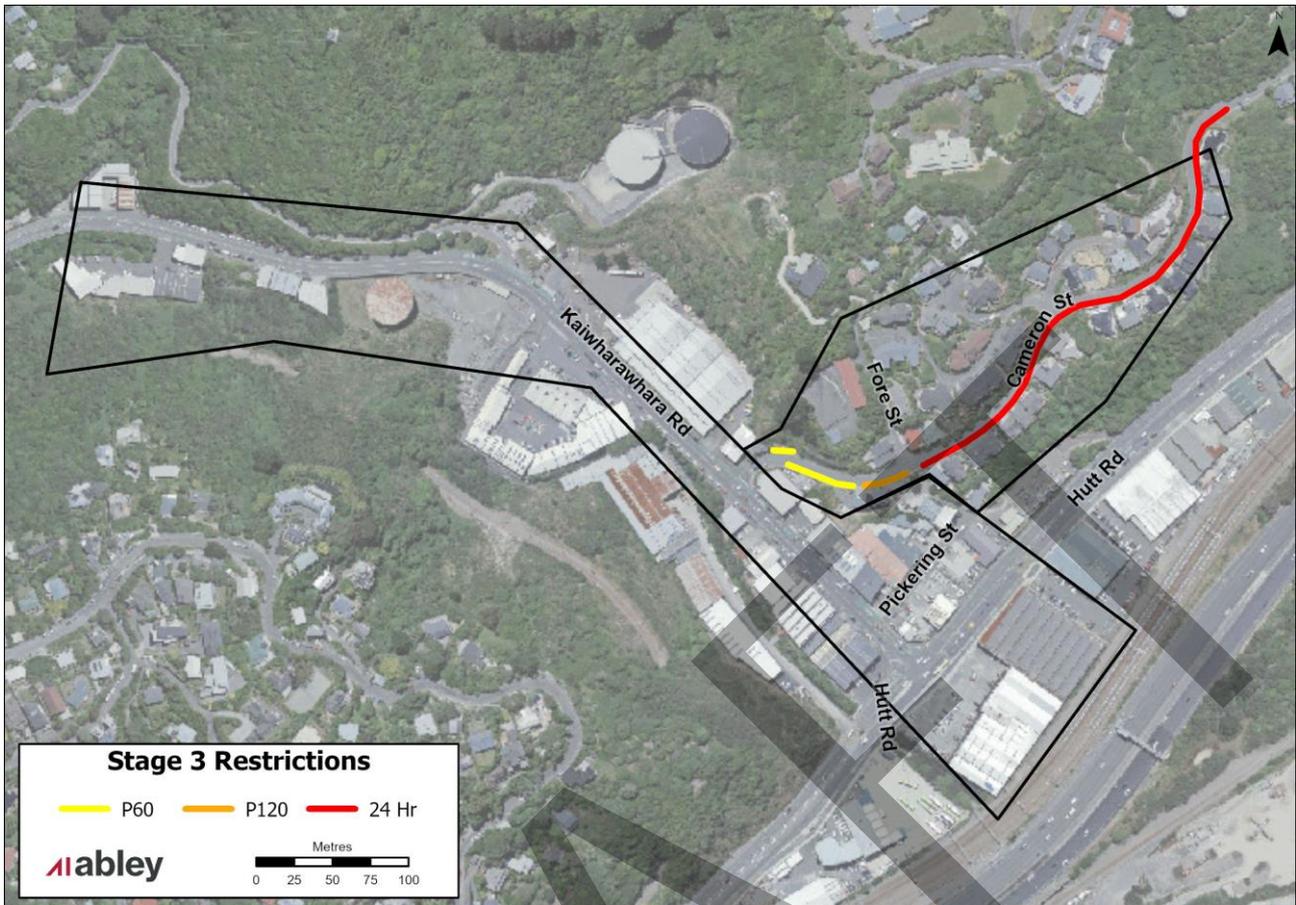


Figure 12 Stage 3 parking restrictions of parking spaces affected by the cycleway project

2.4. Mitigation of parking impacts

Unrestricted Parking

The most suitable mitigation is to encourage the users of the removed parking spaces to park in the Cameron Street section. This allows the excess capacity in the Cameron Street section to be used efficiently. The parking on the west side of Kaiwharawhara Road is split into two distinct sections. The southern section, which is primarily focused on the commercial centre (near the Courier Post hub and the retail shops) and the northern section, which is focused around the industrial areas. Figure 13 shows the centroid of the parking removed on Kaiwharawhara Road in proximity to the commercial centre, and the parking available within a 5-minute walking radius from this centroid. This is a Moderate impact from the parking removal (See Table 2). Figure 14 shows the centroid of the parking removed in the northern section of Kaiwharawhara Road (industrial centre) and the parking available within a 10-minute walking distance in the Cameron Street section. This parking removal has a high impact (See Table 2).

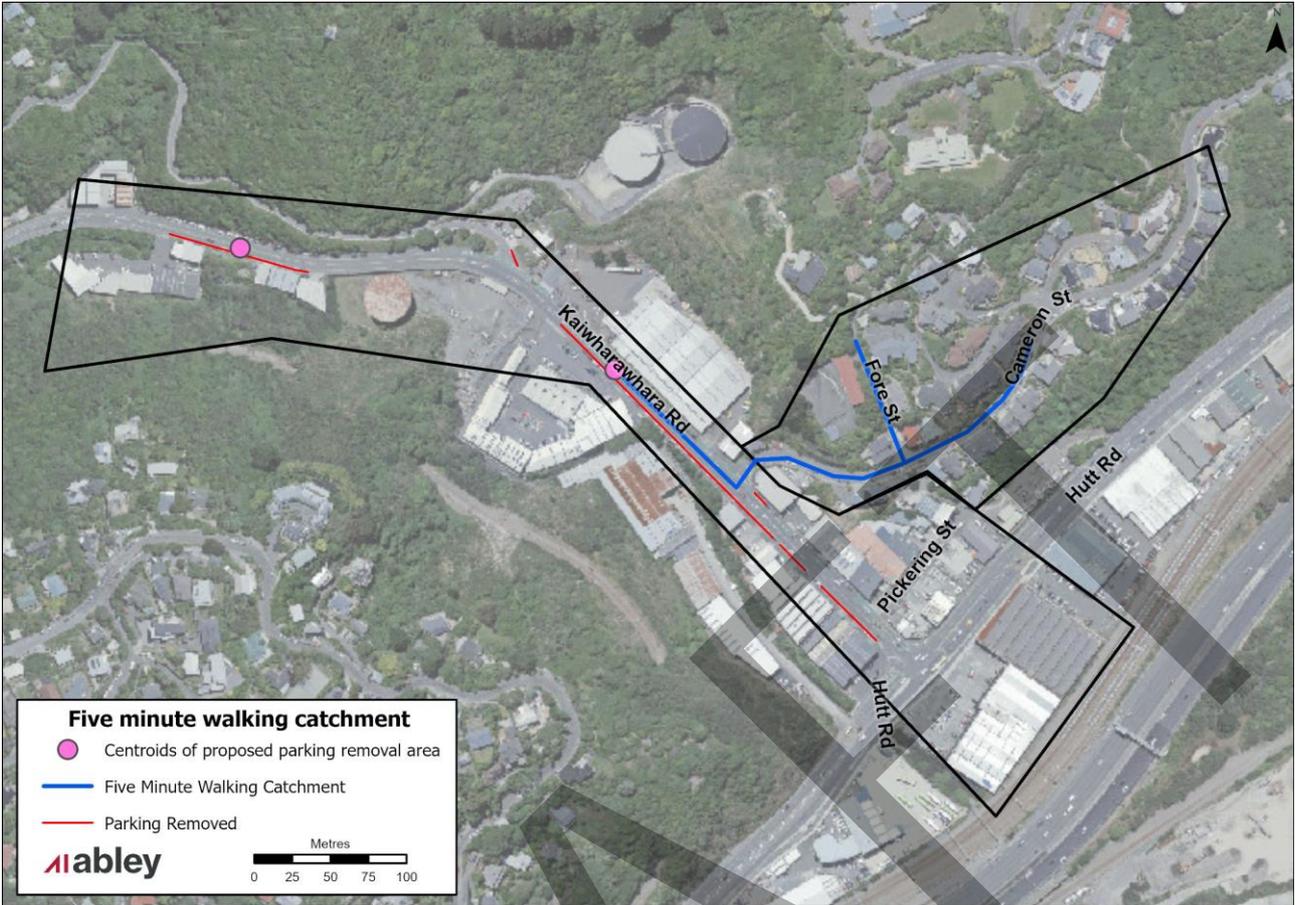


Figure 13 5-minute walking catchment from the commercial parking on Kaiwharawhara Road

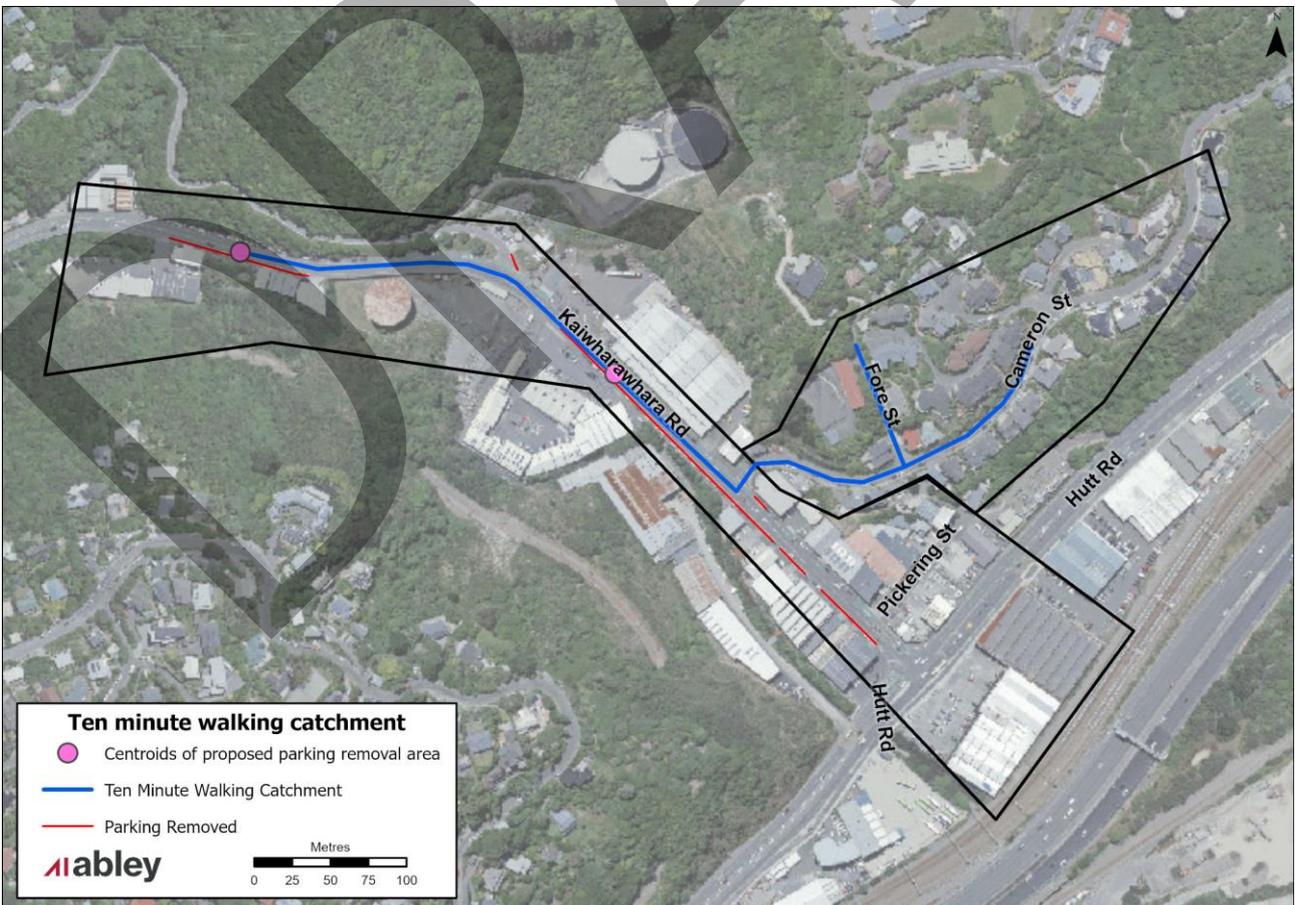


Figure 14 Available parking within a 10-minute walking catchment from the industrial parking removed on Kaiwharawhara Road

The typical user of the unrestricted spaces in the Kaiwharawhara Road section are commuters/ long-stay users who are not residents. This is evidenced by the approximately 50% overnight occupancy as well as the high proportion of 4+ hour stays (46% of users stay for 4+ hours). The parking on Cameron Street is typically expected to be used by residents' vehicles (See Section 3.2.1). Therefore, a high influx of commuter parking to Cameron Street has the potential to cause issues for the residents of this area. However as described in Section 3.3 the expected overall occupancy of the Cameron Street section is expected to be a maximum of 63% (although higher at the western end near Kaiwharawhara Road).

It is recommended that WCC engages with residents and businesses in the Kaiwharawhara Road area to detail the changes occurring and where there is excess parking available. On top of this, WCC could provide more cycle parking facilities/storage to encourage the commuters to the businesses in Kaiwharawhara Road to use more active transport modes. This will aid the encouragement already provided from the installation of the cycleway.

P30 Parking

There are three P30 spaces proposed to be removed on the west side of Kaiwharawhara Road and two spaces on the east side as shown in Figure 13. The occupancy of the P30 parking had a peak of 84% from the surveys undertaken. After the removal of parking for the cycleway this occupancy is expected to increase to over 100%. To achieve a parking occupancy at or below the 85% threshold again, extra time restricted spaces have been added at the lower end of Cameron Street. These carparks are a mix of P60 and P120 spaces. The users of the removed P30 spaces are expected to park in these time restricted spaces, mitigating the potential issues and helping the occupancy stay below the 85% threshold.

3. Cameron Street

3.1. About the area

The Cameron Street section is a primarily residential section which connects the main cycleway route on Kaiwharawhara Road to the Bridle Track (which connects to Khandallah). There were no roadworks occurring in this section during the survey days which would affect the parking survey. The section is shown as Figure 15.



Figure 15 Cameron Street parking study area

3.2. Current parking and usage

All of the parking in this section is unrestricted on-street parking. There is a small section of P60 parking at the southern end of Cameron Street (See Appendix A), however, this is considered to be part of the Kaiwharawhara Road section as part of this analysis. There are 84 car parks in this area, of which, 43 of these are on Cameron Street (see Table 9).

Table 9 Parking inventory for the Cameron Street section

Restriction	Overall section inventory	Cameron Street uphill inventory	Cameron Street downhill inventory
Unrestricted	84	0	43

Figure 16 shows the parking occupancy throughout the day on Cameron Street, and in the overall section. This shows the occupancy on both days. In this section, throughout the day, the

occupancy decreases relatively consistently. On the Thursday survey, particularly on Cameron Street, the occupancy starts high (75%) and slowly decreases throughout the day. The occupancy in the entire section is approximately 20% higher from the Thursday survey compared to the Saturday survey. Most of the parking that occurs in this section is on Cameron Street. The surrounding streets (Fore Street, Sargeson Way, etc.) have very little parking demand. Most of the houses in this area have off-street parking facilities.

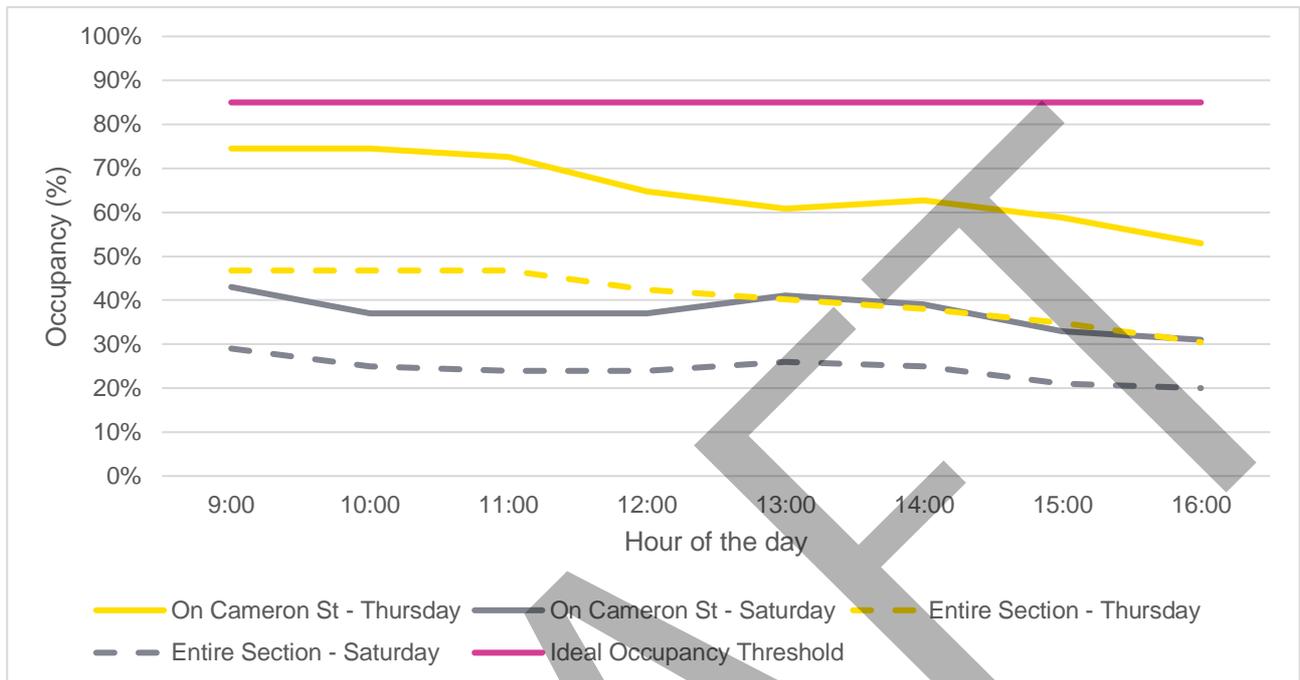


Figure 16 Parking occupancy from both survey days compared to the ideal parking occupancy of 85%

The duration of stay data for this area shows that most users are staying for 4+ hours (55%). This indicates there is a high proportion of residents parking occurring in this area. On top of this, it is expected that some users will be parking and commuting to work (either on Kaiwharawhara Road or in the CBD).

Figure 17 and Figure 18 show the peak parking occupancy per street in this section from both the Thursday and the Saturday survey. This shows the difference in occupancy between the surrounding streets and Cameron Street. On the Thursday survey (Figure 17), Cameron Street is over 60% occupied, whilst the surrounding streets never exceed 20% peak occupancy.

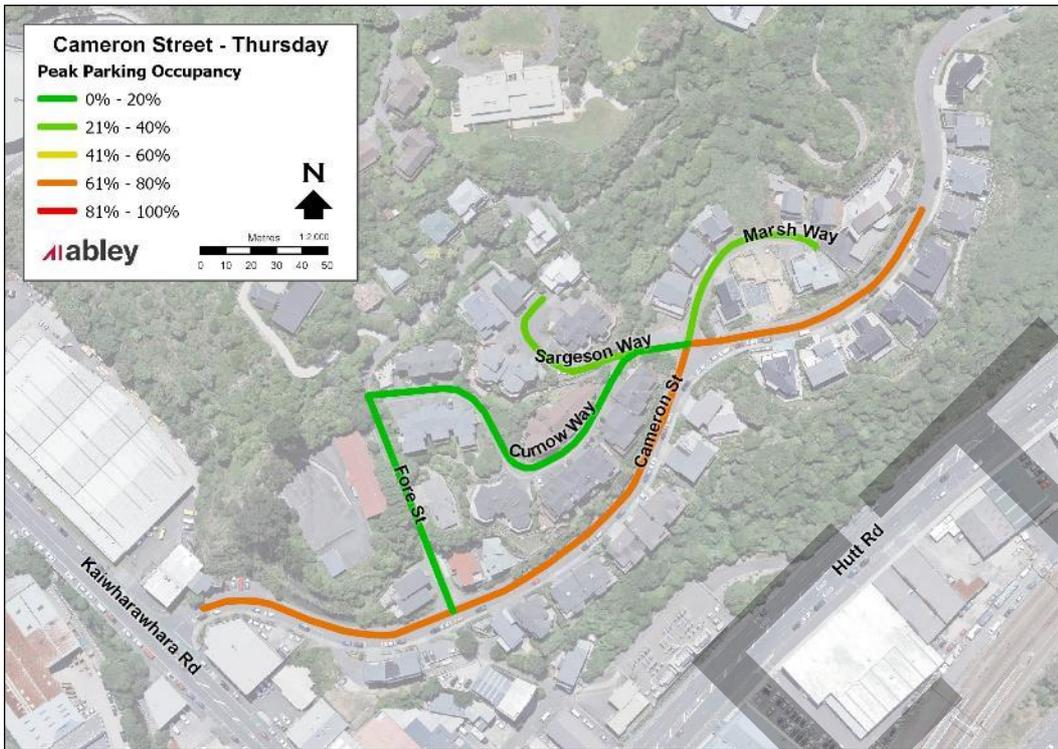


Figure 17 Cameron Street section Thursday peak parking occupancy per street

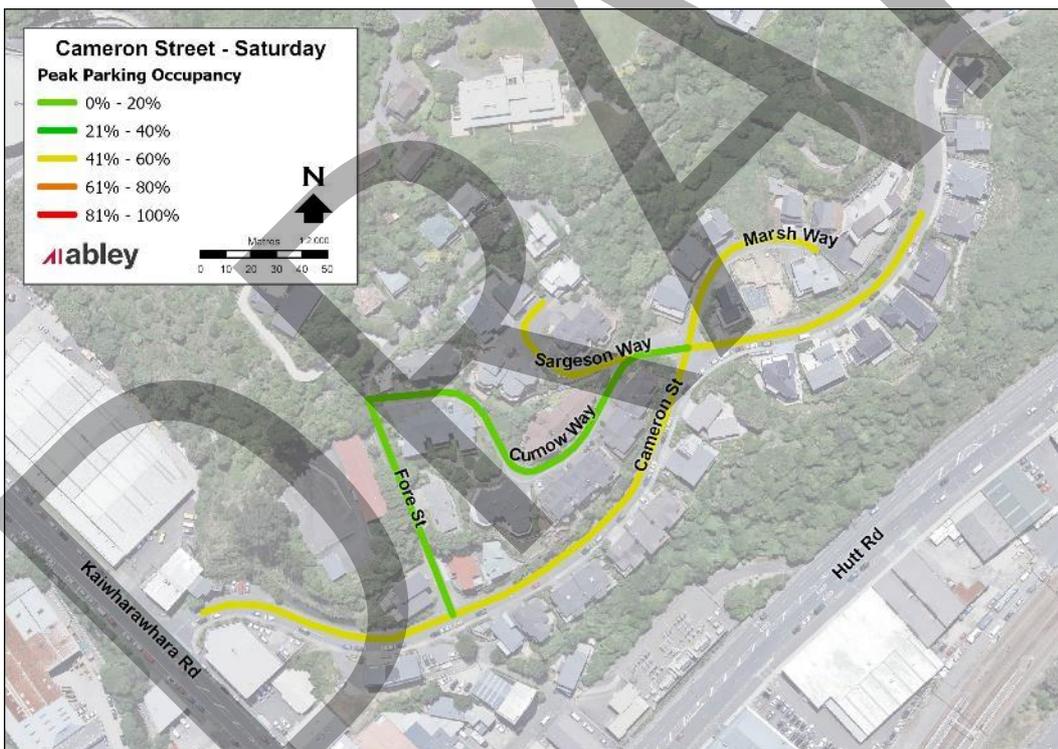


Figure 18 Cameron Street section Saturday peak parking occupancy per street

3.2.1. Overnight parking

When the overnight parking is assessed, it shows there is relatively low occupancy throughout the entire area (see Figure 19). This implies that the parking on Cameron Street is likely being driven by commuters making the most of the unrestricted parking. It was observed on-site that there was a high incidence of tradesperson’s vehicles in the area. The large variation in occupancy between the average overnight occupancy (Figure 19) and the Thursday peak occupancy (Figure 17)

reinforces the notion that people are parking in Cameron Street to commute (as implied from the duration of stay data). There is a high level of residents parking occurring, and it is expected that the spare capacity in this section is taken up by people parking and commuting. Then, throughout the day, more of the residents in this section will leave the area. This is evidenced by the occupancy trending down throughout both survey days as more residents leave and use their vehicles.

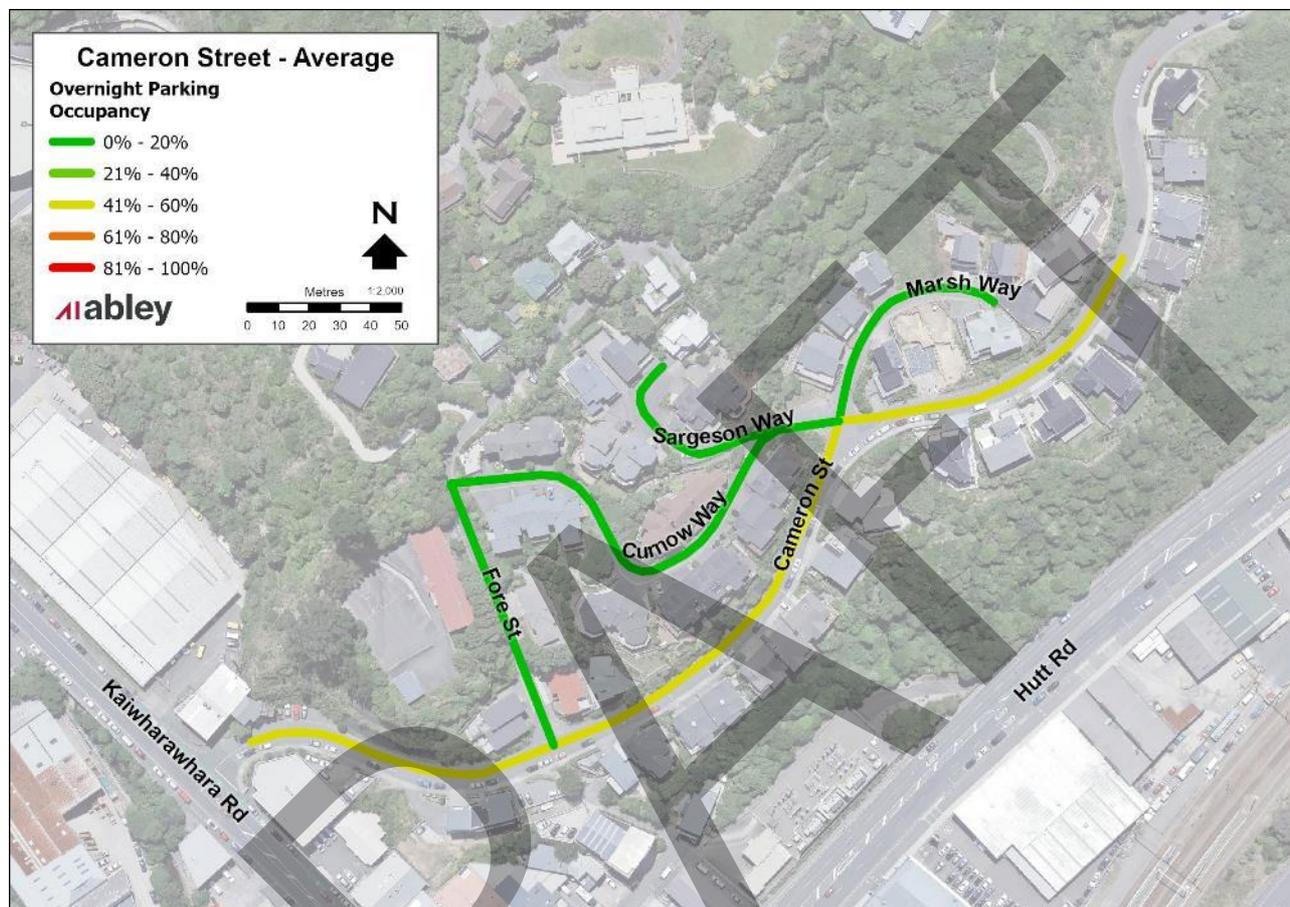


Figure 19 Cameron Street section overnight average parking occupancy from the two survey days

3.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed cycle facility on this section includes an uphill cycle lane and a downhill shared lane. This results in the following changes to the parking on Cameron Street. These are all occurring during the first stage of the Kaiwharawhara Road improvements (see Section 2.3):

- No stopping lines added on Cameron Street opposite the entrance to Fore Street – this results in the removal of one unrestricted parking space.
- Four unrestricted spaces at the Kaiwharawhara Road end of Cameron Street (in the downhill direction) will be changed to P60 spaces. This expands on two P60 spaces in the downhill direction that are to be retained.
- Five unrestricted spaces in the downhill direction on Cameron Street are to be changed to P120 spaces.
- The remaining unrestricted spaces on Cameron Street up to number 60 (39 spaces) are to have a P24 Hr restriction. Our survey area only included up to #52 on Cameron Street. 34 of the 39 unrestricted spaces being turned into P24 Hr have been surveyed. The spaces being converted extend beyond the study area after consultation with local residents.

Table 10 shows the parking inventory existing and the parking inventory after these changes in the entire Cameron Street section.

Table 10 Cameron Street section inventory before and after upgrades

Restriction	Existing Inventory	Inventory after changes
Unrestricted	84	41
P60	0	4
P120	0	5
P24 Hr	0	33
Total	84	83

These changes, as well as the overflow parking from Kaiwharawhara Road (approx. 22 users), result in an increase in demand for the remaining unrestricted spaces in this area. The unrestricted parking on Cameron Street was found to have a peak occupancy of 77% from the Thursday survey and 42% on the weekend survey. The 24Hr parking being implemented on Cameron Street is expected to be used by the same users as the unrestricted parking at present. Given it is residents exempt, residents can park freely in the same way they do currently. The commuters that use Cameron Street currently are still able to park in these 24Hr parks, as it is expected they arrive and leave during the same day. It is expected there will be some change to the parking patterns on Cameron Street due to this restriction, however, it is not quantifiable.

There are nine spaces being changed from unrestricted spaces to time-restricted: four P60 and five P120. There is also one unrestricted space being removed. This decreases the number of unrestricted spaces on Cameron Street by 23%. This means that the peak occupancy on Cameron Street (before the changes on Kaiwharawhara Road are considered) is expected to be greater than the ideal 85% threshold on the weekday. The unrestricted spaces in the wider Cameron Street section were found to be only 45% occupied during the AM peak. Therefore, the unrestricted parking users will be able to find spaces in the streets surrounding Cameron Street. After the removal of the 10 spaces, the occupancy of the entire section during the weekday is expected to increase from 45% to 51%. During the weekend there is expected to be sufficient capacity on Cameron Street to accommodate the removed spaces.

The removals of parking on Kaiwharawhara Road are expected to result in 22 users who would typically park on Kaiwharawhara Road searching for a parking space in the Cameron Street section during the Kaiwharawhara Road peak period. This increases the demand for unrestricted parking spaces in the section. This is likely to only occur on weekdays, not the weekend (see Table 6). When the parking on Kaiwharawhara Road is at its peak occupancy (1pm) the occupancy in the Cameron Street section was only found to be 39% from the Thursday survey. When the removal of the 10 unrestricted spaces on Cameron Street is factored in, this increases to 44%. The addition of 22 vehicles from Kaiwharawhara Road is expected to increase the occupancy of the overall Cameron St section from 44% to approximately 74%.

3.4. Mitigation of parking impacts

The parking in the entire Cameron Street section is not expected to be at or above the 85% occupancy threshold at its peak. It is expected most users who have been displaced from Kaiwharawhara Road to Cameron Street are commuters. These users are 'Lower Priority' (See Table 1). Therefore, no mitigation is proposed to accommodate these users. However, if the occupancy in the Cameron Street section is seen to be exceeding this 85% occupancy threshold

significantly, further mitigation may be required to ensure the residents of the section have parking available. A medium-term parking restriction (P120/P180 with the option of residents exempt) could be installed instead of the 24Hr spaces currently proposed. Given most dwellings in this area have off-street parking, it will encourage the residents to use that parking. On top of that, it will deter the commuter parkers, and provide available spaces for visitors to both residences in the area as well as the businesses on Kaiwharawhara Road. This should be considered if the occupancy of Cameron Street gets significantly higher than the desired occupancy threshold of 85%.

The time restricted parking is not expected to exceed the 85% occupancy threshold in this section. The removal of time restricted spaces on Kaiwharawhara Road is expected to cause short term users to transfer over into the new P60/P120 spaces at the bottom of Cameron Street. However, these are not expected to exceed the 85% occupancy threshold.

DRAFT

4. Ngaio Gorge Road

4.1. About the area

This section is a low activity residential section that connects the industrial area of Kaiwharawhara to the residential parts of Ngaio (See Figure 20). There are few areas on this section for stopping, and most of the parking in this section is associated directly with the houses (through off-street pull in areas). At the time of the survey, major construction works were happening in the lower section of Ngaio Gorge Road. These works have resulted in the removal of parking spaces, however, given this has been the case for the last approximately five years, it is expected that the users of parking in the Ngaio Gorge have adjusted. This construction is not expected to affect the parking behaviour on Ngaio Gorge Road. It has been indicated by WCC that there is a plan to reinstate 15 car parks in the lower end of the gorge.

Given most of the parking in this section is available in pull over areas, which almost act as private parking areas for the residences close to them, a full survey has not been performed. Instead, the occupancy in this section has been assessed every two hours.

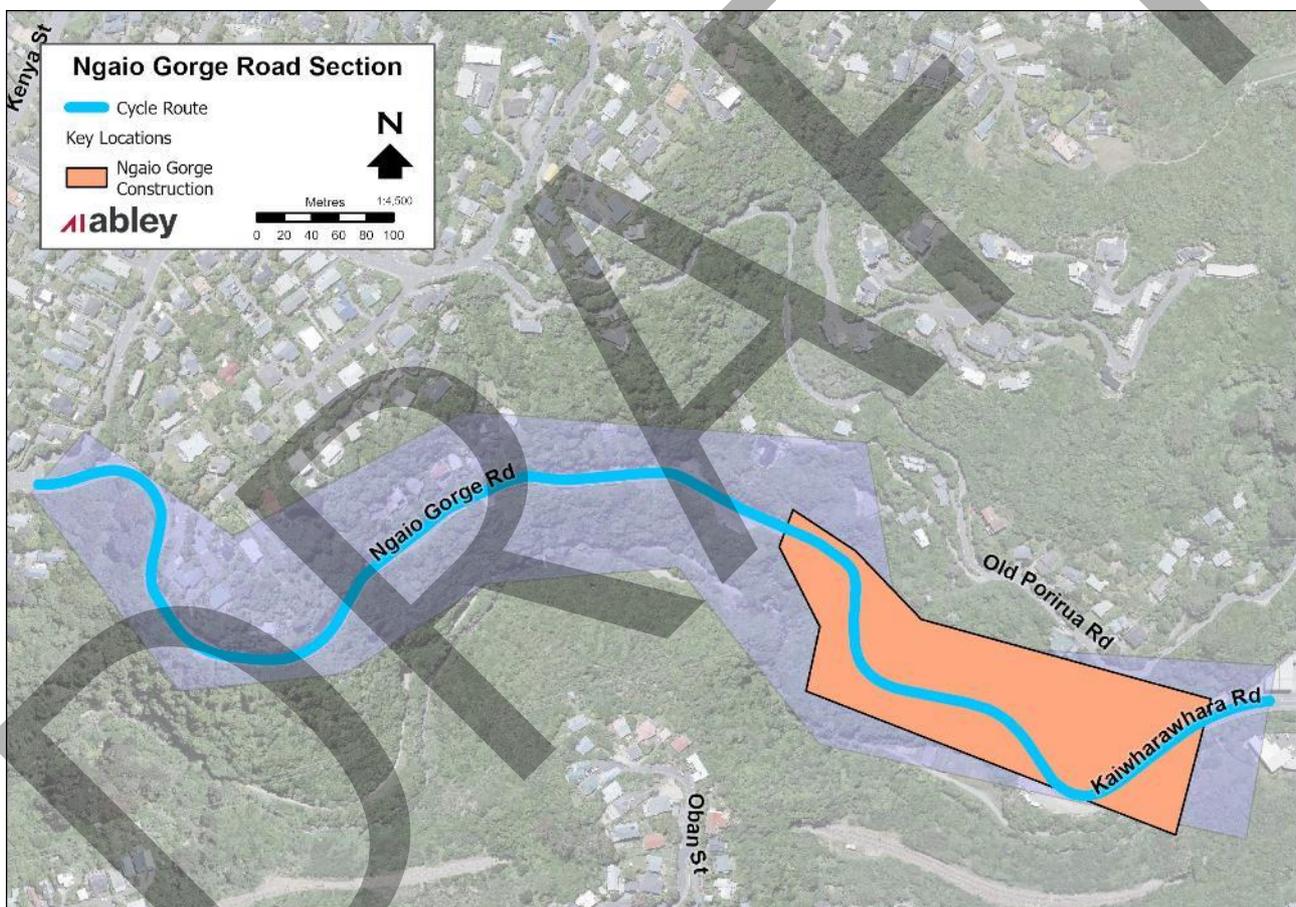


Figure 20 Ngaio Gorge Road study area

4.2. Current parking and usage

All the parking in this section is unrestricted parking and it is all in off-street pull in areas, which are only expected to be used by residents. There was an instance of vehicles who were observed to park illegally on the berm (see Figure 21) and an instance of vehicles who were observed to park illegally on the roadside; either with one wheel on the kerb or not (see Figure 22). Given this, it is hard to quantify exactly what the parking occupancy is in this section. However, given there are no spaces being proposed to be removed as part of this cycleway project in this section (as there are none on-street) this is not an issue.



Figure 21 Cars parked on the berm on Ngaio Gorge Road



Figure 22 Cars parked on the kerb in the Ngaio Gorge

Table 11 shows the number of vehicles parked in this section during the day on both survey days. This has not been assessed as an occupancy. This data shows the level of parking in this section

is relatively consistent throughout the day. There is more variation on the Saturday compared with the Thursday. This is expected, as the people who typically would have commuted to work (and left cars at home) are now not at work and likely using their cars (for errands, recreation, etc.).

Table 11 Parking counts on Ngaio Gorge Road on both survey days

Time	Thursday Count	Saturday Count
9am	18	19
11am	17	16
1pm	17	15
4pm	15	17

4.3. Impacts of Ngaio Transitional Cycleway on parking

The preferred option is a separated cycleway uphill and a shared lane towards the city. This results in no parking removal in this area. There is very limited parking in this section anyway, as most occurs in off-street pull in areas, typically associated with nearby residents.

When the Ngaio Gorge Slope Stabilisation project is complete, there will be 15 parking spaces provided in the former pull-in bays (which were used as parking for access to Trelissick Park prior to the slips occurring). They will be unrestricted publicly available spaces.

4.4. Mitigation of parking impacts

Given there is no parking removed in this section, little mitigation is required. As shown in Figure 22, vehicles park on the kerb on the uphill side of the road on Ngaio Gorge Road. This should be continually monitored after the cycleway is installed as vehicle parking in this section has the potential to block the cycleway, pushing cyclists into the live lane.

5. Kenya Street

5.1. About the area

The Kenya Street section is residential in nature and the entire section includes the top of Ngaio Gorge Road, and Trelissick Crescent. There are no shops or commercial activities in this area (aside from small businesses run out of residences). On both the survey days, roadworks were affecting the parking supply in this area (on both Kenya St and Trelissick Crescent). The parking occupancy data has been adjusted accordingly (see Section 5.2). The area assessed, with the cycleway location included, is shown in Figure 23.

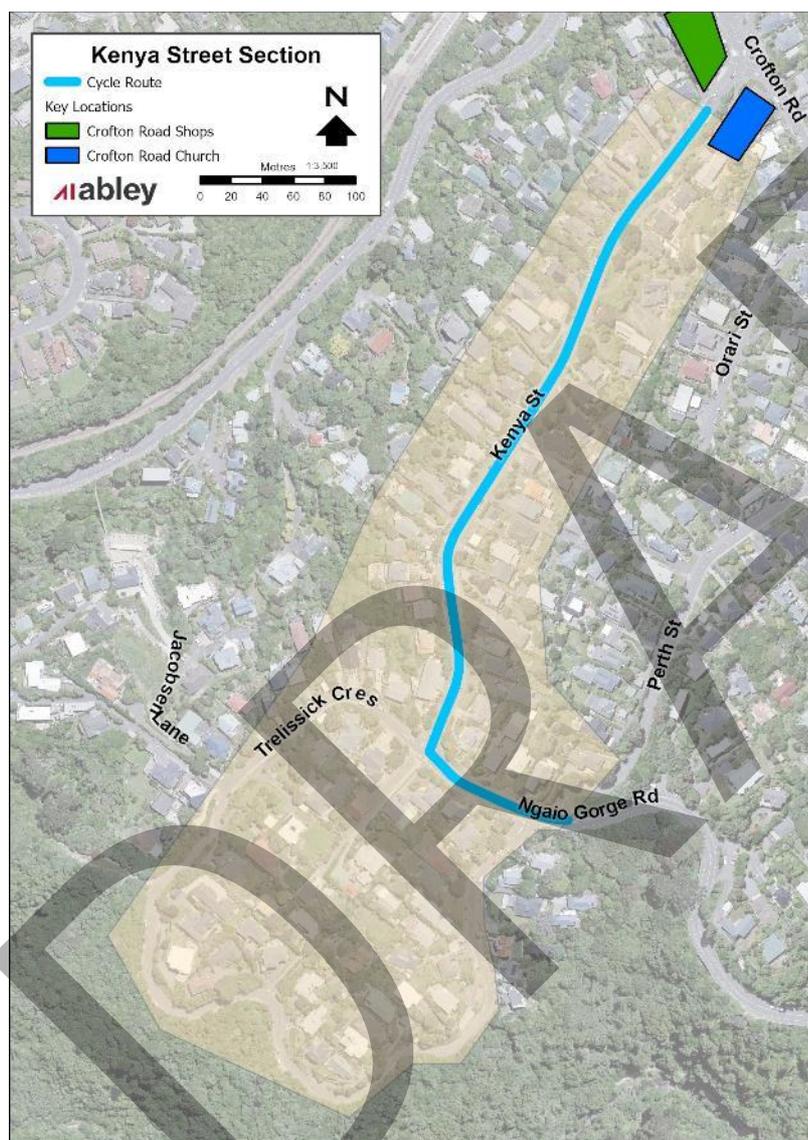


Figure 23 Kenya Street parking study area

5.2. Current parking and usage

All the parking (162 spaces) in this section are unrestricted on-street parks that are typically unmarked. Table 12 shows the parking inventory in this area. This includes the number of spaces on the roads in the parking study area. The construction in this section occurred at the southern end of Kenya Street. This was found to have closed a total of 25 spaces on both sides of Kenya Street. No spaces on Kenya Street were closed on the Saturday. The construction extended to Trelissick Crescent, this closed 20 spaces on the Thursday and 25 on the Saturday on Trelissick Crescent. Table 12 shows the number of spaces assuming no construction.

Table 12 Kenya Street section parking inventory

Restriction	Overall section inventory	Kenya Street inventory (towards Ngaio)	Kenya Street inventory (towards City)
Unrestricted	205	39	59

The occupancy throughout the day, on both days, is shown as Figure 24. This occupancy has also been compared to the occupancy threshold of 85% (See Section 1.3). Again, this data has been considered for the entire section and the parking spaces on Kenya St itself. The parking occupancy overall and on Kenya St are similar throughout the day. This is expected given approximately 50% of the spaces in this section are on Kenya St. The parking in this section is consistent throughout the day (variation of 5% on the Thursday and 6% on the Saturday). This low variation implies that a high proportion of the vehicles in this area are resident vehicles (long-stay parkers). This is expected, given there are no destinations (shops etc.) in this section or parking restrictions. Also, it is common for houses in this area to have off-street parking available. This is particularly the case on the south side of Kenya Street (approximately 90% of residences have off-street parking facilities). It was observed that some off-street parking was not being used, even during the overnight occupancy checks.

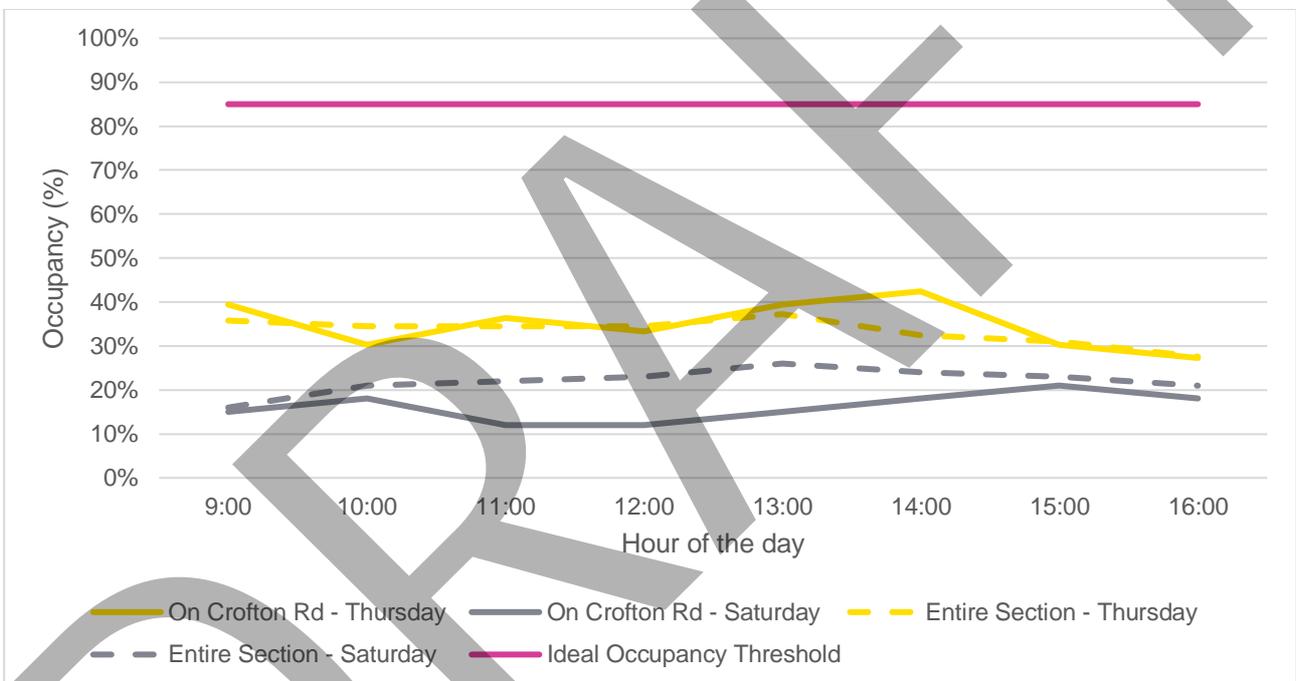


Figure 24 Kenya Street parking occupancy throughout both survey days compared to the occupancy threshold of 85%

The duration of stay data for this section shows that most users (38% on Thursday, 39% on Saturday) stay for 4+ hours. This further indicates the high proportion of residents parking occurring, as these users are not expected to move their cars during the day.

The peak occupancy on each street in this section is shown in Figure 25 and Figure 26. These figures show this entire section has an occupancy less than 40%.

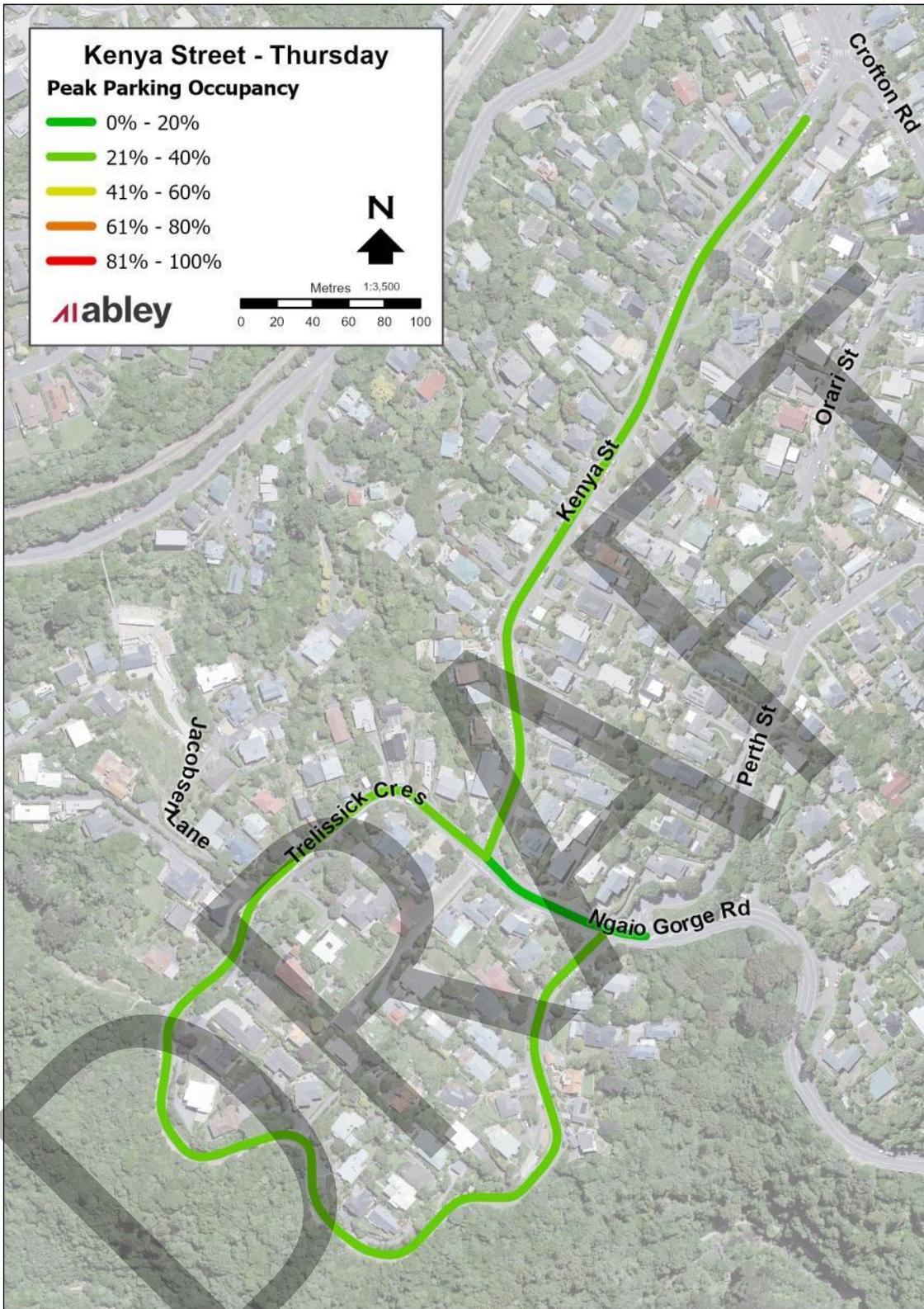


Figure 25 Kenya Street section Thursday peak parking occupancy per street

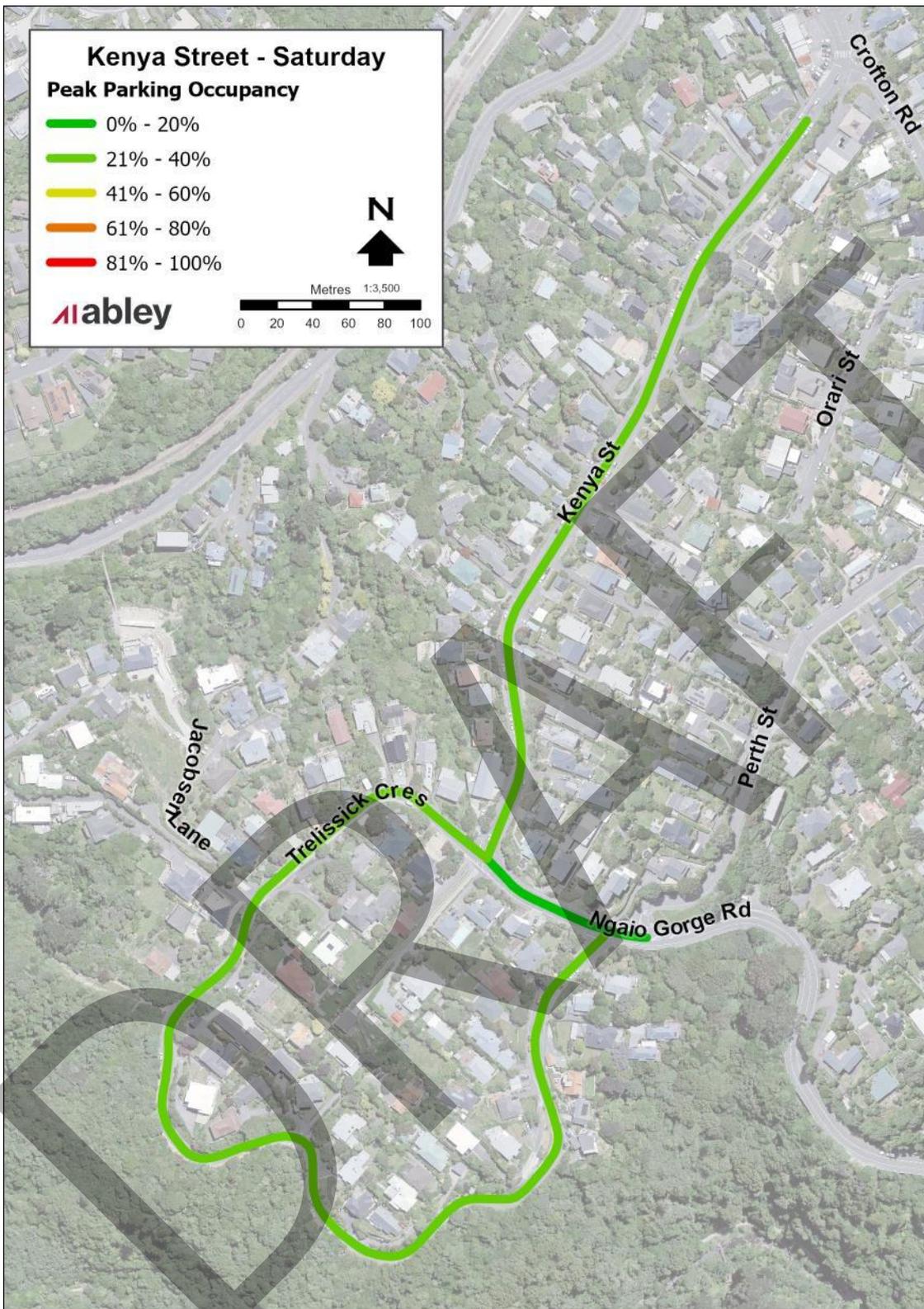


Figure 26 Kenya Street section Saturday peak parking occupancy per street

5.2.1. Overnight parking

Figure 27 shows the average parking occupancy from the two overnight occupancy checks taken. This occupancy shown is very similar to the peak occupancy data shown in Figure 25 and Figure 26. This is expected given the low levels of variation in parking demand throughout the day and is due to the limited destinations in this section. Most users of the parking are expected to be

residents/long-stay parkers. This is evidenced by the duration of stay data showing approximately 40% of users on both days stay for 4+ hours.

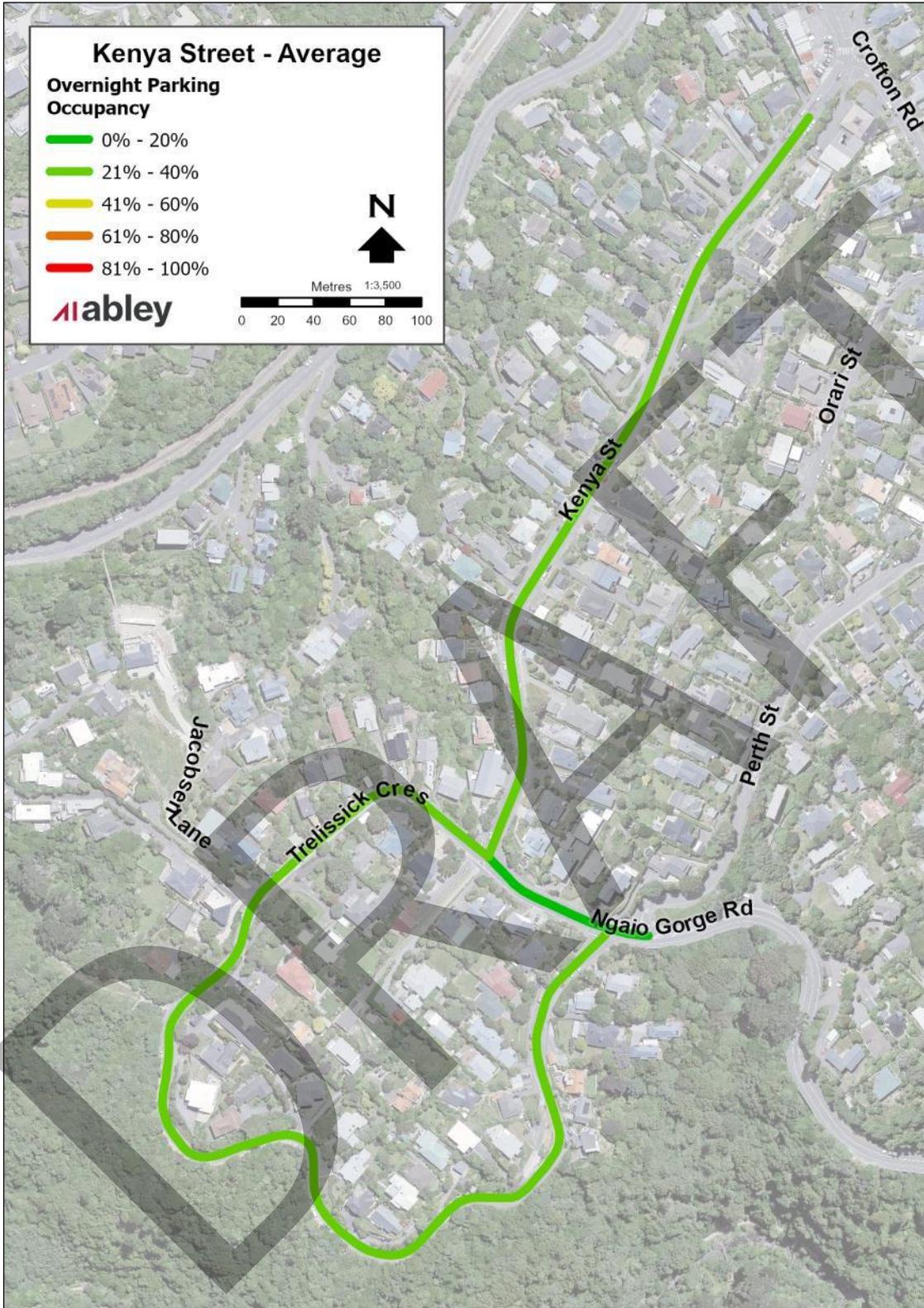


Figure 27 Kenya Street average overnight parking occupancy from the two study days

5.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed option in this section provides a shared traffic lane towards Ngaio and a separated cycleway in the uphill direction away from Ngaio. This will result in parking only on the side of the road towards Ngaio. This is a removal of 59 parking spaces in the towards City direction, and four

spaces in the towards Ngaio direction (for tracking around corners). This removal is approximately 31% of the entire parking in this section and 64% of the parking spaces on Kenya Street.

The peak occupancy on Kenya Street from both days, along with the expected peak occupancy after the installation of the cycleway is shown in Table 13. It is noted that the parking inventory on the Thursday survey was decreased due to roadworks on the southern section of Kenya Street. The existing occupancy appears higher, however, the actual number of vehicles parked is very similar to the Saturday.

Table 13 Kenya Street peak occupancy on both days and the expected occupancy after the parking removal

Day	Peak occupancy currently	Expected peak occupancy after installation of cycleway
Thursday	41%	87%
Saturday	31%	87%

After the parking is removed to accommodate the cycleway, it is expected that the parking occupancy will exceed the 85% occupancy threshold. The excess parking can easily be accommodated by Trelissick Crescent and Abbott Street/ Crofton Road (part of the adjacent section). The parking removed is all unrestricted on-street parking. This duration of stay data and overnight parking occupancy heavily implies a high number of residents parkers in this section. Therefore, it is expected that the majority of parking removed will be used by residents. Residents parking is high priority (See Table 1). The adjacent streets where parking is available are close, and it is likely that users will be able to find a space within a 5-minute walking distance at most. This is likely to be the most distance someone will need to walk to find a space, so is a conservative estimate. All the available spaces within a 5-minute walk from the centroid of the parking removal are shown as Figure 28.

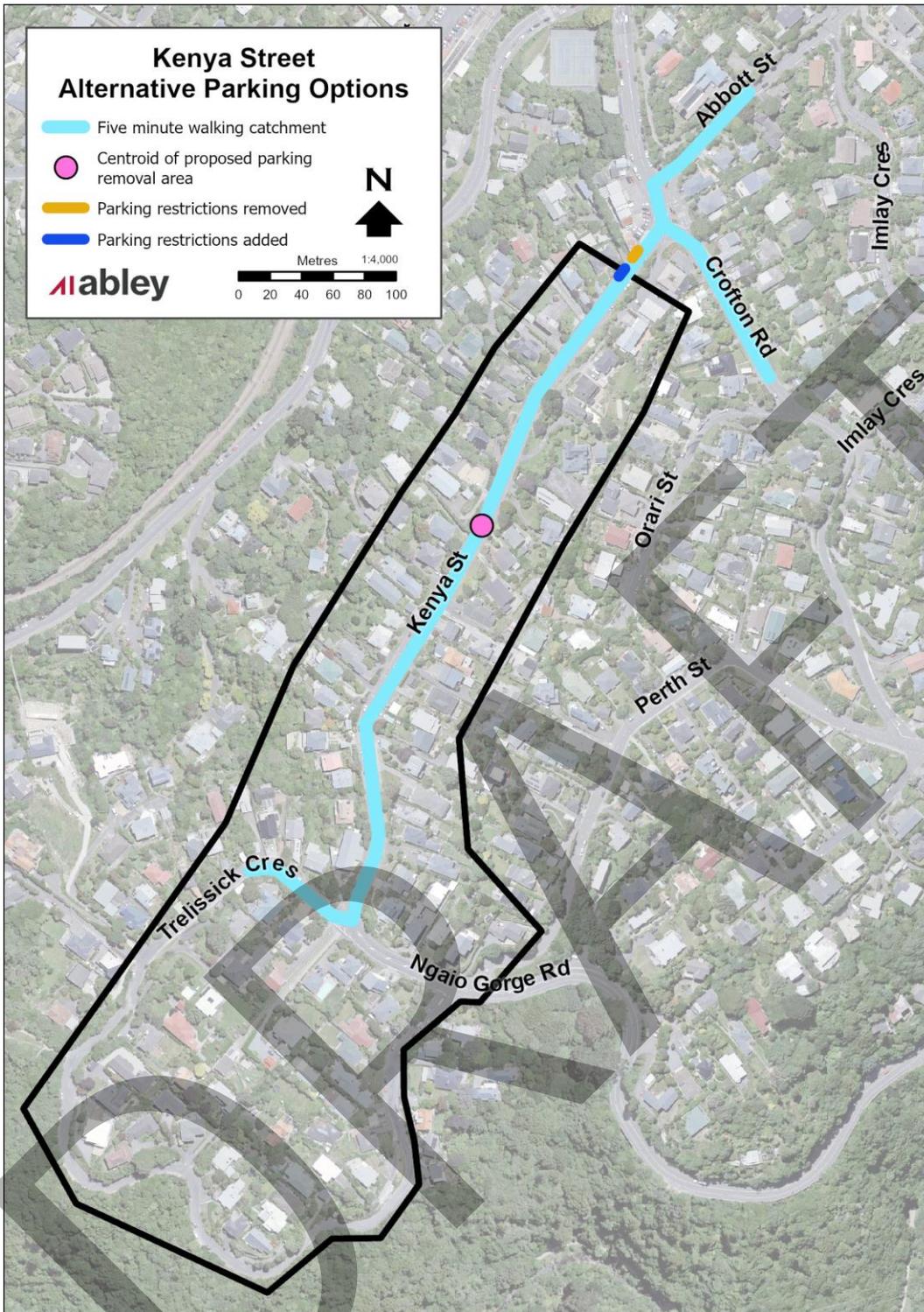


Figure 28 Parking within a five-minute walk from the centroid of the spaces removed on Kenya Street

5.4. Mitigation of parking impacts

The occupancy on Kenya Street with the removal of the 59 parking spaces is expected to exceed the 85% occupancy threshold (maximum 87% on Saturday). It is expected there is sufficient spare capacity in Trelissick Crescent, Abbott Street and Crofton Road to accommodate the excess vehicles. The occupancy of the entire area is expected to peak at 41% on the weekday after the removal of these spaces. This is well below the 85% occupancy threshold. No mitigation is proposed to account for the impacts of this parking removal. It is recommended that WCC engages with residents to understand any issues and concerns associated with the removal of the unrestricted parking along Kenya Street.

6. Crofton Road

6.1. About the area

The Crofton Road section is located to the north at the top of the Ngaio Gorge. This area includes Crofton Road and Abbott Street, as well as the off-street car parks at the Ottawa Road / Crofton Road intersection. This area is primarily residential but includes a small section of business activity on the western side of Crofton Road. There is a dairy, takeaway shop, butcher and auto repair shop. Ngaio School is at the top of Abbott Street, however, it is beyond the area surveyed. On the survey days, there were no roadworks or parking closures affecting the parking supply in this area. The section assessed, with the cycleway location, is shown in Figure 29.



Figure 29 Crofton Road study area (with cycleway route shown)

6.2. Current parking and usage

Most of the parking in this section is unrestricted on-street parking. There is a small section of P30 parking around the shops on Crofton Road (See Appendix A). There are two off-street council owned facilities at the northern end of Crofton Road. These facilities have both unrestricted and time restricted parking. Table 14 shows the inventory of the different parking restrictions on Crofton Road and in the entire section area.

Table 14 Crofton Road section parking inventory

Restriction	Overall section inventory	Crofton Road inventory (towards Ngaio)	Crofton Road inventory (towards City)
Unrestricted	132	9	16
P30	9	8	0
P10	2	0	0
Mobility	2	0	0
Total	145	17	16

The parking occupancy throughout the day on both survey days is shown in Figure 30. This is compared to the occupancy threshold of 85% (See Section 1.3). The occupancy of the entire section is shown as well as the parking occupancy on Crofton Road itself. The occupancy in this section on both days was significantly lower than the occupancy threshold of 85%. The occupancy in this section does not often exceed 40%, with the peak on Crofton Road being at 2pm on the Thursday. The time restricted parking in this area has a peak occupancy of 55% at 2pm also on the Thursday. The unrestricted parking in this section peaked at 1pm during the Thursday survey (37%).

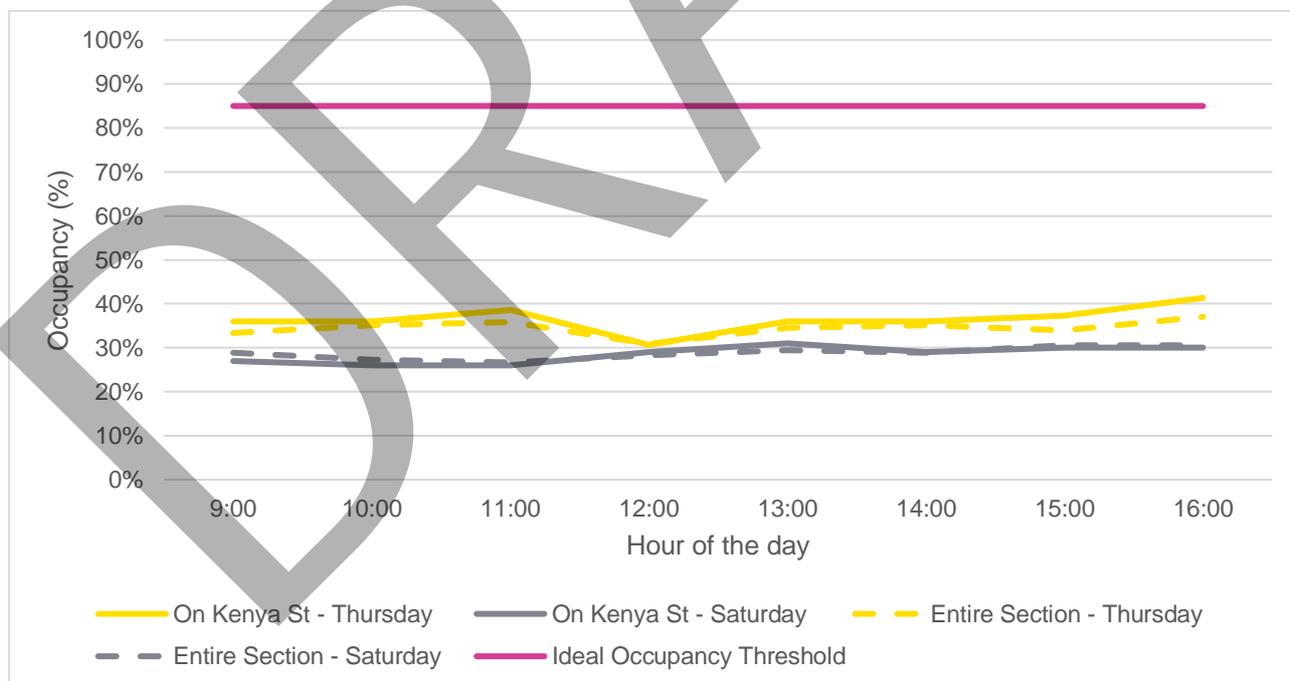


Figure 30 Crofton Road parking occupancy on both days compared to the occupancy threshold of 85%

The peak parking occupancy on each street in this area is shown in Figure 31 and Figure 32 for the Thursday and Saturday surveys respectively. The results show the following:

- The Ottawa Road car park was more than 80% occupied at its peak. No other street in this area was more than 60% occupied at any point during the two survey days.

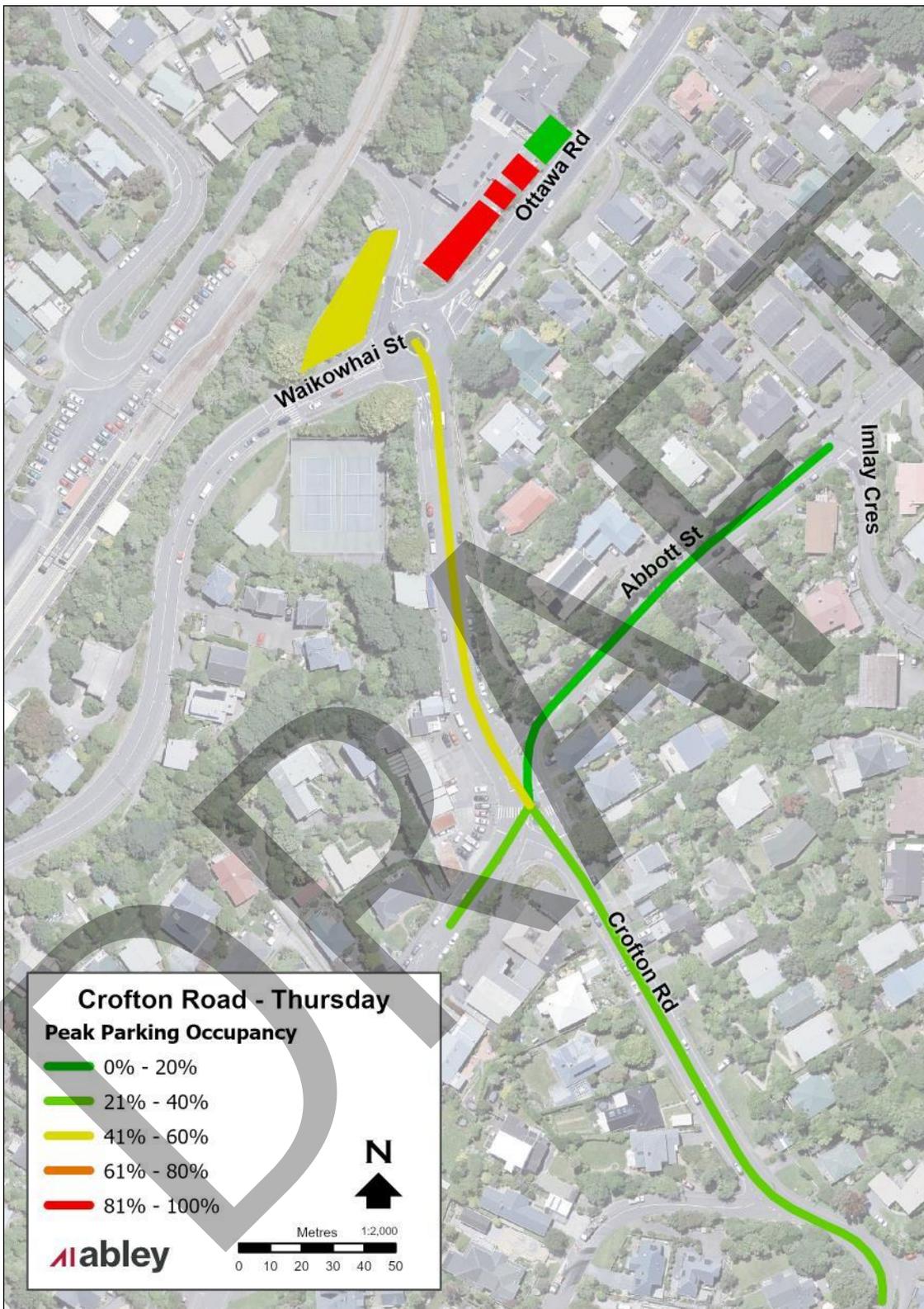


Figure 31 Crofton Road section Thursday peak parking occupancy per street

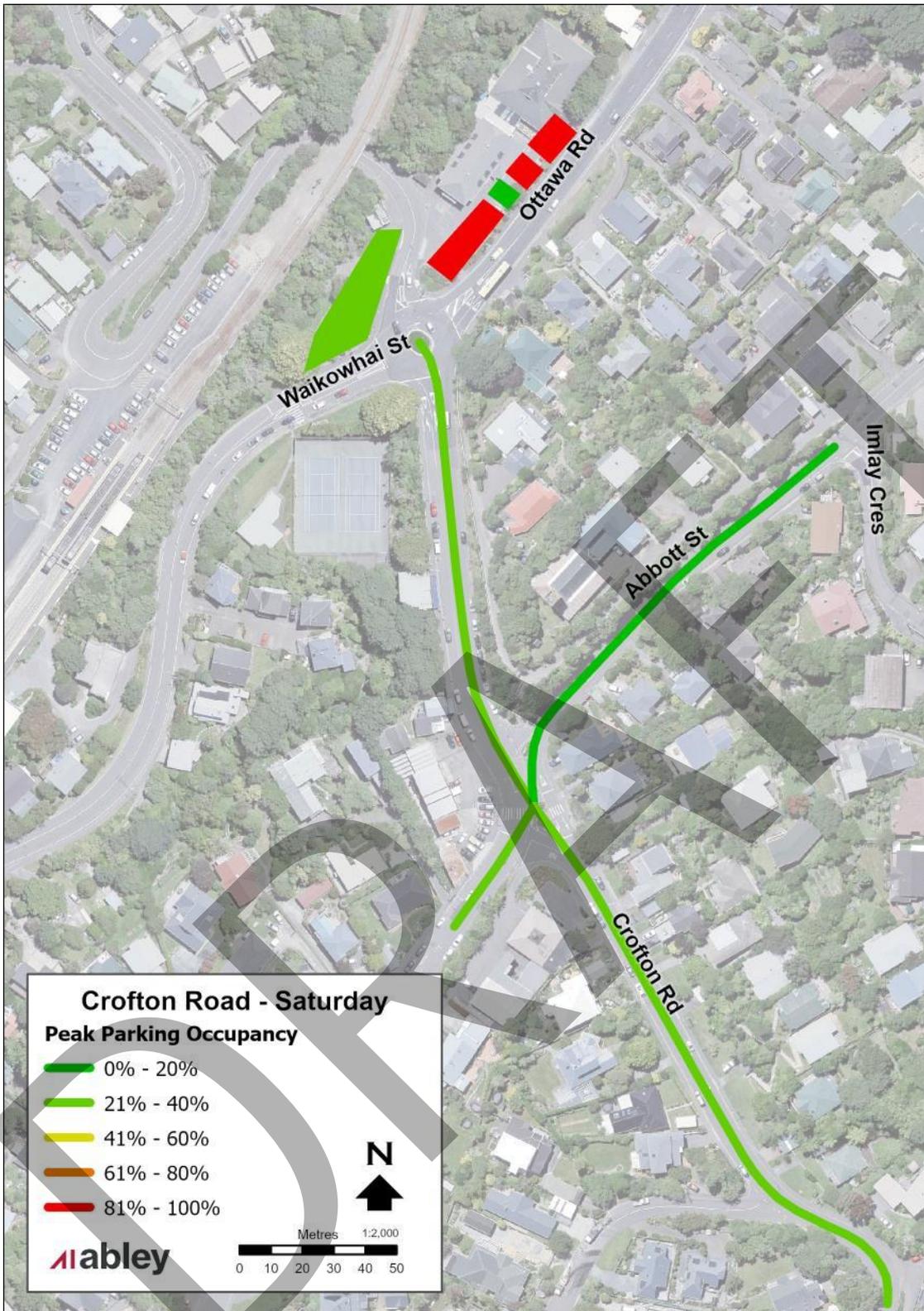


Figure 32 Crofton Road section Saturday peak parking occupancy per street

6.2.1. Duration of stay

The duration of stay data can be used to assess the parking behaviour around the shops on Crofton Road. Figure 33 shows the average duration of stay profile for the two survey days. The time restricted parking is used for short stay (<1 hour) parking by 76% of users on the Thursday and 93% of users on the Saturday. The unrestricted parking typically has a similar proportion of short stay and long stay users (41% <1-hour compared to 35% 4+ hours).

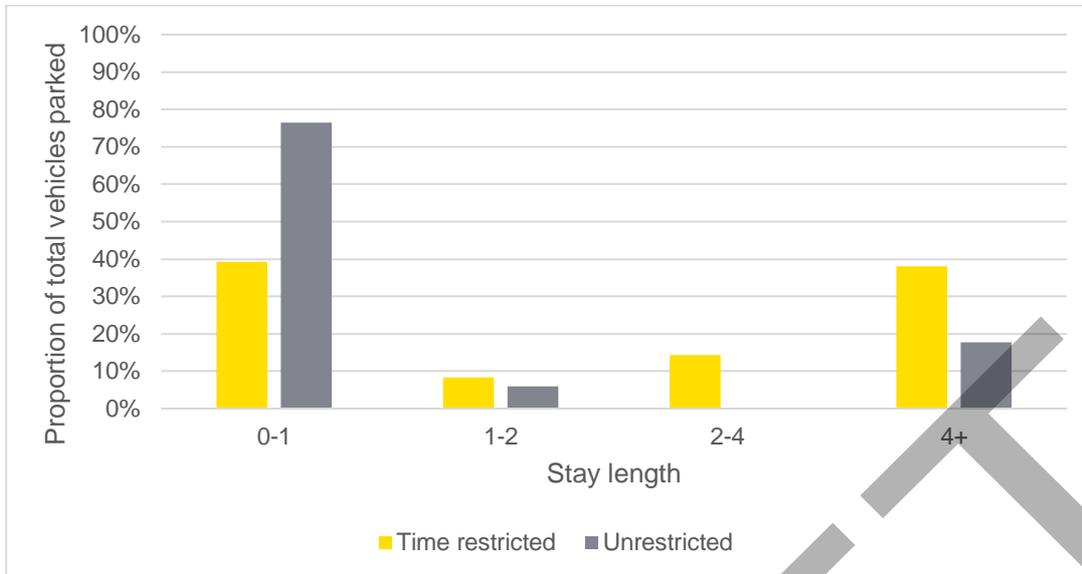


Figure 33 Parking duration of stay behaviour comparing the time restricted and unrestricted parking spaces in the entire section

As described in Section 2.4 this data can be used to assess the extent to which drivers are non-compliant with parking restrictions in this area. It has been assumed that everyone who stays for longer than one hour is non-compliant.

The survey found that an average of 16% of users of the time restricted parks stayed longer than the restriction allows. Table 15 shows the level of non-compliance for each parking restriction type on both survey days.

Table 15 Non-compliance with parking restrictions

Restriction	Thursday	Saturday
P30	23%	7%
P10	25%	No users surveyed

6.2.2. Overnight parking

In addition to the surveys throughout the day, overnight snapshot surveys were undertaken to assess the evening occupancy in the area. Figure 34 shows the average overnight occupancy of this section from both the weeknight and weekend checks. The occupancy overnight never exceeded 40% for any of the streets in this section. It is expected that most of the vehicles parked in this section overnight are residents' vehicles, so this data provides an insight into the residents' parking behaviour on Crofton Road.

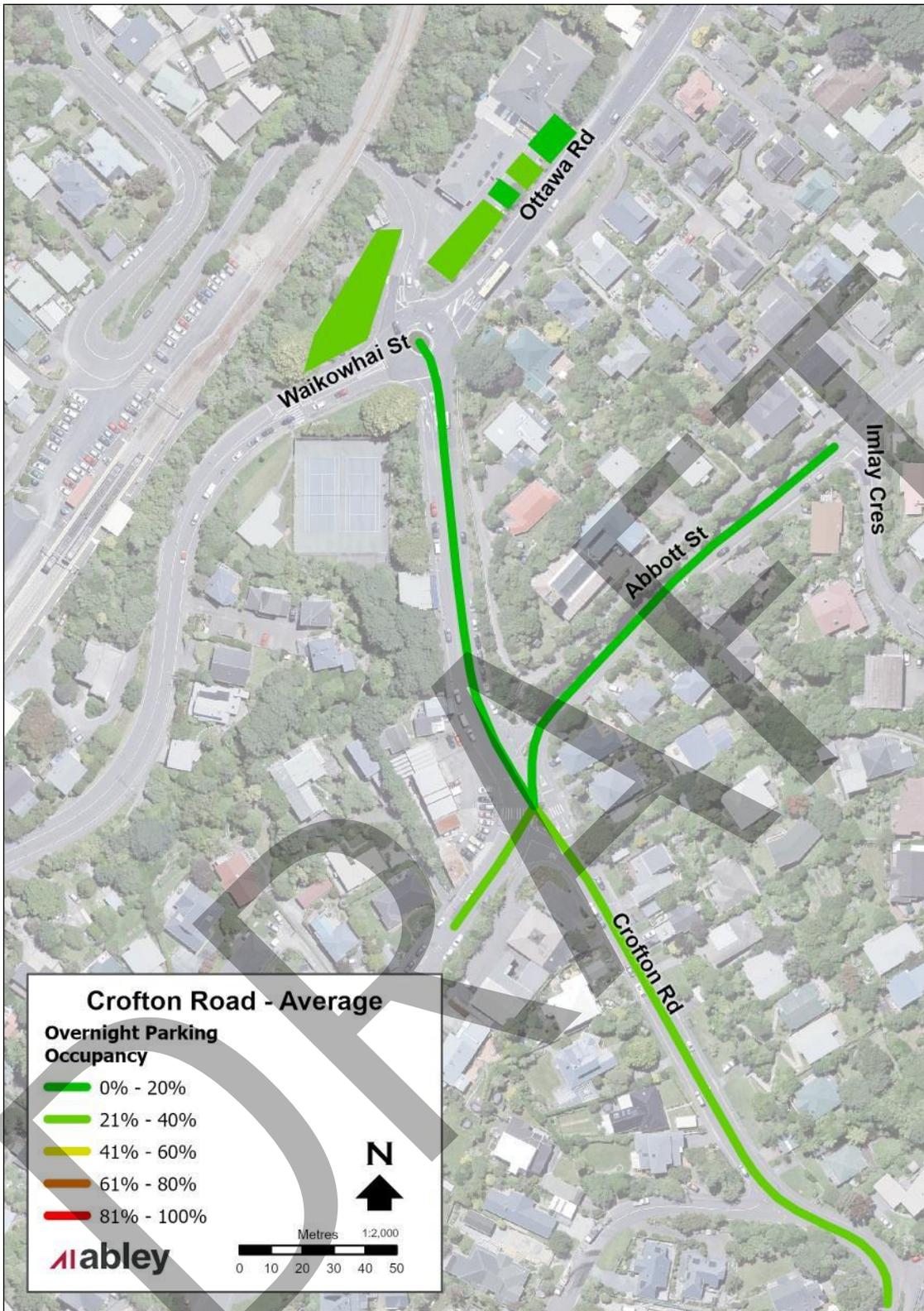


Figure 34 Crofton Road overnight average occupancy from the two survey days

6.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed cycleway in this section is the same as the Kenya Street section, a shared lane in the towards Ngaio direction and a separated cycleway in the opposite direction. This has resulted in parking only being available on the side of the road in the towards Ngaio direction. This results in the removal of 16 unrestricted spaces. This is approximately 50% of the parking spaces on Crofton Road where the cycleway is and approximately 11% of the parking spaces in the entire

section. The P30 space outside the takeaway shop on Kenya Street has been removed to provide sufficient space for vehicle tracking. All of the other P30 spaces on Crofton Road in the towards Ngaio direction outside the shops and businesses would remain.

Table 16 shows the peak occupancy of the unrestricted spaces on Crofton Road currently from both surveys as well as the expected occupancy after the removal of the parking spaces.

Table 16 Peak occupancy of the unrestricted spaces on Crofton Road and the expected occupancy after the removal

Day	Surveyed peak occupancy	Expected peak occupancy after parking removals
Thursday	36%	100%
Saturday	17%	47%

All the parking removed is unrestricted all-day on street parking. Given the high proportion of expected residents parking occurring in the unrestricted parking on Crofton Road, the parking removed is expected to be used mainly by residents. This is high priority parking (See Table 1). On top of this, given the proximity to the shops, it is expected that there is a reasonable number of short-term visitors parking on the towards city side of the road currently to access the shops and businesses. This is evidenced by the duration of stay data, see Section 6.2.1 (41% of users stay <1 hour). This parking is low priority parking according to Table 1. These users are expected to still be able to use the parking on the towards Ngaio side of the road, which is P30 to aid turnover around the shops. Therefore, this is not expected to be significant.

6.4. Mitigation of parking impacts

In the entire area, after the removal of the 16 parking spaces, the occupancy of the unrestricted spaces is expected to peak at 42% at maximum (on the Thursday). On Crofton Road, the occupancy of the unrestricted parks is expected to reach 100% on the Saturday. This is above the occupancy threshold of 85%. To achieve the 85% ideal threshold on Crofton Road, one vehicle needs to park in an adjacent street.

The most suitable mitigation for this is to encourage users of these spaces to park in the surrounding streets and off-street car parks and make them aware of the available occupancy in these spaces. Parking on Abbott Street has a peak occupancy of 14% (24 unoccupied spaces) on the Saturday. This parking is within a five-minute walk from centroid of the parking removal on Crofton Road (See Figure 35). This impact is considered low according to the Wellington Parking Policy 2020. The parking considered in this assessment is only the unrestricted parking on Abbott Street. This is because of the low existing occupancy. It is expected some users will park on other streets in the area (Kenya Street, Crofton Road to the east of the cycleway section, and north and west of the Ottawa Road roundabout). However, this has not been assessed.

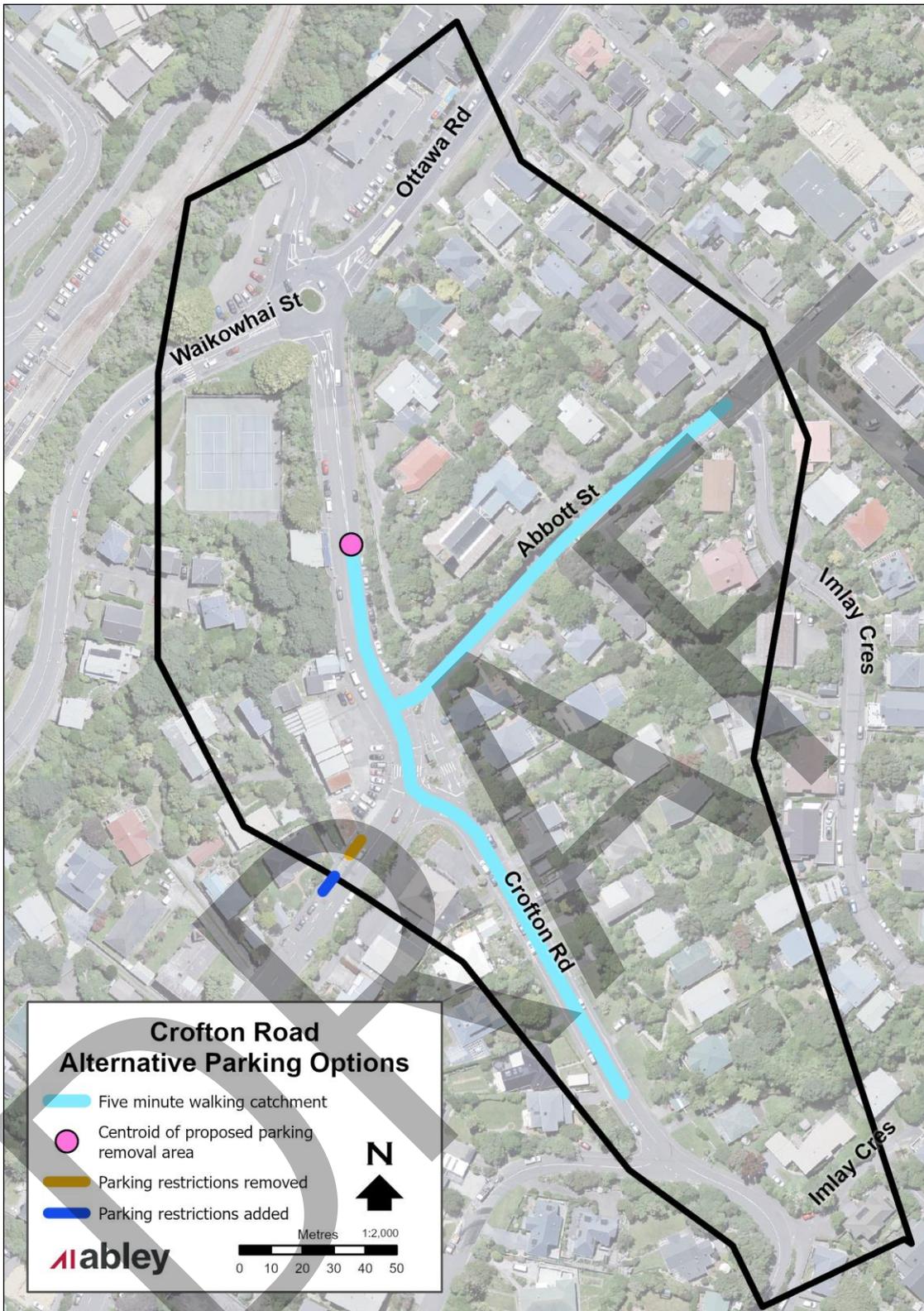


Figure 35 Parking within a five-minute walk from the centroid of the spaces removed on Crofton Road (not including the section to the north)

To account for the removal of the P30 space outside the takeaway shop, the adjacent unrestricted car park on Kenya Street is recommended to be converted to a P10 space (See Figure 35 for the location of this). This will still provide a short-term space to allow visitors to the shops to find a space. Given the nature of the business this space services, a shorter time restriction seems more appropriate. This is a very low impact, as the parking is available within a one-minute walk. There are P30 spaces available on Crofton Road in the towards Ngaio direction. The addition of a P10 space will remove one more unrestricted space in the Crofton Road section. However, since this is a small fraction of the removals already considered, it has not been assessed. There is adequate

capacity on Crofton Road (south of Kenya St) and Abbott Street already, so one more space is expected to be easily accounted for.

It is recommended that WCC engages with residents to understand any issues or concerns with the removal of this unrestricted parking. Given the high proportion of long duration stay (35% of users in this area stay for 4+ in unrestricted spaces) it is expected that most vehicles are residents' parking.

DRAFT

7. Conclusions

The key observations from this assessment are as follows:

- In all study sections, the parking occupancy on the streets where the cycleway is proposed is typically higher than the surrounding areas. This is largely due to the destinations located on the corridor.
- The parking occupancy is the highest around the commercial centres at the bottom of the Ngaio Gorge (on Kaiwharawhara Road and Westminster Street).
- In most areas, the overnight occupancy observed is like that observed throughout the day in the unrestricted parking sections. The main difference is on Kaiwharawhara Road during the weekday, where the occupancy was significantly higher through the day than at night.
- Of the entire parking in this area, 43% of users stay for one hour or less. Overall, 33% of users stay for 4+ hours. This is driven by the high number of users accessing the Kaiwharawhara Road commercial centres.
- There is a significant occurrence of users overstaying time restricted parks on Kaiwharawhara Road (approximately 40% from the weekday survey).
- The average occupancy throughout the entire study area is 44% on the Thursday and 28% on the Saturday.

Table 17 shows the summary of parking changes in each section as a result of the Ngaio transitional cycleway project. This shows the peak occupancy expected on the roads with cycleway sections before and after the parking changes and mitigation. All of the roads where the cycleway is proposed are expected to have a parking occupancy at or near the ideal peak occupancy. There is adequate capacity in the surrounding streets to account for the removal of the spaces on the cycleway corridor.

Table 17 Summary of proposed changes to parking for the Ngaio Transitional Cycleway Project (after completion of Stage 3)

Section	Current peak occupancy	Expected peak occupancy	Proposed changes
Kaiwharawhara Road	79% (1pm)	85%* (1pm)	<p>Loss of 43 unrestricted spaces on Kaiwharawhara Road. Loss of five P30 spaces on Kaiwharawhara Road. Loss of two angle spaces (P60) at the bottom of Cameron Street – north side Four P60 spaces and five P120 spaces to be added on Cameron Street (changed from unrestricted currently) to accommodate short term parking users displaced from Kaiwharawhara Road. Monitor the parking occupancy in Cameron Street (due to the overflow of vehicles from Kaiwharawhara Road), and if significant issues occur implement a parking restriction (P120/ P180 with the option for residents exempt)</p>
Cameron Street	77% (9am)	85%* (1pm)	<p>Change Four Unrestricted spaces at the bottom of Cameron Street to P60 – east side Change Four Unrestricted spaces at the bottom of Cameron Street to P120 – east side Change 39 Unrestricted spaces on the east side of Cameron St to P24Hr (this has been extended beyond the study area – see Section 3.3) Monitor the parking occupancy in Cameron Street (due to the overflow of vehicles from Kaiwharawhara Road), and if</p>

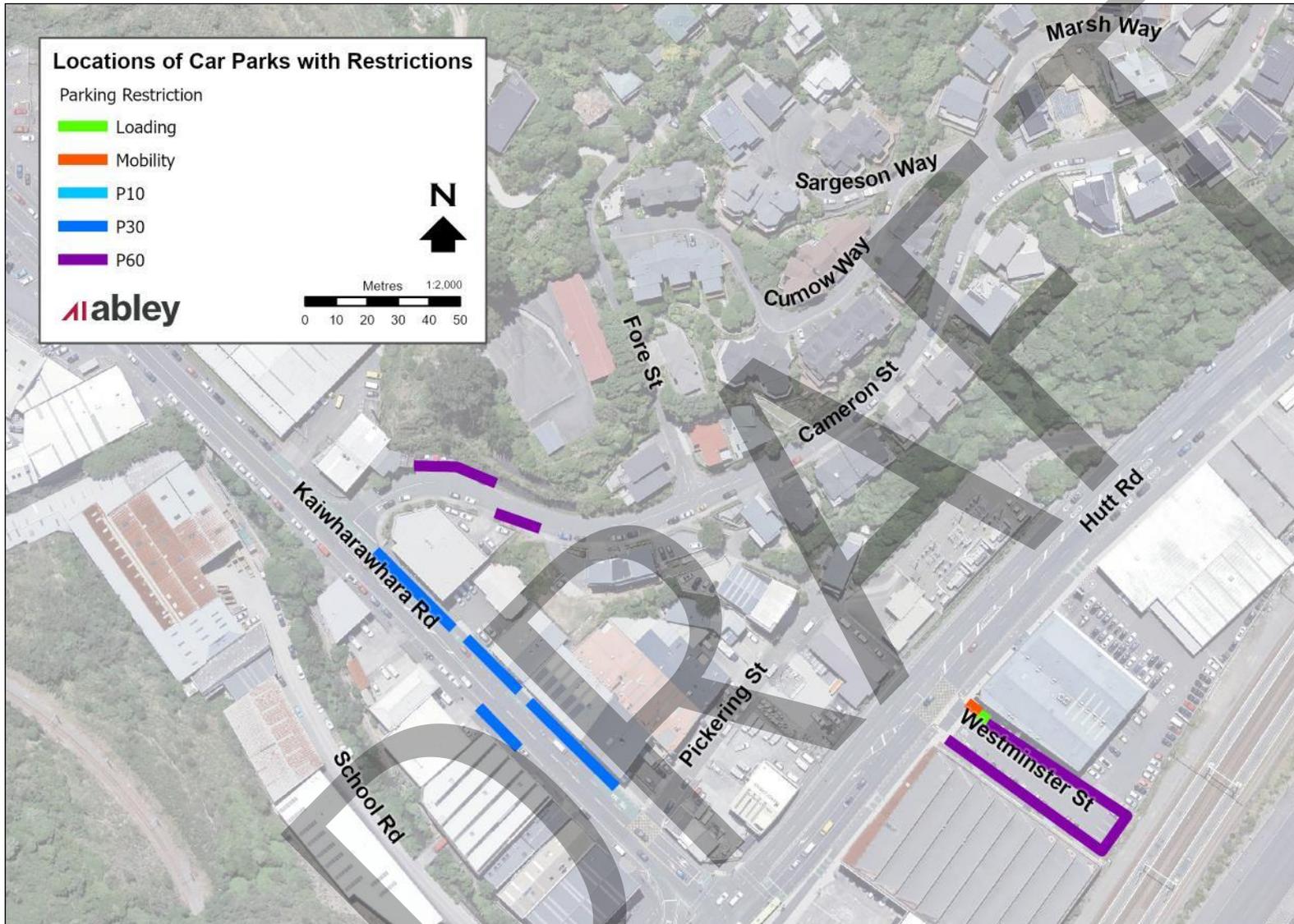
			significant issues occur implement a parking restriction (P120/ P180 with the option for residents exempt)
Ngaio Gorge Road	-	-	No parking loss in this section as there is no on-street parking available. Addition of 15 unrestricted spaces after completion of the slope stabilisation project
Kenya Street	41% (4pm)	85%* (4pm)	Loss of 63 unrestricted spaces on Kenya Street.
Crofton Road	42% (2pm)	85%* (2pm)	Loss of 16 unrestricted spaces on Crofton Road across the road from the shops. Loss of one P30 space outside the takeaway on Kenya Street Convert one unrestricted space on Kenya Street to a P10.

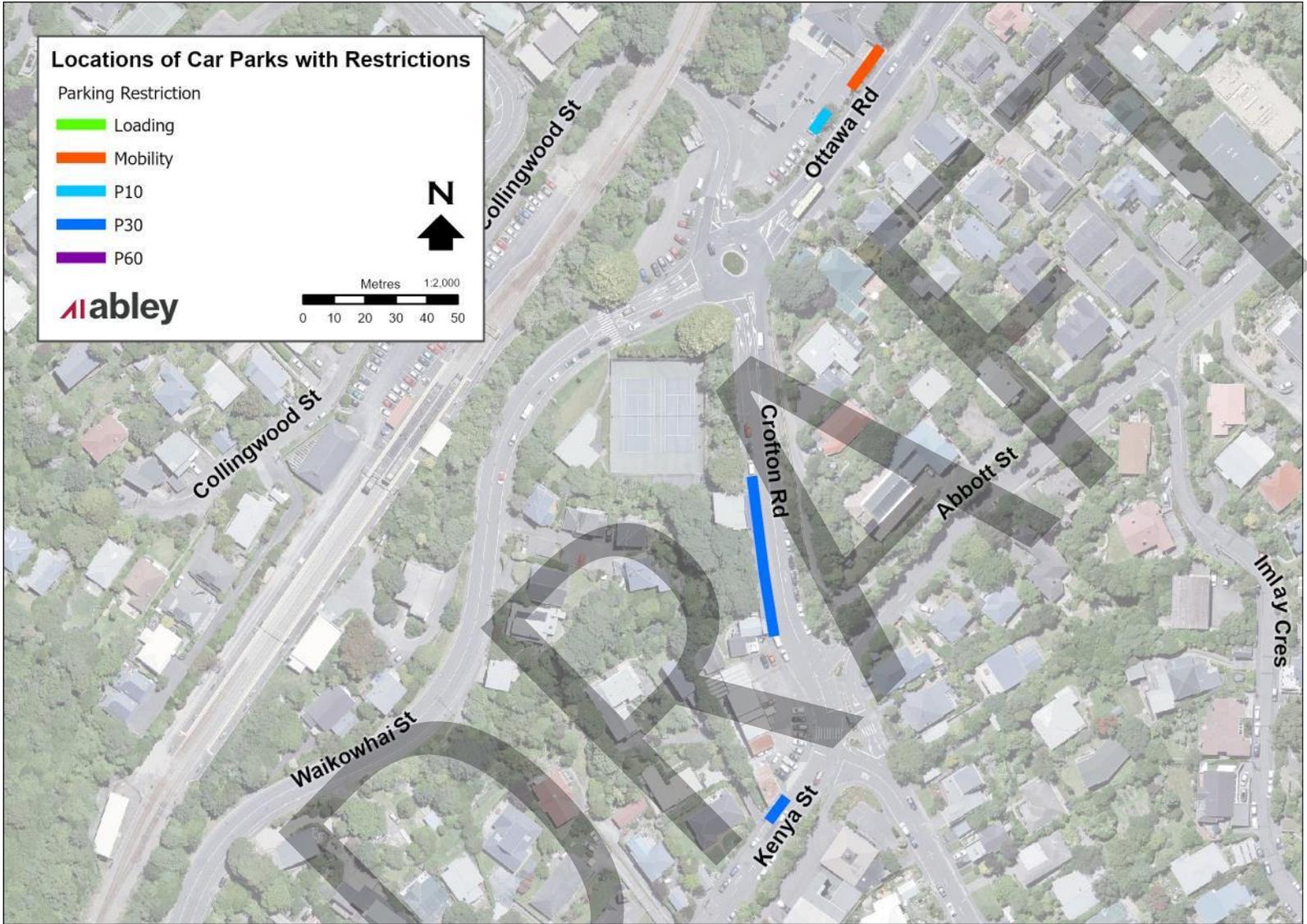
*Maximum expected capacity due to the occupancy outlined in Section 1.2.

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Appendix A – Parking Restriction Maps

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Wellington City Council**

Me Heke Ki Pōneke

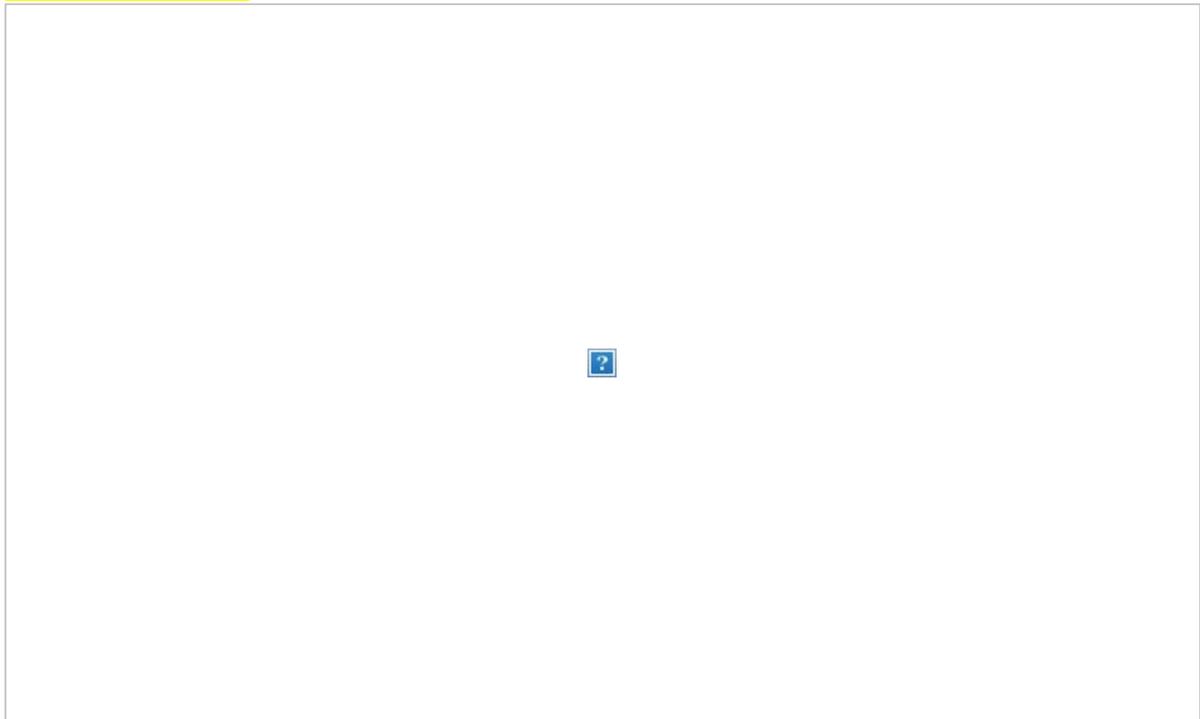
<https://wellington.govt.nz/parking-roads-and-transport/transport/cycling>

From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ann-Marie Head](#)
Subject: Draft Ngaio PMP - Cameron Street updates
Date: Tuesday, 13 December 2022 8:12:53 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[ATT00001.jpg](#)
[Pages 9 to 23 from DRAFT Ngaio Parking Management Plan_traffic.resolution_CameronStChanges.pdf](#)

Hi Jonathan.

Kaiwharawhara and Cameron Street sections of the PMP attached. Key for you are probably Sections 2.4 and 3.4 explaining the mitigation of parking impacts, see also snips below. We will wait before finalising as the proposed TR changes include changes to parking on Kenya Street and Crofton Road also which require updating.

[Kaiwharawhara Road]



[Cameron Street]



Thanks,

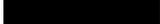
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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2. Kaiwharawhara Road

2.1. About the area

The Kaiwharawhara Road section is a primarily commercial section at the southern end of the proposed cycleway corridor. This section connects the bottom of the Ngaio Gorge to Hutt Road. This area has multiple cafes, retail businesses, commercial/industrial businesses, a courier post depot, and other various commercial activities. At the northern end of this section, roadworks were occurring on the Ngaio Gorge during the parking surveys (major works relating to the large slip at the lower end of the gorge). These roadworks did not affect the parking supply in this area. This section is shown as Figure 3.

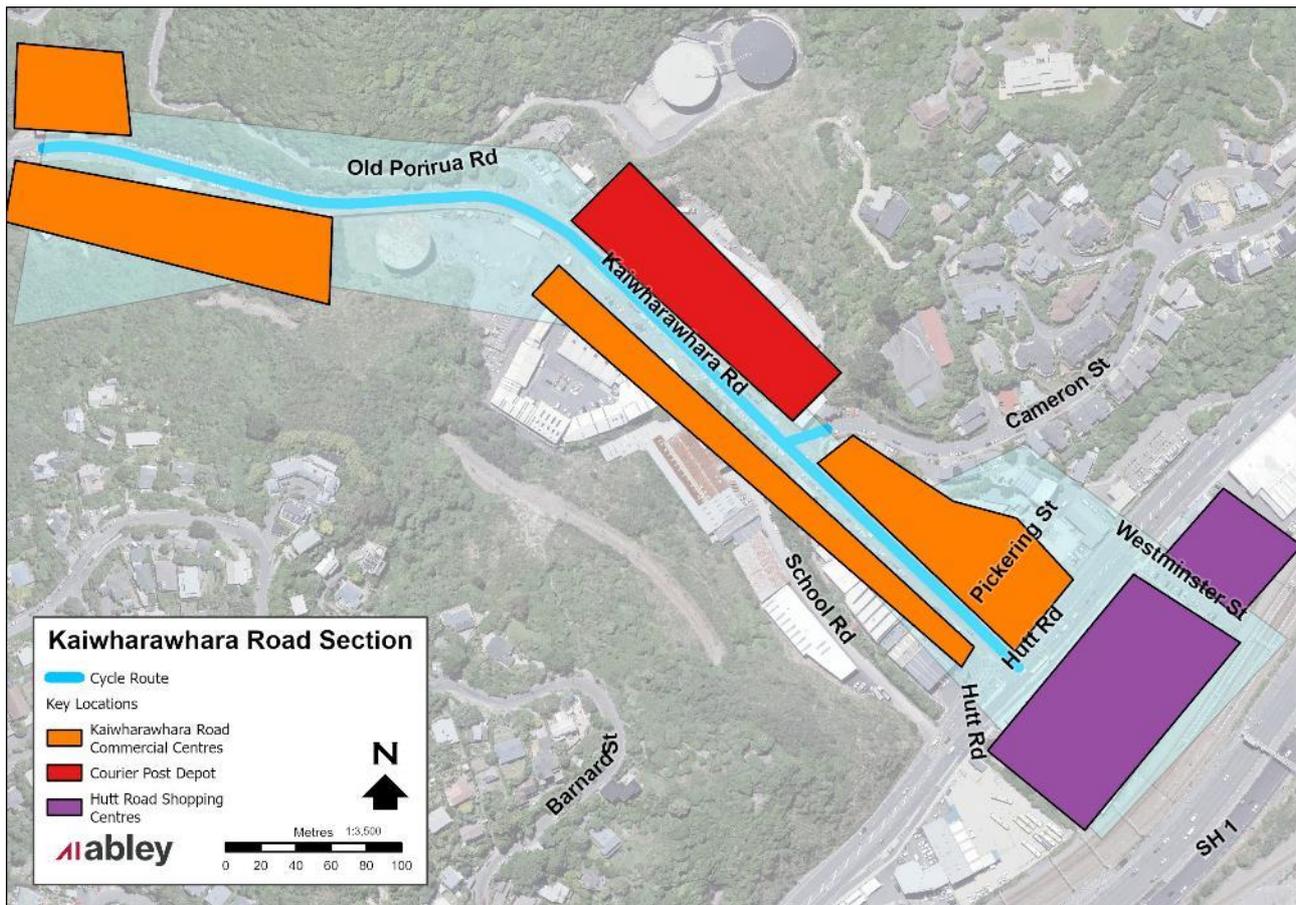


Figure 3 Kaiwharawhara Road parking study area

2.2. Current parking and usage

The parking in this section is a mixture of both unrestricted and time restricted parking (See Appendix A). The inventory and type of parking in this section is shown in Table 3. The parking in this section is all on-street, however, given the low volumes and no exit nature of Westminister Street, it functions as a pseudo off-street parking facility. There is one mobility park and one loading zone on Westminister Street. The seven (7) P60 parking spaces on Cameron Street are considered to be part of this section for the analysis. This is because it is expected that most people who use these parks are accessing the businesses on Kaiwharawhara Road. The parking spaces on the north-eastern side of Kaiwharawhara Road are not available when the bus lane is operating between 7-9am on weekdays. The parking spaces on School Road were not surveyed.

Table 3 Parking inventory in the Kaiwharawhara section (including number of spaces on Kaiwharawhara Road)

Restriction	Overall section inventory	Kaiwharawhara Road inventory (towards Ngaio)	Kaiwharawhara Road inventory (towards City)*
Unrestricted	125	43	74
P60	31	0	0
P30	19	3	16
Mobility	1	0	0
Loading	1	0	0
Total	177	46	90

*Note this inventory is the parking spaces available when the bus lane is not operating

The parking occupancy on both days of this survey is shown as Figure 4. The occupancy of the overall area and the parking just on Kaiwharawhara road is shown. This occupancy has been compared to the occupancy threshold of 85%. The parking occupancy in this section varies significantly between the two survey days. The parking occupancy on the Thursday peaks at 85% on Kaiwharawhara Road. On the Saturday the peak on Kaiwharawhara Road is 45%. This is expected given the high level of business activity that occurs along Kaiwharawhara Road. During the Thursday survey, the occupancy of this section overall was above 70% for most of the day (10am-2pm). This is a high occupancy and is driven by visitors/staff of the businesses in this area.

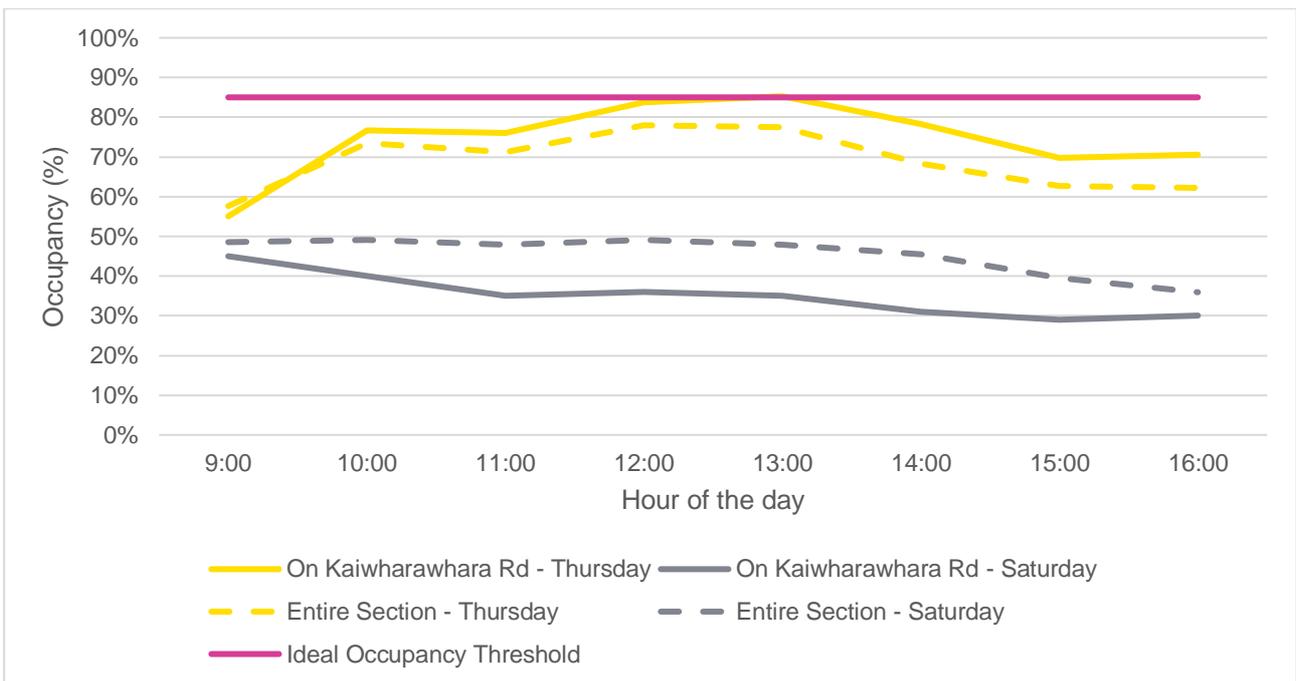


Figure 4 Kaiwharawhara Road parking occupancy on both survey days compared to the occupancy threshold of 85%.

Figure 5 shows the parking occupancy for the different restriction types in this section. The time restricted parking is typically less occupied than the unrestricted parking on the Thursday, however, on the Saturday, the time restricted parking is significantly more occupied.

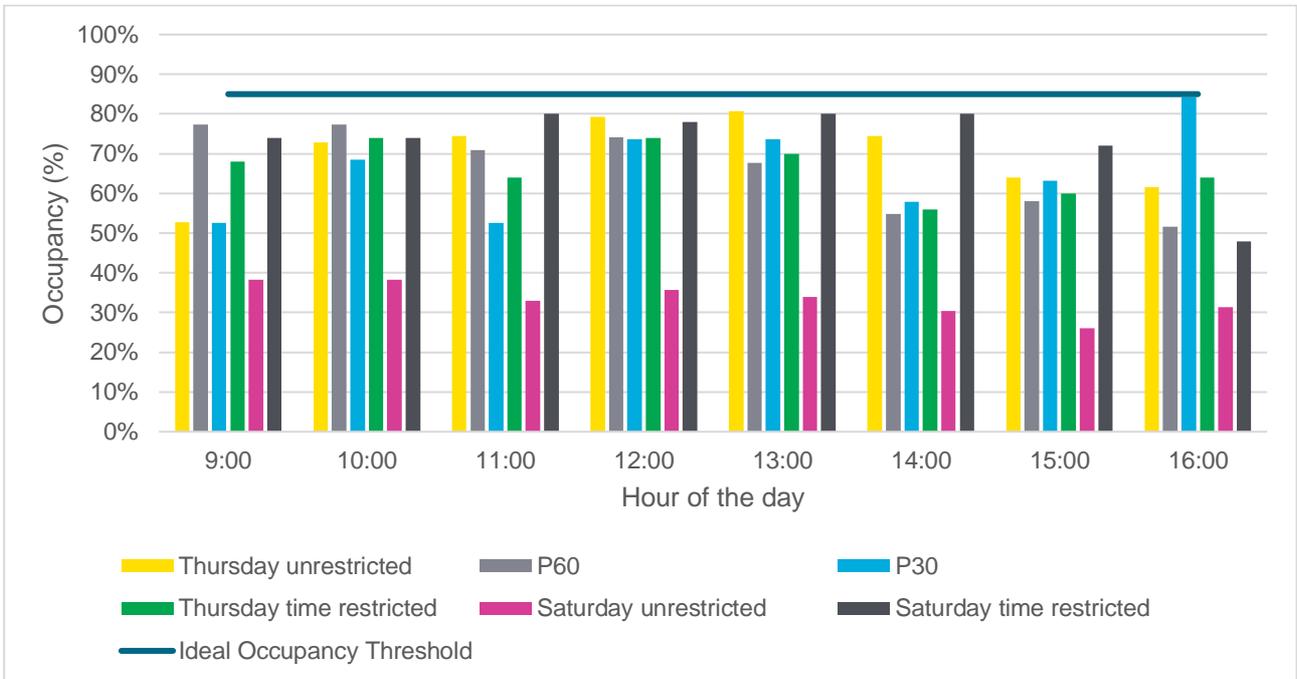


Figure 5 Comparison of parking occupancy between unrestricted and time restricted parking spaces

Figure 6 and Figure 7 show the parking occupancy per street in this section. This data shows that there is uniform parking demand on the Thursday, as nearly all the section is over 80%. This is not the case on Saturday, as there are concentrated areas of high parking demand on Pickering Street and Westminster Street, however, the rest of the section is relatively unoccupied. This demand is expected on Westminster Street given the proximity of the local businesses (Animates, cafes, etc.).

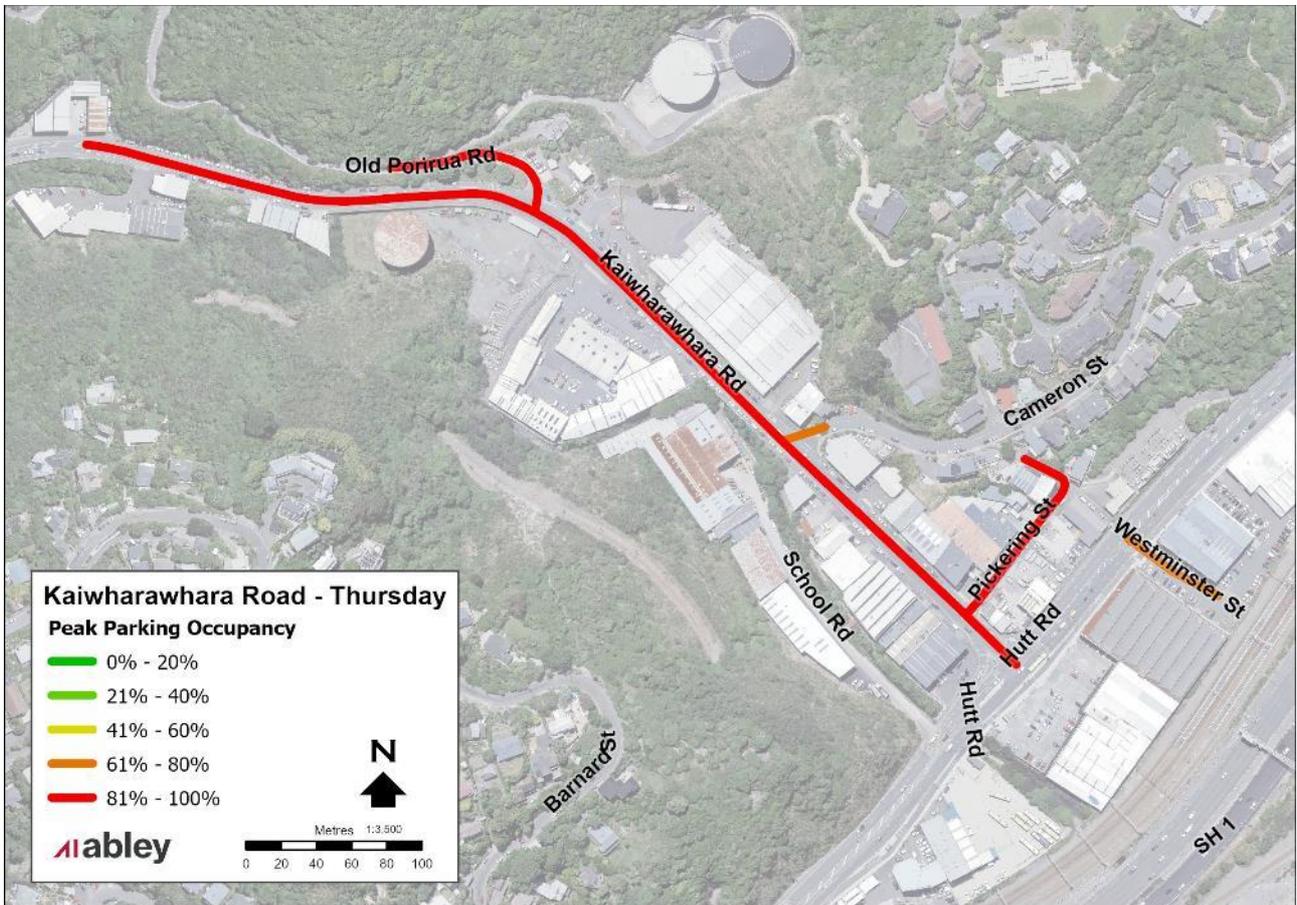


Figure 6 Kaiwharawhara Road section Thursday peak parking occupancy per street

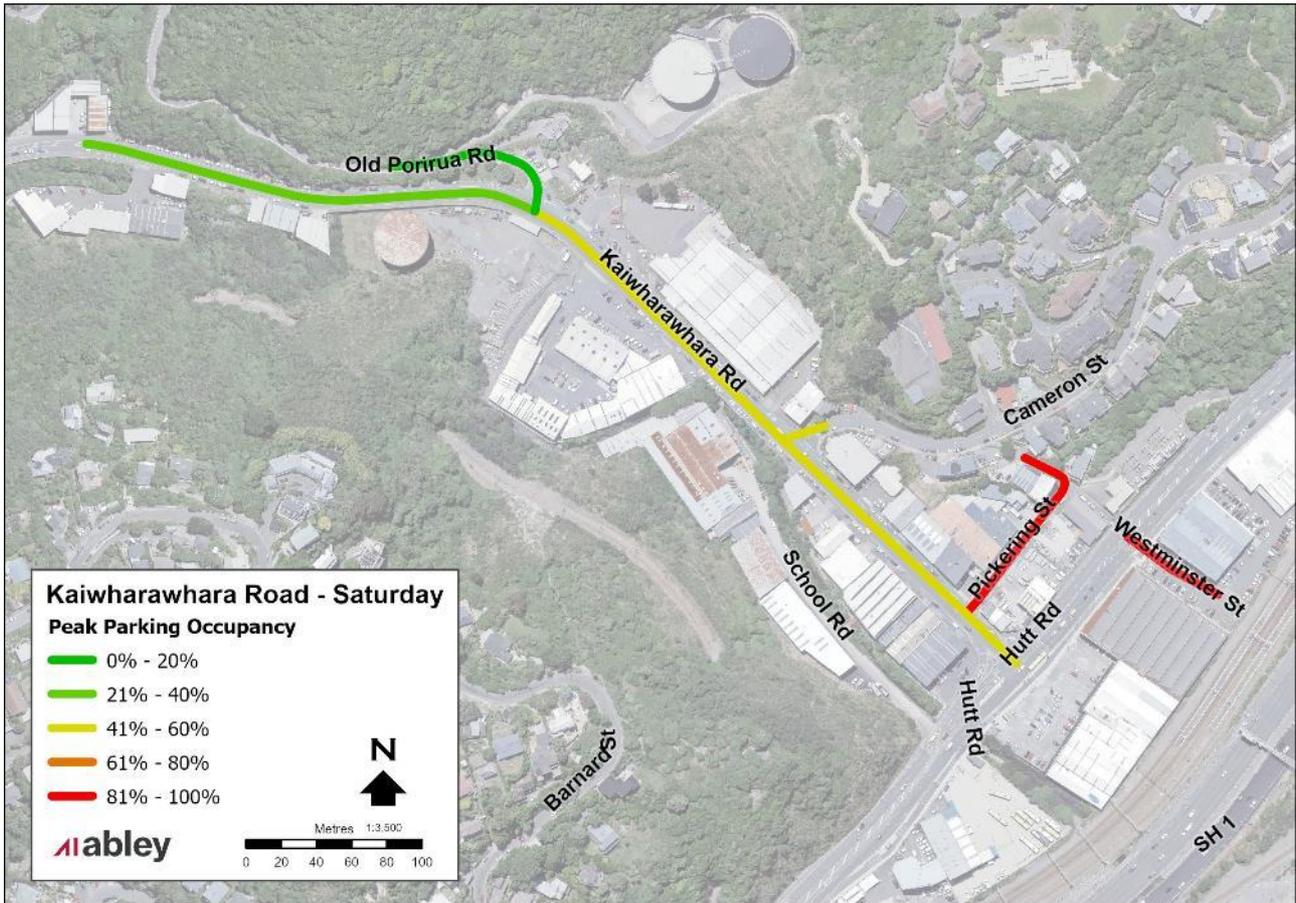


Figure 7 Kaiwharawhara Road section Saturday peak parking occupancy per street

2.2.1. Duration of stay

The duration of stay data can be assessed in this section to get a picture of the different user types of the parking. Figure 8 shows the duration of stay data on both the Thursday and the Saturday. The parking behaviour changes significantly on the two days. On the Saturday survey, the majority of users of the unrestricted parking were short stay users (45% <1 hour compared to 11% 4+hour). On the Thursday, 18% of users were short stay compared to 27% long stay. This shows there is a significant portion of users parking in this area for the entire day as part of their commute. It is not clear whether these users are working in the Kaiwharawhara businesses or commuting further into Wellington CBD (this parking is free compared to parking on Hutt Road which is paid).

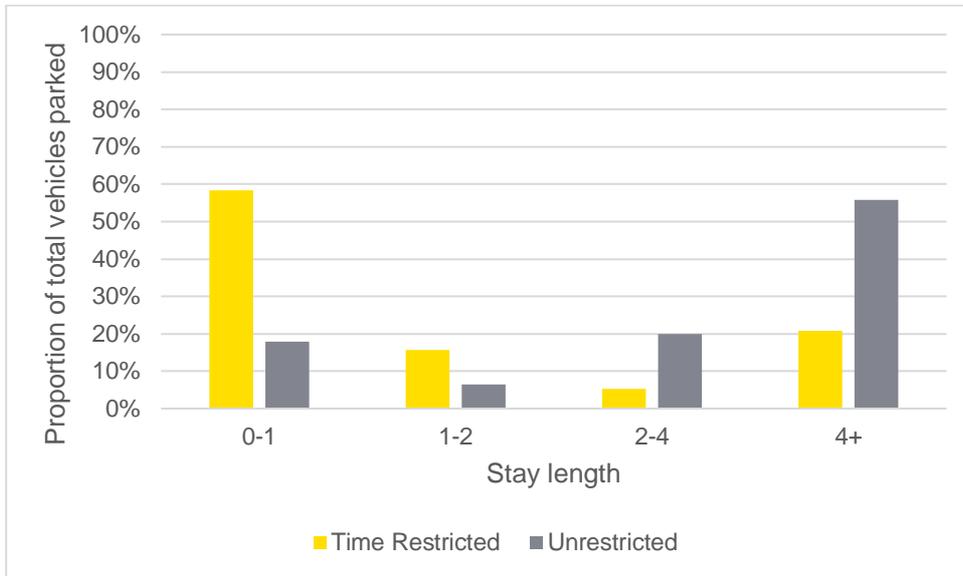


Figure 8 Duration of stay comparison for time restricted and unrestricted parking spaces

This data can be used to assess the extent to which drivers are non-compliant with parking restrictions in this area. Given the duration of stay data is taken every hour, the exact extent of the restriction compliance cannot be assessed (as the restrictions in this section are P10 and P30). It has been assumed that everyone who stays for longer than one hour is non-compliant.

Table 4 shows the level of non-compliance for the different parking restrictions. This data shows a relatively high amount of non-compliance with the parking restrictions.

Table 4 Non-compliance rates for the time-restricted parks on both survey days

Restriction	Thursday	Saturday
P60	41%	20%
P30	42%	32%

2.2.2. Overnight parking

The overnight parking shows a similar behaviour to the weekend occupancy (see Figure 9). There is relatively low occupancy throughout the area, which is driven by the high proportion of commercial land use in this area, rather than residential. One thing which was noticed was there was a significant number of courier post vehicles parked on the south side of Kaiwharawhara Road (opposite courier post). These vehicles were not observed during the 9am survey, which implies they are purely overnight parkers, resulting from staff. There is a bus lane which operates from 7am-9am during the weekdays. During the overnight weekday survey, there were no vehicles present in this bus lane. However, by the 10am survey period, the lane was significantly filled. This implies that people are parking in this section to commute, either to Kaiwharawhara Road or Wellington CBD. This is evidenced by most parkers in this section being long-stay users (46% stay for 4+ hours).

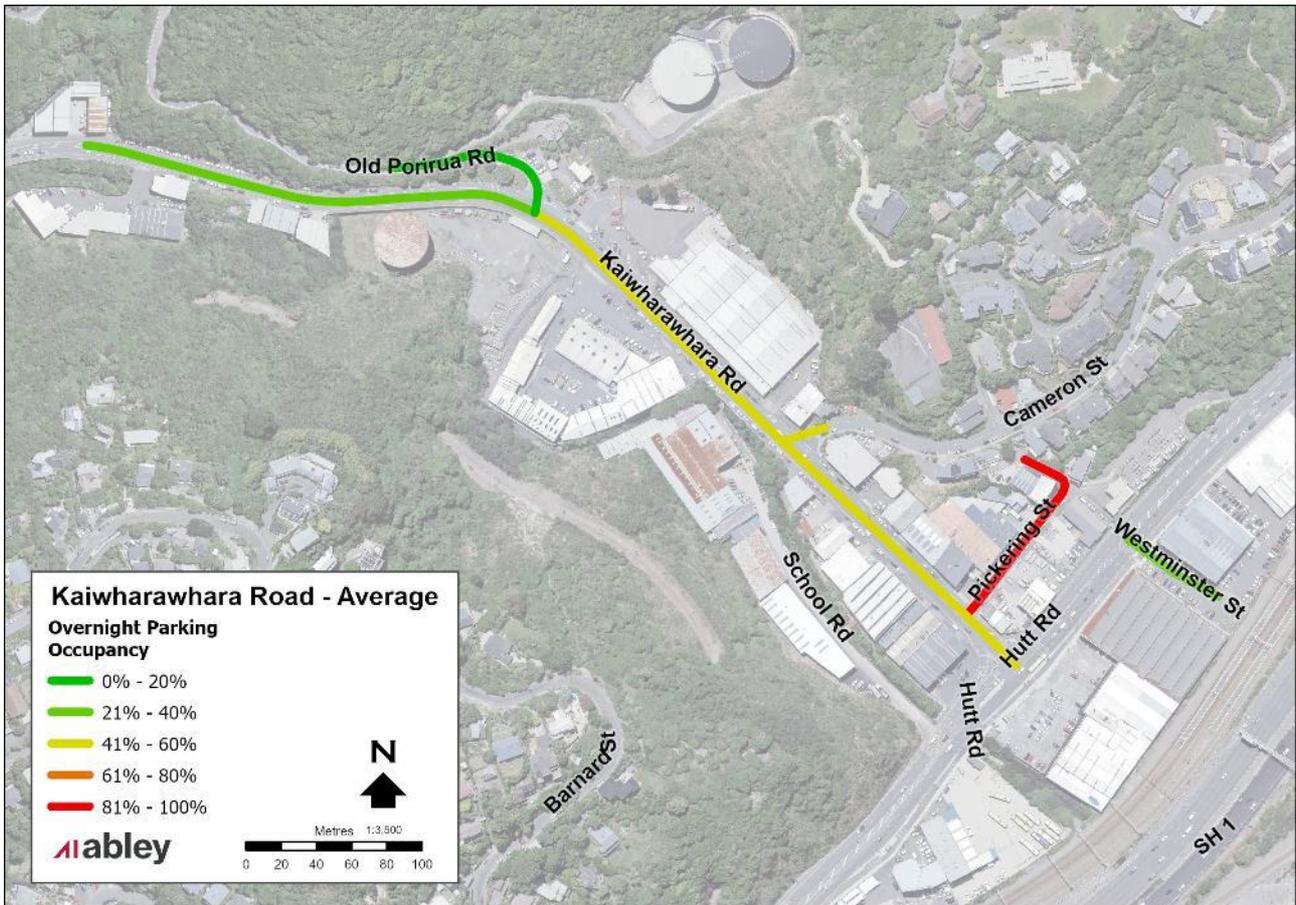


Figure 9 Kaiwharawhara Road overnight average occupancy from the two survey days

2.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed cycle facility in this section is to be implemented in phases. The impacts on parking are outlined below.

Stage 1 – completion in 2024

The transitional cycleway project will maintain the existing peak hour bus lane with off-street parking on the east side of Kaiwharawhara Road. On the west side, the transitional project will initially include a combination of peak hour clearway with parking permitted off-peak (from #1 to #25 and #53 to #57 Kaiwharawhara Road) and a separated cycleway along the remainder of this section. Overall, there are 46 existing parking spaces on the west side of Kaiwharawhara Road. The following details Stage 1 for the west side:

- From #1 to #25 Kaiwharawhara Road, there will be a clearway from 4-7pm. This means during this three-hour period, parking will not be permitted in this location. This equates to a removal of nine unrestricted spaces and three P30 spaces between 4-7pm. When the clearway is not active (i.e. 7pm to 4pm) the remaining spaces will be P30 at all times.
- From #27 to #53 Kaiwharawhara Road, the facility will be a separated/buffered cycleway. This results in the removal of all the parking on the west side of Kaiwharawhara Road in this section (at all times). This amounts to a removal of 23 parking spaces.
- From #53 to #57 Kaiwharawhara Road, the cycle facility will be a clearway from 4pm-7pm. During this three-hour period, parking or loading will not be permitted in this location. Three of the spaces in this section will be removed. Four of the spaces will be converted into a Loading Zone P10 from 7pm to 4pm and clearway from 4-7pm. Four of the spaces will be converted to P30 from 7-4pm and clearway from 4-7pm.

One unrestricted parking space will be removed due to the installation of no stopping lines at the bottom of Old Porirua Road. On the eastern side of Kaiwharawhara Road (in the downhill direction) the P30 parking will be converted into P60 parking to allow a longer stay (eg. lunch or a meeting). This changes 16 P30 spaces to 16 P60 spaces.

Stage 2 – completion in 2025

The second stage is to construct a separated cycleway in the uphill direction along the entire length of Kaiwharawhara Road. This results in the removal of all parking spaces on the west side (46 spaces on Kaiwharawhara Road). The loading zone outside #55 Kaiwharawhara Road will be retained.

Parking inventory through the stages

The parking inventory in this section throughout the changes is shown in Table 5. This shows the existing inventory, Stage 1 and Stage 2. There is a total removal of 33 spaces in Stage 1. There is a total removal of 49 spaces in Stage 2. There is one existing loading zone, and one loading zone being added. These are not included in Table 5. The parking inventory per street before and after the changes is shown as Table 6.

Table 5 Parking inventory in the Kaiwharawhara Road section before and after the cycleway project

Restriction	Existing inventory	Stage 1 inventory	Stage 2 inventory
Unrestricted	125	79	79
P60	31	63	47
P30	19	0	0
Mobility	1	1	1
Total	176	143	127

*Two P60 spaces removed and two added

Table 6 Inventory per street for the entire Kaiwharawhara Road section existing and after the Stage 2 changes

Street	Existing Inventory	Inventory after Stage 2
Kaiwharawhara Road	Unrestricted: 120 P30: 19 P60: 0	Unrestricted: 79 P30: 0 P60: 16
Old Porirua Road	Unrestricted: 5	Unrestricted: 4
Westminster Street	P60: 15 Mobility: 1 Loading: 1	P60: 15 Mobility: 1 Loading: 1
Pickering Street	P60: 9	P60: 9
Cameron Street	P60: 7	P60: 7

Anticipated parking occupancy after Stage 2

At the end of Stage 2, there will be 49 parking spaces permanently removed. This is approximately 27% of the entire section inventory and 33% of the inventory on Kaiwharawhara Road. When only the unrestricted spaces are considered, this is a removal of 37% of the unrestricted spaces in the entire section and 36% of the unrestricted spaces on Kaiwharawhara Road. For the time restricted spaces, this is a 10% decrease in the entire section and 16% on Kaiwharawhara Road.

Table 7 shows the peak occupancy of the unrestricted and the time restricted (P30 and P60) parking on Kaiwharawhara Road from the surveys as well as the expected peak occupancy after the parking is removed. The time restricted parking is assessed together as it is all less than an hour. It is assumed that the users who would previously use the P30 spaces would now use the P60 spaces.

Table 7 Occupancy on Kaiwharawhara Road from survey and the expected occupancy after parking removal

Day	Restriction	Peak from survey	Expected peak after parking removals
Thursday	Time Restricted	84%	100%
	Unrestricted	80%	100%**
Saturday	Time Restricted	74%*	100%**
	Unrestricted	37%*	60%

*It should be noted that on the Saturday the two peak values were the first recordings of the day

** This is the maximum capacity achievable; the actual demand is higher than this

During weekdays after Stage 2, it is expected that there will be insufficient parking supply on Kaiwharawhara Road to achieve the desired occupancy threshold of 85%. On Saturdays, the time restricted parking is likely to be over capacity and the unrestricted parking is likely to have sufficient capacity.

During the weekday, the unrestricted parking removed is typically used by commuters (See Section 2.2.1). This parking is classed as lower priority according to the Wellington Parking Policy 2020 (See Table 1). The occupancy of the Kaiwharawhara Road section is relatively high (approximately 81% throughout the day on the Thursday). After the removal of parking spaces, there are approximately 33 unrestricted parking users who will need to find an alternative car park to reduce the occupancy to 85% at the peak. The other unrestricted parking in the Kaiwharawhara Road section is already over the 85% occupancy threshold. When the Kaiwharawhara Road section is at its peak (1pm - weekday), the Cameron Street section is only expected to be at 66%. This section will have capacity to accommodate approximately 8 displaced users from Kaiwharawhara Road. There will still be several users (approx. 25) who cannot be accommodated in either the Kaiwharawhara Road section or the Cameron Street section. Mitigation measures for these users are discussed in Section 2.4.

When the weekday time restricted parking is considered, the occupancy is currently 84%. After the removal of the three time restricted parks, there is not enough capacity to accommodate all users in the Kaiwharawhara Road section. However, several unrestricted spaces in the Cameron Street section are proposed to be converted to P60 & P120 restrictions. It is expected the excess users from Kaiwharawhara Road (approx. two vehicles) will use these (See Section 2.4).

In the weekend, the occupancy of the unrestricted spaces on Kaiwharawhara Road are not expected to exceed the 85% occupancy threshold, however, the time restricted parking will. There are adequate unrestricted spaces to accommodate the short-term users who have been displaced. Therefore, no users are expected to transfer into the Cameron Street section on weekends. This change is reflected in Table 9.

Table 8 Thursday peak occupancy and expected occupancy of the unrestricted spaces after removals in the Kaiwharawhara Road and Cameron Street sections due to the cycleway

Section	Current occupancy (Kaiwharawhara Road peak)	Expected peak occupancy (Kaiwharawhara Road peak)
Kaiwharawhara Road	81%	85%*
Cameron Street	52%	85%*

*This is capped at the occupancy threshold. The demand exceeds 100%.

Table 9 Saturday peak occupancy and expected occupancy of the unrestricted spaces after removals in the Kaiwharawhara Road and Cameron Street sections due to the cycleway

Section	Current occupancy (Kaiwharawhara Road peak)	Expected peak occupancy (Kaiwharawhara Road peak)
Kaiwharawhara Road	38%	63%
Cameron Street	27%	27%

The parking changes through the stages are shown in Figure 10, Figure 11, and Figure 12. These figures show all spaces which are being removed/ modified as a result of the cycleway project. Figure 10 shows the existing restriction of these spaces, Figure 11 shows the restrictions in Stage 1, and Figure 12 shows the restrictions in Stage 2. Where parking has been removed all together for the improvements, it has been removed from the maps.

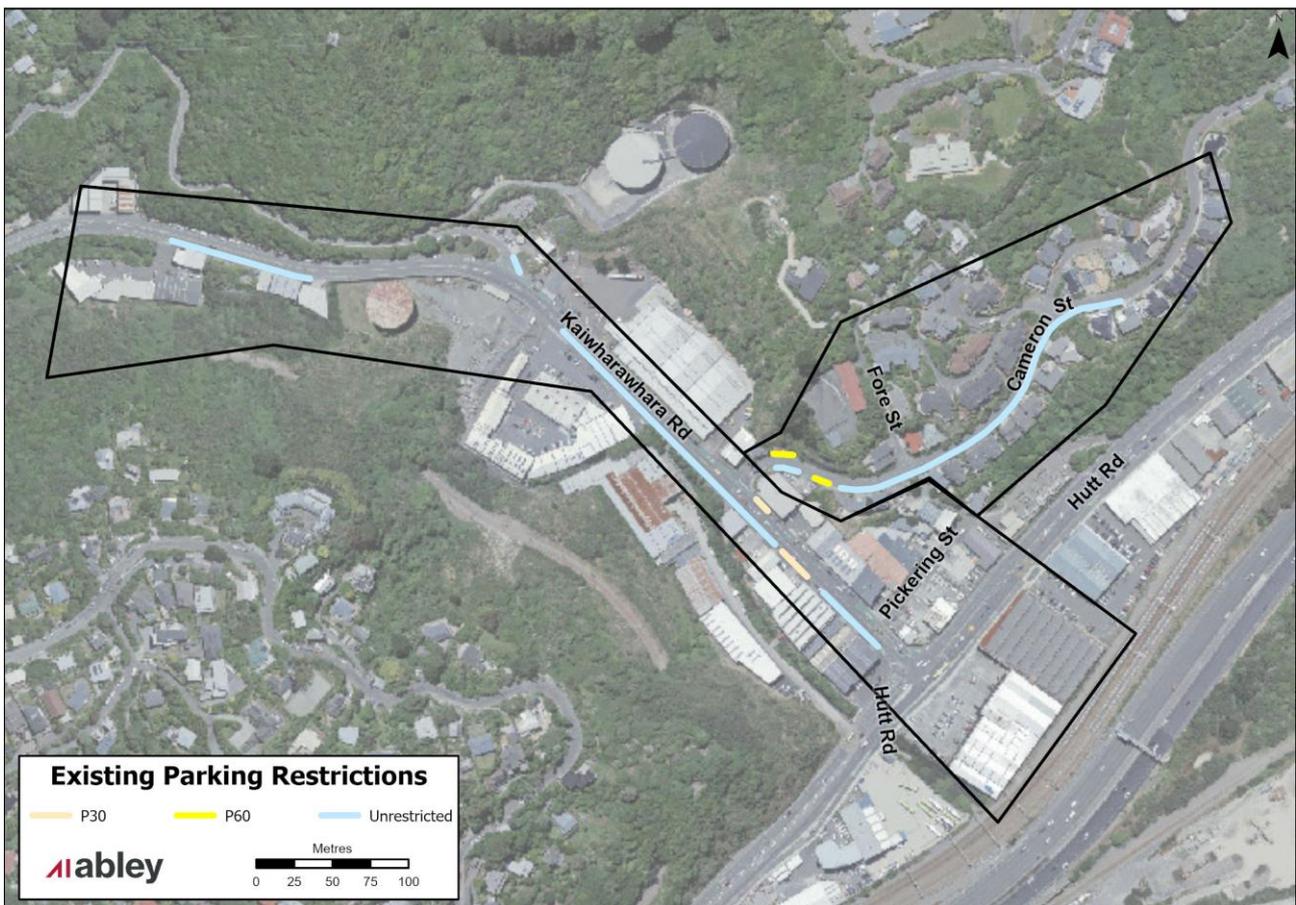


Figure 10 Existing parking restrictions of parking spaces affected by the cycleway project

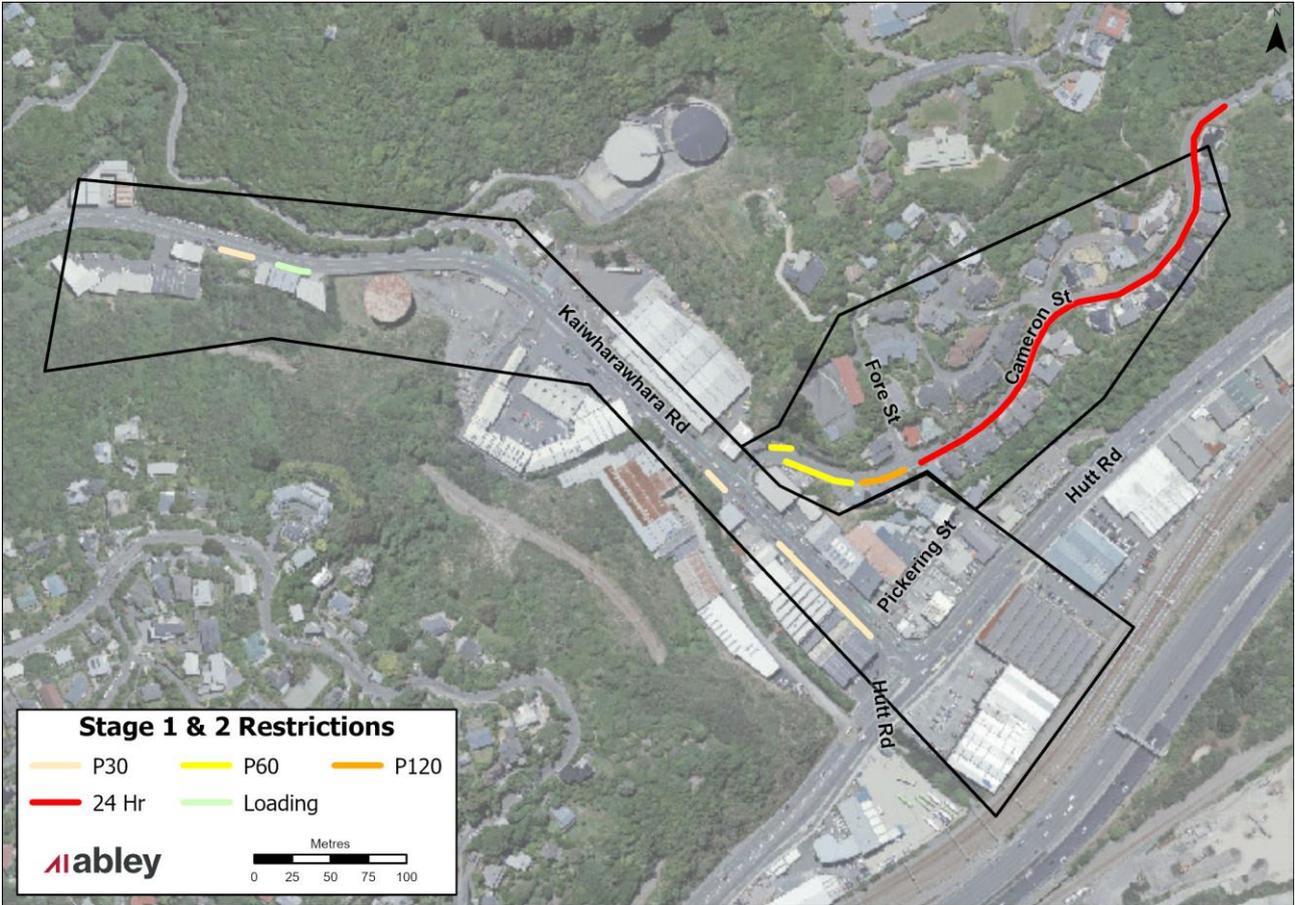


Figure 11 Stage 1 & 2 parking restrictions of parking spaces affected by the cycleway project (Note: Parking spaces and the loading zone on the west side of Kaiwharawhara Road will be subject to a clearway during the afternoon period)

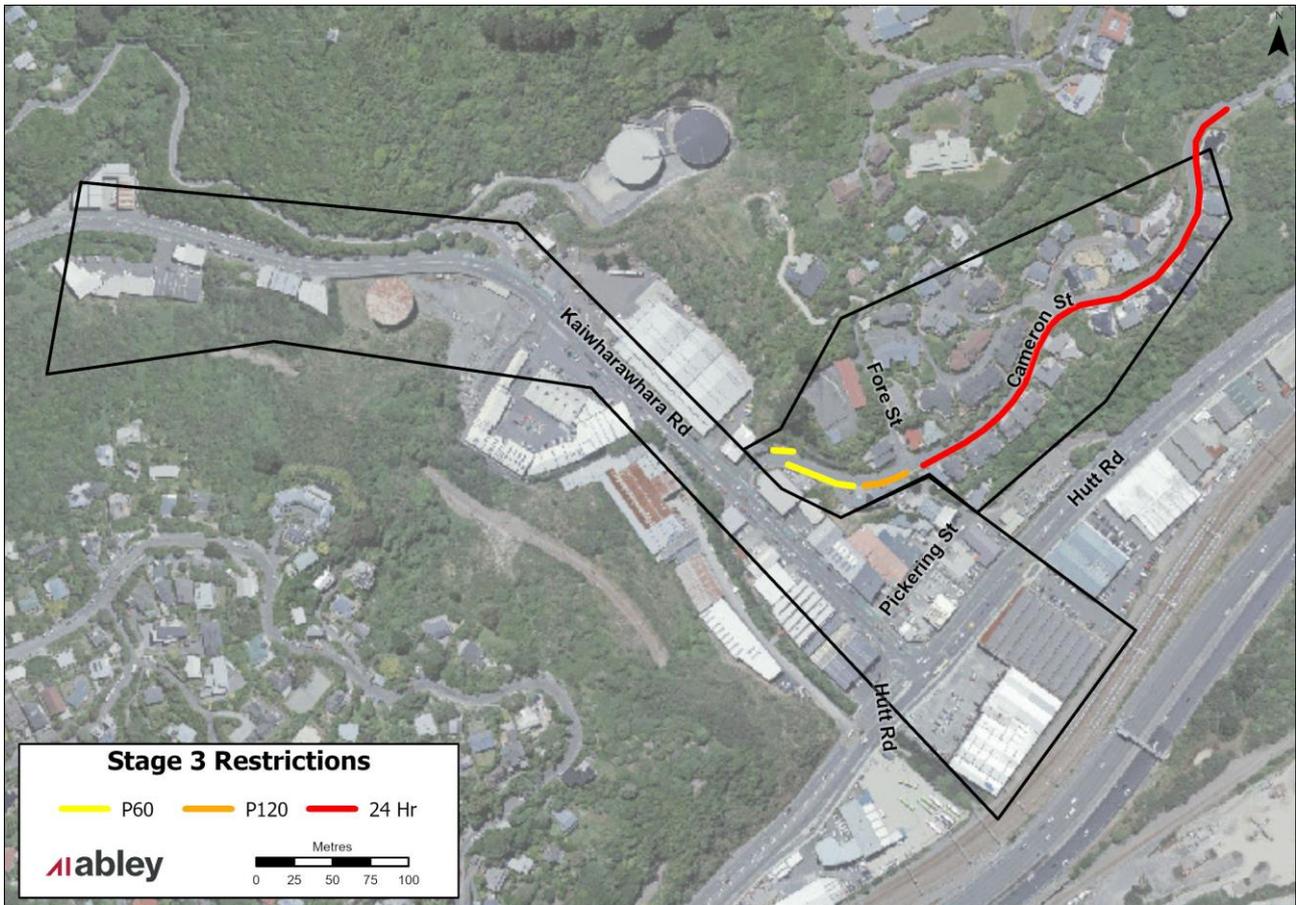


Figure 12 Stage 3 parking restrictions of parking spaces affected by the cycleway project

2.4. Mitigation of parking impacts

There is inadequate capacity to accommodate all of the displaced users from Kaiwharawhara Road. Both the Kaiwharawhara Road and Cameron Street sections are at or above the 85% threshold after the changes due to the cycleway project. Table 10 shows the proposed mitigation for both commuters and short stay users.

Table 10 Proposed Mitigation

Parking Type	Proposed Mitigation	Level of Impact after
Commuter (Lower Priority)	Encourage other modes of travel where possible. Commuter parking is lower priority (See Table 1). No additional commuter spaces are provided.	Very High
Short Stay	Encourage short stay users to park in the P60 and P120 parking on Cameron Street.	Low – this parking is available within a three-minute walk from the removed P30 spaces

It is recommended that WCC engages with residents and businesses in the Kaiwharawhara Road area to detail the changes occurring and where there is excess parking available. On top of this, WCC could provide more cycle parking facilities/storage to encourage the commuters to the businesses in Kaiwharawhara Road to use more active transport modes. This will aid the encouragement already provided from the installation of the cycleway.

3. Cameron Street

3.1. About the area

The Cameron Street section is a primarily residential section which connects the main cycleway route on Kaiwharawhara Road to the Bridle Track (which connects to Khandallah). There were no roadworks occurring in this section during the survey days which would affect the parking survey. The section is shown as Figure 13.

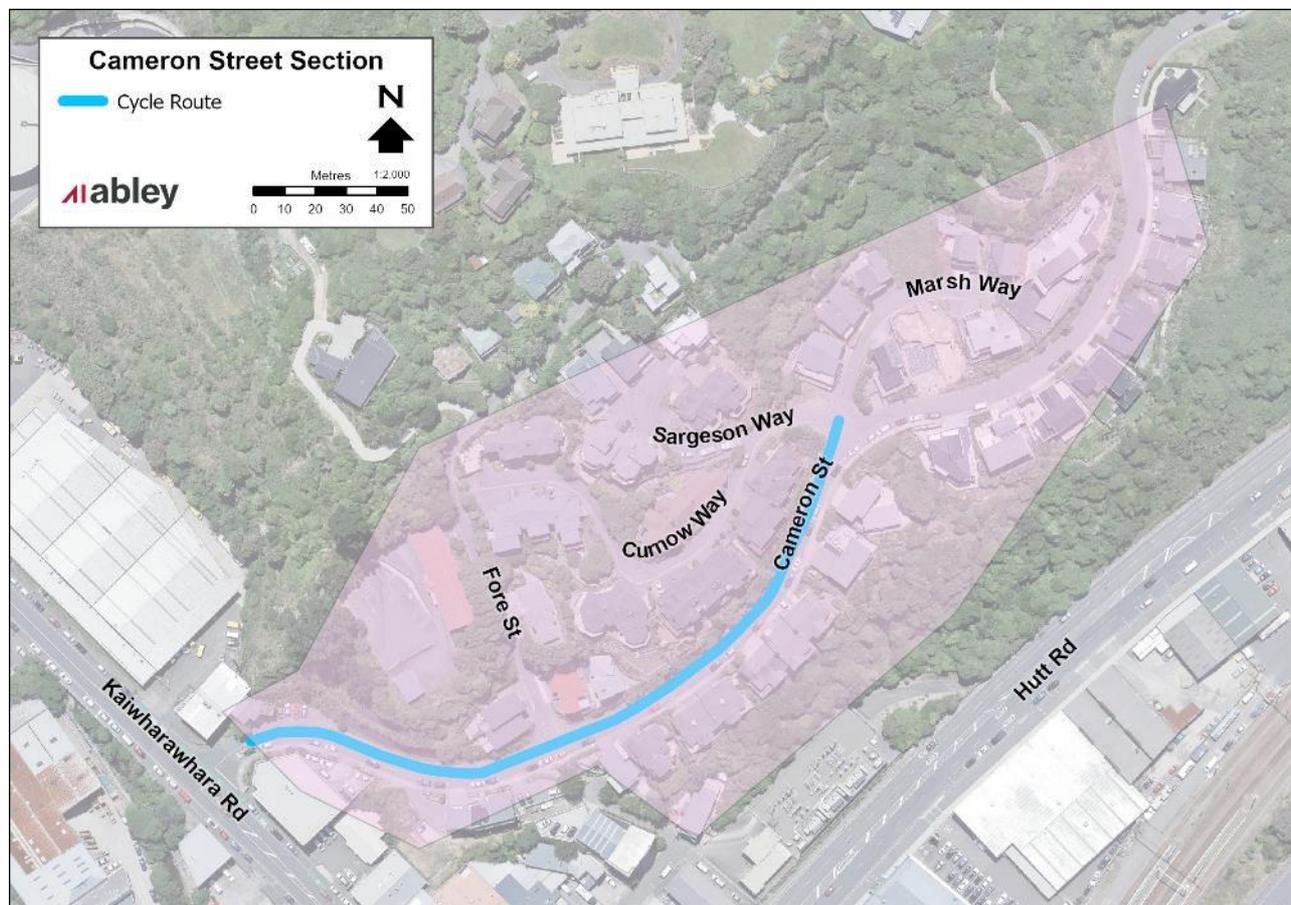


Figure 13 Cameron Street parking study area

3.2. Current parking and usage

All of the parking in this section is unrestricted on-street parking. There is a small section of P60 parking at the southern end of Cameron Street (See Appendix A), however, this is considered to be part of the Kaiwharawhara Road section as part of this analysis. There are 52 spaces on Cameron Street in the parking survey (noting the survey only extended to #54 Cameron Street (see Table 11)). Marsh Way, Cumow Way and Sargeson Way are private roads which are all accessed from Cameron Street. During the surveys it has been observed that there is a small number of vehicles parking on the roadside on these roads. These are a mix of what is assumed to be residential parking and some visitors (tradesperson vans were observed). Given the private nature of these roads, they have not been included in this analysis. Given the narrow width and steep gradient of Fore Street it has been assumed that it is not suitable for on-street parking.

Table 11 Parking inventory for the Cameron Street section

Restriction	Overall section inventory	Cameron Street uphill inventory	Cameron Street downhill inventory
Unrestricted	52	9	43

Figure 14 shows the parking occupancy throughout the day on Cameron Street (which is all the parking in the section). This shows the occupancy on both days. In this section, throughout the day, the occupancy decreases relatively consistently. On the Thursday survey the occupancy starts high (63%) and slowly decreases throughout the day. The occupancy in the entire section is approximately 20% higher from the Thursday survey compared to the Saturday survey. Most of the houses in this area have off-street parking facilities.

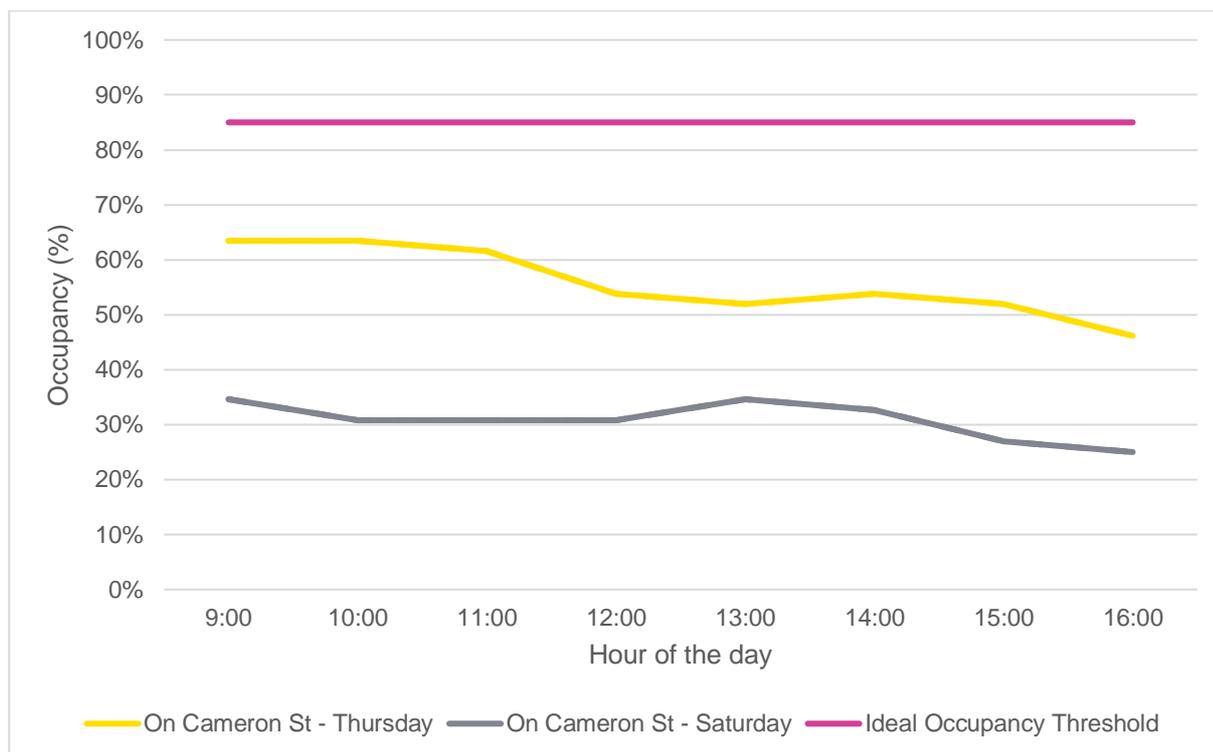


Figure 14 Parking occupancy from both survey days compared to the ideal parking occupancy of 85%

The duration of stay data for this area shows that most users are staying for 4+ hours (52%). This indicates there is a high proportion of residents parking occurring in this area. On top of this, it is expected that some users will be parking and commuting to work (either on Kaiwharawhara Road or in the CBD).

3.2.1. Overnight parking

Overnight, the average parking occupancy was 45% (from both survey days). When compared to the occupancy found during the weekday survey, this is notably lower. It is expected that a significant number of the users on Cameron Street are commuters, which drives the occupancy up during the day. During the weekend, the occupancy throughout the day is similar to the overnight occupancy, which also implies commuter parking is occurring during the week on Cameron Street. Overall, there is a mix of residents and commuters using this parking.

3.3. Impacts of Ngaio Transitional Cycleway on parking

The proposed cycle facility on this section includes an uphill cycle lane and a downhill shared lane. This results in the following changes to the parking on Cameron Street. These are all occurring during the first stage of the Kaiwharawhara Road improvements (see Section 2.3):

- No stopping lines added on Cameron Street opposite the entrance to Fore Street – this results in the removal of one unrestricted parking space.
- Four unrestricted spaces at the Kaiwharawhara Road end of Cameron Street (in the downhill direction) will be changed to two P60 spaces. Two of these spaces are removed to aid with safety at the Cameron Street/ Kaiwharawhara Road intersection. The 7 P60 spaces at the bottom of Cameron Road that have been analysed as part of the Kaiwharawhara Road section will be retained.
- Five unrestricted spaces in the downhill direction on Cameron Street are to be changed to P120 spaces.
- The remaining unrestricted spaces on Cameron Street will remain unrestricted. This includes 42 surveyed spaces. There are more spaces further up Cameron Street, however, these were not surveyed.

Table 12 shows the existing parking inventory and the parking inventory after these changes in the entire Cameron Street section.

Table 12 Cameron Street section inventory before and after upgrades

Restriction	Existing Inventory	Inventory after changes
Unrestricted	52	42
P60	0 (7 P60 are counted in Kaiwharawhara Road section)	2 (7 P60 are counted in Kaiwharawhara Road section)
P120	0	5
Total	52	49

Table 13 shows the current peak inventory and expected peak inventory after the removals of parking on Cameron Street. Both surveyed days are still below the 85% occupancy threshold.

Table 13 Occupancy of the Cameron Street section existing and after the proposed changes

Day	Restriction	Peak from survey	Expected peak after parking removals
Thursday	Unrestricted	63%	79%
Saturday	Unrestricted	35%	44%

There are expected to be vehicles who can no longer park in the Kaiwharawhara Road unrestricted spaces who will search for a car park on Cameron Street. The unrestricted parking being retained on Cameron Street is expected to be used by the same users (residents, commuters) as the unrestricted parking at present. There is enough space in the unrestricted parking on Cameron Street to accommodate approximately 8 additional users displaced from Kaiwharawhara Road before it exceeds the 85% occupancy target (during the Kaiwharawhara

Road peak). Table 14 shows the expected occupancy on the two survey days with the addition of users displaced from Kaiwharawhara Road. These users are expected to be commuters, which are 'lower priority' according to Table 1. Therefore, there are still a number of users from the Kaiwharawhara Road section who will be unable to find a park (See Section 2.3).

Table 14 Occupancy of the Cameron Street section existing and after the proposed changes – including the users transferred over from Kaiwharawhara Road

Day	Restriction	Occupancy during Kaiwharawhara Road peak - existing	Expected peak after parking removals
Thursday	Unrestricted	52%	85%
Saturday	Unrestricted	44%	44%

3.4. Mitigation of parking impacts

There is inadequate capacity to accommodate all the displaced users from Kaiwharawhara Road in the Cameron Street section. Both the Kaiwharawhara Road and Cameron Street sections are at or above the 85% threshold after the changes due to the cycleway. Table 15 shows the proposed mitigation for both residents and commuters. As discussed in Section 3.2, both residents and commuters currently use Cameron Street to park.

Table 15 Proposed Mitigation

Parking Type	Proposed Mitigation	Level of Impact after
Commuter (Lower Priority)	Encourage other modes of travel where possible. Commuter parking is lower priority (See Table 1). No additional commuter spaces are provided.	Very High
Resident (High Priority)	Encourage residents to use off-street parking where available. If it is found that there is significant issue with commuters taking all the available resident parking, a more rigid residents parking restriction should be implemented (e.g. P120 with residents exempt).	Very Low – none of the current residential users in Cameron Street are displaced more than a one-minute walk from the currently available parking spaces after the upgrades.

The time restricted parking is not expected to exceed the 85% occupancy threshold in this section. The removal of time restricted spaces on Kaiwharawhara Road is expected to cause short term users to transfer over into the new P60/P120 spaces at the bottom of Cameron Street. However, these are not expected to exceed the 85% occupancy threshold. When the occupancy begins to exceed 85% it is expected that some users will attempt to park on Fore Street or the private roads off Cameron Street. WCC should actively monitor this and provide no stopping lines where necessary. It is recommended that no stopping lines be added on the western side of Fore Street, as there is not sufficient width for safe vehicle passage when vehicles are parked.

From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ryan Dunn](#); [Ann-Marie Head](#)
Subject: Ngaio Traffic Resolution deliverables
Date: Monday, 10 October 2022 7:58:29 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[ATT00001.jpg](#)
[Schedule of Quantities \(SOQ\) - Draft for Traffic Resolution issue.xlsx](#)
[TR180-22 Ngaio Transitional Cycleway with StepChange comments.docx](#)
[Transitional Cycleways Comment Tracker - Ngaio 90% Design - Sept 2022 with all responses.xlsx](#)
[SCH-TC-NGAIOT-DRG-TR-combined_Optimized.pdf](#)

Hi Jonathan,

Please find attached the following traffic resolution documents;

- Traffic Resolution Drawing issue (optimised). This has both a header and a watermark referring to the TR number, do you have a preference?
- TR report with legal descriptions added. I have also added some track changes and comments to your text in the TR report
- Comments tracker with completed responses
- Draft Schedule of Quantities

The design decisions report is going through final reviews and expected to be issued later today

The Parking Management Plan (PMP) needs to be updated to reflect the final changes from Friday and will follow later this week.

Ngā mihi,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

Tonkin + Taylor - *Exceptional thinking together*

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www.tonkintaylor.co.nz



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<https://transfer.tonkinandtaylorgroup.com/filedrop/brodenburg@tonkintaylor.co.nz>

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Item	Unit	Total length (m)
Road Markings		
100mm White Road Marking - Solid	Length	481
100mm White Road Marking - Dashed	Length	
Audio Tactile Profile (ATP) Markings	Length	
300mm Limit Line	Length	7
600mm Chevrons	Length	4
100mm Yellow Non-passing Line - Solid	Length	163
100mm Yellow Non-stopping Line - Dashed	Length	
Pedestrian Crossing stripes	Length	
Cycle buffer (two edgelines and stripes)	Length	
Flush Median Stripes	Area	
No Stopping Zone (Hatch)	Area	
Green Cycle Lane	Area	
Green Bus Lane	Number	
Cycle Symbols	Number	
Sharrows	Number	
Arrow - Straight	Number	
Arrow - Straight Left/Right	Number	
Arrow - Right/Left	Number	
Dragons Teeth	Number	
Pedestrian crossing diamond	Number	
Give Way	Number	
Speed Marking - Slow	Number	
Speed marking (30km)	Number	
Bus Stop - Marking and lettering	Number	
Loading Zone - Marking and lettering	Number	
Pavement messages	Number	
Civil		
Kerb and Channel	Length	10
Footpath	Area	
Road Surface	Area	
Pedestrian Ramps	Number	
Cycleway Separator with Bollards	Number	
Tactile Paver	Number	
Directional Tactile Indicators	Number	
Raised Pedestrian Crossing	Number	
Speed Hump	Number	
Traffic Island	Number	
Cycle Friendly Sump	Number	
Sump lead	Length	
Manhole	Number	
Manhole Cover	Number	
Bicycle Stand	Number	
Vegetation - Trimmed	Length	
700mm Planting Bed	Number	
Signs		

Parking	Number	
Regulatory, Advisory, Warning	Number	
Wayfinding	Number	
Signals		

Totals	653.4
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Exclusions

				12
				14
				1

	1	• 1370.6	285.7	163
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Based on TR drawings only and subject to change

Comments

Subtracting new 22.6m outside of Kaiwharawhara page extents

Look right at pedestrian refuge

Cameron St - Kaiwharawhara footpath corner build-out

Including x4 outside of page extents

Vanguard interlocking rubber speed hump

20MPa precast concrete islands 170mm thick with three hold down bars evenly spaced around each island, includes x2 removable handrails

Sheltered bicycle stands at 0.8m centres

Including x10 "P30 all other times" associated with clearways and x2 relocated existing parking signs
Including new clearway and RG-17 signs and x2 relocated clearway signs
Kaiwharawhara Bridle Path sign

Item	Unit	Total length (m)
Road Markings		
100mm White Road Marking - Solid	Length	45
100mm White Road Marking - Dashed	Length	
Audio Tactile Profile (ATP) Markings	Length	
300mm Limit Line	Length	
600mm Chevrons	Length	
100mm Yellow Non-passing Line - Solid	Length	
100mm Yellow Non-stopping Line - Dashed	Length	
Pedestrian Crossing stripes	Length	
Cycle buffer (two edgelines and stripes)	Length	
Flush Median Stripes	Area	
No Stopping Zone (Hatch)	Area	
Green Cycle Lane	Area	
Green Bus Lane	Number	
Cycle Symbols	Number	
Sharrows	Number	
Arrow - Straight	Number	
Arrow - Straight Left/Right	Number	
Arrow - Right/Left	Number	
Dragons Teeth	Number	
Pedestrian crossing diamond	Number	
Give Way	Number	
Speed Marking - Slow	Number	
Speed marking (30km)	Number	
Bus Stop - Marking and lettering	Number	
Loading Zone - Marking and lettering	Number	
Pavement messages	Number	
Civil		
Kerb and Channel	Length	
Footpath	Area	
Road Surface	Area	
Pedestrian Ramps	Number	
Cycleway Separator with Bollards	Number	
Tactile Paver	Number	
Directional Tactile Indicators	Number	
Raised Pedestrian Crossing	Number	
Speed Hump	Number	
Traffic Island	Number	
Cycle Friendly Sump	Number	
Sump lead	Length	
Manhole	Number	
Manhole Cover	Number	
Bicycle Stand	Number	
Vegetation - Trimmed	Length	
700mm Planting Bed	Number	
Signs		
Parking	Number	

Regulatory, Advisory, Warning	Number	
Wayfinding	Number	
Signals		

Totals	44.8
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Exclusions

				8
				6

		638.9	23	57
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Based on TR drawings only and subject to change

Comments

Including Green box showing entrance to Bridle Path

*Kerb buildout and the associated tactile pavers and pedestrian ramp included in Kaiwharawhara quantities

Cut back up to 2.2m high to improve visibility

Including "Cameron Street Slow Zone" sign
Including relocating sign at the entrance to the Kaiwharawhara Bridle Path

Item	Unit	Total length (m)
Road Markings		
100mm White Road Marking - Solid	Length	452
100mm White Road Marking - Dashed	Length	
Audio Tactile Profile (ATP) Markings	Length	
300mm Limit Line	Length	
600mm Chevrons	Length	
100mm Yellow Non-passing Line - Solid	Length	743
100mm Yellow Non-stopping Line - Dashed	Length	
Pedestrian Crossing stripes	Length	
Cycle buffer (two edgelines and stripes)	Length	
Flush Median Stripes	Area	
No Stopping Zone (Hatch)	Area	
Green Cycle Lane	Area	
Green Bus Lane	Number	
Cycle Symbols	Number	
Sharrows	Number	
Arrow - Straight	Number	
Arrow - Straight Left/Right	Number	
Arrow - Right/Left	Number	
Dragons Teeth	Number	
Pedestrian crossing diamond	Number	
Give Way	Number	
Speed Marking - Slow	Number	
Speed marking (30km)	Number	
Bus Stop - Marking and lettering	Number	
Loading Zone - Marking and lettering	Number	
Pavement messages	Number	
Civil		
Kerb and Channel	Length	12
Footpath	Area	
Road Surface	Area	
Pedestrian Ramps	Number	
Cycleway Separator with Bollards	Number	
Tactile Paver	Number	
Directional Tactile Indicators	Number	
Raised Pedestrian Crossing	Number	
Speed Hump	Number	
Traffic Island	Number	
Cycle Friendly Sump	Number	
Sump lead	Length	
Manhole	Number	
Manhole Cover	Number	
Bicycle Stand	Number	
700mm Planting Bed	Number	
Signs		

Parking	Number	
Regulatory, Advisory, Warning	Number	
Wayfinding	Number	
Signals		

Totals	1206.37
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Exclusions

				20
				1

		6801	234.8	254
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Based on TR drawings only and subject to change

Comments

58m of removed added from overlap from Kenya Street. 66m of new added from overlap from Kenya street

x2 lines either side of pedestrian crossing

18.2m new added from Kenya Street overlap. *Need to deduct dashed bus stop lines*

Total length of x7 zebra markings

Including traffic median strips and strips located within cycleway separators. 0.7m new added from Kenya Street overlap

1.6m new added from overlap from Kenya street

x2 cycle symbols added from overlap of Kenya street

Trellisick Cres corner kerb removal

Trellisick Cres footpath corner build-out

Trellisick Cres footpath corner pedestrian ramp

Combined area of both speed humps

Connected to existing sump lead

Includes belisha beacon signs and poles

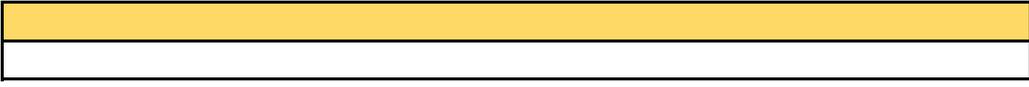
Item	Unit	Total length (m)
Road Markings		
100mm White Road Marking - Solid	Length	52
100mm White Road Marking - Dashed	Length	
Audio Tactile Profile (ATP) Markings	Length	
300mm Limit Line	Length	
600mm Chevrons	Length	
100mm Yellow Non-passing Line - Solid	Length	
100mm Yellow Non-stopping Line - Dashed	Length	
Pedestrian Crossing stripes	Length	
Cycle buffer (two edgelines and stripes)	Length	
Flush Median Stripes	Area	
No Stopping Zone (Hatch)	Area	
Green Cycle Lane	Area	
Green Bus Lane	Number	
Cycle Symbols	Number	
Sharrows	Number	
Arrow - Straight	Number	
Arrow - Straight Left/Right	Number	
Arrow - Right/Left	Number	
Dragons Teeth	Number	
Pedestrian crossing diamond	Number	
Give Way	Number	
Speed Marking - Slow	Number	
Speed marking (30km)	Number	
Bus Stop - Marking and lettering	Number	
Loading Zone - Marking and lettering	Number	
Pavement messages	Number	
Civil		
Kerb and Channel	Length	
Footpath	Area	
Road Surface	Area	
Pedestrian Ramps	Number	
Cycleway Separator with Bollards	Number	
Tactile Paver	Number	
Directional Tactile Indicators	Number	
Raised Pedestrian Crossing	Number	
Speed Hump	Number	
Traffic Island	Number	
Cycle Friendly Sump	Number	
Sump lead	Length	
Manhole	Number	
Manhole Cover	Number	
Bicycle Stand	Number	
700mm Planting Bed	Number	
Signs		
Parking	Number	
Regulatory, Advisory, Warning	Number	
Wayfinding	Number	

Signals		

Totals	52.3
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Exclusions

		933.4	39	75
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Item	Unit	To be removed	
		Total length (m)	Total Area (m ²)
Road Markings			
100mm White Road Marking - Solid	Length	189	
100mm White Road Marking - Dashed	Length	18	
Audio Tactile Profile (ATP) Markings	Length		
300mm Limit Line	Length	1	
600mm Chevrons	Length		
100mm Yellow Non-passing Line - Solid	Length		
100mm Yellow Non-stopping Line - Dashed	Length		
Pedestrian Crossing stripes	Length		
Cycle buffer (two edgelines and stripes)	Length		
Flush Median Stripes	Area		15
No Stopping Zone (Hatch)	Area		
Green Cycle Lane	Area		
Green Bus Lane	Number		
Cycle Symbols	Number		
Sharrows	Number		
Arrow - Straight	Number		
Arrow - Straight Left/Right	Number		
Arrow - Right/Left	Number		
Dragons Teeth	Number		
Pedestrian crossing diamond	Number		
Give Way	Number		
Speed Marking - Slow	Number		
Speed marking (30km)	Number		
Bus Stop - Marking and lettering	Number		
Loading Zone - Marking and lettering	Number		
Pavement messages	Number		
Civil			
Kerb and Channel	Length		
Footpath	Area		
Road Surface	Area		
Pedestrian Ramps	Number		
Cycleway Separator with Bollards	Number		
Tactile Paver	Number		
Directional Tactile Indicators	Number		
Raised Pedestrian Crossing	Number		
Speed Hump	Number		
Traffic Island	Number		
Cycle Friendly Sump	Number		
Sump lead	Length		
Manhole	Number		
Manhole Cover	Number		
Bicycle Stand	Number		
700mm Planting Bed	Number		
Signs			
Parking	Number		2
Regulatory, Advisory, Warning	Number		
Wayfinding	Number		

Signals			

Totals	208	17
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Exclusions

	New		
Total number	Total length (m)	Total Area (m ²)	Total number
	303		
	25		
	7		
	160		
	34		
		22	
			9
			9
		27	
			1
			2
			1
			16
			64
			2
			2
			4
	22		
			1
			26

	551	49	137
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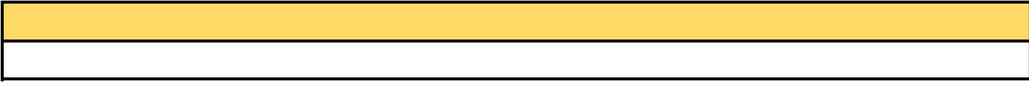
Based on TR drawings only and subject to change

Comments

8 x 3m zebra crossing stripes= 24m. 4 x 2.6m zebra crossing stripes = 10.4m

Third speed marking included in Kenya street quantities

Also including replaced and relocated signs



We are proposing a change in your area

TR180-00 Ngaio Transitional Cycleway.

Kia ora,

This leaflet is to let you know about a change we are proposing to make in your neighbourhood.

Proposal:

Reference	<ul style="list-style-type: none">• TR180-00 Ngaio Transitional Cycleway.• Kaiwharawhara Road, Ngaio Gorge Road, Kenya Street, Crofton Road, Cameron Street.• Cycle lanes and pedestrian crossings
Location – where we propose to make the change	<ul style="list-style-type: none">• Kaiwharawhara Road, Old Porirua Road, Ngaio Gorge Road, Kenya Street, Crofton Road, Cameron Street, Sargeson Way, Marsh Way.• Kaiwharawhara and Ngaio, Wellington.
What we'd like to do	<p>On Kaiwharawhara Road the changes would be made over three stages (2023, 2024, and 2025) to give businesses time to adapt to the removal of parking, as there are currently not enough alternative parking places for the number of cars being driven into the area each day.</p> <p>Kaiwharawhara Road</p> <ul style="list-style-type: none">• Resolute/install a 4-7pm Clearway on the southern side of Kaiwharawhara Road for 145 metres (from Hutt Road to 25 Kaiwharawhara Rd) in 2023 (Stage 1).• Resolute/Install 320 metres of cycle lane from 25 Kaiwharawhara Road to Capital Auto Electrics Ltd in 2023 (Stage 1).• Resolute/Install a loading zone with a Clearway from 4-7pm for 30 metres outside Capital Auto Electrics Ltd and Hanging Around.• Resolute four P30 carparking spaces with a Clearway from 4-7pm on Kaiwharawhara Road, west of the loading zone outside Hanging Around in 2023 (Stage 1).• Resolute a cycle lane (and no stopping lines) in 2023 (Stage 1) from the new P30 carparks on Kaiwharawhara Road along the remainder of the southern side of Kaiwharawhara Road, which ends at the current slip stabilisation work. This will tie in with 480 metres of cycle lane being built as part of the Ngaio Gorge slip stabilisation work (due for completion in Dec 2022).• Extend the Clearway times on the southern side of Kaiwharawhara Road described above (Hutt Road to 25 Kaiwharawhara Road and outside Capital Auto Electrics and Hanging Around businesses) from 4-7pm to 2-9pm in 2024 (Stage 2).

- Resolute the sections of clearway along Kaiwharawhara Road described above as cycle lanes (from Hutt Road to 25 Kaiwharawhara Road and outside Capital Auto Electrics and Hanging Around businesses) in 2025.
- Resolute a 5 metre long no stopping (BYLs) section outside 19 Kaiwharawhara Road where a new pedestrian crossing will be built (2023).
- Resolute a 8.5 metre long no stopping (BYLs) section outside 24 Kaiwharawhara Road where a new pedestrian crossing will be built (2023).
- Extend existing no stopping hatching on Kaiwharawhara Road at Pickering Street to improve sightlines (2023).

Old Porirua Road

- Resolute no stopping (BYLs) for 8 metres back from the limit line where Old Porirua Road meets Kaiwharawhara Road (2023).

Ngaio Gorge Road (all 2023)

- Install an uphill cycle lane (and no stopping lines) from Kaiwharawhara Road to Kenya Street.
- Paint cycle sharrows at regular intervals down Ngaio Gorge Road.
- Install raised pedestrian crossing between 5 and 7 Ngaio Gorge Road.

Kenya Street and Crofton Road (all 2023)

- Paint cycle sharrows at regular intervals heading towards Ngaio on all of Kenya Street and from the end of Kenya St down Crofton Road to the roundabout at Waikowhai Street.
- Install a cycle lane in the uphill direction, from Waikowhai Street roundabout up Crofton Road to Kenya Street and Kenya Street (heading towards the city).
- Resolute no stopping (BYLs) for 36 metres on the bend outside 54, 56, and 58 Kenya Street – up to the existing bus stop (this involves removing 4 carparks).
- Resolute no stopping in place of an existing P30 carpark outside 2 Kenya Street (Takeaways shop)
- Resolute one P10 carpark outside 6 Kenya Street.
- Extend the Ngaio Village 30 kph Zone from 11 Ottowa Road to 6 Kenya Street.

Cameron Street (all 2023)

- Resolute an uphill 'cycle shoulder' on Cameron St from Kaiwharawhara Road to Curnow Way. This 'cycle shoulder' will be a trial in partnership with Waka Kotahi.
- Install cycle sharrows down Cameron Street from Curnow Way to Kaiwharawhara Road.
- Resolute to reduce the speed limit from 50kph to 30kph on Cameron Street
- Resolute six unrestricted carparks outside 6 Cameron St to six P60s.

We are proposing a change in your area

	<ul style="list-style-type: none"> • Resolute six unrestricted carparks outside 8 – 14 Camerson St to five P120s and one no stopping section six metres long at the existing kerb ramp in front of 10 Cameron St (opposite Fore St). • Resolute 39 unrestricted carparks from 16 – 60 Cameron St (eastern side) to 39 P24hr Except Residents. • Resolute no stopping (BYLs) around the corner of Cameron St into Sargeson Way and around Sargeson Way into Marsh Way and around Marsh Way into Cameron St. • Resolute no stopping (BYLs) on both sides of the narrow section of Cameron St from 60 Cameron Street to Brasch Way. • Resolute to change four angle parks at the bottom of Cameron Street (west side) to two parallel parks.
<p>Why we are proposing the change</p>	<ul style="list-style-type: none"> • This primary bike route is part of the Wellington Bike Network, which was approved by council in March 2022. • This route will support the strategic goals outlined in Te Atakura (by reducing transport emissions) and the 2020 Parking Policy (by reducing parking on arterial routes). • It leads from the Hutt Road Cycleway to the suburbs of Ngaio, Crofton Downs and Khandallah. • The change will make it safer for more people to bike/scoot these suburbs and Wellington City. • It takes approximately 20 minutes to ride into the CBD from Ngaio, and 25-30 minutes from the CBD to Ngaio. • The route also includes improvements for pedestrians to cross Kaiwharawhara Rd, Ngaio Gorge Road and Crofton Road. • Kaiwharawhara Road will have a new median refuge crossing near Cameron St. • There will be a new raised pedestrian crossing installed at the top of Ngaio Gorge Road near three bus stops. • The existing pedestrian crossing at the bus stop on Crofton Road will be raised. • The changes on Kaiwharawhara Road will be made over three stages – 2023, 2024, and 2025 – to give businesses time to adjust to the removal of carparks on one side of the road. • These changes have been developed through the business case process and have involved initial engagement with residents and businesses along the route.
<p>Impact</p>	<ul style="list-style-type: none"> • Net parking impact <ul style="list-style-type: none"> - removal of 1 P30 parking spaces on Kaiwharawhara Rd - removal of 43 unrestricted parking spaces on Kaiwharawhara Rd - removal of 1 P30 parking space on Kenya St - installation of 1 P10 parking space on Kenya St - removal of 59 unrestricted parking spaces on Kenya St, taking maximum occupancy from a current peak of 41% to a new peak of 87%. - removal of 16 unrestricted parking spaces on Crofton Rd (assessed low impact).

	<ul style="list-style-type: none"> - removal of 2 P60 car parks on Cameron St - change 4 unrestricted car parks to 4 P60s on Cameron St - change 7 unrestricted car parks to 6 P120s on Cameron St - change 39 unrestricted carp parks to 39 P24hr car parks on Cameron St - Note: Cameron St is not expected to exceed 85% occupancy - removal of one unrestricted car park at the bottom of Old Porirua Road. <ul style="list-style-type: none"> • The impact on people driving along this arterial route will be improved traffic efficiency and safety. • Pedestrian impact is positive: The impact on pedestrians will be improved safety due to two new crossings and one existing crossing be upgraded. • Business impact will vary depending on where they are: <ul style="list-style-type: none"> - On Crofton Road the impact will be low/none as parking will be retained directly outside the businesses. However, Motor Doctors have been using the street to park their customers vehicles (10-20/day) and they will need to park them either on their car yard or on a nearby side street (most likely the upper part of Crofton Road). If Motor Doctors chose to park outside the Church on Crofton Road this would impact on parking availability for Church visitors. The Church has limited off-street parking. - Kaiwharawhara Road businesses in the uphill (Ngaio bound) direction will have their parking removed between 2023 and 2025. They will need park on the opposite side of the road (although this is a clearway from 7-9am) or optimise their off-road parking and/or use alternative parking space in side streets. • The impact on public transport will be improved access to bus stops and improved efficiency for buses, which currently have difficulty on narrow parts of Kenya St when cars are parked on both sides of the street leaving less than 6 metre lane width. • Impact on parking revenue will be none. • With bike trips made safer and more attractive, this project will provide improved transport options between Ngaio and the City.
<p>How this relates to the parking policy</p>	<ul style="list-style-type: none"> • Supports mode shift to using active modes (e.g. walking and cycling) and public transport through parking management, to move more people driving fewer vehicles. • Supports safe movement – facilitates the safe and efficient movement of people and goods by focusing on people moving along transport corridors rather than people parking or storing stationary vehicles. • A parking management plan was undertaken to inform the design of these changes and feedback from the community has subsequently been incorporated into the proposed parking changes • As outlined in the Parking Policy 2020, short term and residents parking has been prioritised over commuter parking which is the lowest priority for road space allocation.

We are proposing a change in your area

<p>Additional Information</p>	<ul style="list-style-type: none"> • This Ngaio to City project is a key initiative to deliver on several strategic outcomes for the city, including the Spatial Plan, Te Atakura and Paneke Pōneke plan for a citywide network of connected bike/scooter routes. • The improvements are being made using lower cost, adaptable materials that can be tweaked based on public feedback, and data, once these initial installations are in place. • Designs were developed by technical experts and with input from a technical working group to ensure the project delivers on its objectives without compromising road safety, within a constrained road corridor. • Average monthly passenger number - (number – see Wendy if unsure) • Average daily traffic count – 12,400 AADT on Kaiwharawhara, 9,800 AADT on Ngaio Gorge Road, 8,000 AADT on Kenya St, 690 AADT on Cameron St. • Annual parking revenue impact - none • To view the legal description for this Traffic Resolution, an electronic copy of the report will be available on the Council’s website from 9.00am Monday 31 October 2022 at https://www.letstalk.wellington.govt.nz/ or you can call (04) 499 4444 and we will send one out to you.
<p>Feedback</p>	<ul style="list-style-type: none"> • If you would like to provide us with specific feedback, which will be added to the Traffic Resolution following consultation and made public in full, you can do so by filling out an online submission form, downloading a printable submission form on https://www.letstalk.wellington.govt.nz/ or emailing us at trfeedback@wcc.govt.nz. • Please note if you are giving feedback the consultation period opens at 9.00 am Monday 31 October 2022 and finishes at 5.00 pm Sunday 21 November 2022. • What we do with your personal information: All submissions (including your name, but not contact details) are provided in their entirety to elected members and made available to the public on our website and at our office. Personal information (including contact details) will also be used for the administration of the consultation process including informing you of the outcome of the consultation. All information collected will be held by Wellington City Council, 113 The Terrace, Wellington, with submitters having the right to access and correct personal information.
<p>Next Steps</p>	<ol style="list-style-type: none"> 1. Feedback collated by Monday 8 November 2022. 2. The proposal will go to the Regulatory Processes Committee on Wednesday 7 December 2022. 3. If approved, the proposal will be installed within the following three years.

Traffic Resolution Plan: **TR00-20 Street name and suburb – Reference eg: No Stopping At All Times**

Plans

We are proposing a change in your area

- Auto Cad
- North Point on top right and visible – Can move if can't see it.
- Font and speech bubble – (Proposed X metres of broken yellow line)
- Centre the change – Show a side street to give relevance
- Show the house numbers
- Wellington Logo current
- Scale - 1:100/ 1:150 or even amount
- Measurements to the 1st decimal.
- Map title on bottom – to match document title and bold section written above plan;
TRXX-XX Street Name, Suburb- proposed changes.

Traffic Resolution plan guidelines;

- All lines must be to scale for proposed and existing lines 1metre on and one metre off
- All lines proposed to be removed must be 50cm on and 50cm off so we can delineate between proposed, existing and removed lines.

Broken yellow lines colours

- Yellow- proposed broken yellow lines
- Blue- road markings to be removed
- Red- existing broken yellow lines
-

White road markings

- White- proposed new white road markings
- Blue- road markings to be removed
- Red- existing white road markings

Parking and other

- Black lettering or black hatching box- Proposed new parking restrictions
- Black- Indication of new pole or sign
- Red- Restrictions that are retained
- Red- existing pole
- Yellow- Existing taxi stand

Lettering colour guidelines

- Speech bubbles of proposed changes- Black lettering with white background
- Name of business- Blue lettering with white background, first letter capitalised
- House numbers- Red with white background

We are proposing a change in your area

- Street name- Blue capital letters

Further details;

- Zack to written in as draft checker
- Soon to be written in as approved Project Director
- Bottom right of plan title and description should match title of Traffic resolution document and bold section of TR above plan as follows-
TRXX-XX Street Name, Suburb- proposed changes.

We are proposing a change in your area

Legal Description:

Delete from Schedule A (Time limited) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	P30	South side, commencing 98 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 5.5 metres.
Kaiwharawhara Road	P30	South side, commencing 108 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 5.5 metres.
Kaiwharawhara Road	P30, Monday - Sunday, 9:00am - 6:00pm	North side, commencing 6 metres from its intersection with Cameron Street and extending in an easterly direction following the northern kerbline for 32 metres.
Cameron Street	P60	North side, commencing 30 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates x= 1749805.8 m, y= 5430949.9 m), and extending in a south-easterly direction following the northern kerbline for 19 metres. (4 angle and 1 parallel parking space)
Kenya Street	P30	East side, commencing 16 metres south of its intersection with Crofton Rd and extending in a southerly direction for 7 metres

Add to Schedule A (Time limited) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 23m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 8 metres.

We are proposing a change in your area

Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 46 metres.
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 133.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 15.5 metres.
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 531 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 23.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 23m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 8 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 46 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 133.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 15.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 531 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 23.5 metres.
Kaiwharawhara Road	P30, Monday - Sunday, 9:00am - 6:00pm	East side, commencing 6 metres southeast of its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in a southeasterly direction following the eastern kerbline for 11 metres.

We are proposing a change in your area

Kaiwharawhara Road	<i>P30, Monday - Sunday, 9:00am - 6:00pm</i>	<i>East side, commencing 25.5 metres southeast of its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in a southeasterly direction following the eastern kerbline for 15 metres.</i>
Cameron Street	<i>P60</i>	<i>South side, commencing 31 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 48 metres.</i>
Cameron Street	<i>P120</i>	<i>South side, commencing 84 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 36 metres.</i>
Cameron Street	<i>P24hr</i>	<i>South side, commencing 144 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 367 metres.</i>
Cameron Street	<i>P24hr</i>	<i>North Side, commencing 75m east of its intersection with Marsh Way (Grid coordinates x=1750015.56 y=5431061.32) and extending in an easterly direction following the northern kerb line for 146 metres.</i>
Cameron Street	<i>P60</i>	<i>North side, commencing 30 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates x= 1749805.8 m, y= 5430949.9 m), and extending in a south-easterly direction following the northern kerbline for 19 metres.</i>
Kenya Street	<i>P10</i>	<i>North side, commencing 38.5 metres southwest of its intersection with Crofton Road (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a southwesterly direction following the northern kerbline for 6 metres.</i>

We are proposing a change in your area

Delete from Schedule B (Class restricted) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	Bus Stop – At All Times	South side 296.5m from its intersection with Hutt Road and extending in a westerly direction for 12m
Kenya Street	Bus Stop – At All Times	East side, commencing 190 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 24.5 metres.
Kenya Street	Bus Stop – At All Times	West side, commencing 148.5 metres north of its intersection with Trelissick Cres and extending in a northerly direction following the western kerbline for 14.5 metres.

Add to Schedule B (Class restricted) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	Bus Stop – At All Times	West side, commencing 50 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 11 metres.
Kaiwharawhara Road	Bus Stop – At All Times	West side, commencing 289 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 11.5 metres.
Kaiwharawhara Road (Stage 1, 2023)	Loading Zone, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 488.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 21.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	Loading Zone, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 488.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 21.5 metres.

We are proposing a change in your area

Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 134.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 15 metres.</i>
Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 364 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 13 metres.</i>
Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 14 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 15 metres.</i>
Kenya Street	<i>Bus Stop – At All Times</i>	<i>North side, commencing 143.5 metres north of its intersection with Trelissick Crescent (Grid Coordinates X = 1748383.625m, Y = 5431327.364m) and extending in a northerly direction following the northern kerbline for 15 metres.</i>
Kenya Street	<i>Bus Stop – At All Times</i>	<i>South side, commencing 178.5 metres north of its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 14.5 metres.</i>

We are proposing a change in your area

Add to Schedule C (Direction, Placement and Lane Use) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.

We are proposing a change in your area

Delete from Schedule D (No Stopping) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	No Stopping – At All Times	West side, commencing 43.5 metres north of its intersection with Hutt Road (Grid coordinates x= 1,749,909.8 m, y= 5,430,820.5m), and extending in a northerly direction following the western kerb line for 7.5 metres.
Kaiwharawhara Road	No Stopping – At All Times	South side, commencing at a point 580 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 154 metres.
Kaiwharawhara Road	No Stopping – At All Times	West side, commencing 149.5 metres north of its intersection with Hutt Road (Grid coordinates x= 1,749,913.1m, y= 5,430,822.8 m), and extending in a northerly direction following the western kerb line for 8.5 metres."
Kaiwharawhara Road	No Stopping – At All Times	South side 308.5m from its intersection with Hutt Road and extending in a westerly direction for 105.5m
Cameron Street	No Stopping – At All Times	Southeast side, commencing from its intersection with Kaiwharawhara Road (Grid Coordinates X=1,749,813.33m Y=5,430,941.57m) and extending in a north-easterly direction, following the southeastern kerb line for 18 metres
Cameron Street	No Stopping – At All Times	North Side, commencing 43.67 metres east of its intersection with Marsh Way (Grid coordinates x=1750011.04 y=5431057.11) and extending in an easterly direction following the northern kerb line for 31 metres.
Ngaio Gorge Road	No Stopping – At All Times	Northwest side, commencing at its intersection with Ngaio Gorge Road and extending in a southwesterly direction following the northwestern kerbline for 24.5 metres.
Ngaio Gorge Road	No Stopping – At All Times	Southeast side, commencing at its intersection with Ngaio Gorge Road and extending in a southwesterly direction following the southeastern kerbline for 24.5 metres.

We are proposing a change in your area

Kenya Street	<i>No Stopping – At All Times</i>	<i>West side, commencing from its intersection with Crofton Road and extending in a southerly direction following the western kerbline for 26 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing from its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 33.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing 97.5 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 9.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing 127.5 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 24 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>Northeast side, commencing 6 metres from its intersection with Kenya Street and extending in a northwesterly direction following the northeastern kerbline for 14 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Kenya Street and extending in a northerly direction following the western kerbline for 3 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>South side, commencing 5.5 metres west of its intersection with Kenya Street and extending in a westerly direction following the western kerbline for 12 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Abbott St and extending in an westerly direction following the northern kerbline for 7 metres.</i>

Add to Schedule D (No Stopping) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction</i>

We are proposing a change in your area

		following the western kerbline for 23 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 31m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 19 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 107 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 26.5 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 149 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 140 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 187.5 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 554.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 146.5 metres.
Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.

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Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Kaiwharawhara Road	No Stopping – At All Times	East side, commencing at its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in an southeasterly direction following the eastern kerbline for 6 metres.
Kaiwharawhara Road	No Stopping – At All Times	East side, commencing 17 metres southeast of its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in a southeasterly direction following the eastern kerbline for 8.5 metres.
Cameron Street	No Stopping – At All Times	South side, commencing 130 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in an easterly direction following the southern kerbline for 6 metres.
Cameron Street	No Stopping – At All Times	South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in an easterly direction following the southern kerbline for 31 metres.
Cameron Street	No Stopping – At All Times	North side, commencing at its intersection with Marsh Way (Grid coordinates x=1750011.04 y=5431057.11) and extending in an easterly direction following the northern kerb line for 75 metres.
Marsh Way	No Stopping – At All Times	South side, commencing at its intersection with Marsh Way (Grid coordinates x=1750015.56 y=5431061.32) and extending in an easterly direction following the southern kerb line for 8 metres.
Marsh Way	No Stopping – At All Times	North side, commencing at its intersection with Sargeson Way (Grid coordinates x=1750008.14 y=5431066.07) and extending in an easterly direction following the northern kerb line for 12 metres.

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Sargeson Way	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Marsh Way (Grid coordinates x=1750011.04 y=5431057.11) and extending in a westerly direction following the southern kerb line for 27 metres.</i>
Sargeson Way	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Marsh Way (Grid coordinates x=1750008.14 y=5431066.07) and extending in a westerly direction following the northern kerb line for 10 metres.</i>
Old Porirua Road	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749650.973m, Y = 5431089.75m and extending in a westerly direction following the northern kerbline for 8 metres.</i>
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 133.5 metres.</i>
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	<i>South side, commencing 149.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 214 metres.</i>
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Trelissick Crescent (Grid Coordinates X = 1748458.814m, Y = 5431272.233m) and extending in an easterly direction following the southern kerbline for 432 metres.</i>
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	<i>South side, commencing 29 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 36 metres.</i>
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Kenya Street (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in an easterly direction following the northern kerbline for 83 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>North side, commencing 107.5 metres north of its intersection with Trelissick Crescent (Grid Coordinates X = 1748383.625m, Y = 5431327.364m) and</i>

We are proposing a change in your area

		<i>extending in a northerly direction following the northern kerbline for 36 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Crofton Road (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a westerly direction following the northern kerbline for 38 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 178.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Crofton Road (Grid Coordinates X = 1748587.606m, Y = 5431757.751m) and extending in a southwesterly direction following the southern kerbline for 300 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Kenya Street (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a northerly direction following the western kerbline for 26 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>East side, commencing at its intersection with Ottawa Road (Grid Coordinates X = 1748563.806m, Y = 5431918.344m) and extending in a southeasterly direction following the eastern kerbline for 115 metres.</i>

We are proposing a change in your area

Add to Schedule G (Give Way and Stop) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Marsh Way	<i>Give Way</i>	<i>West bound, at its intersection with Sargeson Way.</i>
Marsh Way	<i>Give Way</i>	<i>South bound, at its intersection with Cameron Street.</i>
Fore Street	<i>Give Way</i>	<i>South bound, at its intersection with Cameron Street.</i>

We are proposing a change in your area

Add to Schedule H (Pedestrian crossing) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
<i>Ngaio Gorge Road</i>	<i>Pedestrian Crossing</i>	<i>South side, commencing 7.5 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 3.5 metres.</i>

We are proposing a change in your area

Add to Schedule I (Cycle lane) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 133.5 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing 149.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 214 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing at its intersection with Trelissick Crescent (Grid Coordinates X = 1748458.814m, Y = 5431272.233m) and extending in an easterly direction following the southern kerbline for 432 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing 29 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 36 metres.

We are proposing a change in your area

Ngaio Gorge Road	Cycle Lane	North side, commencing at its intersection with Kenya Street (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in an easterly direction following the northern kerbline for 26 metres.
Kenya Street	Cycle Lane	South side, commencing at its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 178.5 metres.
Kenya Street	Cycle Lane	South side, commencing 27.5 metres southwest of its intersection with Crofton Road (Grid Coordinates X = 1748587.606m, Y = 5431757.751m) and extending in a southwesterly direction following the southern kerbline for 272.5 metres.
Crofton Road	Cycle Lane	East side, commencing at its intersection with Ottawa Road (Grid Coordinates X = 1748563.806m, Y = 5431918.344m) and extending in a southeasterly direction following the eastern kerbline for 115 metres.

Prepared By: ? (?)

Approved By: Zackary Moodie (Team Leader Transport Engineering)

Date:

1 - Minor = To be fixed for final version. Does not impact validity of document (e.g. typo, grammar, format)
 2 - Moderate = To be resolved during next stage of development. Not required for support/endorsement/approval.
 3 - Critical = Resolution or approach to it must be agreed (not necessarily implemented) prior to support/endorsement/approval.

Organisation	Comment From (Name)	Document	Location (Cl / Para / Ref)	Comment	Comment Category (REFER GUIDE)	Response/comments -WCC/Consortium	Consortium comments	Action	Action status	Column13	Column14	Column15	Column16
4 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 1	Are the No Stopping restrictions Mon- Fri or everyday?		Every day.	As per WCC response, no action required						
5 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 2	While I understand this is a transitional cycleway, the design does not come close to fitting in with the safe system approach. (We still have bikes sharing with traffic downhill at 50km/h which is unsafe. The downhill bike riders share a 4.2m downhill lane with parked cars offpeak. The bike riders will be travelling at high speed and are at risk of dooring and poor visibility from driveways and side roads. (Safety is a requirement for mass cycling uplift and we are not targeting the areas most likely to be risk for people on bikes. Are we bold enough to remove parking from both sides of the road? At the very minimum we should try and address the driveway and side road safety issue. Can the parking be removed from both sides from Hutt Road to Old Porirua Road?	2 - Moderate	Carparking will be removed from both sides from 7am to 9am, as the bus lane is a clearway. Then parking will be available on the downhill side only for the rest of the day. But we are widening the bus/bike lane to 4.2m so that we virtually eliminate the risk of being doored.	As per WCC response, noting that the widening occurs in Stage 3. Refer also to Safety Audit Finding 2.2. No action required						
6 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 3	How easy is it to get from the Hutt Road cycleway into the uphill protected lane?	2 - Moderate	The detailed design will ensure it is easy.	Comment included in the design decisions report describing the cycle signals						
7 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 1	Can the Council commit to a date when STAGE 2 and STAGE 3 will take effect or is the plan to reconsult with a future traffic resolution.	1 - Minor	We will have one TR, and commit to dates if needed.	As per WCC response, no action required.						
8 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Large Signs	Check to see if large signs will effect the wind loading when installed on existing utility poles	1 - Minor	Noted and agree.	No large signs proposed for this project. Note to check future projects included in design decisions report.						
9 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 4	Where there is only a white painted buffer, a yellow no stopping line should be painted next the kerb. (This is consistent with NZTA TN004 Buffered cycleway design. (See figure 3 in the guideline).	2 - Moderate	Noted and agree.	This is generally applied, and we have doubled checked to confirm. We note that for Sheet 4 there are existing no-stopping lines for this section so new markings aren't required						
10 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Sheet 5	Put the words LOADING ZONE in the yellow loading zone box consistent with MOTSAM.	1 - Minor	Noted and agree.	Updated in TR design						
11 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Cameron Street	The cycle symbols every 10m is unnecessary	1 - Minor	Noted. Discuss with designer changing to every 20 metres and check RSA and Waka Kotahi advice.	As per WCC response, updated in TR design						
12 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	37 Kenya Street and other BUS STOPS	Put the words BUS STOP in the yellow box consistent with MOTSAM.	1 - Minor	Noted and Agree.	WCC to confirm, this isn't typically provided around Wellington and not a legal requirement. It will add additional ongoing maintenance of the markings	Add BUS STOP markings to make bus stops more prominent.	Complete				
13 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Abbott / Crofton Intersection	The raised crossing is located on a curve. Vehicles hitting the ramp at speed may lose control. It is recommended to attempt to install the ramps perpendicular to the direction of travel.	1 - Minor	Noted and agreed.	This location aligns with the existing crossing, matches desires lines and achieves sight distance. No change proposed	Stay with this location but V-shape the platform.	Complete				
15 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	67 Ngaio George Road	Green cycle marking in BUS STOP Box which should be amended.	1 - Minor	Noted and agreed.	Updated on TR plans						
18 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	7 Ngaio George Road	Check visibility for the zebra crossing meets the standards. The zebra crossing must have lighting in accordance with the standard. Install zebra crossing lighting. Check that all crossings have directional tactile pavers.	2 - Moderate	Noted and agreed.	Refer also comment 136. If the distance between the warning tactiles and the back of the footpath is less than 1m then Waka Kotahi Tactile indicator installation note does not require directional pavers. The visibility is suitable for approach speeds of 25 km/hr, refer design decisions report. There is an existing streetlight, comment added to drawings to check and upgrade luminaire if required during installation						
19 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	3 & 20 Ngaio George Road	Traffic will likely swerve into cycle lane rather than go over the speed cushions.	1 - Minor	Seek advice on this matter.	Refer to Safety Audit Finding 2.14 Feedback also recommended that the speed cushions are changed to speed humps. Providing speed humps across the full width of the carriageway will address the CAT finding regarding vehicles trying to skip around the speed cushions.	Updated on TR plans					
20 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Abbott / Crofton Intersection	Additional green markings should be added across the end of the side roads.	1 - Minor	Noted and agreed.	Updated on TR plans						
22 WCC	Daniel Cairncross	Ngaio Transitional cycleway 90% design	Speed cushions	Road markings should be added to cushions consistent with TCD manual.	1 - Minor	Noted and agreed.	Updated on TR plans						
25 wcc	Paige Boyd	Ngaio Transitional cycleway 90% design	Sheet 8	Add a strip of cyclalane vertical protection adjacent to the speed cushions to prevent cars swerving into the cyclalane.		Noted and agreed.	Refer to Safety Audit Finding 2.14 Feedback also recommended that the speed cushions are changed to speed humps. Providing speed humps across the full width of the carriageway will address the CAT finding regarding vehicles trying to skip around the speed cushions.	Updated on TR plans					
26 wcc	Paige Boyd	Ngaio Transitional cycleway 90% design	Sheet 8	Any chance of widening the footpath on either side of the pedestrian crossing? The footpath is very narrow which would prevent/reduce movement past someone waiting to cross, especially wheelchairs, prams, etc. This could be widened by extending further back from the kerb.		Not at this stage (out of scope of transitional approach but can be done when transformation occurs in 5 years).	As per WCC response, no action required						
30 wcc	Paige Boyd	Ngaio Transitional cycleway 90% design	Sheet 8	Does the cyclalane also need a give way line for the pedestrian crossing? Extend the Give-way line further out to fall over cyclalane.		Discuss with designer	Updated on TR plans						
32	Paige Boyd	Ngaio Transitional cycleway 90% design	Sheet 8	A small raised median on either side of the Pedestrian crossing could draw attention to the crossing and ensure vehicles move through carefully. Also would be a location to install an additional Black and White pedestrian crossing pole.		May not be space for this. Ask designer to consider.	There isn't enough width to provide this, and it would also impact vehicles turning out of Perth Street and Trellisick Crescent. Comment added in design decisions report						
35	Paige Boyd	Ngaio Transitional cycleway 90% design		Why are no placemaking locations shown?		These are in a separate report. Ask designer to add them to these plans.	WCC to provide report	Refer to Design Decisions report to see list, four locations in total.	Updated on TR plans				

Cover Page - LGWM Document Review

Package/Workstream Lead to Complete

Workstream: Ngaio Gorge
Review Title: Working group and Project Team review and comment on Transitional cycleways

Release of Documents on: #####
Response Required: Fri, 10-Jun-22 Response Received: {Rec'd. Date}
Distribution Medium: Email
Purpose of Review: Comment and support

Ref	Document Title	Filename	Version No.	Doc. Provided On	Notes
1	Ngaio MCA & Options overview			1	30-May-22

TRANSITIONAL CYCLEWAYS



NGAIO TRANSITIONAL TRAFFIC RESOLUTION DESIGN

07/10/2022

TRANSITIONAL CYCLEWAY
NGAIO TRANSITIONAL
TRAFFIC RESOLUTION ISSUE

DRAWING

GENERAL

- SCH-TC-NGAIO-DRG-TR-902000
- SCH-TC-NGAIO-DRG-TR-902010
- SCH-TC-NGAIO-DRG-TR-902011

KAIWHARAWHARA ROAD

- SCH-TC-NGAIO-DRG-TR-902100
- SCH-TC-NGAIO-DRG-TR-902110
- SCH-TC-NGAIO-DRG-TR-902111
- SCH-TC-NGAIO-DRG-TR-902112
- SCH-TC-NGAIO-DRG-TR-902113
- SCH-TC-NGAIO-DRG-TR-902114

CAMERON STREET

- SCH-TC-NGAIO-DRG-TR-902200
- SCH-TC-NGAIO-DRG-TR-902210
- SCH-TC-NGAIO-DRG-TR-902211

NGAIO GORGE ROAD

- SCH-TC-NGAIO-DRG-TR-902300
- SCH-TC-NGAIO-DRG-TR-902310
- SCH-TC-NGAIO-DRG-TR-902311
- SCH-TC-NGAIO-DRG-TR-902312
- SCH-TC-NGAIO-DRG-TR-902313
- SCH-TC-NGAIO-DRG-TR-902314
- SCH-TC-NGAIO-DRG-TR-902315
- SCH-TC-NGAIO-DRG-TR-902316
- SCH-TC-NGAIO-DRG-TR-902317

KENYA STREET

- SCH-TC-NGAIO-DRG-TR-902400
- SCH-TC-NGAIO-DRG-TR-902410
- SCH-TC-NGAIO-DRG-TR-902411
- SCH-TC-NGAIO-DRG-TR-902412

CROFTON ROAD

- SCH-TC-NGAIO-DRG-TR-902500
- SCH-TC-NGAIO-DRG-TR-902510
- SCH-TC-NGAIO-DRG-TR-902511
- SCH-TC-NGAIO-DRG-TR-902512
- SCH-TC-NGAIO-DRG-TR-902513

REV TITLE

C03	PROJECT COVERSHEET
C03	DRAWING LIST & KEY PLAN
C03	GENERAL NOTES & LEGEND
C03	SECTION COVERSHEET
C03	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 1
C03	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 2
C03	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 3
C03	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 4
C03	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 5
C03	CAMERON STREET - SECTION COVERSHEET
C03	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 1
C03	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 2
C03	NGAIO GORGE ROAD - SECTION COVERSHEET
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 1
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 2
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 3
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 4
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 5
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 6
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 7
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 8
C03	KENYA STREET - SECTION COVERSHEET
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
C03	CROFTON ROAD - SECTION COVERSHEET
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 4



PROJECT OVERVIEW PLAN
 SCALE 1:5000

CLIENT

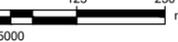
ORIGINATOR



REVISIONS

REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	07/ 10/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE



DRAWING SCALE: 1:5000

DESIGN STAGE

DETAILED DESIGN

GATE

TR

DRAWING STATUS

NOT FOR CONSTRUCTION

APPROVED BY:	DESIGNED:	B.RODENBURG
J.WARD	DES. CHECK:	J.WARD
APPROVED DATE:	DRAWN:	C.LI
07/ 10/ 2022	DRG. CHECK:	H.BENDLE

PROJECT:

TRANSITIONAL CYCLEWAYS
 NGAIO TRANSITIONAL

TITLE:

DRAWING LIST & KEY PLAN

DOCUMENT:

ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
SCH - TC	- NGAIO	- DRG	- TR	- 902010	C03	

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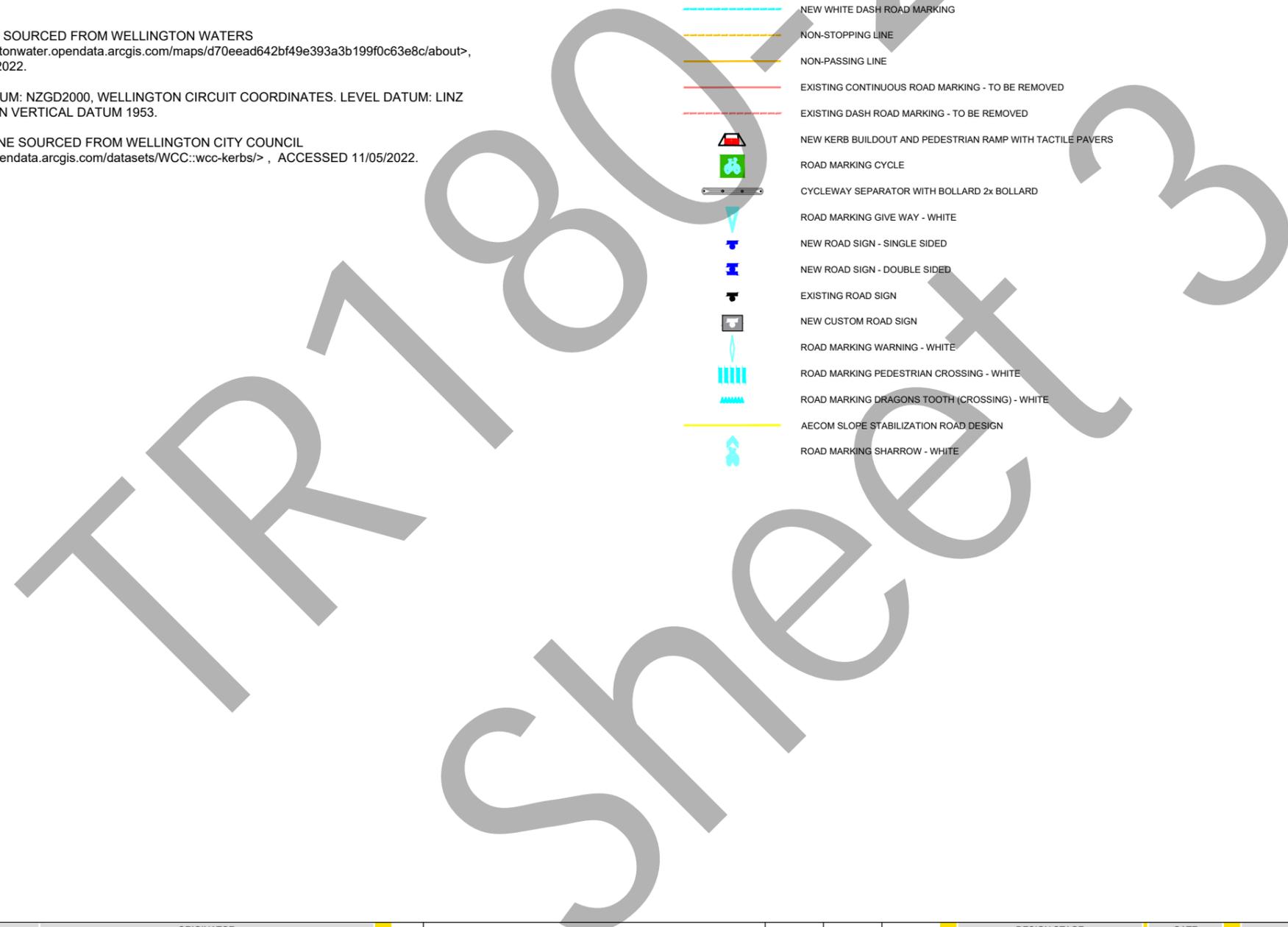
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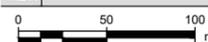
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<<https://data-wellingtonwater.opendata.arcgis.com/maps/d70eead642bf49e393a3b199f0c63e8c/about>> , ACCESSED 11/05/2022.
6. COORDINATE DATUM: NZGD2000, WELLINGTON CIRCUIT COORDINATES. LEVEL DATUM: LINZ (MSL) WELLINGTON VERTICAL DATUM 1953.
7. EXISTING KERB LINE SOURCED FROM WELLINGTON CITY COUNCIL
<<https://data-wcc.opendata.arcgis.com/datasets/WCC::wcc-kerbs/>> , ACCESSED 11/05/2022.

GENERAL LEGEND

- PARCEL BOUNDARY
- HOUSE EXTENT
- PLOT NUMBER
- 130
- KERB - EXISTING
- KERB - NEW
- NEW CYCLIST LANES
- NEW / EXTENDED RAISED MEDIAN ISLAND
- NEW WHITE ROAD MARKING
- NEW WHITE DASH ROAD MARKING
- NON-STOPPING LINE
- NON-PASSING LINE
- EXISTING CONTINUOUS ROAD MARKING - TO BE REMOVED
- EXISTING DASH ROAD MARKING - TO BE REMOVED
- NEW KERB BUILDOUT AND PEDESTRIAN RAMP WITH TACTILE PAVERS
- ROAD MARKING CYCLE
- CYCLEWAY SEPARATOR WITH BOLLARD 2x BOLLARD
- ROAD MARKING GIVE WAY - WHITE
- NEW ROAD SIGN - SINGLE SIDED
- NEW ROAD SIGN - DOUBLE SIDED
- EXISTING ROAD SIGN
- NEW CUSTOM ROAD SIGN
- ROAD MARKING WARNING - WHITE
- ROAD MARKING PEDESTRIAN CROSSING - WHITE
- ROAD MARKING DRAGONS TOOTH (CROSSING) - WHITE
- AECOM SLOPE STABILIZATION ROAD DESIGN
- ROAD MARKING SHARROW - WHITE



CLIENT 	ORIGINATOR 	REVISIONS <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>REV.</th> <th>DESCRIPTION</th> <th>DRAWN BY</th> <th>APPRVD. BY</th> <th>DATE</th> </tr> <tr> <td>C01</td> <td>30% DETAILED DESIGN</td> <td>C.LI</td> <td>A.HEAD</td> <td>25/ 07/ 2022</td> </tr> <tr> <td>C02</td> <td>90% DETAILED DESIGN</td> <td>C.LI</td> <td>A.HEAD</td> <td>07/ 10/ 2022</td> </tr> <tr> <td>C03</td> <td>TRAFFIC RESOLUTION ISSUE</td> <td>C.LI</td> <td>J.WARD</td> <td>07/ 10/ 2022</td> </tr> </table>	REV.	DESCRIPTION	DRAWN BY	APPRVD. BY	DATE	C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022	C02	90% DETAILED DESIGN	C.LI	A.HEAD	07/ 10/ 2022	C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022	SCALE  1:2000	DRAWING INFORMATION DESIGN STAGE DETAILED DESIGN GATE TR DRAWING STATUS NOT FOR CONSTRUCTION	APPROVED BY: J.WARD DESIGNED: B.RODENBURG DES. CHECK: J.WARD APPROVED DATE: 07/ 10/ 2022 DRAWN: C.LI DRG. CHECK: H.BENDLE	PROJECT: TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL TITLE: GENERAL NOTES & LEGEND	DOCUMENT: SCH - TC - NGAIoT - DRG - TR - 902011 C03
REV.	DESCRIPTION	DRAWN BY	APPRVD. BY	DATE																							
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022																							
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022																							

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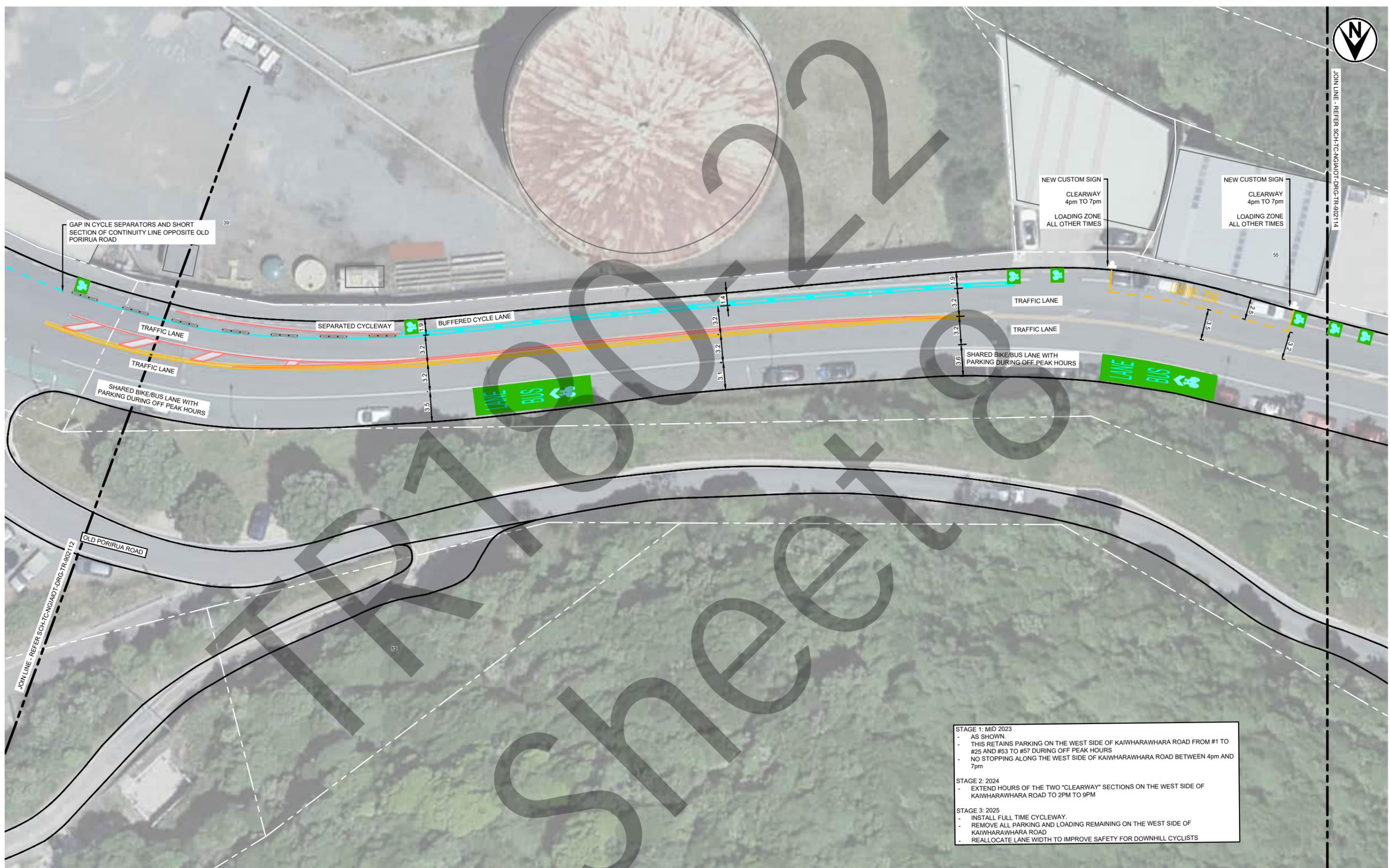
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STAGE 1: MID 2023
 - AS SHOWN.
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - INSTALL FULL TIME CYCLEWAY.
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS

<p>CLIENT</p> 	<p>ORIGINATOR</p>   
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE: 1:200

DRAWING SCALE: 1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL			
TITLE:	KAIWHARAWHARA ROAD GENERAL ARRANGEMENT PLAN SHEET 4			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902113	REVISION:	C03	



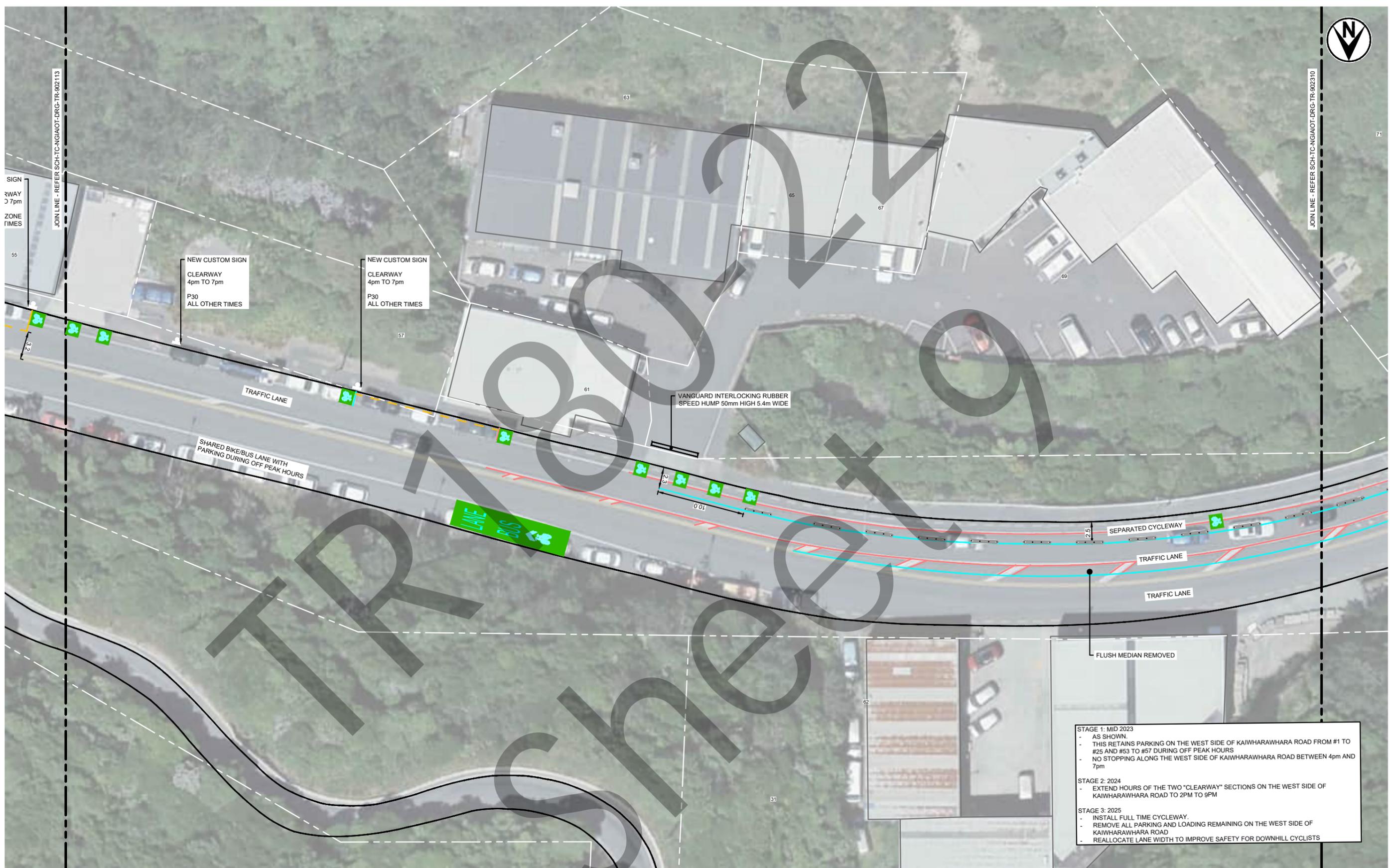
JOIN LINE - REFER SCH:TC-NGAIOT-DRG-TR-902114

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STAGE 1: MID 2023
 - AS SHOWN.
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - INSTALL FULL TIME CYCLEWAY.
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS

CLIENT

Mc Heker 90 Pūkaha

ORIGINATOR

Isthmus.

REV.	DESCRIPTION	DRAWN BY	APPRD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE: 1:200

DRAWING SCALE: 1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR

DRAWING STATUS

NOT FOR CONSTRUCTION

APPROVED BY:	DESIGNED:	B.RODENBURG
J.WARD	DES. CHECK:	J.WARD
APPROVED DATE:	DRAWN:	C.LI
07/ 10/ 2022	DRG. CHECK:	H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	KAIWHARAWHARA ROAD			
	GENERAL ARRANGEMENT PLAN			
	SHEET 5			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902114	C03		

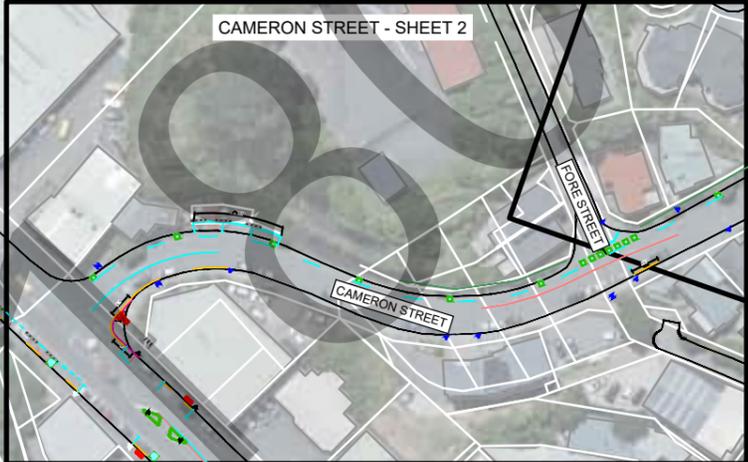
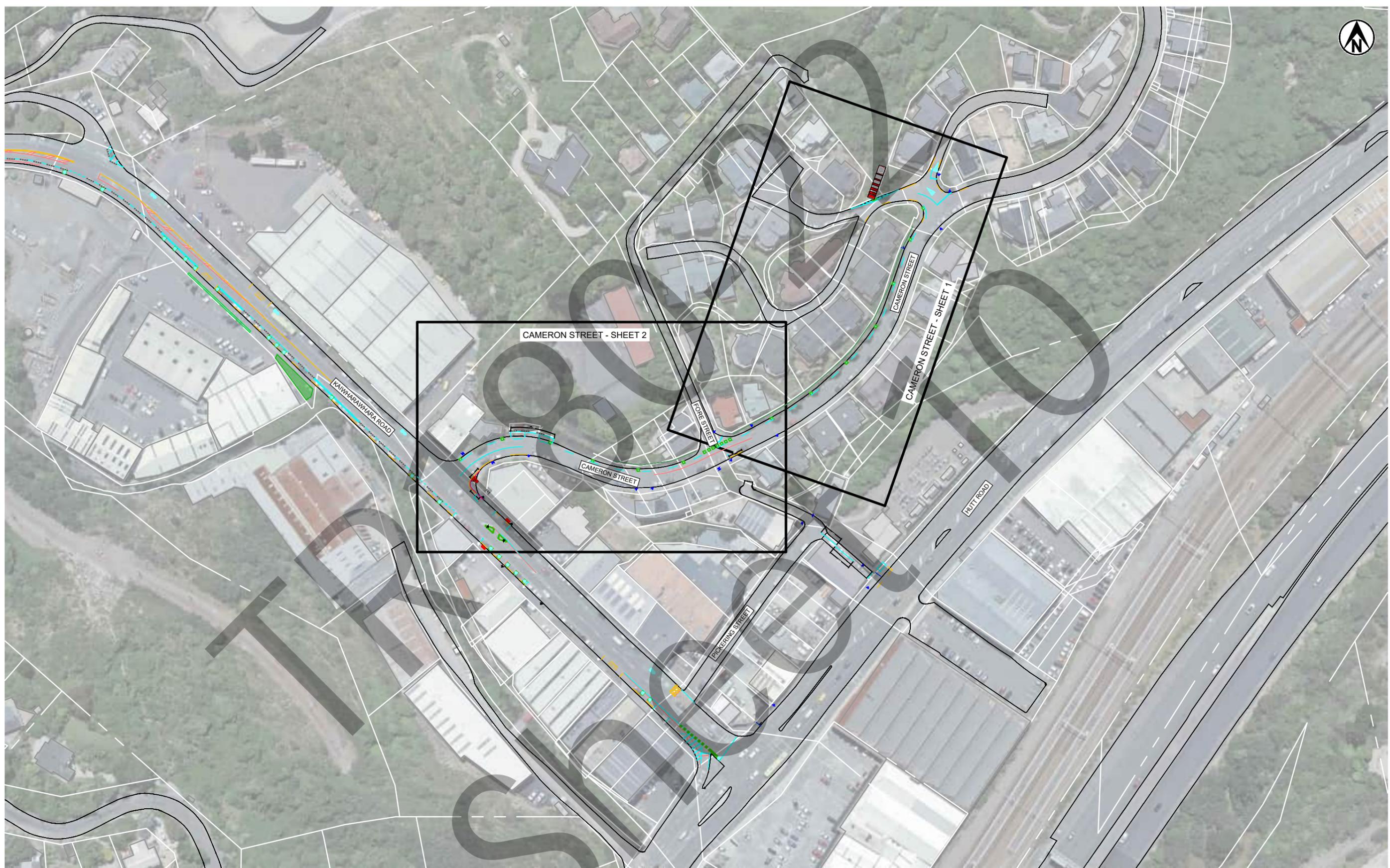
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REV.	DESCRIPTION	DRAWN BY	APPRD. BY	DATE
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C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DETAILED DESIGN	TR
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NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: J.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

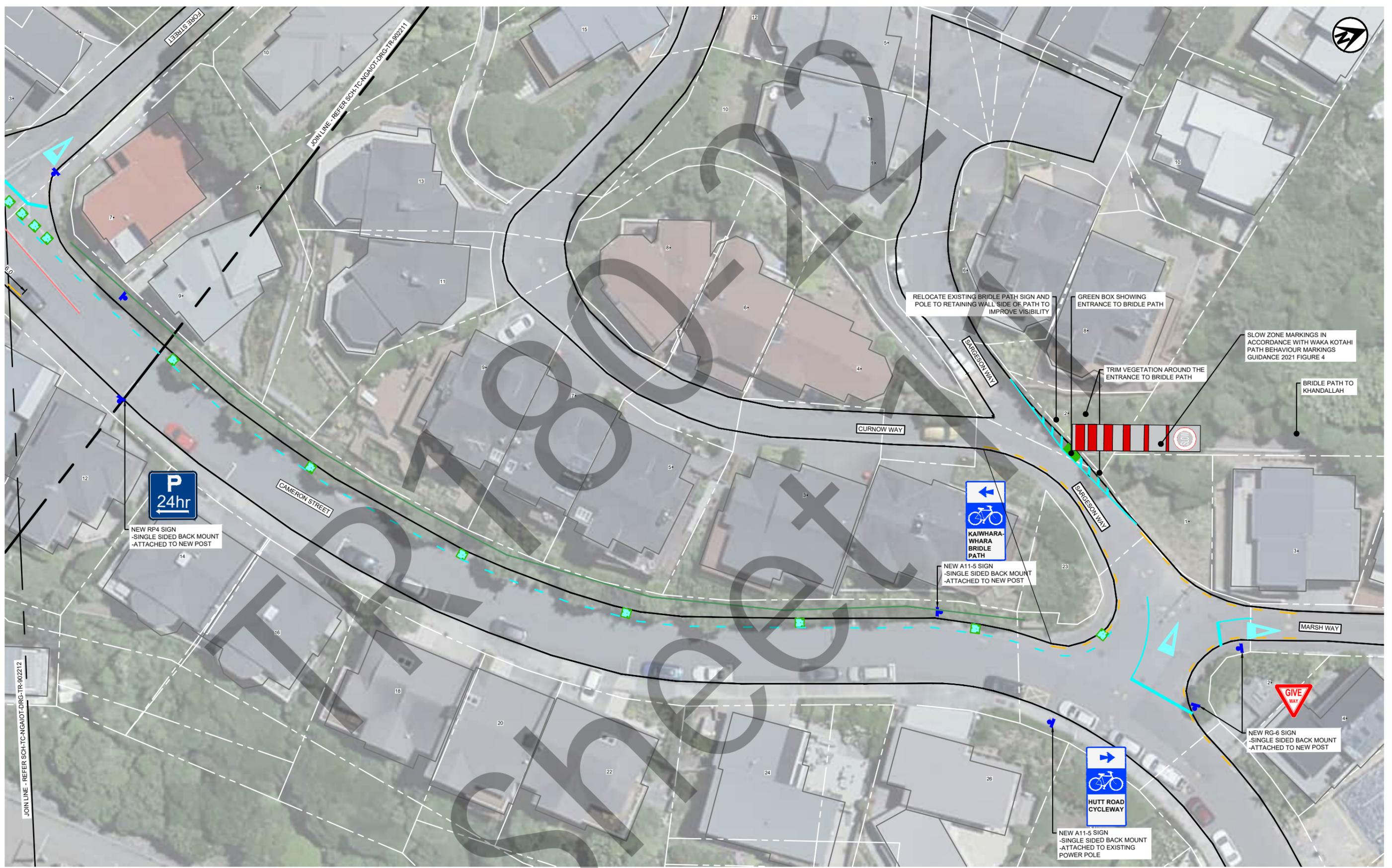
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FILE LOCATION:	PRINTED COPIES OF THIS DOCUMENT ARE UNCONTROLLED	ORIGINATOR	PROJECT
		WORK STREAM	TYPE
		DISCIPLINE	SHEET
		REVISION	C03

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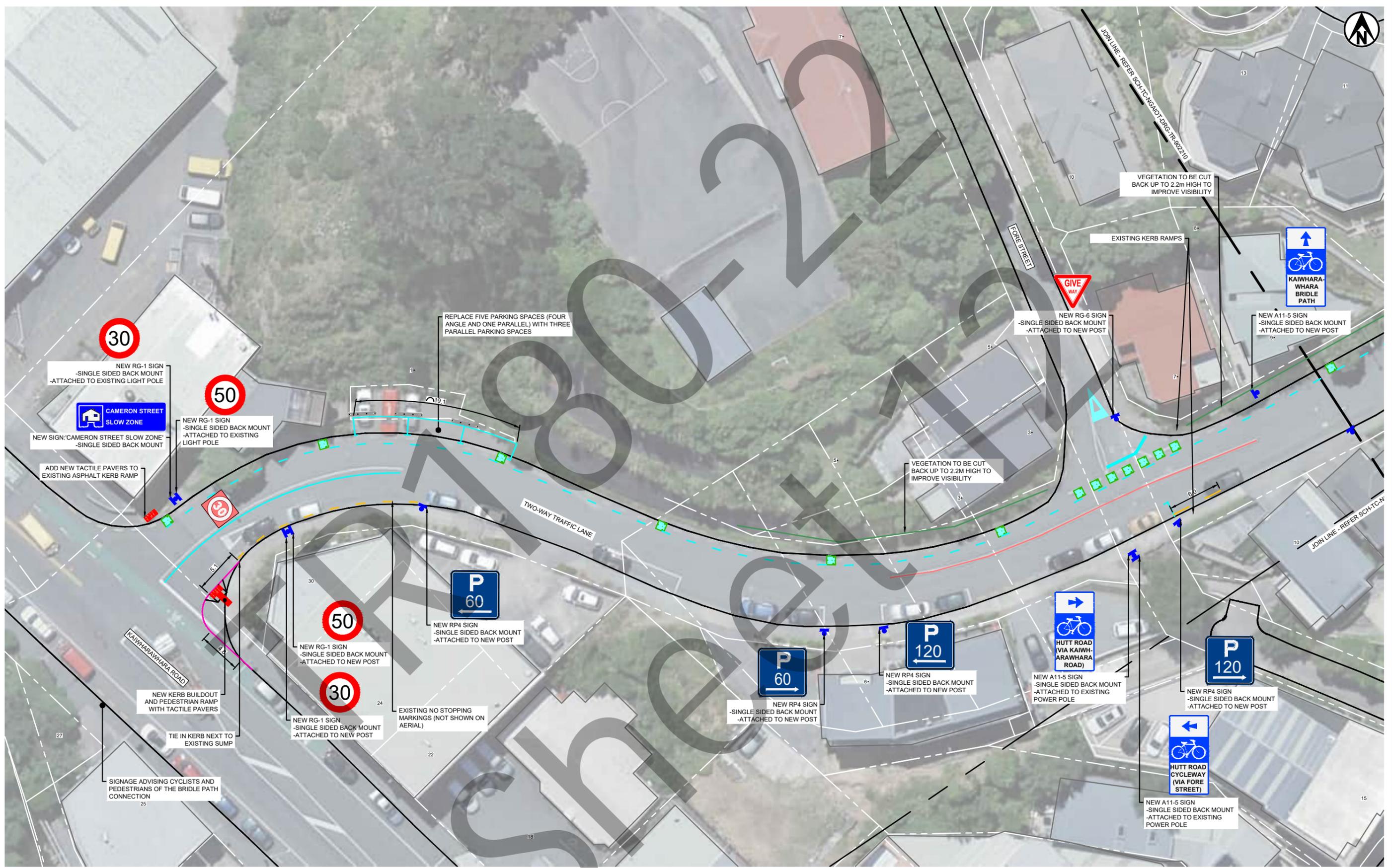
DESIGN STAGE	GATE
DETAILED DESIGN	TR

DRAWING STATUS: NOT FOR CONSTRUCTION

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J.WARD	B.RODENBURG
APPROVED DATE:	DES. CHECK:
07/ 10/ 2022	J.WARD
DRAWN:	DRG. CHECK:
C.LI	H.BENDLE

PROJECT:	TITLE:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	CAMERON/ PICKERING STREET GENERAL ARRANGEMENT PLAN SHEET 1
DOCUMENT:	REVISION:
SCH - TC - NGAIoT - DRG - TR - 902210	C03

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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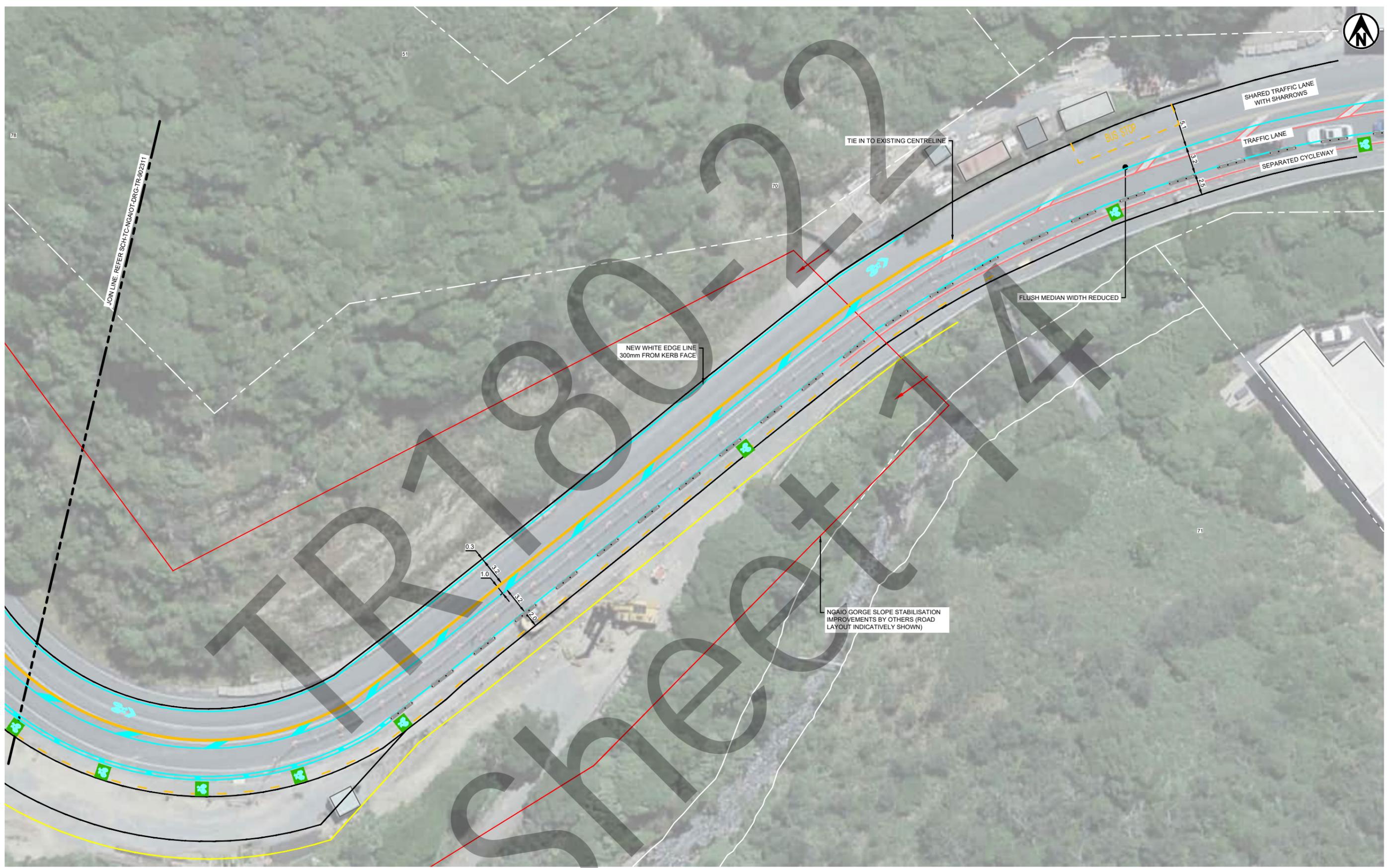
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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SCALE: 1:200			

DESIGN STAGE	GATE
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
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PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 1			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902310	SHEET	C03	REVISION

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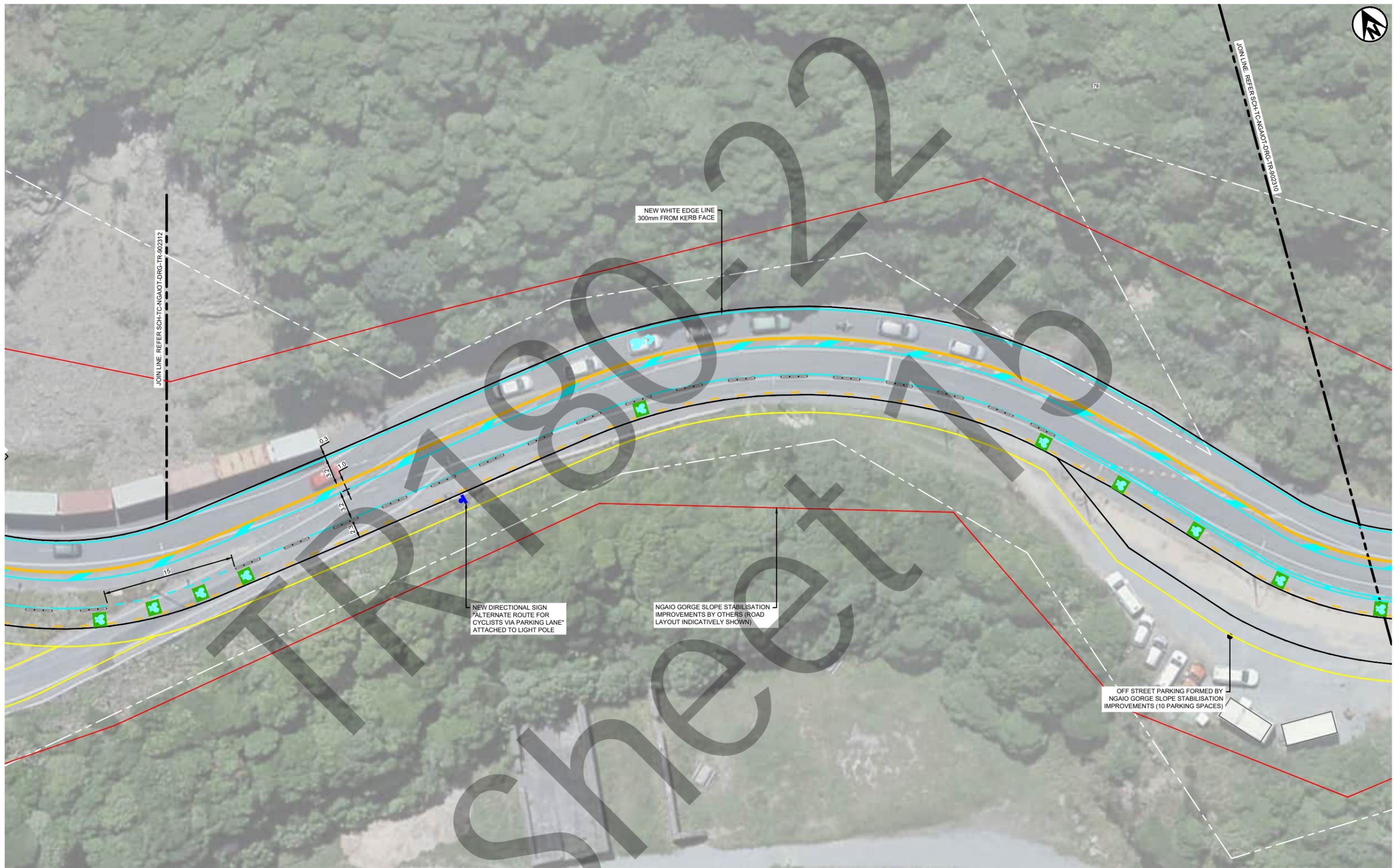
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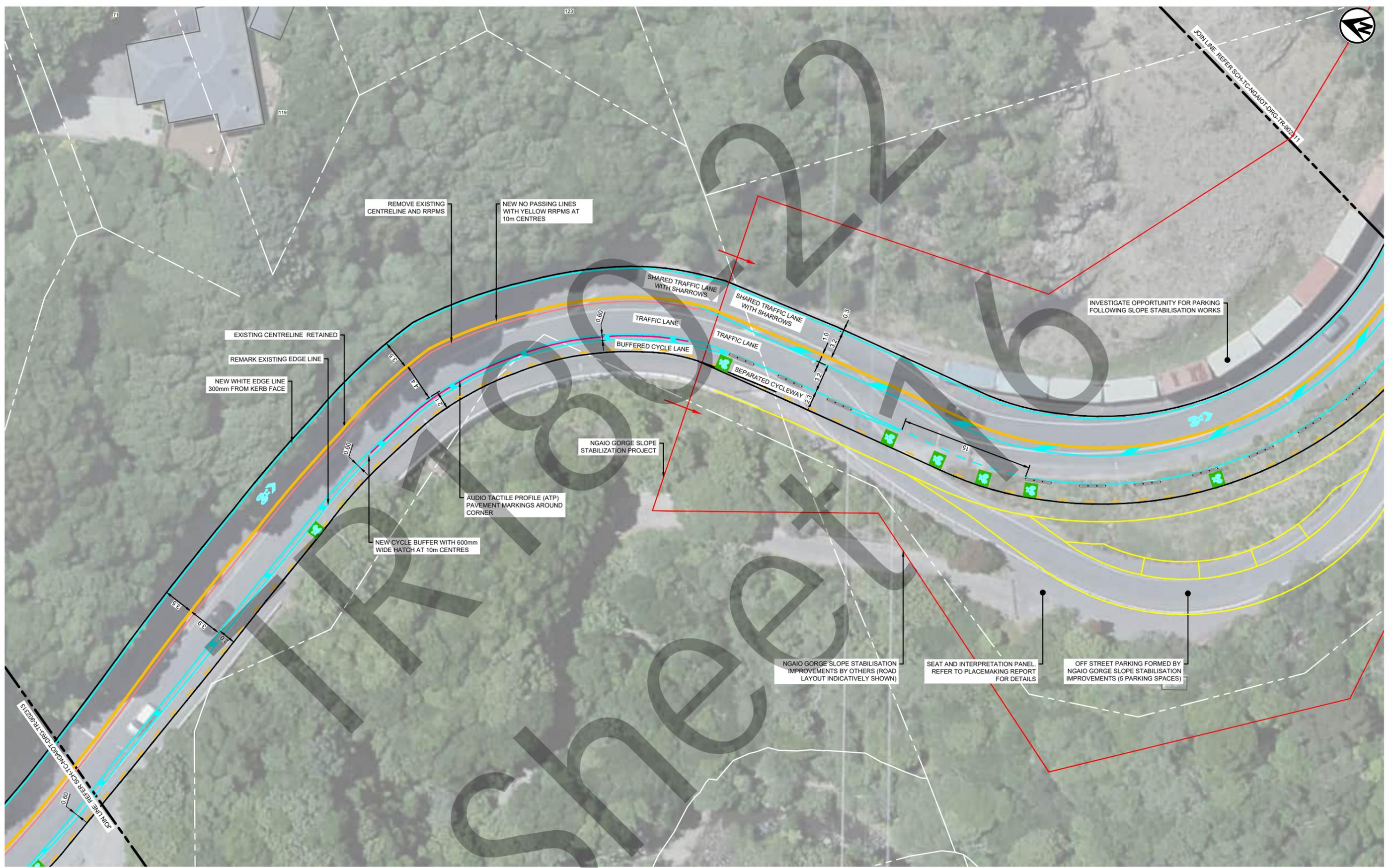
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CLIENT		ORIGINATOR		DESIGN STAGE		GATE		PROJECT:																					
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

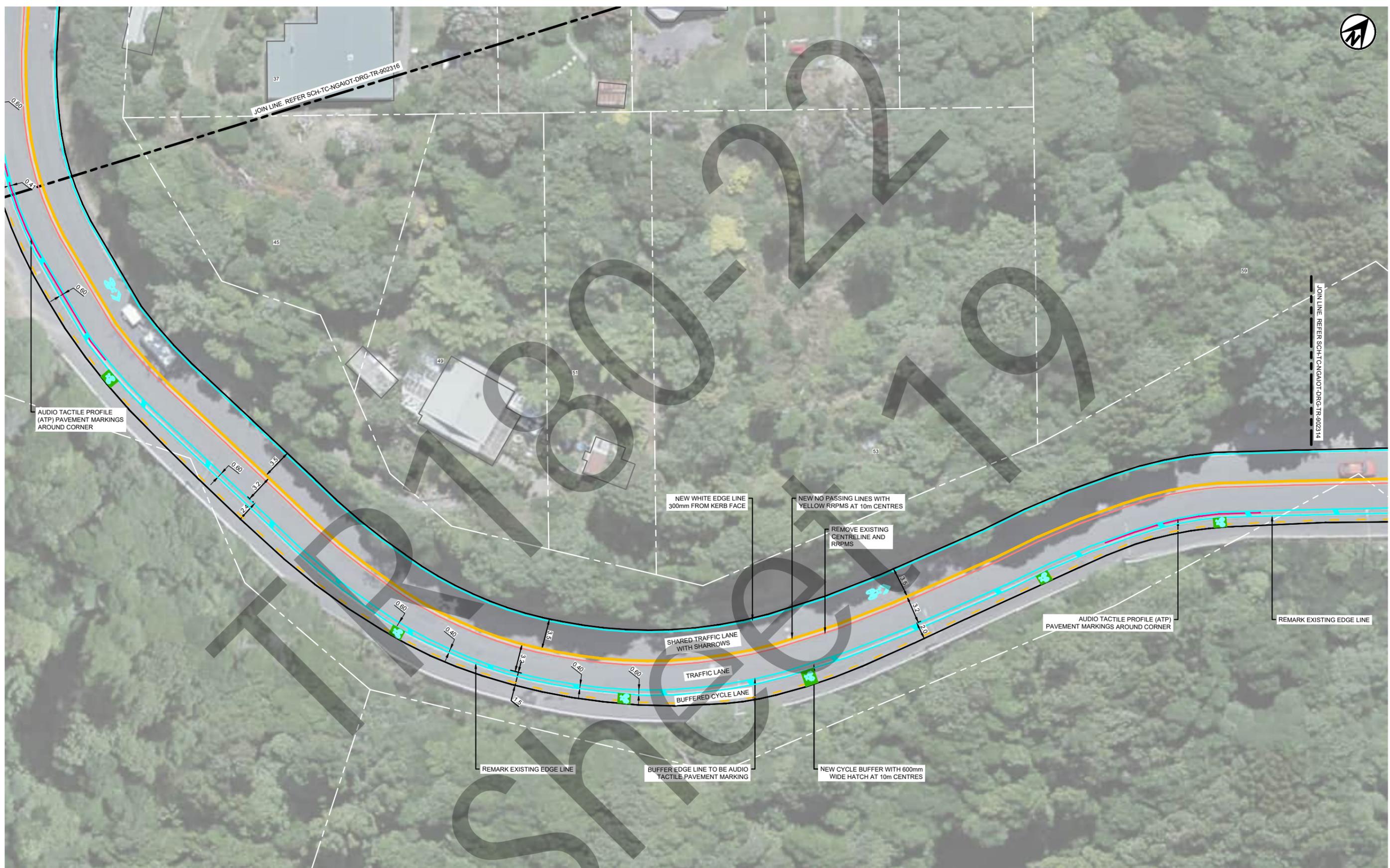
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J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 5			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902314	C03		

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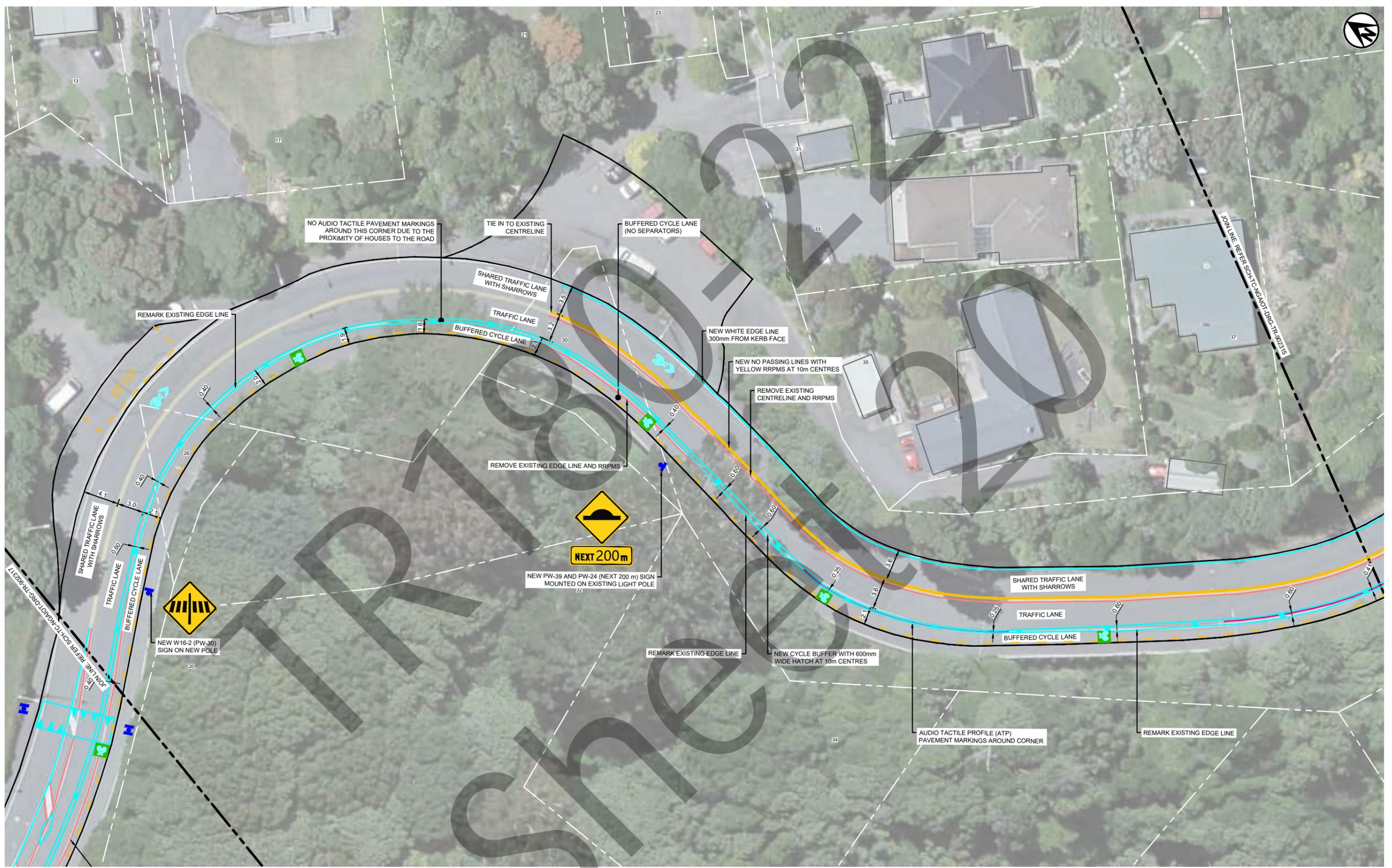
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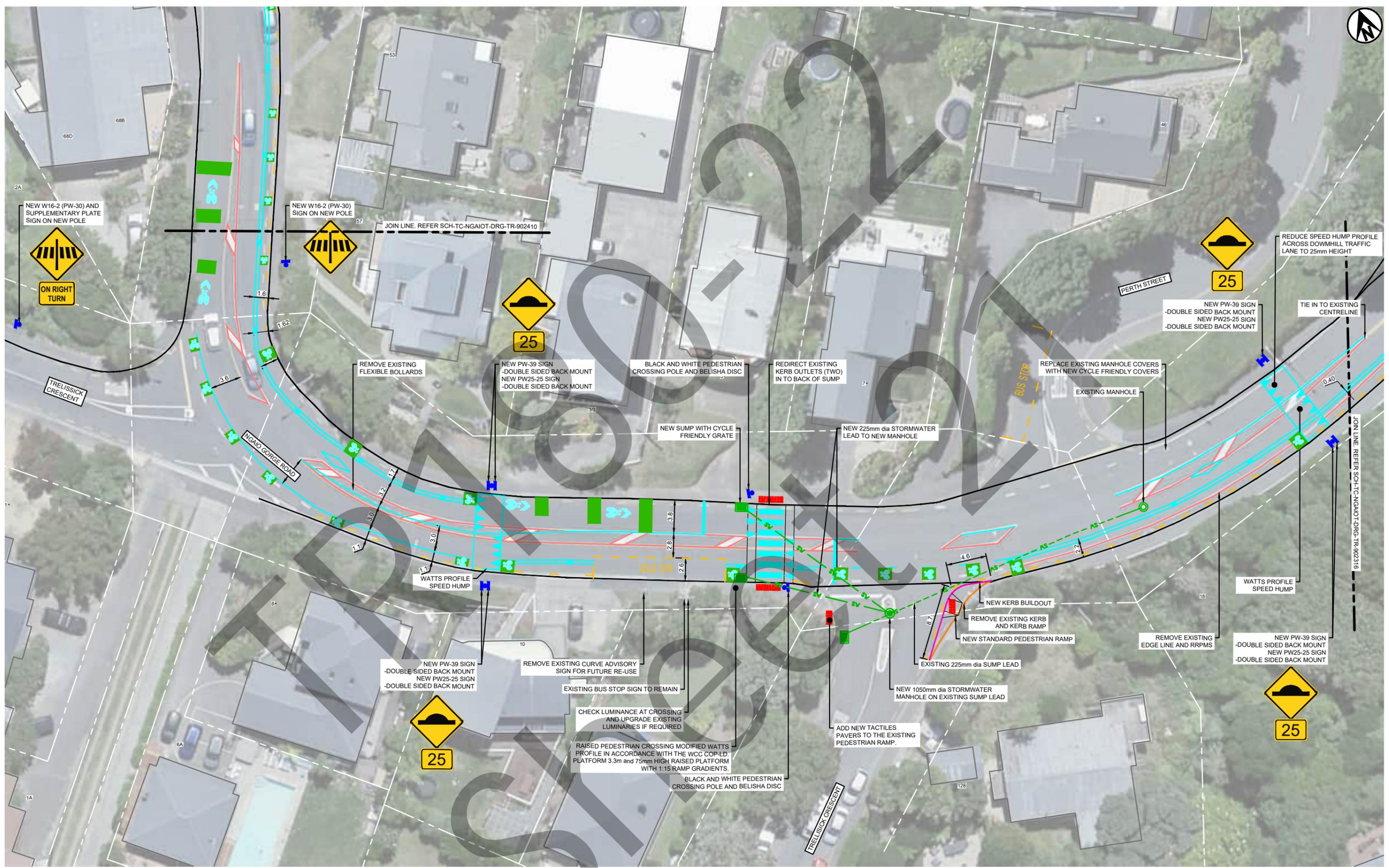
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DETAILED DESIGN		TR
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APPROVED BY:	DESIGNED:	B.RODENBURG
J.WARD	DES. CHECK:	J.WARD
APPROVED DATE:	DRAWN:	C.LI
07/ 10/ 2022	DRG. CHECK:	H.BENDLE

PROJECT:		TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	
TITLE:	KENYA STREET GENERAL ARRANGEMENT SHEET 1		
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902410	SHEET	REVISION
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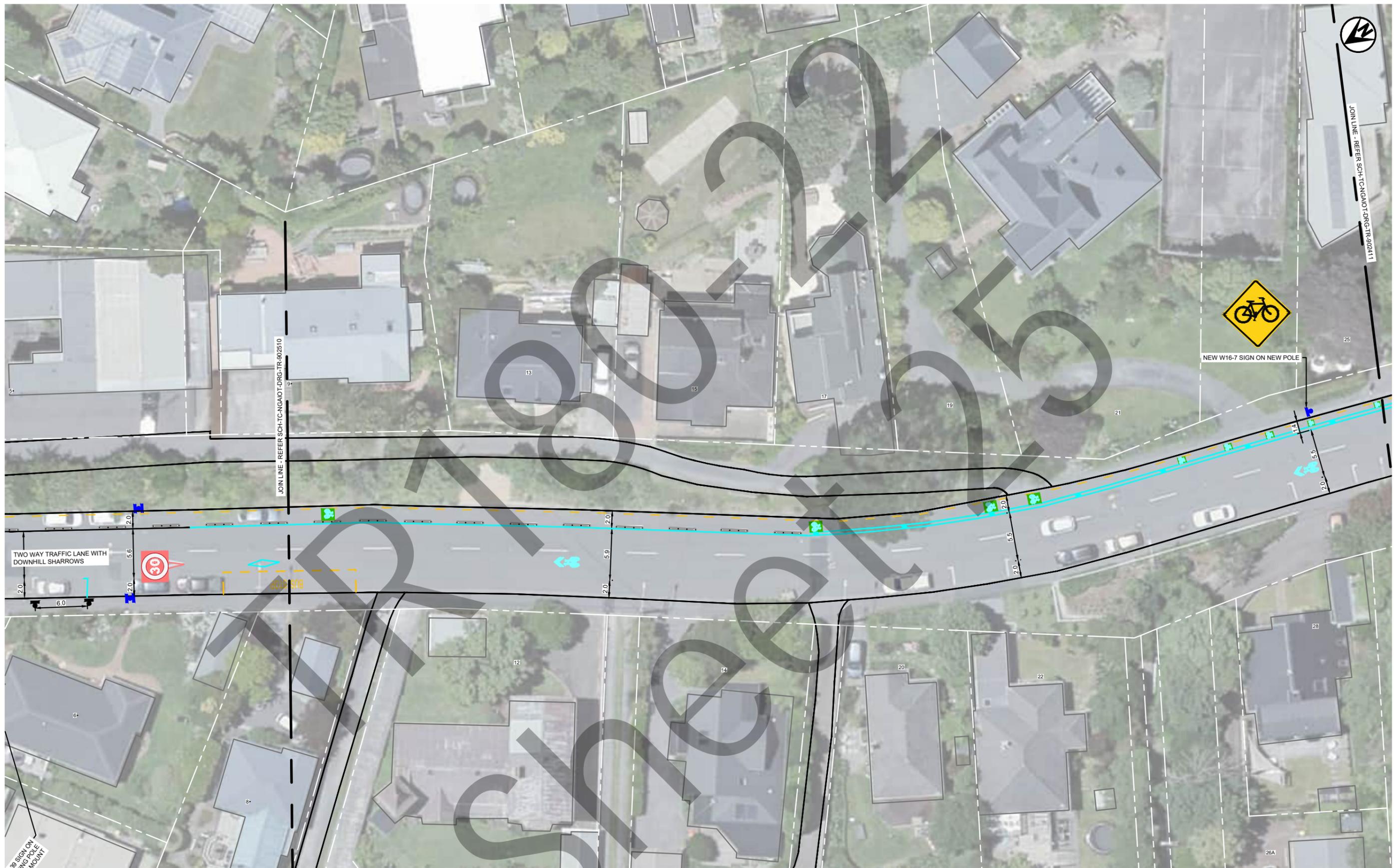
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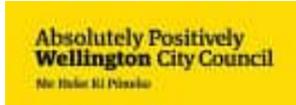
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DESIGN STAGE	GATE
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APPROVED DATE:	DRAWN:	C.LI
07/ 10/ 2022	DRG. CHECK:	H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL				
TITLE:	KENYA STREET GENERAL ARRANGEMENT SHEET 3				
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902412	C03			

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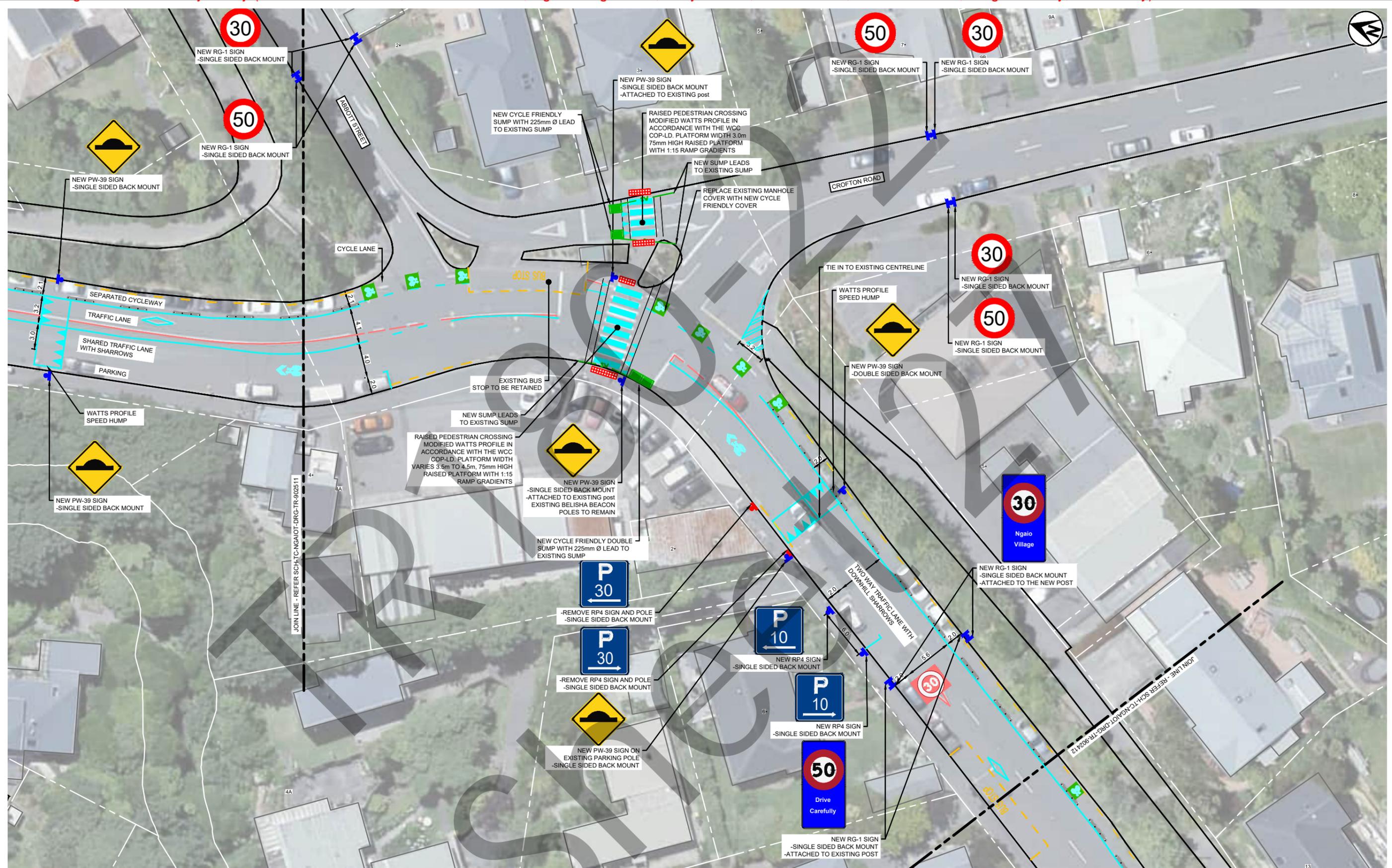
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DES. CHECK:	J.WARD
APPROVED DATE:	07/ 10/ 2022
DRAWN:	C.LI
DRG. CHECK:	H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	
TITLE:	CROFTON ROAD GENERAL ARRANGEMENT SHEET 1	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902510	C03

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J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS				
	NGAIO TRANSITIONAL				
TITLE:	CROFTON ROAD				
	GENERAL ARRANGEMENT				
	SHEET 3				
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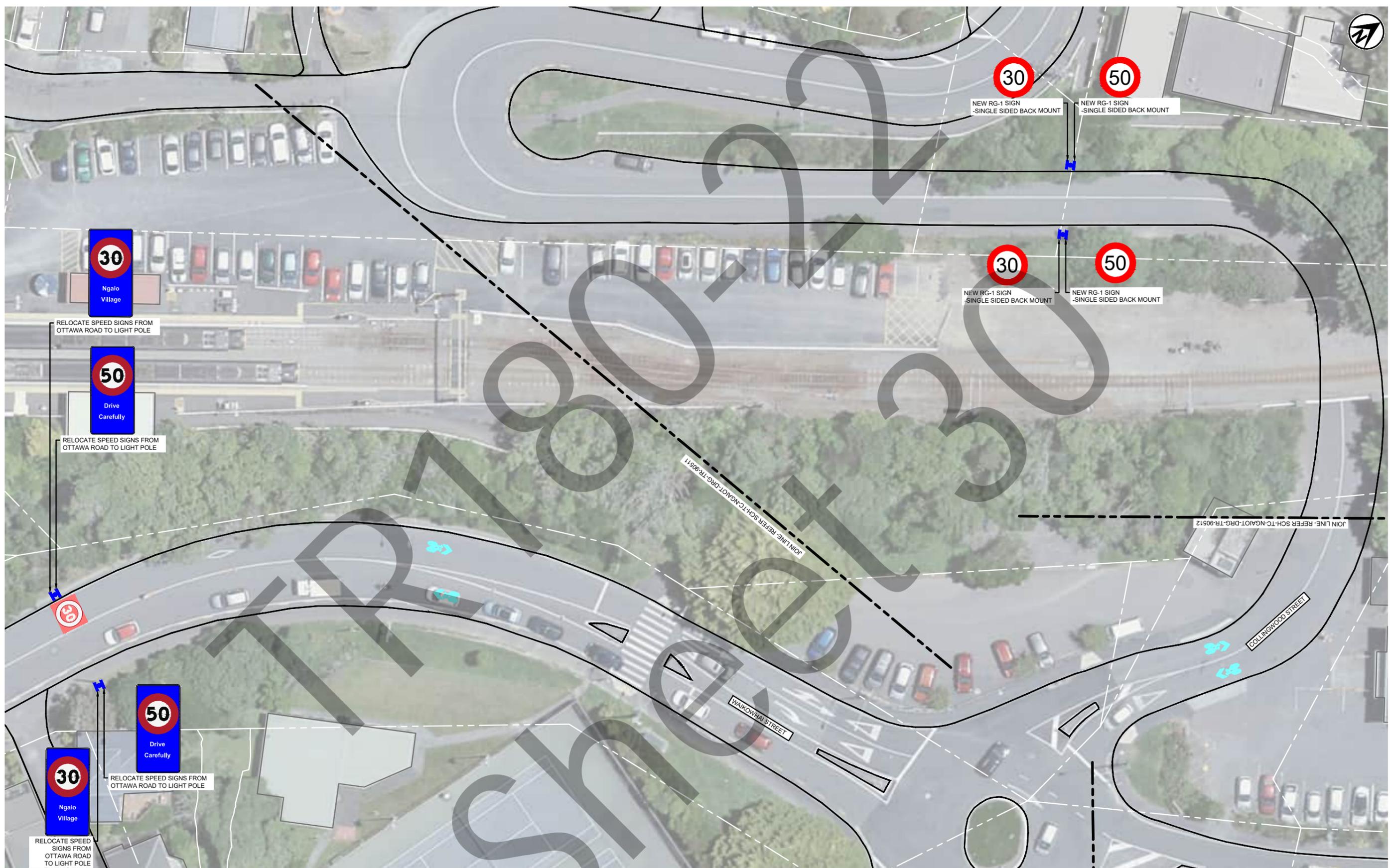
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DESIGN STAGE	GATE
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J.WARD	B.RODENBURG
APPROVED DATE:	DES. CHECK:
07/ 10/ 2022	J.WARD
DRAWN:	DRG. CHECK:
C.LI	H.BENDLE

PROJECT:	TITLE:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	CROFTON ROAD GENERAL ARRANGEMENT SHEET 4

ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: Another Ngaio design question
Date: Tuesday, 29 November 2022 1:35:37 pm
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

Hi Jonathan,

That's a good idea. We would have to move it slightly further back than you have drawn to the other side of the driveway, but don't see any issues with that

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Tuesday, 29 November 2022 12:26 pm

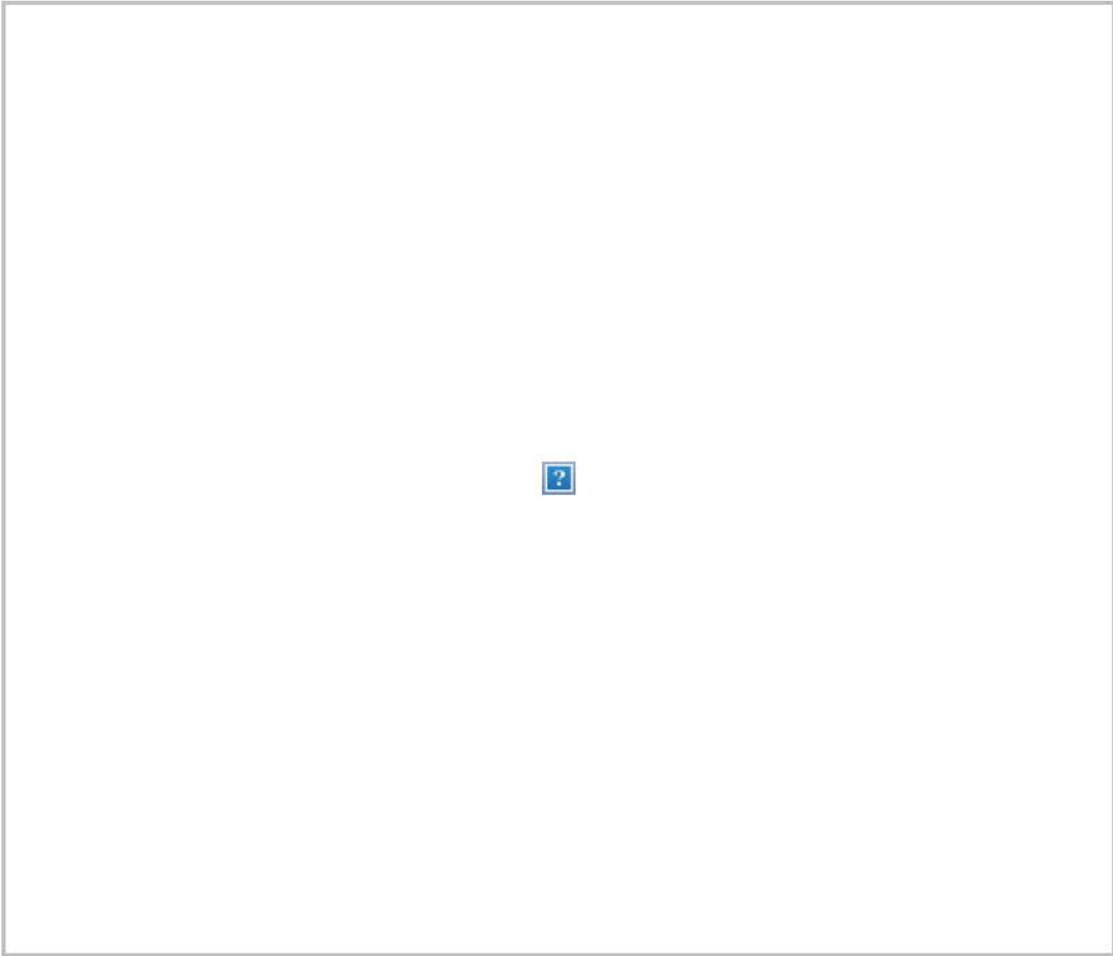
To: Billy Rodenburg <brodensburg@tonkintaylor.co.nz>

Subject: Another Ngaio design question

Hi Billy,

A submitter would like to see the Kenya St bus stop moved back to the south so that we can add one unrestricted carpark.

Do you have any comment about this suggestion?



Cheers
Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#); [Ann-Marie Head](#)
Cc: [Claire Pascoe](#)
Subject: RE: Another small change to the Ngaio Connections design
Date: Wednesday, 14 December 2022 2:42:22 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Hi Jonathan,

There is potential that drivers travel above the posted speed limit. For instance, the average speed recorded in the current 30km/hr section past the Ngaio shops is 35 km/hr (and half of all drivers are travelling faster than this).

The WCC COP-LD Section C1.9 recommends a spacing of 75m spacing to achieve a 30 km/hr speed environment. Removing this speed hump would increase the distance between the roundabout at the raised crossing to approximately 150m increasing the likelihood that traffic speeds exceed the speed limit. This is especially important for the shared lane for northbound cyclists.

In a transitional approach we could install the cycleway without the hump and monitor average traffic speeds following installation. The hump could be installed later if traffic speeds remain high.

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Tuesday, 13 December 2022 4:56 pm
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Cc: Claire Pascoe <claire.pascoe@wcc.govt.nz>
Subject: Another small change to the Ngaio Connections design

Hi Billy,

Can you please make one more small change?



Remove the speed hump to the west of the Crofton Road raised pedestrian crossing (close to the Crofton Road Dairy).

The raised pedestrian crossing is sufficient to improve safety given that drivers are approaching from a 30km/h speed zone.

Happy to discuss, but in a nutshell we don't think this speed hump is needed.

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ann-Marie Head](#); [Ryan Dunn](#)
Subject: RE: Changes for Ngaio TR
Date: Friday, 21 October 2022 2:17:52 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.jpg](#)
[SCH-TC-NGAIOT-DRG-TR-combined - Updated_Optimized.pdf](#)
[TR180-22 Ngaio Transitional Cycleway_with StepChange comments \(1\).docx](#)

Hi Jonathan,

The updated Kaiwharawhara and Cameron Street Drawings and TR Legal descriptions with updates as track changes attached. Changes are;

- Extending the Stage 1 and 2 P30 restrictions to match existing parking on Kaiwharawhara Road at Cameron Street
- Extending no-stopping across the two parking spaces agreed to be removed between Cameron Street and the pedestrian refuge in Stage 3
- Additional section of no stopping on Cameron Street
- Loading zone with P10 restriction

I also note that we will need to update and reissue the design decisions report reflecting these recent changes, and expect to be able to get that through to you mid next week.

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

Tonkin + Taylor - *Exceptional thinking together*

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Thursday, 20 October 2022 11:38 am

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: RE: Changes for Ngaio TR

Thanks Billy,

I have pasted those into the TR now.

Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>
Sent: Thursday, October 20, 2022 11:03 AM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Subject: RE: Changes for Ngaio TR

Hi Jonathan,

Ngaio Gorge Road, Kenya Street and Crofton Road plans without the watermark. Kaiwharawhara Road and Cameron Street will follow once updated

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Thursday, 20 October 2022 8:59 am
To: Billy Rodenburg <brodensburg@tonkintaylor.co.nz>
Subject: RE: Changes for Ngaio TR

Are you there now Billy?

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>
Sent: Thursday, October 20, 2022 8:24 AM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Subject: RE: Changes for Ngaio TR

Thanks Jonathan, when would suit to discuss the raised median island? I am free now to 9.30, or between 10 and 1.

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Wednesday, 19 October 2022 6:22 pm

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: Changes for Ngaio TR

Here were are Billy,

All fairly harmless, which is sign of your good work:

The actions for Billy are highlighted thusly

1. **Remove the watermarks 'TR189-22 Sheet'** – they make it difficult to review the plans

2. **Kaiwharawhara median refuge crossing**

Following further business engagement, we are aware that there is low demand for this pedestrian crossing and businesses are struggling to such an extent that the removal of parking at this time appears untenable. We have also heard from the designer and the safety engineer that this crossing would require the removal of not one, but three, carparks on the downhill side of Kaiwharawhara Road, near Cameron Street.

It appears that at this stage the disbenefits of the crossing outweigh the benefits because there is low demand for a crossing, and high demand for the parking outside the businesses on the north side of the road. This leads us to:

- **Delay the crossing until Stage 3 (early 2025)** by which time there may be more pedestrian traffic and less need for short term visitor parking.

In both cases, we should **discuss with the designer the benefits of installing a raised median island in the same area.**

- This would have a traffic calming effect, and also create some shelter for cyclists turning into Cameron St. This turn is not considered safe, which is why many cyclists ride up Fore Street instead.
- In Christchurch the council have begun installing cycle lanes leading up to a right turn so that there is plenty of time to find a gap and move to the right.

3. **Cameron Street**

Change P24hr Except Residents to just P24hr so that we don't have to introduce a Residents Parking scheme in Cameron Street.

Install No Stopping Restriction (BYLs) on both sides of the narrow section of Carmeron St from No 60 Cameron St to Brasch Way for a length of 35 metres.

4. **Kaiwharawhara Road**

Change the **Loading Zone** unlimited time to **Loading Zone P10**

One final check for inconsistencies of the clearways in the legal description and the plans – ensure they are aligned.

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |  

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TRANSITIONAL CYCLEWAYS



NGAIO TRANSITIONAL TRAFFIC RESOLUTION DESIGN 07/10/2022

TRANSITIONAL CYCLEWAY
NGAIO TRANSITIONAL
TRAFFIC RESOLUTION ISSUE

DRAWING

GENERAL

- SCH-TC-NGAIO-DRG-TR-902000
- SCH-TC-NGAIO-DRG-TR-902010
- SCH-TC-NGAIO-DRG-TR-902011

KAIWHARAWHARA ROAD

- SCH-TC-NGAIO-DRG-TR-902100
- SCH-TC-NGAIO-DRG-TR-902110
- SCH-TC-NGAIO-DRG-TR-902111
- SCH-TC-NGAIO-DRG-TR-902112
- SCH-TC-NGAIO-DRG-TR-902113
- SCH-TC-NGAIO-DRG-TR-902114

CAMERON STREET

- SCH-TC-NGAIO-DRG-TR-902200
- SCH-TC-NGAIO-DRG-TR-902210
- SCH-TC-NGAIO-DRG-TR-902211
- SCH-TC-NGAIO-DRG-TR-902212
- SCH-TC-NGAIO-DRG-TR-902213

NGAIO GORGE ROAD

- SCH-TC-NGAIO-DRG-TR-902300
- SCH-TC-NGAIO-DRG-TR-902310
- SCH-TC-NGAIO-DRG-TR-902311
- SCH-TC-NGAIO-DRG-TR-902312
- SCH-TC-NGAIO-DRG-TR-902313
- SCH-TC-NGAIO-DRG-TR-902314
- SCH-TC-NGAIO-DRG-TR-902315
- SCH-TC-NGAIO-DRG-TR-902316
- SCH-TC-NGAIO-DRG-TR-902317

KENYA STREET

- SCH-TC-NGAIO-DRG-TR-902400
- SCH-TC-NGAIO-DRG-TR-902410
- SCH-TC-NGAIO-DRG-TR-902411
- SCH-TC-NGAIO-DRG-TR-902412

CROFTON ROAD

- SCH-TC-NGAIO-DRG-TR-902500
- SCH-TC-NGAIO-DRG-TR-902510
- SCH-TC-NGAIO-DRG-TR-902511
- SCH-TC-NGAIO-DRG-TR-902512
- SCH-TC-NGAIO-DRG-TR-902513

REV TITLE

C03	PROJECT COVERSHEET
C04	DRAWING LIST & KEYPLAN
C03	GENERAL NOTES & LEGEND
C03	SECTION COVERSHEET
C04	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 1
C04	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 2
C04	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 3
C04	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 4
C04	KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - SHEET 5
C04	CAMERON STREET - SECTION COVERSHEET
C04	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 1
C04	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 2
C01	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 3
C01	CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 4
C03	NGAIO GORGE ROAD - SECTION COVERSHEET
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 1
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 2
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 3
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 4
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 5
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 6
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 7
C03	NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 8
C03	KENYA STREET - SECTION COVERSHEET
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
C03	KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
C03	CROFTON ROAD - SECTION COVERSHEET
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
C03	CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 4



PROJECT OVERVIEW PLAN
 SCALE 1:5000

CLIENT

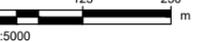
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REVISIONS

REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

SCALE



DESIGN STAGE

DETAILED DESIGN

GATE

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DRAWING STATUS

NOT FOR CONSTRUCTION

APPROVED BY:	DESIGNED:	
A.HEAD	DES. CHECK:	A.HEAD
APPROVED DATE:	DRAWN:	
21/10/2022	DRG. CHECK:	

PROJECT:

TRANSITIONAL CYCLEWAYS
NGAIO TRANSITIONAL

TITLE:

DRAWING LIST & KEY PLAN

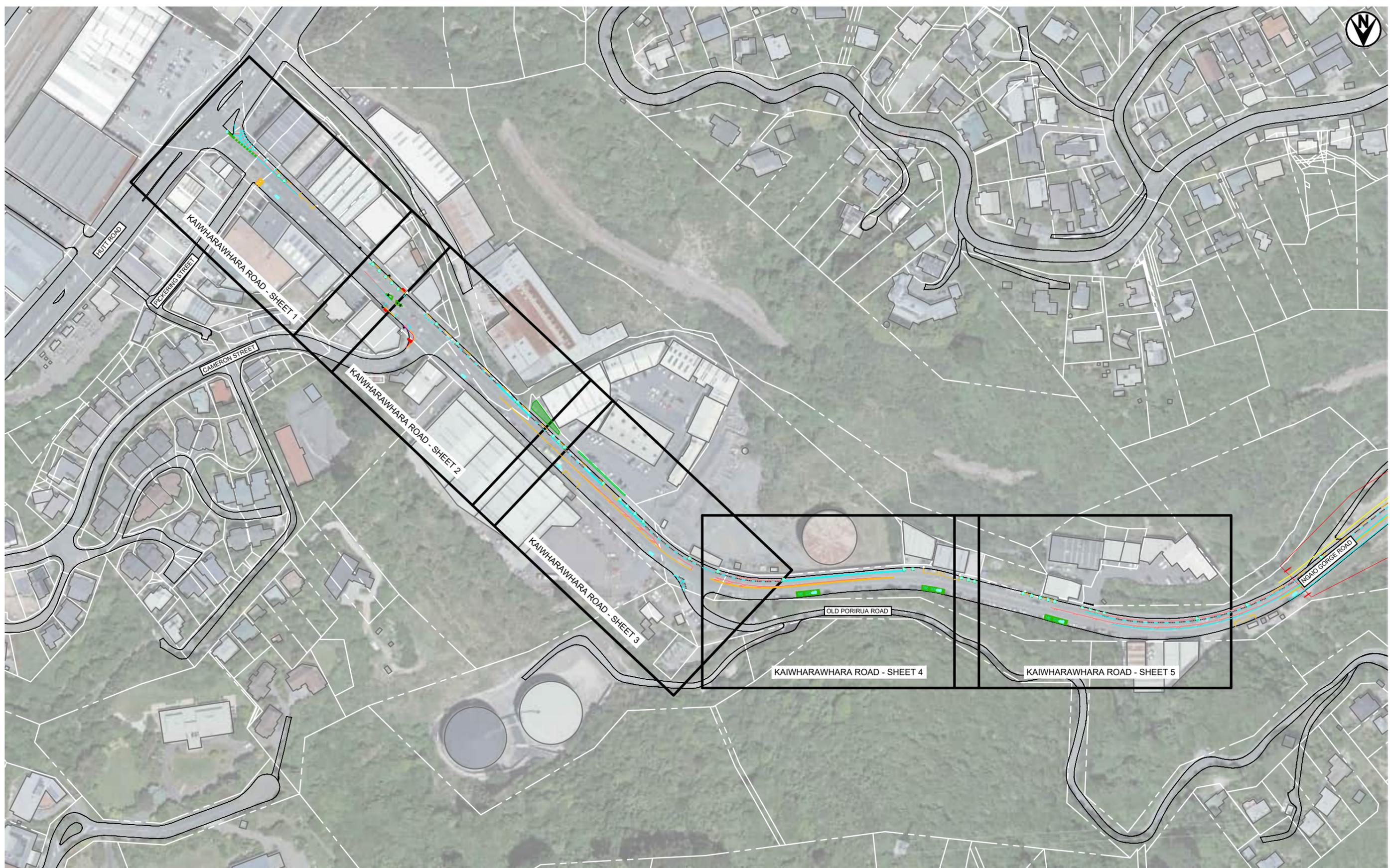
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ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
SCH - TC	- NGAIO	- DRG	- TR	- 902010	C04	

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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

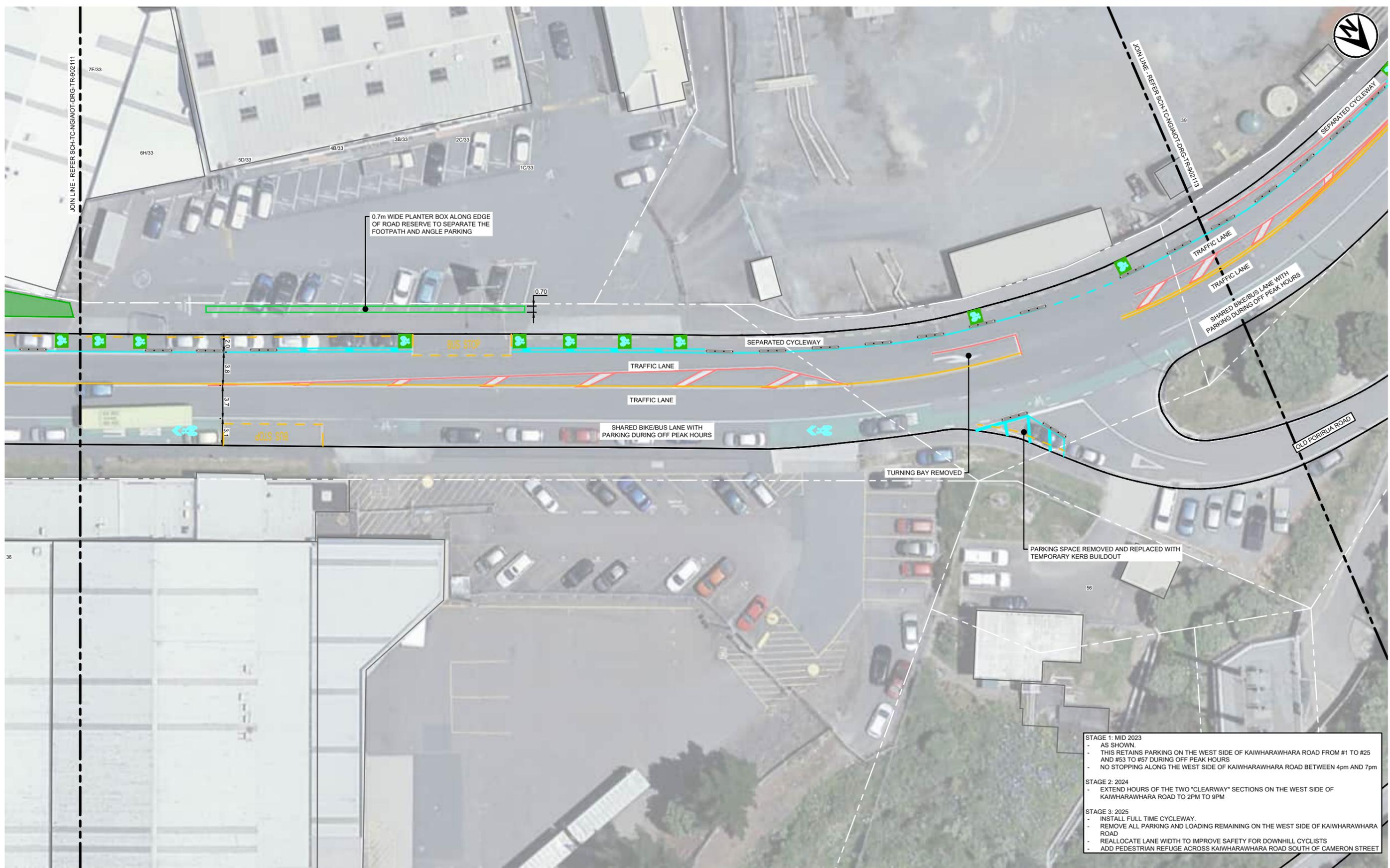
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	KAIWHARAWHARA ROAD SECTION COVERSHEET			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902100	C03	ORIGINATOR	PROJECT
	WORK STREAM	TYPE	DISCIPLINE	SHEET
				REVISION

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STAGE 1: MID 2023
 - AS SHOWN.
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - INSTALL FULL TIME CYCLEWAY.
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

SCALE: 1:200

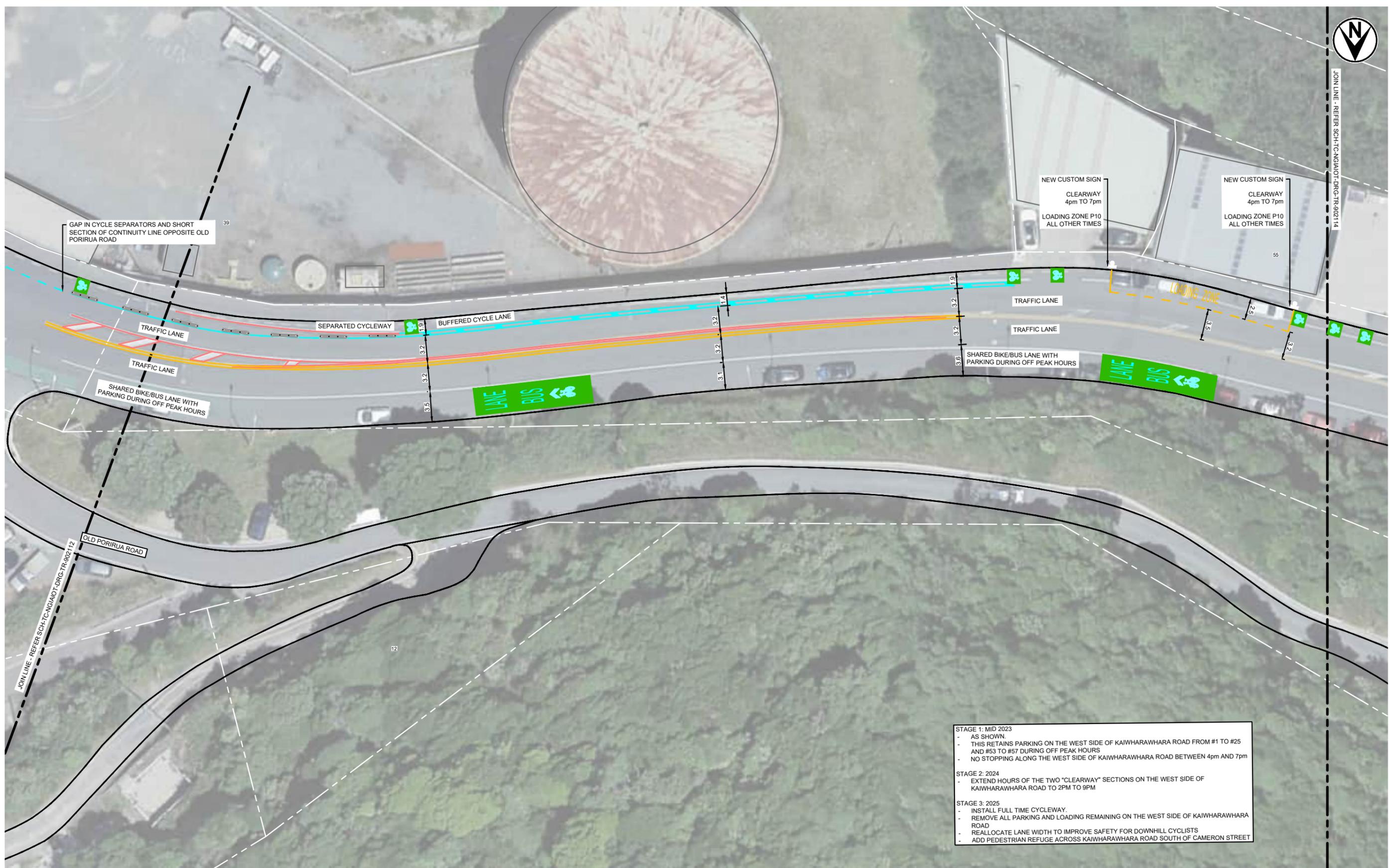
DRAWING SCALE: 1:200

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DETAILED DESIGN	TR
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APPROVED BY:	DESIGNED: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
21/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	KAIWHARAWHARA ROAD	
	GENERAL ARRANGMENT PLAN	
	SHEET 3	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902112	C04

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STAGE 2: 2024
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - INSTALL FULL TIME CYCLEWAY.
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT: Absolutely Positively Wellington City Council
 ORIGINATOR: Abley, Tonkin-Taylor, Isthmus.

REV.	DESCRIPTION	DRAWN BY	APPRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

SCALE: 1:200

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APPROVED BY: A.HEAD	DESIGNED: B.RODENBURG
APPROVED DATE: 21/10/2022	DES. CHECK: A.HEAD
	DRAWN: C.LI
	DRG. CHECK: H.BENDLE

PROJECT: TRANSITIONAL CYCLEWAYS	
TITLE: KAIWHARAWHARA ROAD GENERAL ARRANGEMENT PLAN	
SHEET: SHEET 4	
DOCUMENT: SCH - TC - NGAIoT - DRG - TR - 902113	REVISION: C04

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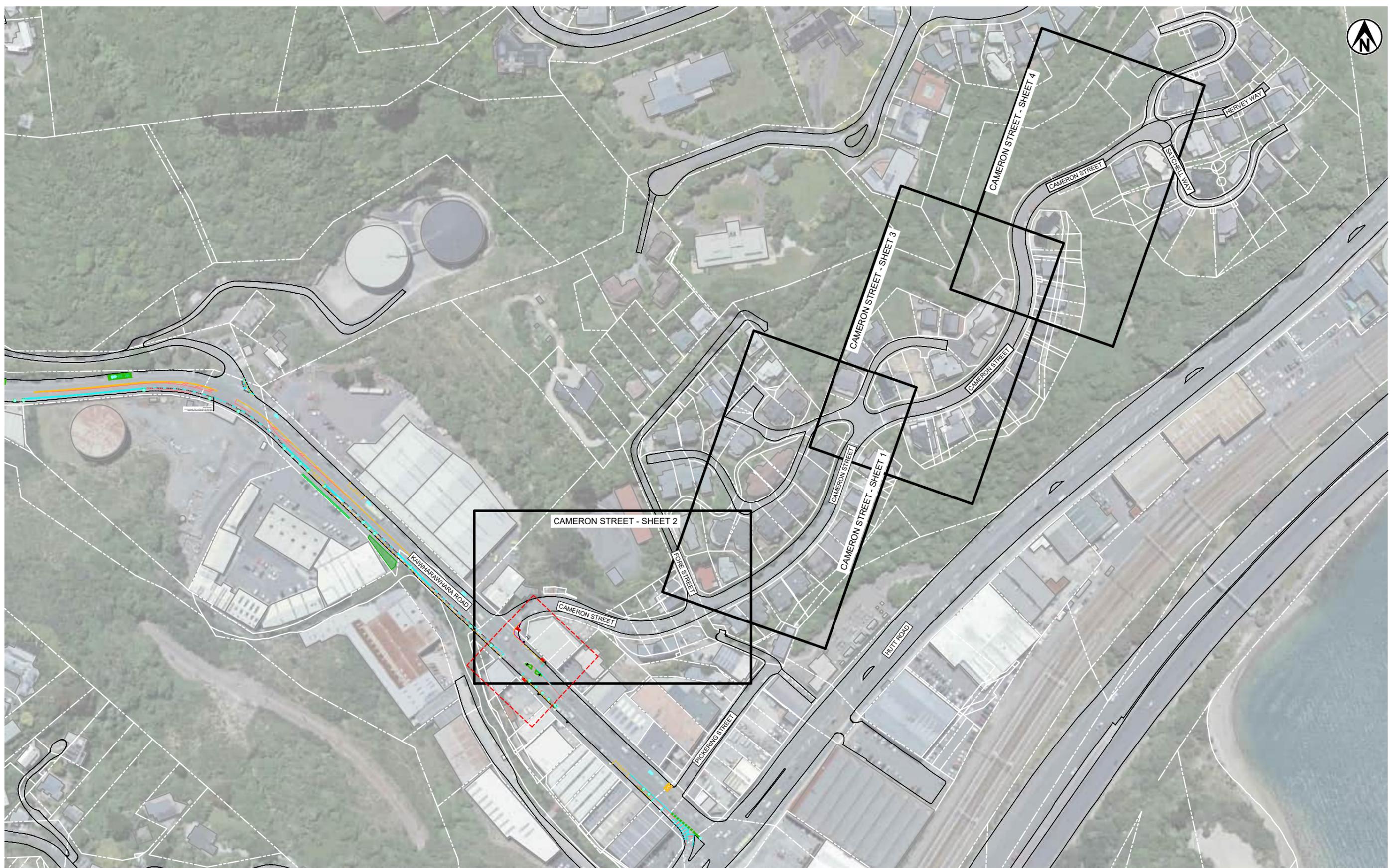
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C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

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A.HEAD		DES. CHECK:		A.HEAD	
APPROVED DATE:		DRAWN:		C.LI	
21/10/2022		DRG. CHECK:		H.BENDLE	
DRAWING INFORMATION			DRAWING INFORMATION		
DRAWING SCALE: 1:1000					

PROJECT:	TRANSITIONAL CYCLEWAYS				
	NGAIO TRANSITIONAL				
TITLE:	CAMERON STREET				
	SECTION COVERSHEET				
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902200	C04	ORIGINATOR	PROJECT	WORK STREAM
			TYPE	DISCIPLINE	SHEET
					REVISION

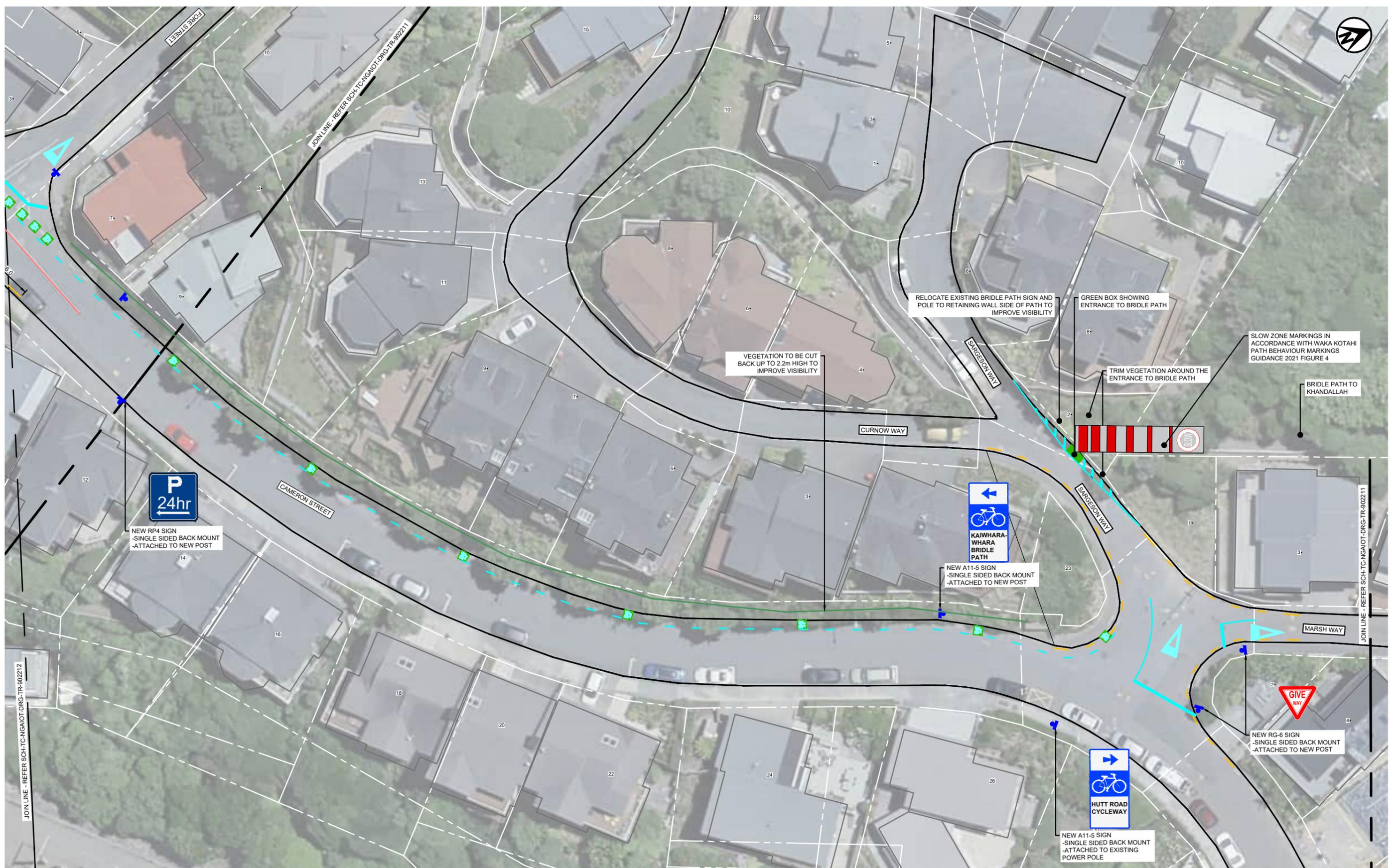
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	 Isthmus.
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C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

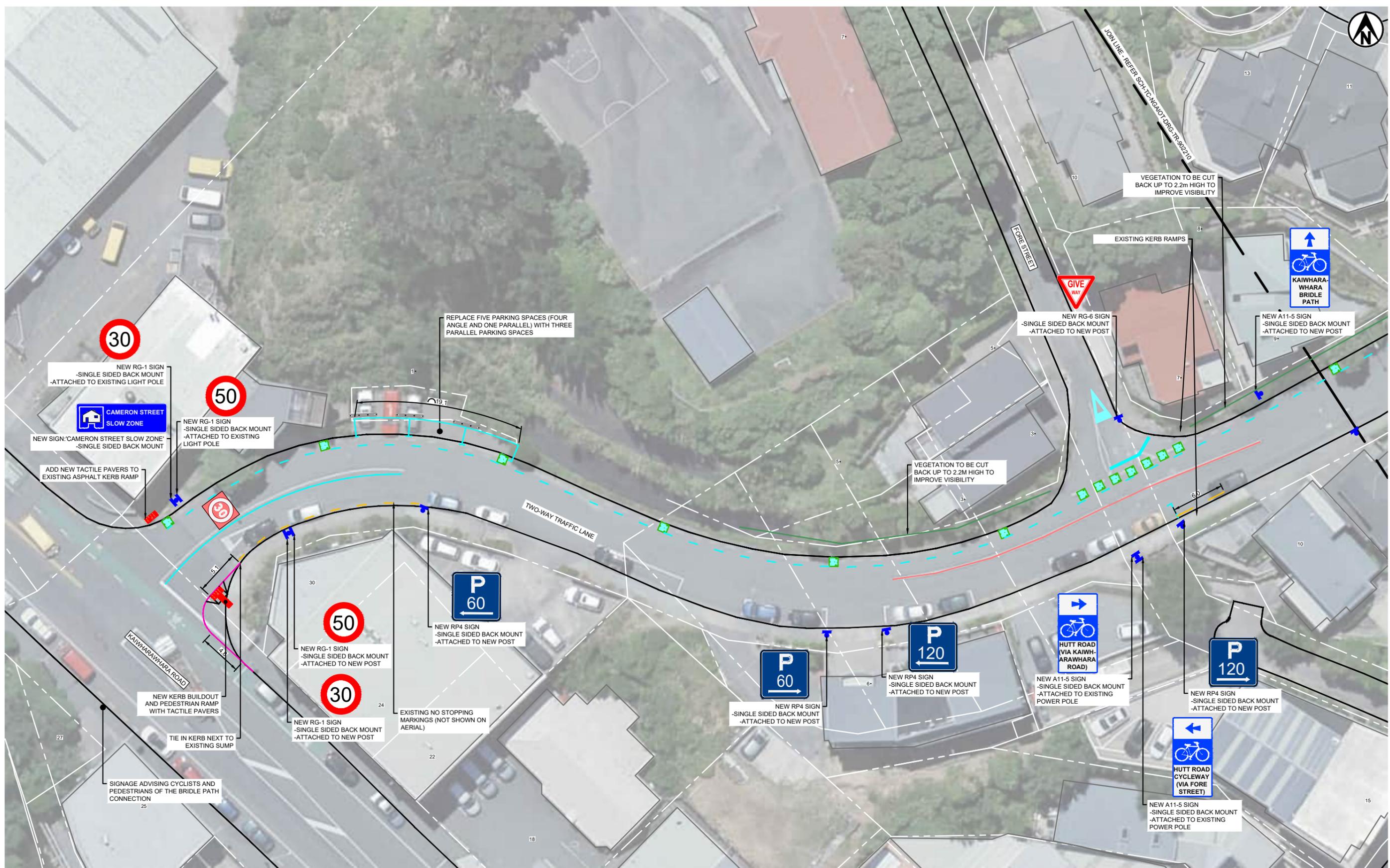
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DESIGN STAGE	GATE
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A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
21/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	CAMERON STREET	
	GENERAL ARRANGEMENT PLAN	
	SHEET 1	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902210	C04

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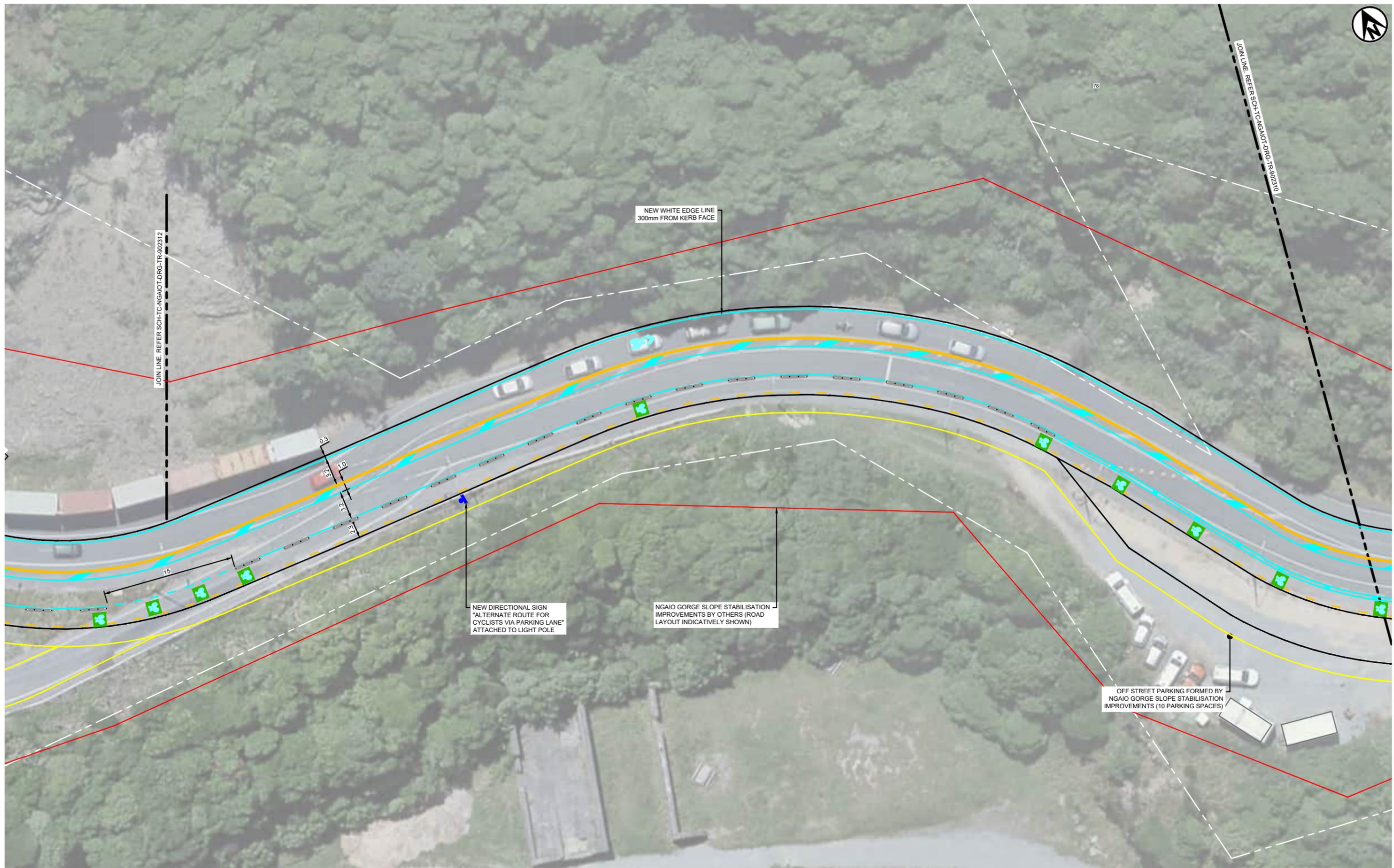


CLIENT		ORIGINATOR		DESIGN STAGE		GATE		PROJECT:	
Absolutely Positively Wellington City Council		abley		DETAILED DESIGN		TR		TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	
Bankin-Taylor Isthmus.		REVISIONS		DRAWING STATUS		DRAWING INFORMATION		TITLE:	
LAST SAVED BY: CHLI(2022-10-20) LAST PLOTTED: 2022-10-20		C01 30% DETAILED DESIGN C.LI A.HEAD 25/ 07/ 2022		NOT FOR CONSTRUCTION		APPROVED BY: A.HEAD DESIGNED: S.RODENBURG		CAMERON STREET	
FILE LOCATION:		C02 90% DETAILED DESIGN C.LI A.HEAD 09/ 09/ 2022		DRAWING SCALE: 1:200		DES. CHECK: A.HEAD		GENERAL ARRANGMENT PLAN	
PRINTED COPIES OF THIS DOCUMENT ARE UNCONTROLLED		C03 TRAFFIC RESOLUTION ISSUE C.LI J.WARD 07/ 10/ 2022		SCALE 0 5 10 m		DRAWN: C.LI		SHEET 2	
		C04 TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING C.LI A.HEAD 21/ 10/ 2022				DRG. CHECK: H.BENDLE		DOCUMENT:	
		REV. DESCRIPTION DRAWN BY APRVD. BY DATE						SCH - TC - NGAIO - DRG - TR - 902211	
		DRAWING INFORMATION						C04	
		DRAWING INFORMATION							

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE	0 5 10 m
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DESIGN STAGE	GATE
DETAILED DESIGN	TR
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 2			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902311	C03		

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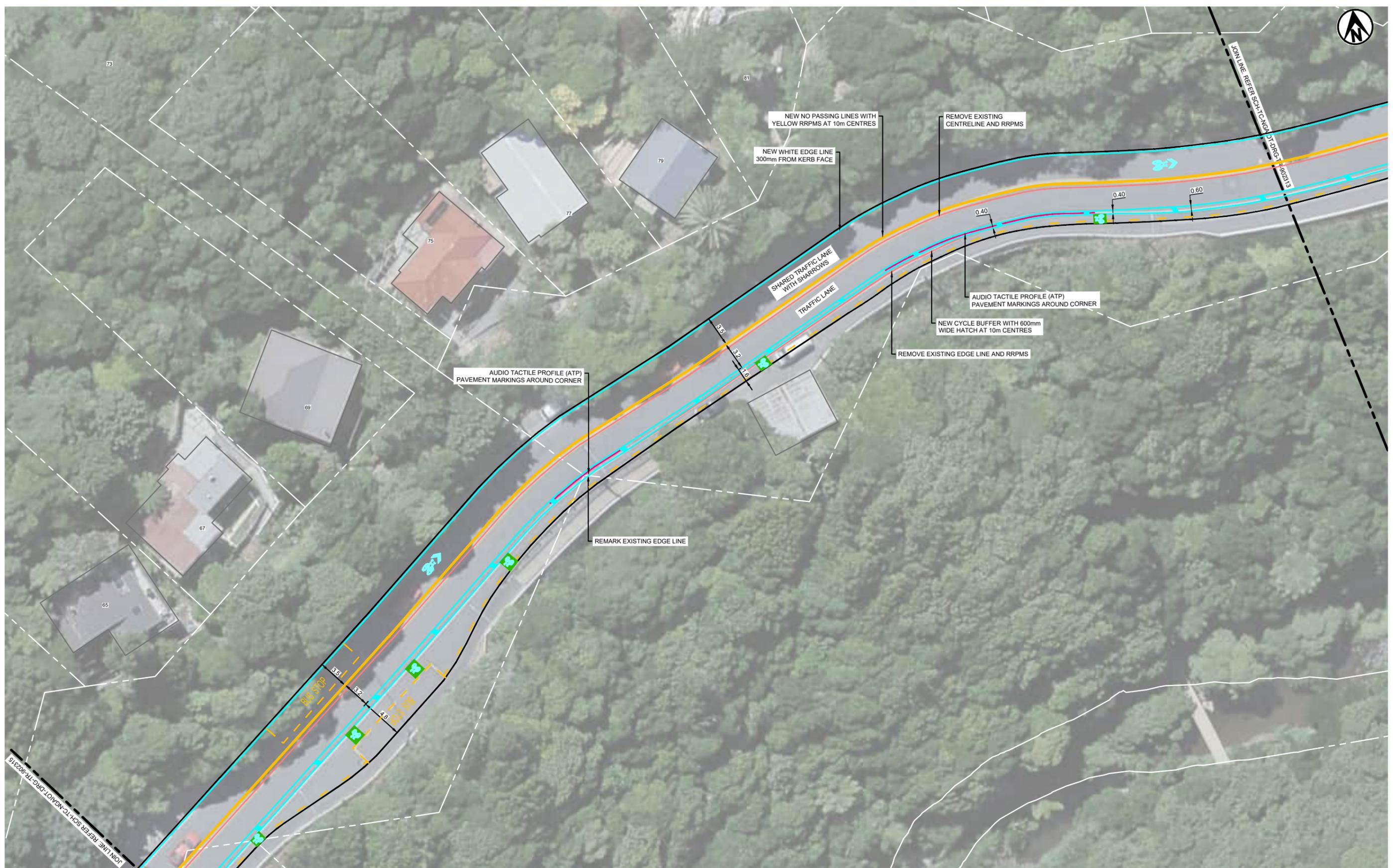
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DESIGN STAGE	GATE
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 5			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902314	C03		

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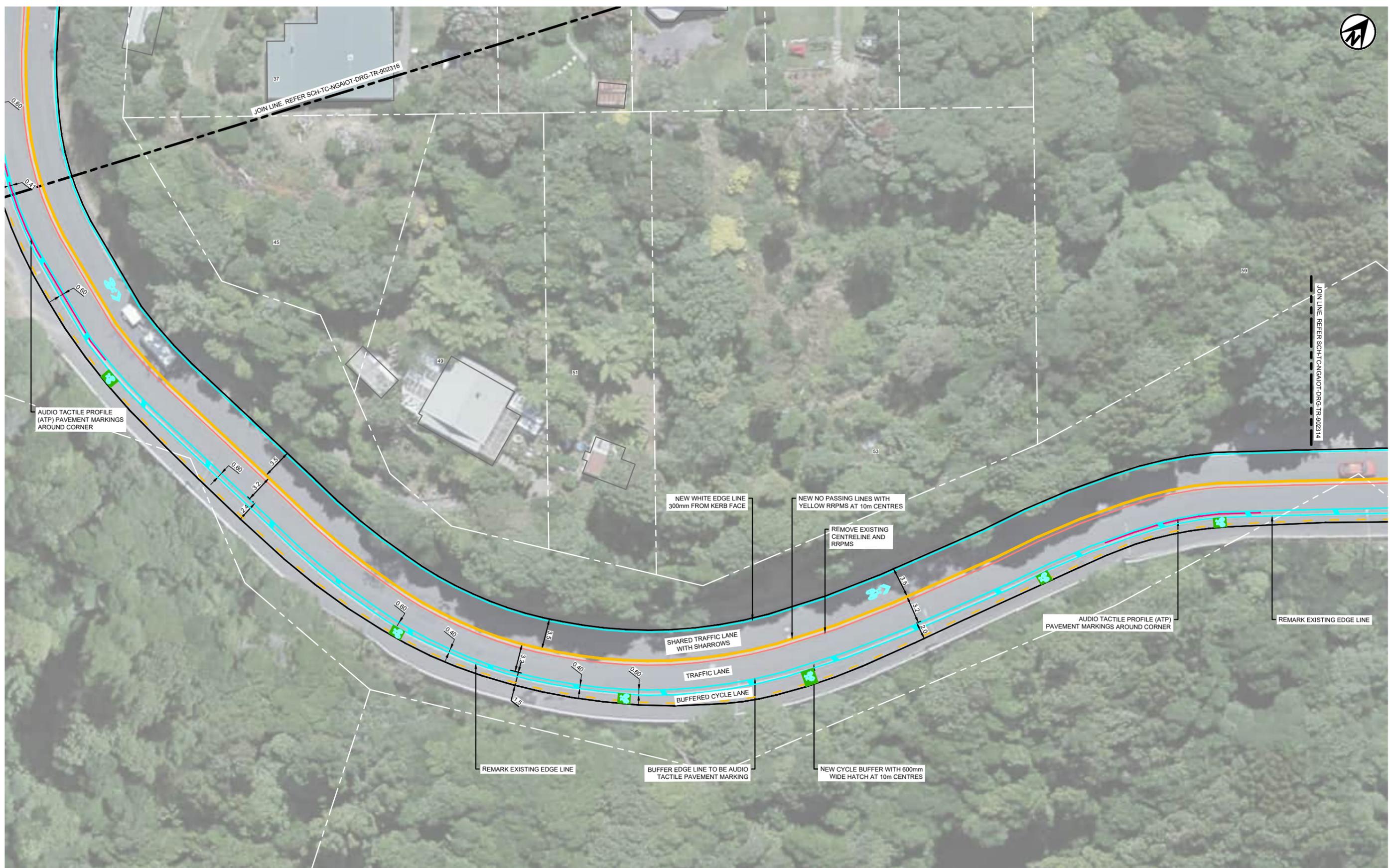
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ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT	ORIGINATOR					DESIGN STAGE	GATE		
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						DRAWING STATUS		TITLE: NGAIO GORGE ROAD GENERAL ARRANGMENT PLAN SHEET 6	
						NOT FOR CONSTRUCTION		DOCUMENT: SCH - TC - NGAIO T - DRG - TR - 902315	
						APPROVED BY: J.WARD	DESIGNED BY: B.RODENBURG	DES. CHECK: J.WARD	DRAWN: C.LI
						APPROVED DATE: 07/ 10/ 2022	DRG. CHECK: H.BENDLE	REVISION: C03	
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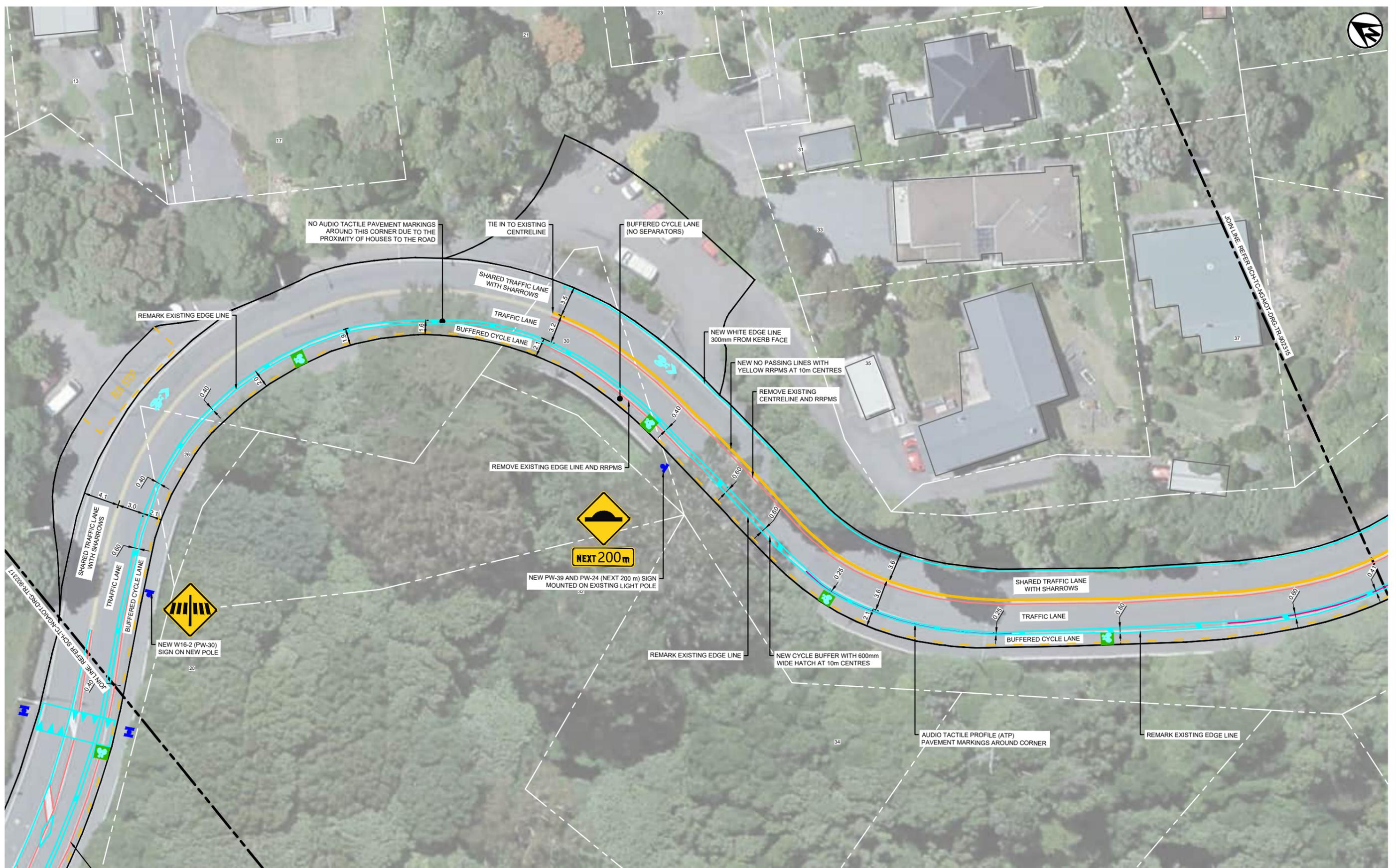
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C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE	0 5 10 m
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DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	NGAIO GORGE ROAD	
	GENERAL ARRANGEMENT PLAN	
	SHEET 7	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902316	C03

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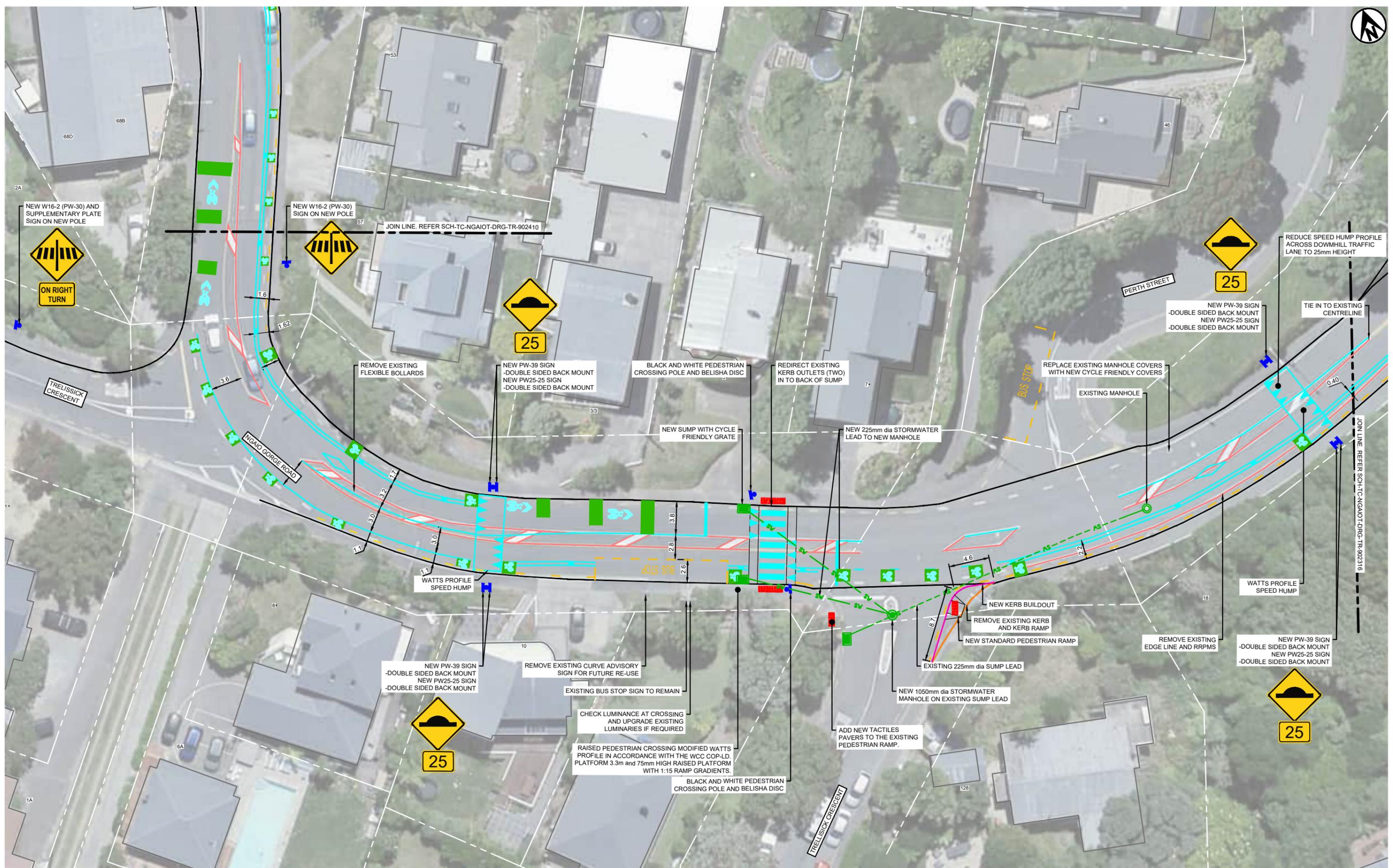
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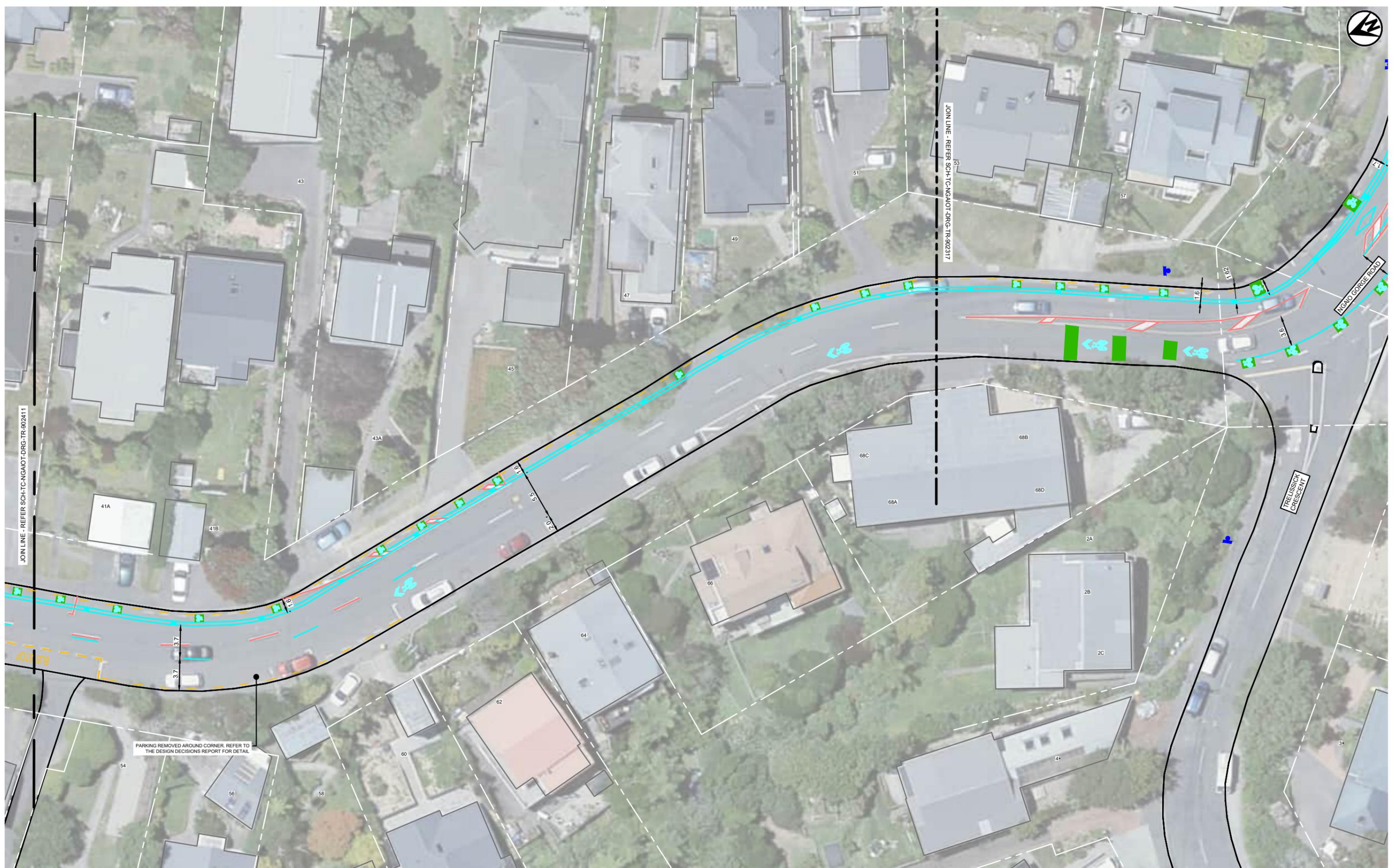
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CLIENT		ORIGINATOR		DESIGN STAGE		GATE		PROJECT:	
Absolutely Positively Wellington City Council		abley TTT Tonkin + Taylor Isthmus.		DETAILED DESIGN		TR		NGAIO GORGE ROAD	
LAST SAVED BY: CHLI(2022-10-07) LAST PLOTTED: 2022-10-07		PRINTED COPIES OF THIS DOCUMENT ARE UNCONTROLLED		DRAWING STATUS		NOT FOR CONSTRUCTION		TITLE:	
FILE LOCATION:		SCALE		DRAWING INFORMATION		DRAWING INFORMATION		DOCUMENT:	
		0 5 10 m 1:200		APPROVED BY: J.WARD APPROVED DATE: 07/10/2022		DESIGNED: B.RODENBURG DES. CHECK: J.WARD DRAWN: C.LI DRG. CHECK: H.BENDLE		NGAIO GORGE ROAD GENERAL ARRANGEMENT PLAN SHEET 8	
		REVISIONS		DRAWING SCALE: 1:200				SCH - TC - NGAIoT - DRG - TR - 902317	
		C01 30% DETAILED DESIGN C02 90% DETAILED DESIGN C03 TRAFFIC RESOLUTION ISSUE		DRAWN BY: APRVD. BY: DATE				C03	

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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DESIGN STAGE	GATE
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DRAWING STATUS

NOT FOR CONSTRUCTION

APPROVED BY:	DESIGNED:	DESIGNED BY:
J.WARD	B.RODENBURG	J.WARD

APPROVED DATE:	DRAWN:	DRG. CHECK:
07/ 10/ 2022	C.LI	H.BENDLE

PROJECT:	TITLE:	DOCUMENT:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	KENYA STREET GENERAL ARRANGEMENT SHEET 1	SCH - TC - NGAIoT - DRG - TR - 902410

ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
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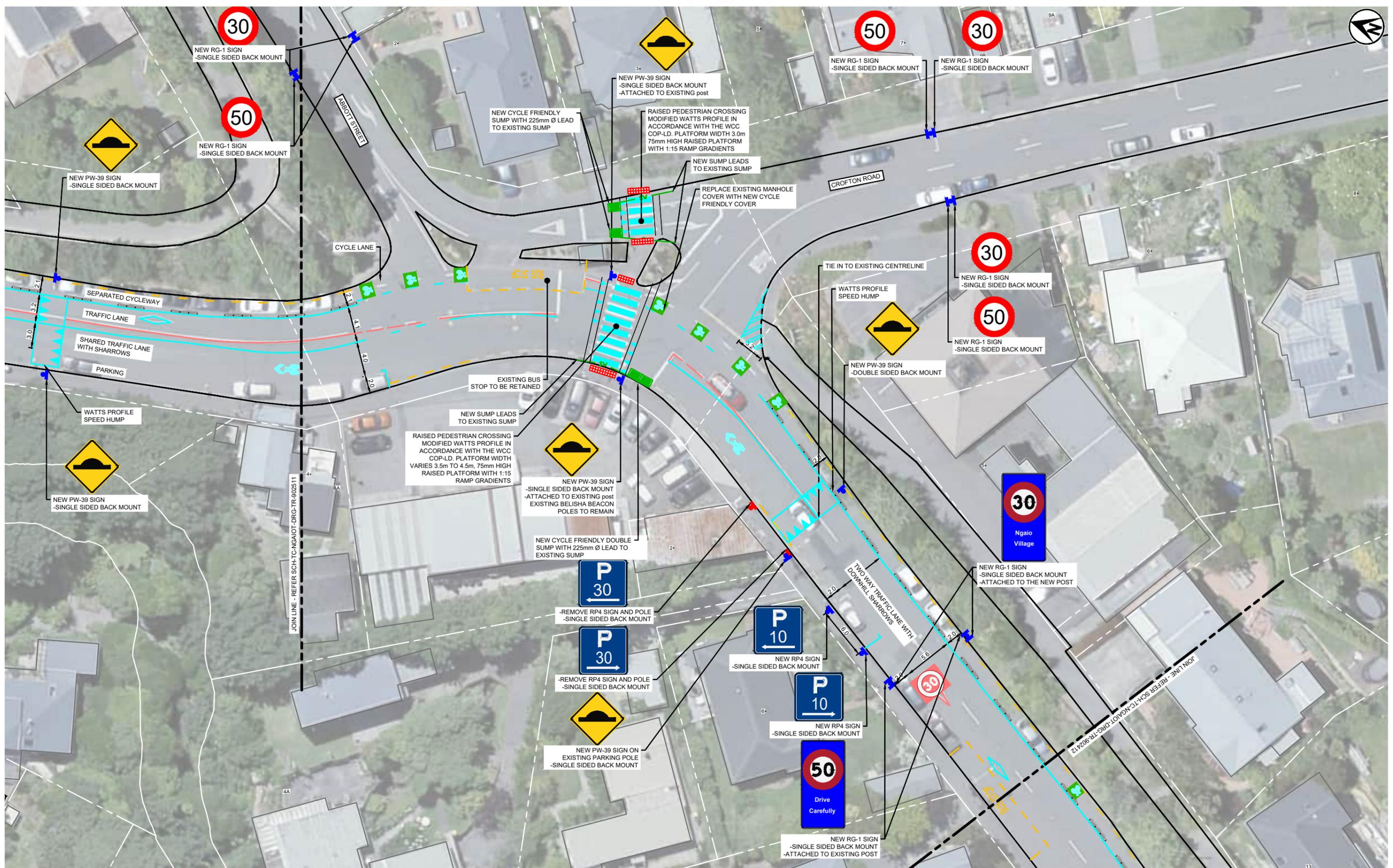
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C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE	0 5 10 m
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DESIGN STAGE	GATE
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	CROFTON ROAD			
	GENERAL ARRANGEMENT			
	SHEET 1			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902510	C03		

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07

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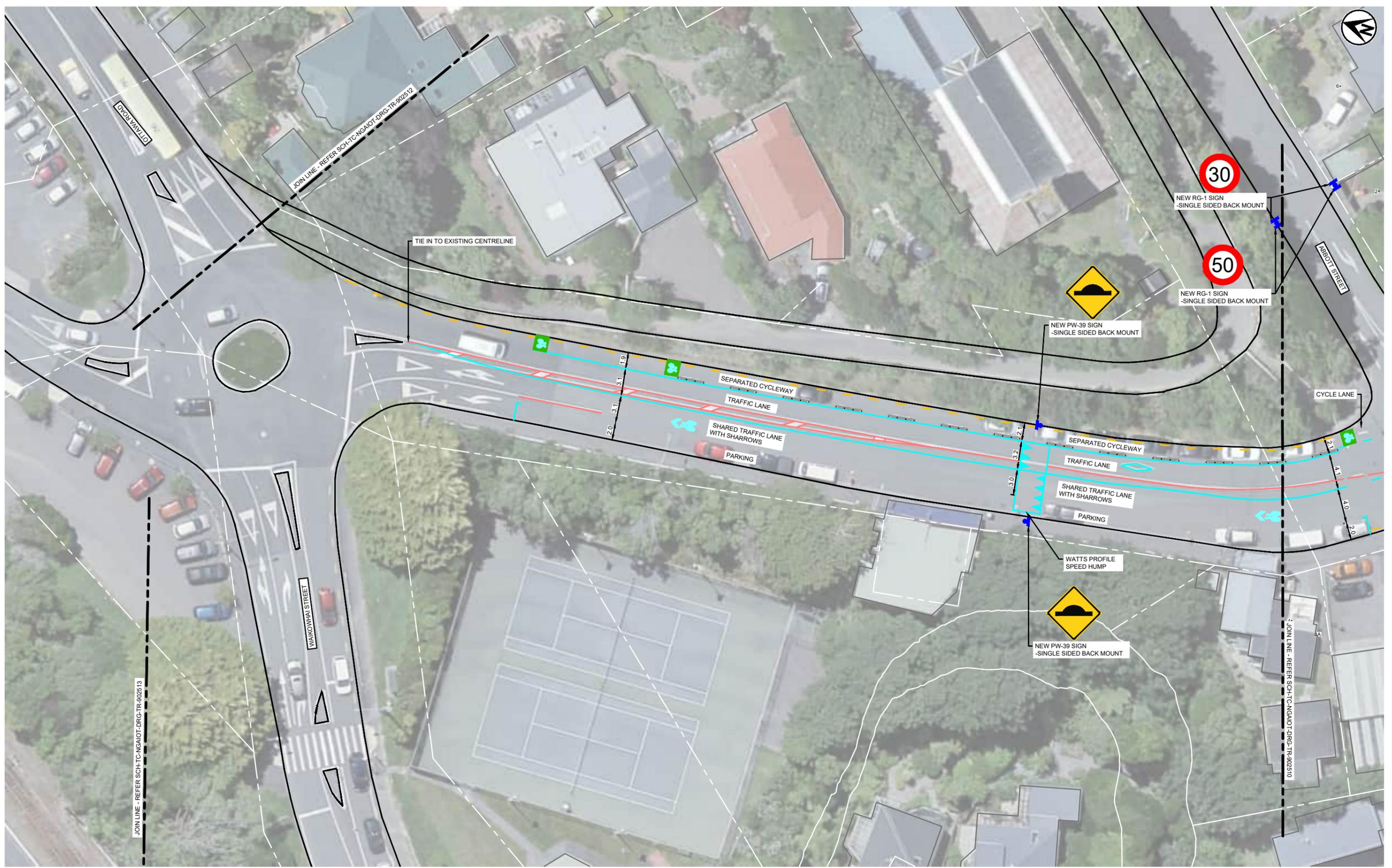
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C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DESIGN STAGE	GATE
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	CROFTON ROAD	
	GENERAL ARRANGEMENT	
	SHEET 2	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902511	C03

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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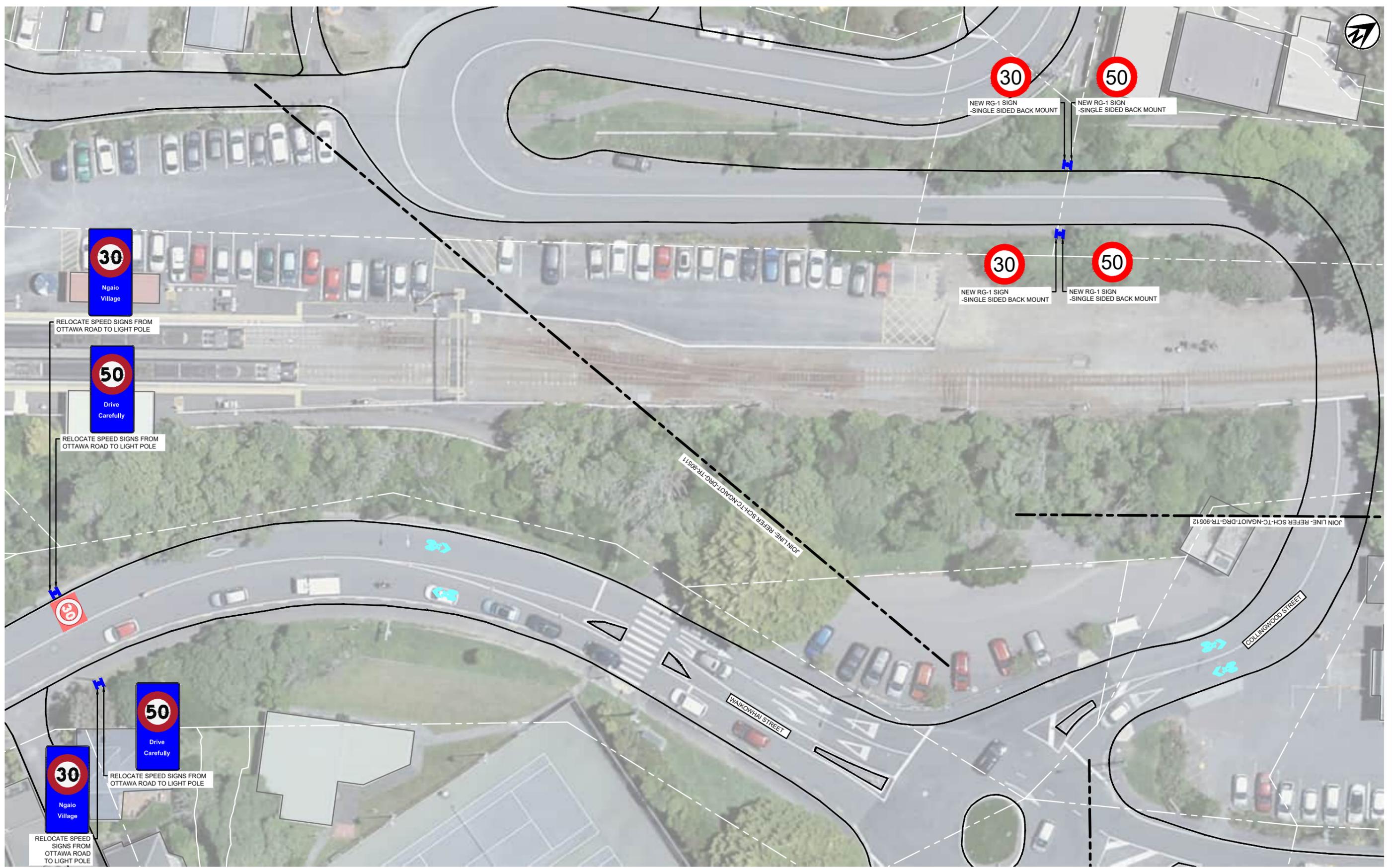
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DESIGN STAGE	GATE
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DRAWING STATUS	
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APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	CROFTON ROAD			
	GENERAL ARRANGEMENT			
	SHEET 4			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902513	C03		

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07

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FILE LOCATION:

We are proposing a change in your area

TR180-00 Ngaio Transitional Cycleway.

Kia ora,

This leaflet is to let you know about a change we are proposing to make in your neighbourhood.

Legal Description:

Delete from Schedule A (Time limited) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	P30	South side, commencing 98 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 5.5 metres.
Kaiwharawhara Road	P30	South side, commencing 108 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 5.5 metres.
Kaiwharawhara Road	P30, Monday - Sunday, 9:00am - 6:00pm	North side, commencing 6 metres from its intersection with Cameron Street and extending in an easterly direction following the northern kerbline for 32 metres.
Cameron Street	P60	North side, commencing 30 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates x= 1749805.8 m, y= 5430949.9 m), and extending in a south-easterly direction following the northern kerbline for 19 metres. (4 angle and 1 parallel parking space)
Kenya Street	P30	East side, commencing 16 metres south of its intersection with Crofton Rd and extending in a southerly direction for 7 metres

Add to Schedule A (Time limited) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
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We are proposing a change in your area

Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 23m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 8 metres.
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 58 metres.
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 128.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 20.5 metres.
Kaiwharawhara Road (Stage 1, 2023)	P30, Monday - Sunday, 7:00pm - 4:00pm	West side, commencing 531 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 23.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 23m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 8 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 58 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 128.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 20.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	P30, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 531 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 23.5 metres.

We are proposing a change in your area

Kaiwharawhara Road (Stage 1 and 2, 2023 and 2024)	P30, Monday - Sunday, 9:00am - 6:00pm	East side, commencing 8.5 metres southeast of its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in a southeasterly direction following the eastern kerbline for 32 metres.
Kaiwharawhara Road (Stage 3, 2025)	P30, Monday - Sunday, 9:00am - 6:00pm	East side, commencing 25.5 metres southeast of its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in a southeasterly direction following the eastern kerbline for 15 metres.
Cameron Street	P60	South side, commencing 31 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 48 metres.
Cameron Street	P120	South side, commencing 84 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 36 metres.
Cameron Street	P24hr	South side, commencing 144 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m) and extending in an easterly direction following the southern kerbline for 367 metres.
Cameron Street	P24hr	North Side, commencing 75m east of its intersection with Marsh Way (Grid coordinates x=1750015.56 y=5431061.32) and extending in an easterly direction following the northern kerb line for 146 metres.
Cameron Street	P60	North side, commencing 30 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates x= 1749805.8 m, y= 5430949.9 m), and extending in a south-easterly direction following the northern kerbline for 19 metres.
Kenya Street	P10	North side, commencing 38.5 metres southwest of its intersection with Crofton Road (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a southwesterly direction following the northern kerbline for 6 metres.

We are proposing a change in your area

Absolutely Positively
Wellington City Council
Me Heke Ki Pōneke

We are proposing a change in your area

Delete from Schedule B (Class restricted) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	Bus Stop – At All Times	South side 296.5m from its intersection with Hutt Road and extending in a westerly direction for 12m
Kenya Street	Bus Stop – At All Times	East side, commencing 190 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 24.5 metres.
Kenya Street	Bus Stop – At All Times	West side, commencing 148.5 metres north of its intersection with Trelissick Cres and extending in a northerly direction following the western kerbline for 14.5 metres.

Add to Schedule B (Class restricted) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	Bus Stop – At All Times	West side, commencing 50 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 11 metres.
Kaiwharawhara Road	Bus Stop – At All Times	West side, commencing 289 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 11.5 metres.
Kaiwharawhara Road (Stage 1, 2023)	Loading Zone P10, Monday – Sunday, 7:00pm – 4:00pm	West side, commencing 488.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 21.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	Loading Zone P10, Monday - Sunday, 9:00pm - 2:00pm	West side, commencing 488.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 21.5 metres.

We are proposing a change in your area

Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 134.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 15 metres.</i>
Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 364 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 13 metres.</i>
Ngaio Gorge Road	<i>Bus Stop – At All Times</i>	<i>South side, commencing 14 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 15 metres.</i>
Kenya Street	<i>Bus Stop – At All Times</i>	<i>North side, commencing 143.5 metres north of its intersection with Trelissick Crescent (Grid Coordinates X = 1748383.625m, Y = 5431327.364m) and extending in a northerly direction following the northern kerbline for 15 metres.</i>
Kenya Street	<i>Bus Stop – At All Times</i>	<i>South side, commencing 178.5 metres north of its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 14.5 metres.</i>

We are proposing a change in your area

Add to Schedule C (Direction, Placement and Lane Use) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 1, 2023)	Clearway, Monday to Friday, 4:00pm - 7:00pm	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 2, 2024)	Clearway, Monday to Friday, 2:00pm - 9:00pm	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.

We are proposing a change in your area

Delete from Schedule D (No Stopping) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road	No Stopping – At All Times	West side, commencing 43.5 metres north of its intersection with Hutt Road (Grid coordinates x= 1,749,909.8 m, y= 5,430,820.5m), and extending in a northerly direction following the western kerb line for 7.5 metres.
Kaiwharawhara Road	No Stopping – At All Times	South side, commencing at a point 580 metres west of its intersection with Hutt Road and extending in a westerly direction following the southern kerbline for 154 metres.
Kaiwharawhara Road	No Stopping – At All Times	West side, commencing 149.5 metres north of its intersection with Hutt Road (Grid coordinates x= 1,749,913.1m, y= 5,430,822.8 m), and extending in a northerly direction following the western kerb line for 8.5 metres."
Kaiwharawhara Road	No Stopping – At All Times	South side 308.5m from its intersection with Hutt Road and extending in a westerly direction for 105.5m
Cameron Street	No Stopping – At All Times	Southeast side, commencing from its intersection with Kaiwharawhara Road (Grid Coordinates X=1,749,813.33m Y=5,430,941.57m) and extending in a north-easterly direction, following the southeastern kerb line for 18 metres
Cameron Street	No Stopping – At All Times	North Side, commencing 43.67 metres east of its intersection with Marsh Way (Grid coordinates x=1750011.04 y=5431057.11) and extending in an easterly direction following the northern kerb line for 31 metres.
Ngaio Gorge Road	No Stopping – At All Times	Northwest side, commencing at its intersection with Ngaio Gorge Road and extending in a southwesterly direction following the northwestern kerbline for 24.5 metres.
Ngaio Gorge Road	No Stopping – At All Times	Southeast side, commencing at its intersection with Ngaio Gorge Road and extending in a southwesterly direction following the southeastern kerbline for 24.5 metres.

We are proposing a change in your area

Kenya Street	<i>No Stopping – At All Times</i>	<i>West side, commencing from its intersection with Crofton Road and extending in a southerly direction following the western kerbline for 26 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing from its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 33.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing 97.5 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 9.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>East side, commencing 127.5 metres north of its intersection with Ngaio Gorge Road and extending in a northerly direction following the eastern kerbline for 24 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>Northeast side, commencing 6 metres from its intersection with Kenya Street and extending in a northwesterly direction following the northeastern kerbline for 14 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Kenya Street and extending in a northerly direction following the western kerbline for 3 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>South side, commencing 5.5 metres west of its intersection with Kenya Street and extending in a westerly direction following the western kerbline for 12 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Abbott St and extending in an westerly direction following the northern kerbline for 7 metres.</i>

Add to Schedule D (No Stopping) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction</i>

We are proposing a change in your area

		following the western kerbline for 23 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 31m northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 19 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 119 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 9.5 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 149 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 140 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 187.5 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	West side, commencing 554.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 146.5 metres.
Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.

We are proposing a change in your area

Kaiwharawhara Road (Stage 3, 2025)	No Stopping – At All Times	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Kaiwharawhara Road (Stages 1 and 2, 2023 to 2024)	No Stopping – At All Times	East side, commencing at its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in a southeasterly direction following the eastern kerbline for 6 metres.
Kaiwharawhara Road Stage 3, 2025)	No Stopping – At All Times	East side, commencing at its intersection with Cameron Street (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in a southeasterly direction following the eastern kerbline for 25.5 metres.
Cameron Street	No Stopping – At All Times	South side, commencing 130 metres east of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in an easterly direction following the southern kerbline for 6 metres.
Cameron Street	No Stopping – At All Times	South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749812.587m, Y = 5430942.588m and extending in an easterly direction following the southern kerbline for 31 metres.
Cameron Street	No Stopping – At All Times	North side, commencing at its intersection with Marsh Way (Grid coordinates x=1750011.04 y=5431057.11) and extending in an easterly direction following the northern kerb line for 75 metres.
Cameron Street	No Stopping – At All Times	North Side, commencing 221m east of its intersection with Marsh Way (Grid coordinates x=1750015.56 y=5431061.32) and extending in an easterly direction following the northern kerb line for 35 metres.
Marsh Way	No Stopping – At All Times	South side, commencing at its intersection with Marsh Way (Grid coordinates x=1750015.56 y=5431061.32) and extending in an easterly direction following the southern kerb line for 8 metres.

We are proposing a change in your area

Marsh Way	<i>No Stopping – At All Times</i>	North side, commencing at its intersection with Sargeson Way (Grid coordinates $x=1750008.14$ $y=5431066.07$) and extending in an easterly direction following the northern kerb line for 12 metres.
Sargeson Way	<i>No Stopping – At All Times</i>	South side, commencing at its intersection with Marsh Way (Grid coordinates $x=1750011.04$ $y=5431057.11$) and extending in a westerly direction following the southern kerb line for 27 metres.
Sargeson Way	<i>No Stopping – At All Times</i>	North side, commencing at its intersection with Marsh Way (Grid coordinates $x=1750008.14$ $y=5431066.07$) and extending in a westerly direction following the northern kerb line for 10 metres.
Old Porirua Road	<i>No Stopping – At All Times</i>	North side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates $X = 1749650.973m$, $Y = 5431089.75m$) and extending in a westerly direction following the northern kerbline for 8 metres.
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates $X = 1749066.529m$, $Y = 5431182.391m$) and extending in a westerly direction following the southern kerbline for 133.5 metres.
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	South side, commencing 149.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates $X = 1749066.529m$, $Y = 5431182.391m$) and extending in a westerly direction following the southern kerbline for 214 metres.
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	South side, commencing at its intersection with Trelissick Crescent (Grid Coordinates $X = 1748458.814m$, $Y = 5431272.233m$) and extending in an easterly direction following the southern kerbline for 432 metres.
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	South side, commencing 29 metres west of its intersection with Trelissick Crescent (Grid Coordinates $X = 1748444.224m$, $Y = 5431277.491m$) and extending in a westerly direction following the southern kerbline for 36 metres.
Ngaio Gorge Road	<i>No Stopping – At All Times</i>	North side, commencing at its intersection with Kenya Street (Grid Coordinates $X = 1748394.214m$, $Y = 5431321.551m$) and extending in an easterly direction following the northern kerbline for 83 metres.

We are proposing a change in your area

Kenya Street	<i>No Stopping – At All Times</i>	<i>North side, commencing 107.5 metres north of its intersection with Trelissick Crescent (Grid Coordinates X = 1748383.625m, Y = 5431327.364m) and extending in a northerly direction following the northern kerbline for 36 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>North side, commencing at its intersection with Crofton Road (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a westerly direction following the northern kerbline for 38 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 178.5 metres.</i>
Kenya Street	<i>No Stopping – At All Times</i>	<i>South side, commencing at its intersection with Crofton Road (Grid Coordinates X = 1748587.606m, Y = 5431757.751m) and extending in a southwesterly direction following the southern kerbline for 300 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>West side, commencing at its intersection with Kenya Street (Grid Coordinates X = 1748584.45m, Y = 5431770.779m) and extending in a northerly direction following the western kerbline for 26 metres.</i>
Crofton Road	<i>No Stopping – At All Times</i>	<i>East side, commencing at its intersection with Ottawa Road (Grid Coordinates X = 1748563.806m, Y = 5431918.344m) and extending in a southeasterly direction following the eastern kerbline for 115 metres.</i>

We are proposing a change in your area

Add to Schedule G (Give Way and Stop) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
<i>Marsh Way</i>	<i>Give Way</i>	<i>West bound, at its intersection with Sargeson Way.</i>
<i>Marsh Way</i>	<i>Give Way</i>	<i>South bound, at its intersection with Cameron Street.</i>
<i>Fore Street</i>	<i>Give Way</i>	<i>South bound, at its intersection with Cameron Street.</i>

We are proposing a change in your area

Add to Schedule H (Pedestrian crossing) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
<i>Ngaio Gorge Road</i>	<i>Pedestrian Crossing</i>	<i>South side, commencing 7.5 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 3.5 metres.</i>

We are proposing a change in your area

Add to Schedule I (Cycle lane) of the Traffic Restrictions Schedule

Column One	Column Two	Column Three
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing at its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 50 metres.
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing 61 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 228 metres.
Kaiwharawhara Road (Stage 3, 2025)	Cycle Lane	West side, commencing 300.5 metres northwest of its intersection with Hutt Road (Grid Coordinates X = 1749912.06m, Y = 5430822.44m and extending in a northwesterly direction following the western kerbline for 390.5 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing at its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 133.5 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing 149.5 metres west of its intersection with Kaiwharawhara Road (Grid Coordinates X = 1749066.529m, Y = 5431182.391m) and extending in a westerly direction following the southern kerbline for 214 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing at its intersection with Trelissick Crescent (Grid Coordinates X = 1748458.814m, Y = 5431272.233m) and extending in an easterly direction following the southern kerbline for 432 metres.
Ngaio Gorge Road	Cycle Lane	South side, commencing 29 metres west of its intersection with Trelissick Crescent (Grid Coordinates X = 1748444.224m, Y = 5431277.491m) and extending in a westerly direction following the southern kerbline for 36 metres.

We are proposing a change in your area

Ngaio Gorge Road	Cycle Lane	North side, commencing at its intersection with Kenya Street (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in an easterly direction following the northern kerbline for 26 metres.
Kenya Street	Cycle Lane	South side, commencing at its intersection with Ngaio Gorge Road (Grid Coordinates X = 1748394.214m, Y = 5431321.551m) and extending in a northerly direction following the southern kerbline for 178.5 metres.
Kenya Street	Cycle Lane	South side, commencing 27.5 metres southwest of its intersection with Crofton Road (Grid Coordinates X = 1748587.606m, Y = 5431757.751m) and extending in a southwesterly direction following the southern kerbline for 272.5 metres.
Crofton Road	Cycle Lane	East side, commencing at its intersection with Ottawa Road (Grid Coordinates X = 1748563.806m, Y = 5431918.344m) and extending in a southeasterly direction following the eastern kerbline for 115 metres.

Prepared By:

?

(?)

Approved By:

Zackary Moodie

**(Team Leader Transport
Engineering)**

Date:

From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ann-Marie Head](#); [Ryan Dunn](#)
Subject: RE: Draft Ngaio Parking Management Plan (traffic resolution issue)
Date: Tuesday, 18 October 2022 2:18:18 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image006.png](#)
[image007.jpg](#)

Hi Jonathan,

[REDACTED]

Ideally there won't be any changes to the parking as a result of the TR feedback. In this case we just remove the draft stamp.

If feedback results in changes to the parking then we think it would be better to update the PMP to match the parking being implemented on site.

So in both cases it would be finalised with the 100% design plans. Do you agree?

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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Level 4, 2 Hunter Street, Wellington 6011, PO Box 2083, Wellington

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Tuesday, 18 October 2022 1:55 pm
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Cc: Ann-Marie Head <ann-marie.head@abley.com>; Ryan Dunn <RDunn@tonkintaylor.co.nz>
Subject: RE: Draft Ngaio Parking Management Plan (traffic resolution issue)

Thanks Billy,

How do you intend this going from Draft to final?

[REDACTED]



Cheers
Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>
Sent: Tuesday, October 18, 2022 11:34 AM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Cc: Ann-Marie Head <ann-marie.head@abley.com>; Ryan Dunn <RDunn@tonkintaylor.co.nz>
Subject: Draft Ngaio Parking Management Plan (traffic resolution issue)

Hi Jonathan

Please find attached the draft Parking Management Plan for the Ngaio Transitional Cycleway traffic resolution.

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Ann-Marie Head](#); [Claire Pascoe](#)
Subject: RE: Final drawings for Ngaio - URGENT
Date: Friday, 28 October 2022 3:32:00 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[SCH-TC-NGAIOT-DRG-TR-combined - Updated incl Kaiwharawhara staging_Optimized.pdf](#)

Hi Jonathan,

Updated drawing set attached.

We also realised that the header on each page (example below) has updated on all the drawings due to the additional staging plans now added (the number of sheets increased from 32 to 42, and numbering changed for the later sections). This might require your screenshots in the TR report to be updated.



Hopefully everything shows correctly and you are happy with the position of the watermarks for the Stage 3 plans. Let me know if anything further is required.

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

Tonkin + Taylor - Exceptional thinking together

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Friday, 28 October 2022 12:28 pm
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Cc: Ann-Marie Head <ann-marie.head@abley.com>; Claire Pascoe <claire.pascoe@wcc.govt.nz>
Subject: Final drawings for Ngaio - URGENT

Hi Billy,

I trust the review of the Ngaio drawings are almost complete?

These drawings need to be sent to councillors today, and will go out for public consultation at 9am on Monday.

Will you be able to send them shortly?

Cheers
Jonathan

Jonathan Kennett
Project Lead – Transitional Cycleways | Wellington City Council
E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |  

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TRANSITIONAL CYCLEWAYS



NGAIO TRANSITIONAL TRAFFIC RESOLUTION DESIGN 07/10/2022

**Absolutely Positively
Wellington City Council**
Me Heke Ki Pōneke

abley
Tonkin+Taylor Isthmus.

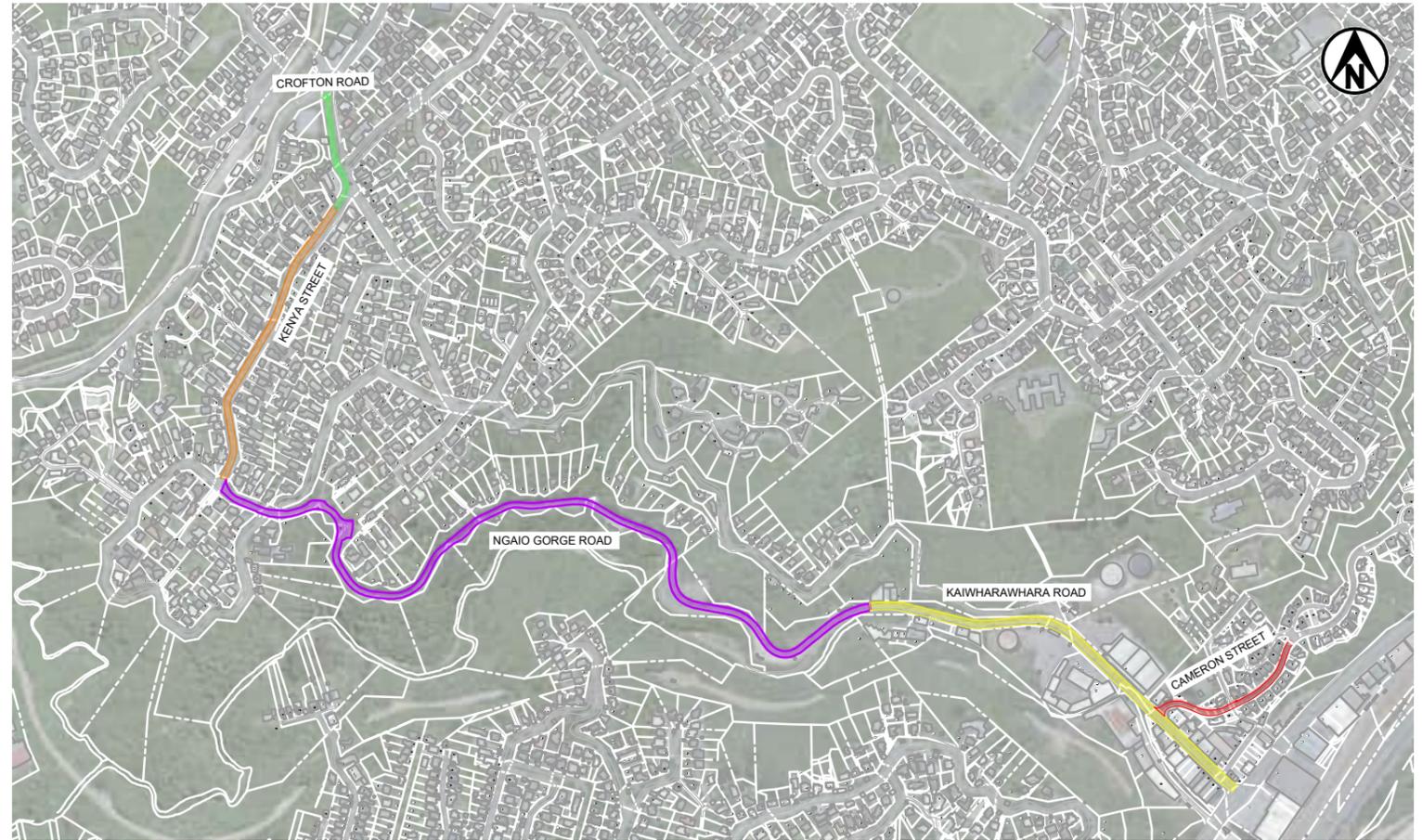
TRANSITIONAL CYCLEWAY
NGAIO TRANSITIONAL
TRAFFIC RESOLUTION ISSUE

DRAWING

- GENERAL**
- SCH-TC-NGAIO-DRG-TR-902000
 - SCH-TC-NGAIO-DRG-TR-902010
 - SCH-TC-NGAIO-DRG-TR-902011
- KAIWHARAWHARA ROAD**
- SCH-TC-NGAIO-DRG-TR-902100
 - SCH-TC-NGAIO-DRG-TR-902110
 - SCH-TC-NGAIO-DRG-TR-902111
 - SCH-TC-NGAIO-DRG-TR-902112
 - SCH-TC-NGAIO-DRG-TR-902113
 - SCH-TC-NGAIO-DRG-TR-902114
 - SCH-TC-NGAIO-DRG-TR-902120
 - SCH-TC-NGAIO-DRG-TR-902121
 - SCH-TC-NGAIO-DRG-TR-902122
 - SCH-TC-NGAIO-DRG-TR-902123
 - SCH-TC-NGAIO-DRG-TR-902124
 - SCH-TC-NGAIO-DRG-TR-902130
 - SCH-TC-NGAIO-DRG-TR-902131
 - SCH-TC-NGAIO-DRG-TR-902132
 - SCH-TC-NGAIO-DRG-TR-902133
 - SCH-TC-NGAIO-DRG-TR-902134
- CAMERON STREET**
- SCH-TC-NGAIO-DRG-TR-902200
 - SCH-TC-NGAIO-DRG-TR-902210
 - SCH-TC-NGAIO-DRG-TR-902211
 - SCH-TC-NGAIO-DRG-TR-902212
 - SCH-TC-NGAIO-DRG-TR-902213
- NGAIO GORGE ROAD**
- SCH-TC-NGAIO-DRG-TR-902300
 - SCH-TC-NGAIO-DRG-TR-902310
 - SCH-TC-NGAIO-DRG-TR-902311
 - SCH-TC-NGAIO-DRG-TR-902312
 - SCH-TC-NGAIO-DRG-TR-902313
 - SCH-TC-NGAIO-DRG-TR-902314
 - SCH-TC-NGAIO-DRG-TR-902315
 - SCH-TC-NGAIO-DRG-TR-902316
 - SCH-TC-NGAIO-DRG-TR-902317
- KENYA STREET**
- SCH-TC-NGAIO-DRG-TR-902400
 - SCH-TC-NGAIO-DRG-TR-902410
 - SCH-TC-NGAIO-DRG-TR-902411
 - SCH-TC-NGAIO-DRG-TR-902412
- CROFTON ROAD**
- SCH-TC-NGAIO-DRG-TR-902500
 - SCH-TC-NGAIO-DRG-TR-902510
 - SCH-TC-NGAIO-DRG-TR-902511
 - SCH-TC-NGAIO-DRG-TR-902512
 - SCH-TC-NGAIO-DRG-TR-902513

REV TITLE

- C03 PROJECT COVERSHEET
 - C05 DRAWING LIST & KEYPLAN
 - C05 GENERAL NOTES & LEGEND
- SECTION COVERSHEET**
- C03 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 1 - SHEET 1
 - C05 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 1 - SHEET 2
 - C05 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 1 - SHEET 3
 - C05 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 1 - SHEET 4
 - C05 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 1 - SHEET 5
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 2 - SHEET 1
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 2 - SHEET 2
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 2 - SHEET 3
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 2 - SHEET 4
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 2 - SHEET 5
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 3 - SHEET 1
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 3 - SHEET 2
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 3 - SHEET 3
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 3 - SHEET 4
 - C01 KAIWHARAWHARA ROAD - GENERAL ARRANGEMENT PLAN - STAGE 3 - SHEET 5
- CAMERON STREET - SECTION COVERSHEET**
- C04 CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 1
 - C04 CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 2
 - C01 CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 3
 - C01 CAMERON STREET - GENERAL ARRANGEMENT PLAN - SHEET 4
- NGAIO GORGE ROAD - SECTION COVERSHEET**
- C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 1
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 2
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 3
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 4
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 5
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 6
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 7
 - C03 NGAIO GORGE ROAD - GENERAL ARRANGEMENT PLAN - SHEET 8
- KENYA STREET - SECTION COVERSHEET**
- C03 KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
 - C03 KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
 - C03 KENYA STREET - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
- CROFTON ROAD - SECTION COVERSHEET**
- C03 CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 1
 - C03 CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 2
 - C03 CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 3
 - C03 CROFTON ROAD - GENERAL ARRANGEMENT LAYOUT PLAN - SHEET 4



PROJECT OVERVIEW PLAN
 SCALE 1:5000

CLIENT

ORIGINATOR



REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
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C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022
C05	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	28/ 10/ 2022

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SCALE 1:5000			

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED BY: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS					
TITLE:	NGAIO TRANSITIONAL					
DOCUMENT:	DRAWING LIST & KEY PLAN					
ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
SCH - TC - NGAIO	TR - 902010				C05	

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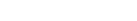
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ORIGINAL SIZE: ISO A1 594mm x 841mm
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150
100
50
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GENERAL NOTES:

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS NOTED OTHERWISE.
2. AERIAL PHOTO SOURCED FROM LINZ DATA SERVICE
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3. CONTOURS SOURCED FROM WELLINGTON CITY COUNCIL
<<https://data-wcc.opendata.arcgis.com/datasets/WCC::wellington-cc-5m-contours-2017/>> , ACCESSED 11/05/2022.
4. PROPERTY BOUNDARIES SOURCED FROM LINZ DATA SERVICE
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6. COORDINATE DATUM: NZGD2000, WELLINGTON CIRCUIT COORDINATES. LEVEL DATUM: LINZ (MSL) WELLINGTON VERTICAL DATUM 1953.
7. EXISTING KERB LINE SOURCED FROM WELLINGTON CITY COUNCIL
<<https://data-wcc.opendata.arcgis.com/datasets/WCC::wcc-kerbs/>> , ACCESSED 11/05/2022.

GENERAL LEGEND

-  PARCEL BOUNDARY
-  HOUSE EXTENT
-  PLOT NUMBER
-  KERB - EXISTING
-  KERB - NEW
-  NEW CYCLIST LANES
-  NEW / EXTENDED RAISED MEDIAN ISLAND
-  NEW WHITE ROAD MARKING
-  NEW WHITE DASH ROAD MARKING
-  NON-STOPPING LINE
-  NON-PASSING LINE
-  EXISTING CONTINUOUS ROAD MARKING - TO BE REMOVED
-  EXISTING DASH ROAD MARKING - TO BE REMOVED
-  NEW KERB BUILDOUT AND PEDESTRIAN RAMP WITH TACTILE PAVERS
-  ROAD MARKING CYCLE
-  CYCLEWAY SEPARATOR WITH BOLLARD 2x BOLLARD
-  ROAD MARKING GIVE WAY - WHITE
-  NEW ROAD SIGN - SINGLE SIDED
-  NEW ROAD SIGN - DOUBLE SIDED
-  EXISTING ROAD SIGN
-  NEW CUSTOM ROAD SIGN
-  ROAD MARKING WARNING - WHITE
-  ROAD MARKING PEDESTRIAN CROSSING - WHITE
-  ROAD MARKING DRAGONS TOOTH (CROSSING) - WHITE
-  AECOM SLOPE STABILIZATION ROAD DESIGN
-  ROAD MARKING SHARROW - WHITE

CLIENT ORIGINATOR

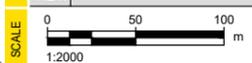


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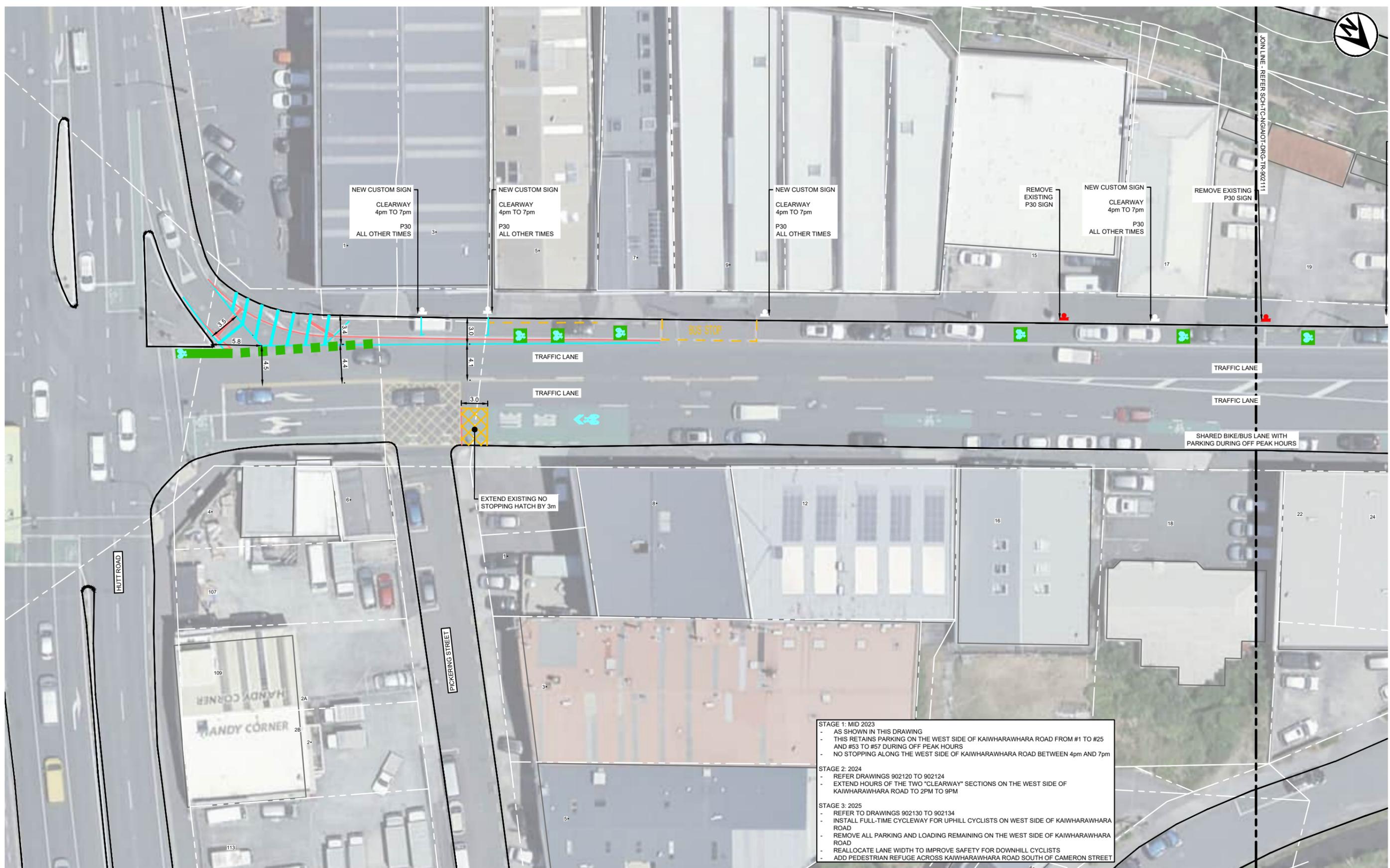


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DETAILED DESIGN	TR
DRAWING STATUS	
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APPROVED BY:	DESIGNED: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TITLE:	DOCUMENT:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	GENERAL NOTES & LEGEND	SCH - TC - NGAIoT - DRG - TR - 902011
ORIGINATOR	PROJECT	WORK STREAM
TYPE	DISCIPLINE	SHEET
REVISION		
		C05

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STAGE 1: MID 2023

- AS SHOWN IN THIS DRAWING
- THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
- NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024

- REFER DRAWINGS 902120 TO 902124
- EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025

- REFER TO DRAWINGS 902130 TO 902134
- INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
- REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
- REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
- ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT	ORIGINATOR
	

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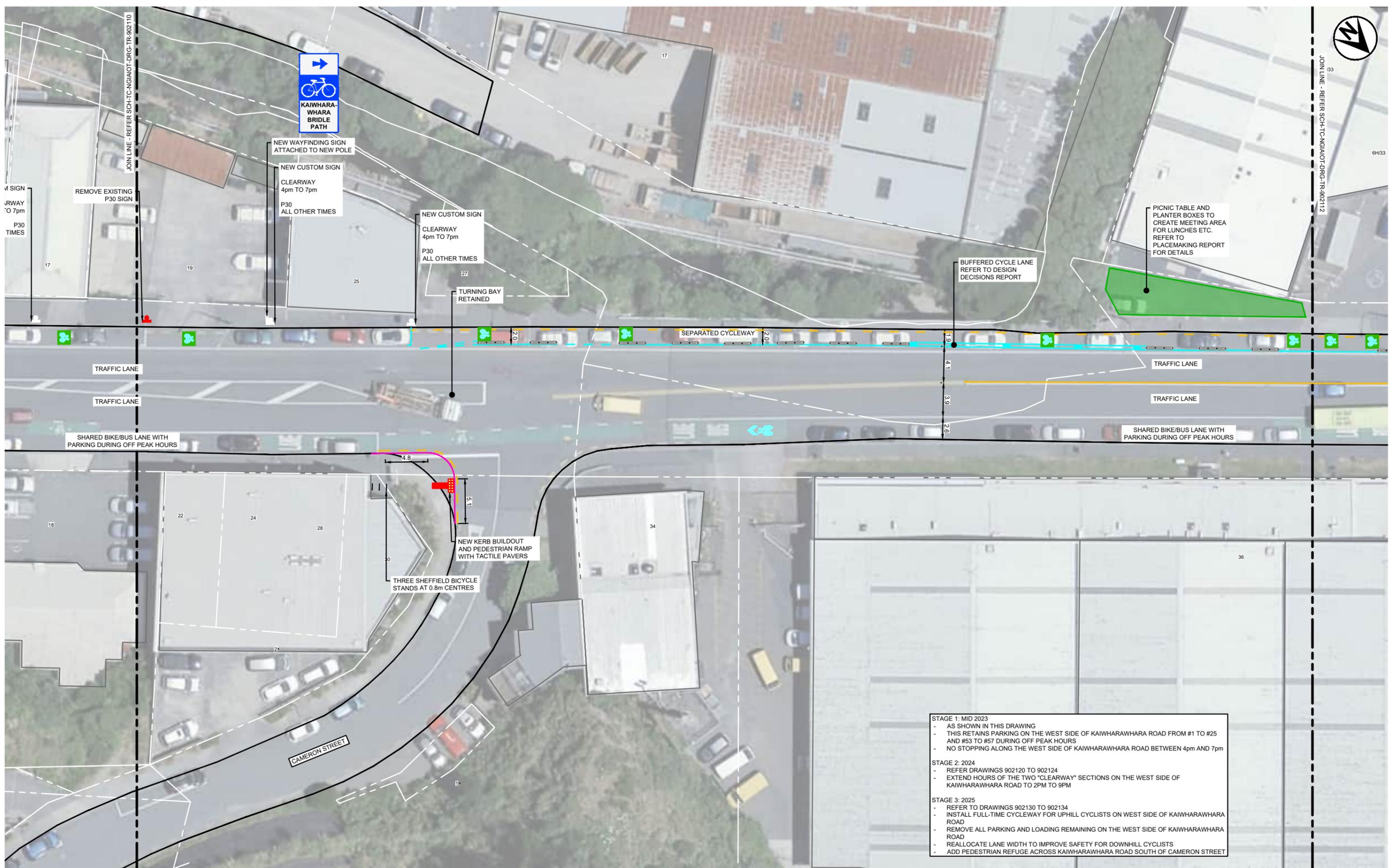
DRAWING SCALE: 1:200

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DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
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A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	KAIWHARAWHARA ROAD	
	GENERAL ARRANGMENT PLAN - STAGE 1	
	SHEET 1	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902110	C05

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 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - REFER DRAWINGS 902120 TO 902124
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - REFER TO DRAWINGS 902130 TO 902134
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT	ORIGINATOR

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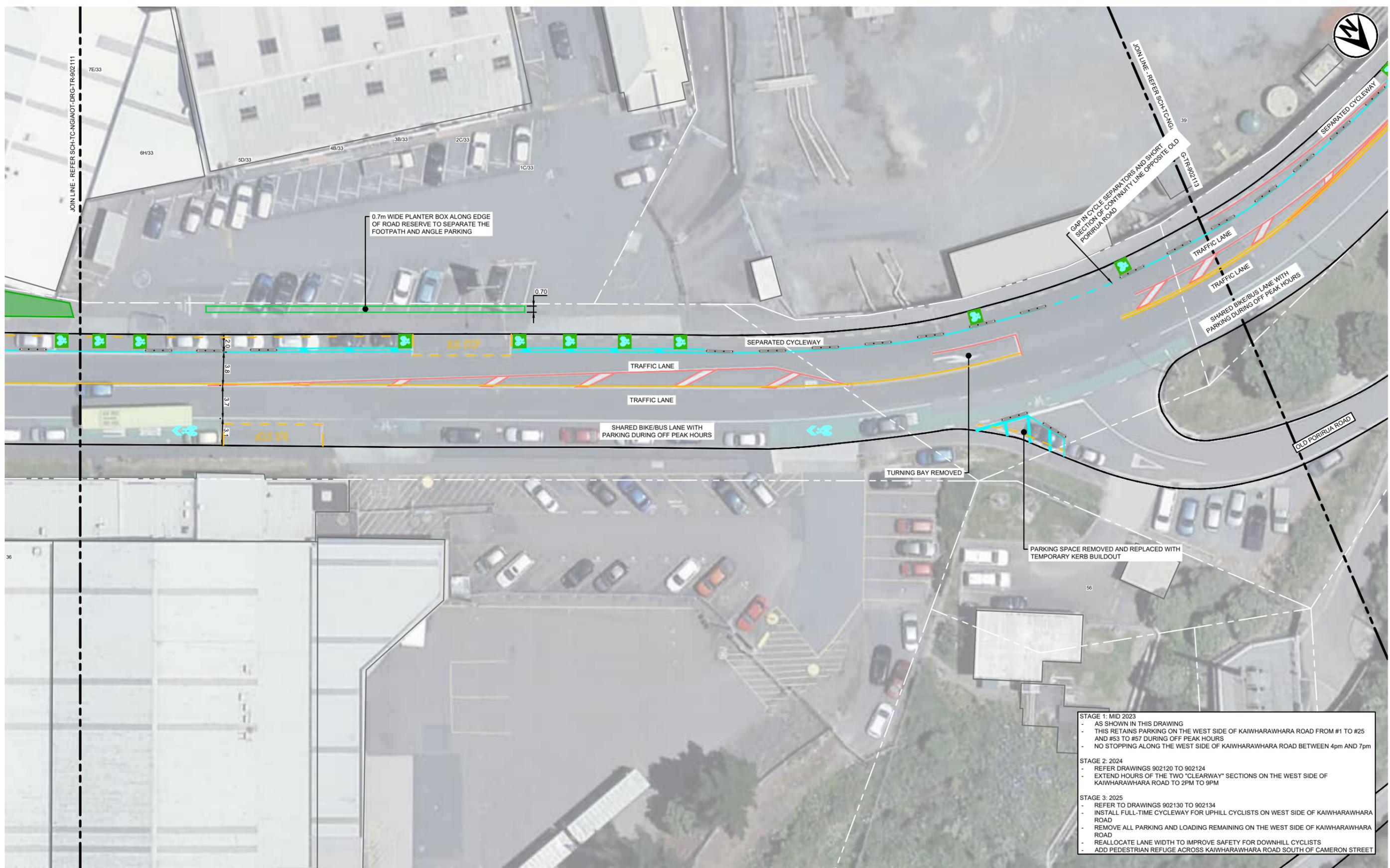
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DETAILED DESIGN	TR
DRAWING STATUS	
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A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	KAIWHARAWHARA ROAD	
	GENERAL ARRANGMENT PLAN - STAGE 1	
	SHEET 2	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902111	C05

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STAGE 1: MID 2023
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 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - REFER DRAWINGS 902120 TO 902124
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - REFER TO DRAWINGS 902130 TO 902134
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 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
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CLIENT		ORIGINATOR		DESIGN STAGE			GATE		PROJECT:																																	
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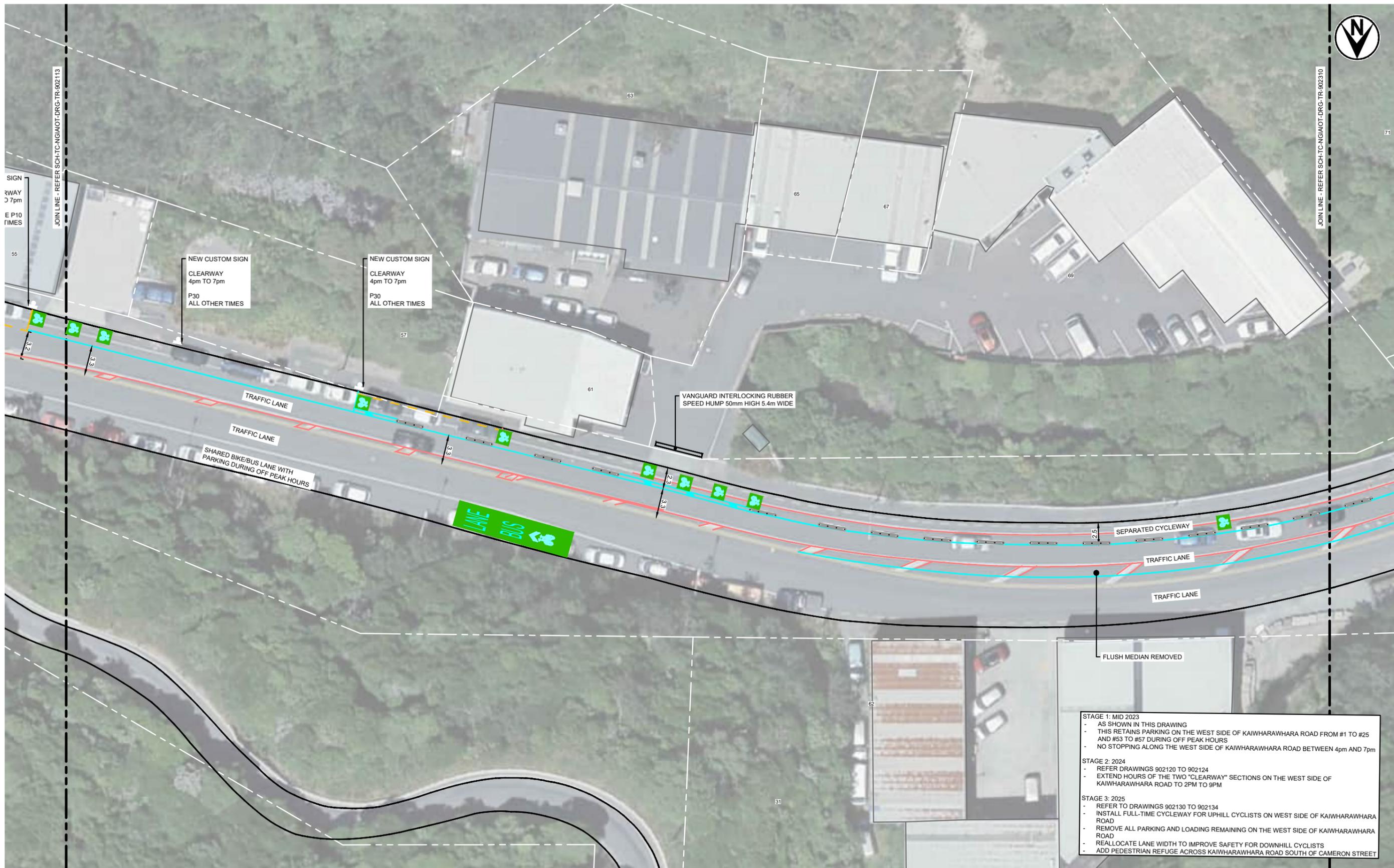
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STAGE 1: MID 2023

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STAGE 2: 2024

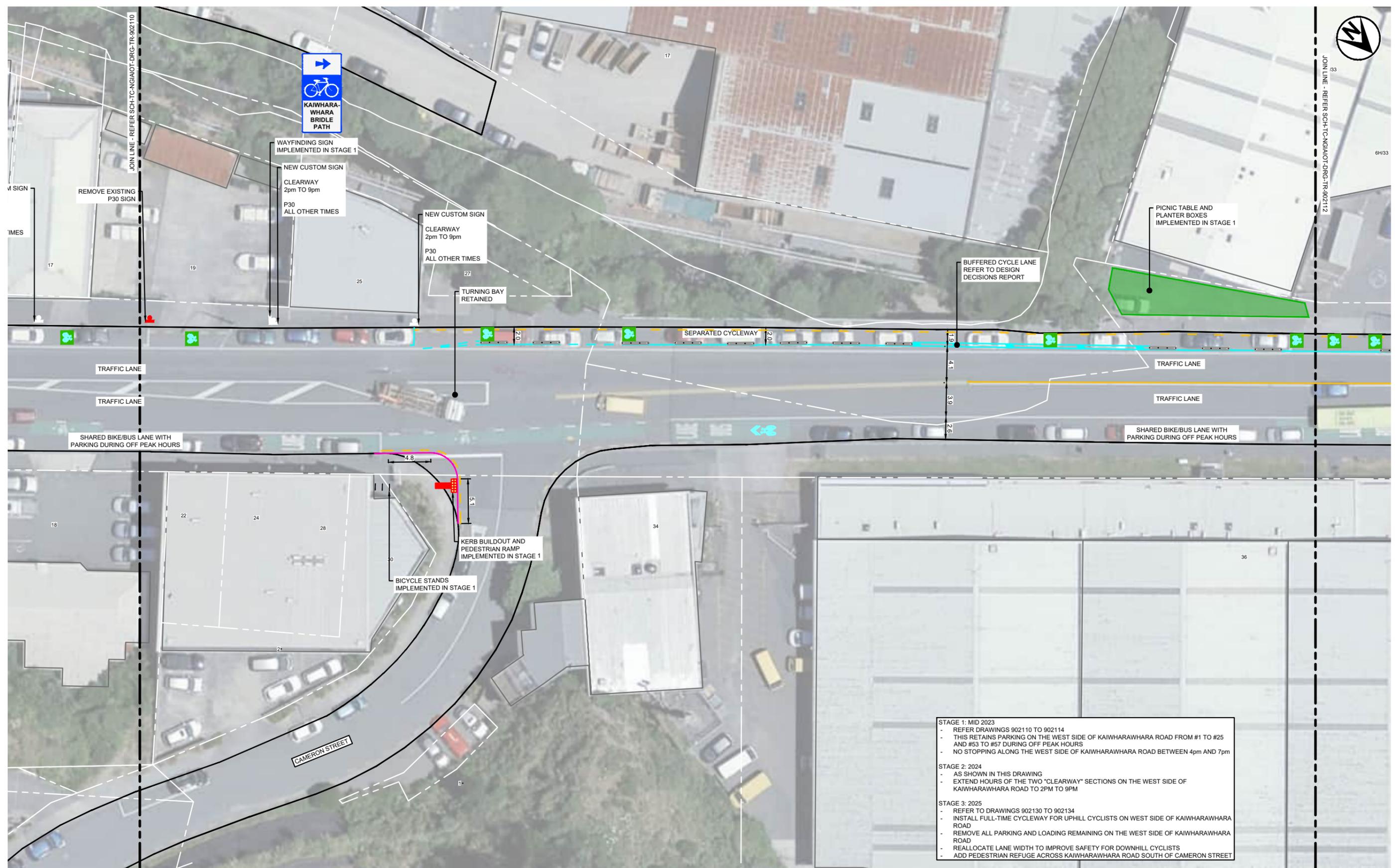
- REFER DRAWINGS 902120 TO 902124
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<p>CLIENT</p> <p>Absolutely Positively Wellington City Council <small>Me Heke Kō Pōkeke</small></p>	<p>ORIGINATOR</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>REV.</th> <th>DESCRIPTION</th> <th>DRAWN BY</th> <th>APRVD. BY</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>C01</td> <td>30% DETAILED DESIGN</td> <td>C.LI</td> <td>A.HEAD</td> <td>25/ 07/ 2022</td> </tr> <tr> <td>C02</td> <td>90% DETAILED DESIGN</td> <td>C.LI</td> <td>A.HEAD</td> <td>09/ 09/ 2022</td> </tr> <tr> <td>C03</td> <td>TRAFFIC RESOLUTION ISSUE</td> <td>C.LI</td> <td>J.WARD</td> <td>07/ 10/ 2022</td> </tr> <tr> <td>C04</td> <td>TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING</td> <td>C.LI</td> <td>A.HEAD</td> <td>21/ 10/ 2022</td> </tr> <tr> <td>C05</td> <td>TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING</td> <td>C.LI</td> <td>A.HEAD</td> <td>28/ 10/ 2022</td> </tr> </tbody> </table>	REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE	C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022	C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022	C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022	C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022	C05	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	28/ 10/ 2022	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>DESIGN STAGE</th> <th>GATE</th> </tr> </thead> <tbody> <tr> <td style="text-align: center;">DETAILED DESIGN</td> <td style="text-align: center;">TR</td> </tr> <tr> <td colspan="2" style="text-align: center;">DRAWING STATUS</td> </tr> <tr> <td colspan="2" style="text-align: center; color: red; font-weight: bold;">NOT FOR CONSTRUCTION</td> </tr> <tr> <td>APPROVED BY:</td> <td>DESIGNED: B.RODENBURG</td> </tr> <tr> <td>A.HEAD</td> <td>DES. CHECK: A.HEAD</td> </tr> <tr> <td>APPROVED DATE:</td> <td>DRAWN: C.LI</td> </tr> <tr> <td>28/10/2022</td> <td>DRG. CHECK: H.BENDLE</td> </tr> </tbody> </table>	DESIGN STAGE	GATE	DETAILED DESIGN	TR	DRAWING STATUS		NOT FOR CONSTRUCTION		APPROVED BY:	DESIGNED: B.RODENBURG	A.HEAD	DES. CHECK: A.HEAD	APPROVED DATE:	DRAWN: C.LI	28/10/2022	DRG. CHECK: H.BENDLE	<p>PROJECT: TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL</p> <p>TITLE: KAIWHARAWHARA ROAD GENERAL ARRANGMENT PLAN - STAGE 1 SHEET 5</p> <p>DOCUMENT: SCH - TC - NGAIoT - DRG - TR - 902114</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>ORIGINATOR</th> <th>PROJECT</th> <th>WORK STREAM</th> <th>TYPE</th> <th>DISCIPLINE</th> <th>SHEET</th> <th>REVISION</th> </tr> </thead> <tbody> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>C05</td> </tr> </tbody> </table>	ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION							C05
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STAGE 2: 2024
 - AS SHOWN IN THIS DRAWING
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - REFER TO DRAWINGS 902130 TO 902134
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

ORIGINAL SIZE: ISO A1 594mm x 841mm
 300
 200
 150
 100
 50
 30
 10
 0

CLIENT	ORIGINATOR

LAST SAVED BY: CHLI(2022-10-28) LAST PLOTTED: 2022-10-28
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	28/10/2022

SCALE: 1:200

DRAWING SCALE: 1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL
TITLE:	KAIWHARAWHARA ROAD GENERAL ARRANGEMENT PLAN - STAGE 2 SHEET 2
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902121 C01

THIS DRAWING MAY CONTAIN COLOUR CONTENT. CORRECTLY PRINTED COPIES OF THIS DOCUMENT WILL DISPLAY THE LOGO IN COLOUR.

ORIGINAL SIZE: ISO A1 594mm x 841mm

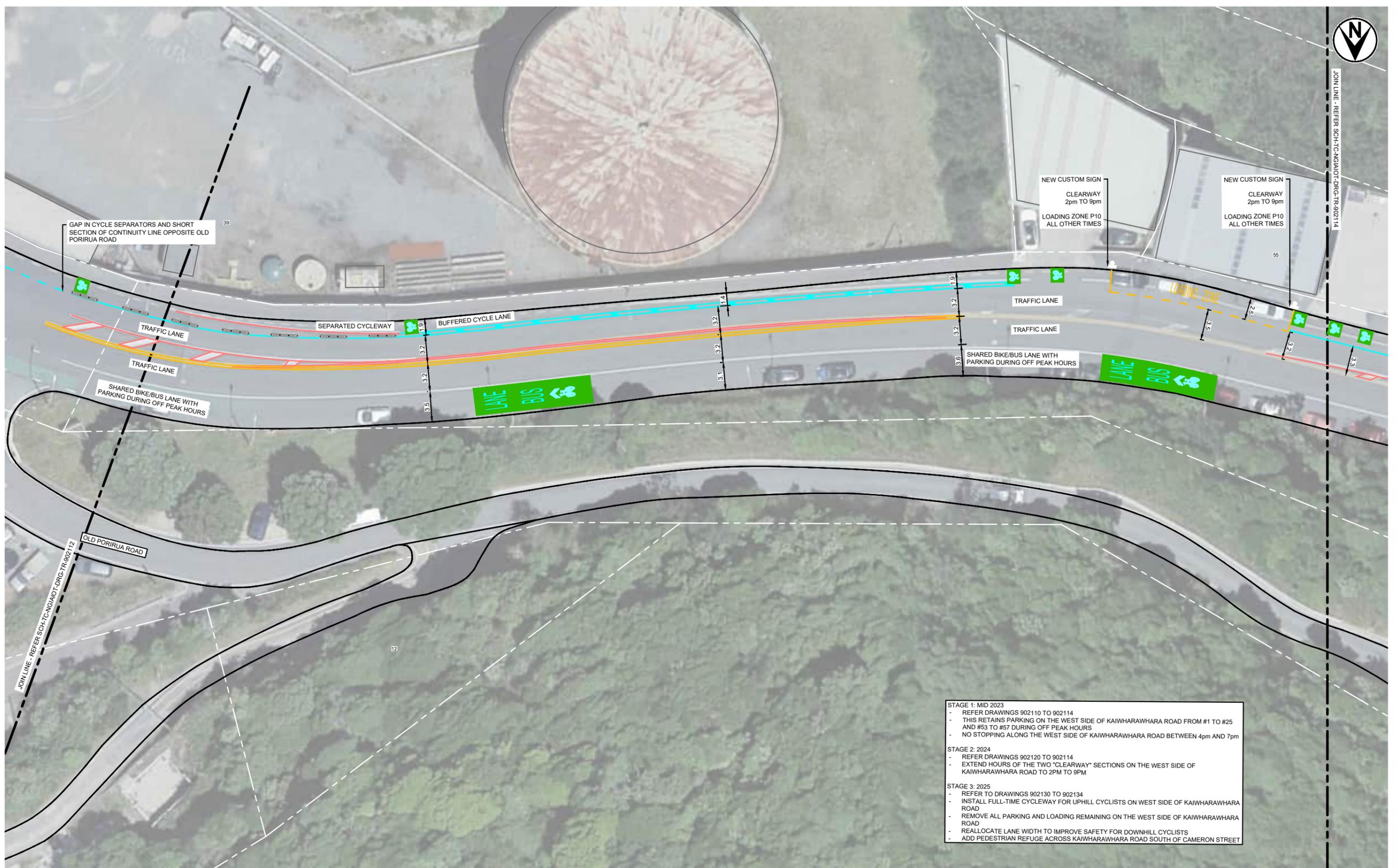
200

150

100

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STAGE 1: MID 2023
 - REFER DRAWINGS 902110 TO 902114
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - REFER DRAWINGS 902120 TO 902114
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - REFER TO DRAWINGS 902130 TO 902134
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT: Absolutely Positively Wellington City Council
 ORIGINATOR: Abley, Tonkin-Taylor, Isthmus.

REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	28/10/2022

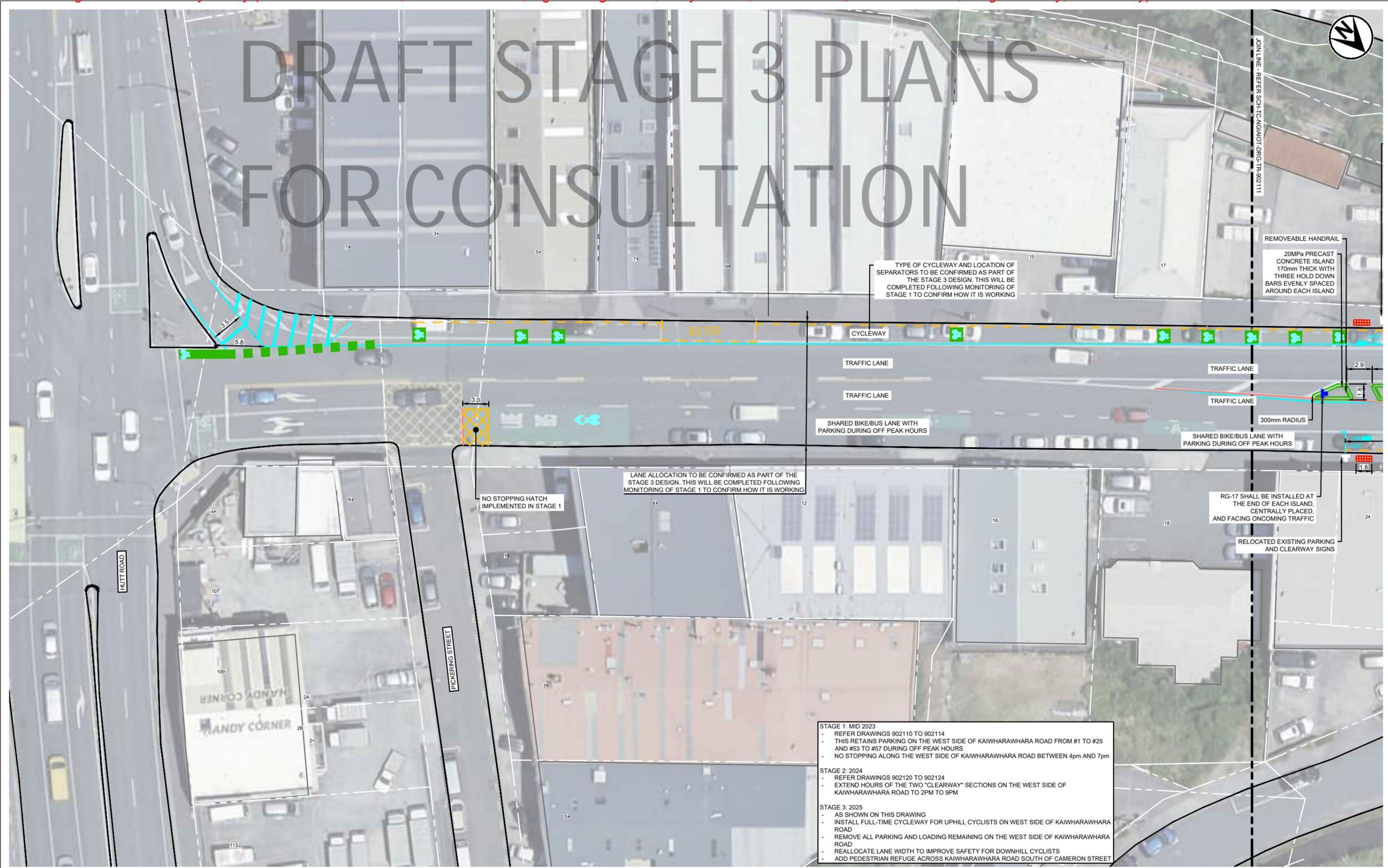
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DESIGN STAGE: DETAILED DESIGN
 GATE: TR
 DRAWING STATUS: NOT FOR CONSTRUCTION
 APPROVED BY: A.HEAD
 DESIGNED: B.RODENBURG
 DES. CHECK: A.HEAD
 APPROVED DATE: 28/10/2022
 DRAWN: C.LI
 DRG. CHECK: H.BENDLE

PROJECT: TRANSITIONAL CYCLEWAYS
 NGAIO TRANSITIONAL
 TITLE: KAIWHARAWHARA ROAD
 GENERAL ARRANGMENT PLAN - STAGE 2
 SHEET 4
 DOCUMENT: SCH - TC - NGAIoT - DRG - TR - 902123 C01

DRAFT STAGE 3 PLANS FOR CONSULTATION

THIS DRAWING MAY CONTAIN COLOUR CONTENT. CORRECTLY PRINTED COPIES OF THIS DOCUMENT WILL DISPLAY THE LOGO IN COLOUR.



STAGE 1: MID 2023
 - REFER DRAWINGS 902110 TO 902114
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - REFER DRAWINGS 902120 TO 902124
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - AS SHOWN ON THIS DRAWING
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT		ORIGINATOR		DESIGN STAGE		GATE		PROJECT:	
Absolutely Positively Wellington City Council		Abley		DETAILED DESIGN		TR		TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	
Tonkin-Taylor Isthmus.		30% DETAILED DESIGN		DRAWING STATUS		NOT FOR CONSTRUCTION		TITLE:	
LAST SAVED BY: CHLI(2022-10-28) LAST PLOTTED: 2022-10-28		DESCRIPTION		APPROVED BY:		DESIGNED: B.RODENBURG		KAIWHARAWHARA ROAD GENERAL ARRANGEMENT PLAN - STAGE 3	
FILE LOCATION:		DRAWN BY: C.LI		APPROVED DATE:		DES. CHECK: A.HEAD		SHEET 1	
SCALE		DRAWING INFORMATION		DRAWING INFORMATION		DRAWN: C.LI		DOCUMENT:	
1:200		DATE: 28/10/2022		DATE: 28/10/2022		DRG. CHECK: H.BENDLE		SCH - TC - NGAIoT - DRG - TR - 902130	
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PRINTED COPIES OF THIS DOCUMENT ARE UNCONTROLLED		DRAWING INFORMATION		DRAWING INFORMATION		SHEET		C01	

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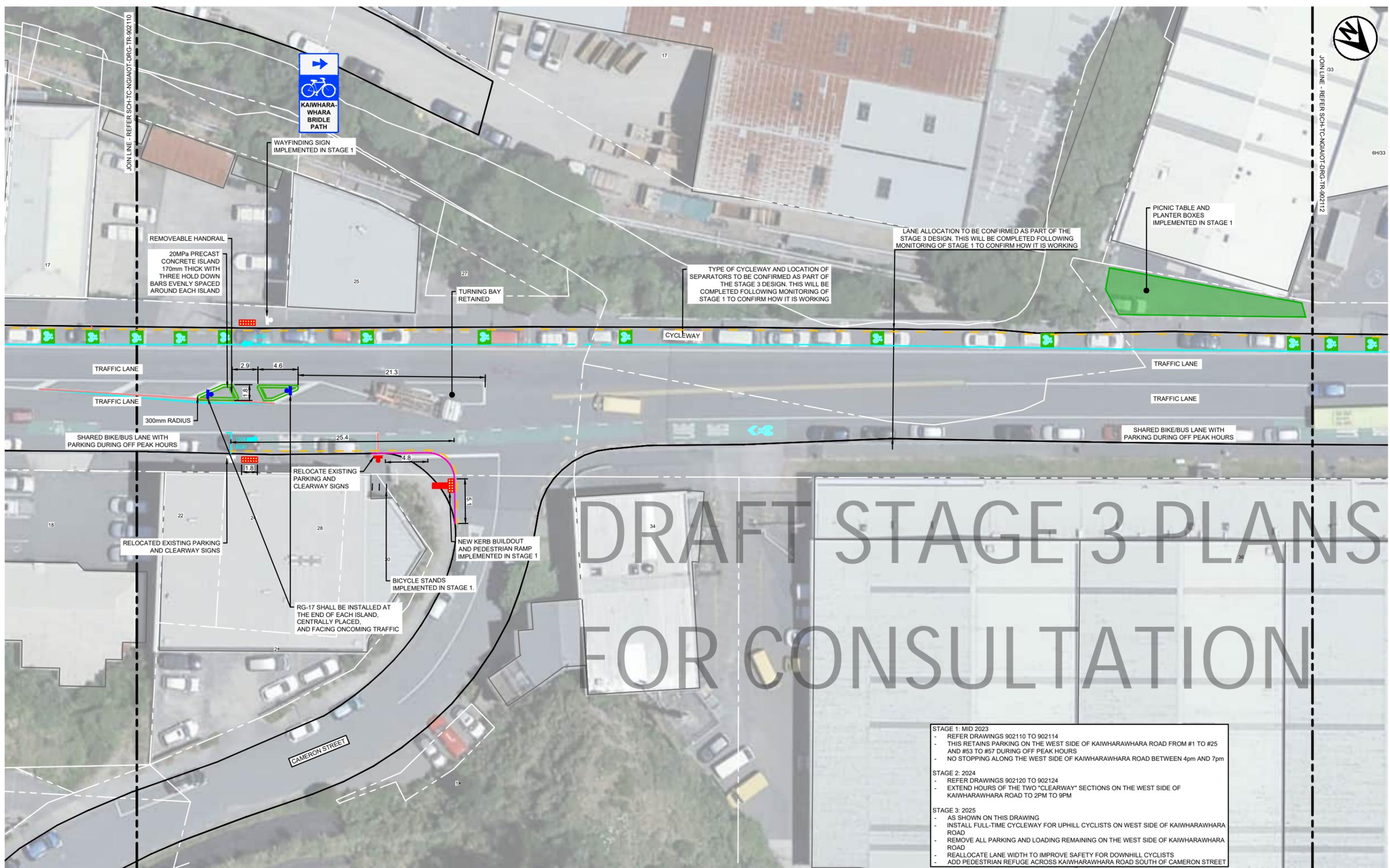
ORIGINAL SIZE: ISO A1 594mm x 841mm

200

100

50

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STAGE 1: MID 2023
 - REFER DRAWINGS 902110 TO 902114
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm

STAGE 2: 2024
 - REFER DRAWINGS 902120 TO 902124
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM

STAGE 3: 2025
 - AS SHOWN ON THIS DRAWING
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

CLIENT	ORIGINATOR

LAST SAVED BY: CHLI(2022-10-28) LAST PLOTTED: 2022-10-28
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	28/10/2022

SCALE: 1:200

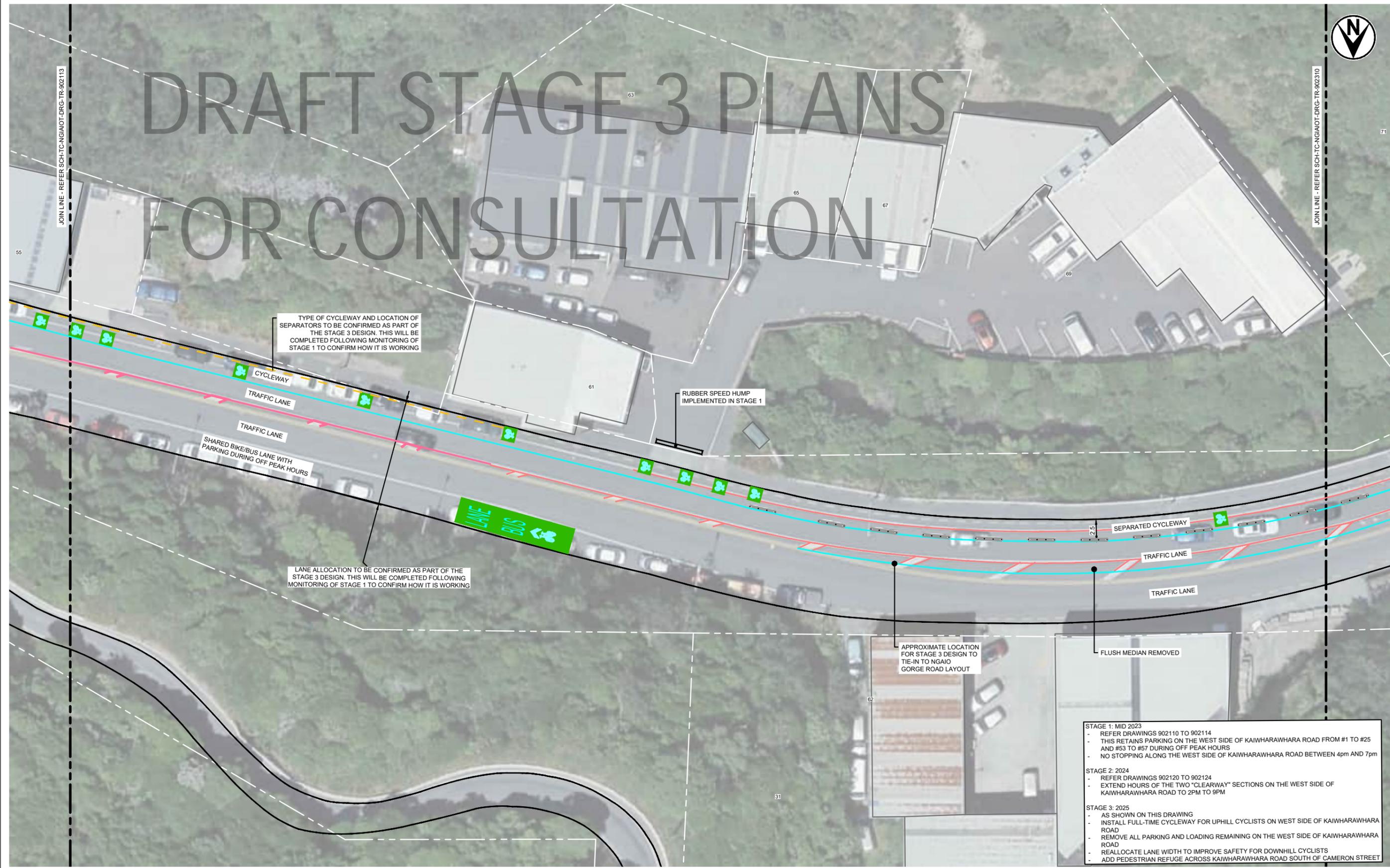
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DETAILED DESIGN	TR		
DRAWING STATUS		TITLE:	KAIWHARAWHARA ROAD GENERAL ARRANGMENT PLAN - STAGE 3 SHEET 2
NOT FOR CONSTRUCTION			
APPROVED BY:	DESIGNED:	DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902131
A.HEAD	B.RODENBURG		
APPROVED DATE:	DES. CHECK:	REVISION	C01
28/10/2022	A.HEAD		
	DRAWN:		
	CL.I		
	DRG. CHECK:		
	H.BENDLE		

DRAFT STAGE 3 PLANS FOR CONSULTATION



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- STAGE 1: MID 2023**
- REFER DRAWINGS 902110 TO 902114
 - THIS RETAINS PARKING ON THE WEST SIDE OF KAIWHARAWHARA ROAD FROM #1 TO #25 AND #53 TO #57 DURING OFF PEAK HOURS
 - NO STOPPING ALONG THE WEST SIDE OF KAIWHARAWHARA ROAD BETWEEN 4pm AND 7pm
- STAGE 2: 2024**
- REFER DRAWINGS 902120 TO 902124
 - EXTEND HOURS OF THE TWO "CLEARWAY" SECTIONS ON THE WEST SIDE OF KAIWHARAWHARA ROAD TO 2PM TO 9PM
- STAGE 3: 2025**
- AS SHOWN ON THIS DRAWING
 - INSTALL FULL-TIME CYCLEWAY FOR UPHILL CYCLISTS ON WEST SIDE OF KAIWHARAWHARA ROAD
 - REMOVE ALL PARKING AND LOADING REMAINING ON THE WEST SIDE OF KAIWHARAWHARA ROAD
 - REALLOCATE LANE WIDTH TO IMPROVE SAFETY FOR DOWNHILL CYCLISTS
 - ADD PEDESTRIAN REFUGE ACROSS KAIWHARAWHARA ROAD SOUTH OF CAMERON STREET

300
200
150
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CLIENT	ORIGINATOR
	

LAST SAVED BY: CHLI(2022-10-28) LAST PLOTTED: 2022-10-28

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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	28/10/2022

DRAWING SCALE: 1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
28/10/2022	DRG. CHECK: H.BENDLE

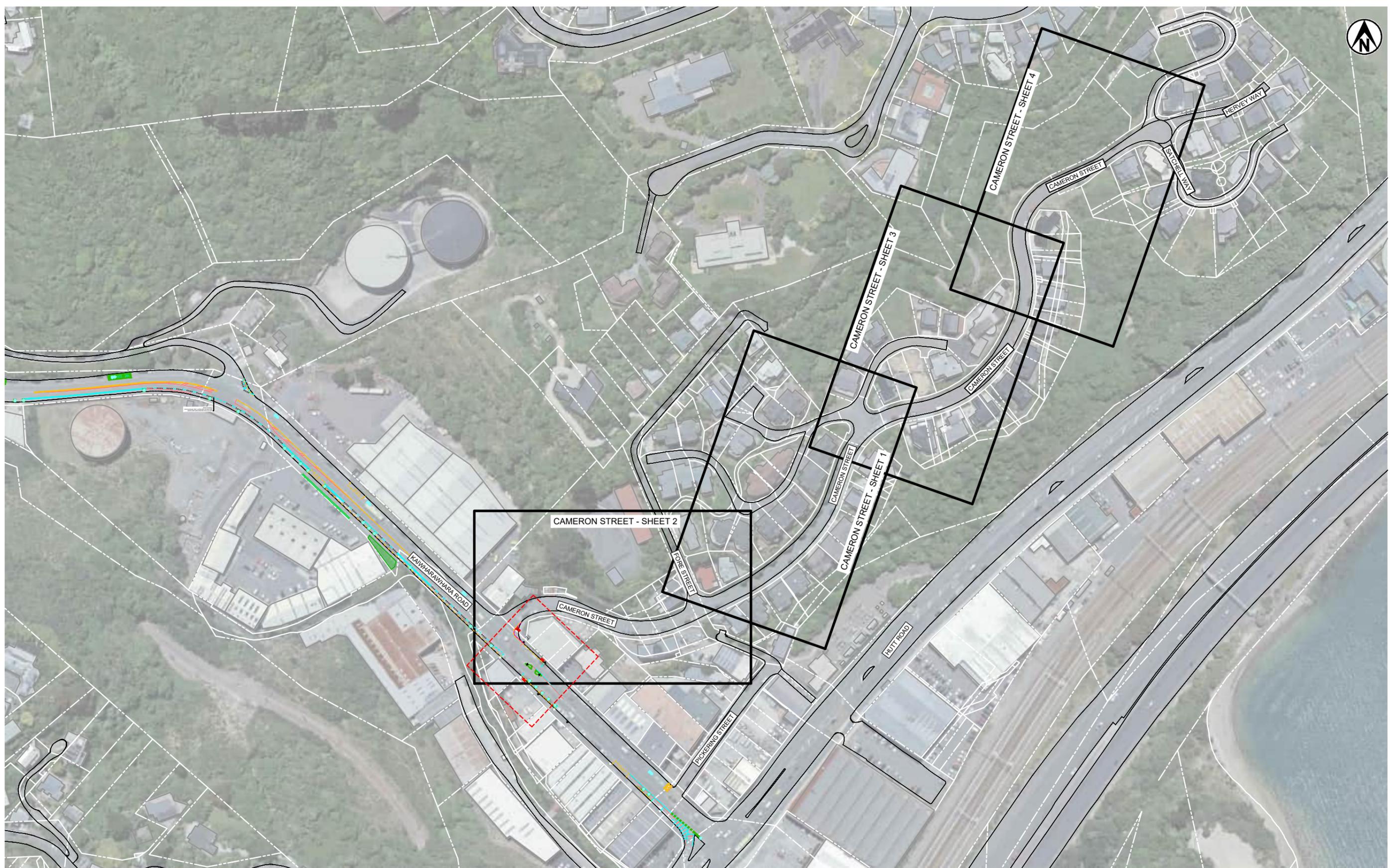
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TITLE:	KAIWHARAWHARA ROAD GENERAL ARRANGEMENT PLAN - STAGE 3 SHEET 5
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902134 C01

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT

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FILE LOCATION:

ORIGINATOR

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C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

SCALE: 1:1000

DESIGN STAGE			GATE		
DETAILED DESIGN			TR		
DRAWING STATUS					
NOT FOR CONSTRUCTION					
APPROVED BY:		DESIGNED:		B.RODENBURG	
A.HEAD		DES. CHECK:		A.HEAD	
APPROVED DATE:		DRAWN:		C.LI	
21/10/2022		DRG. CHECK:		H.BENDLE	
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PROJECT:		TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL			
TITLE:		CAMERON STREET SECTION COVERSHEET			
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DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902200				C04

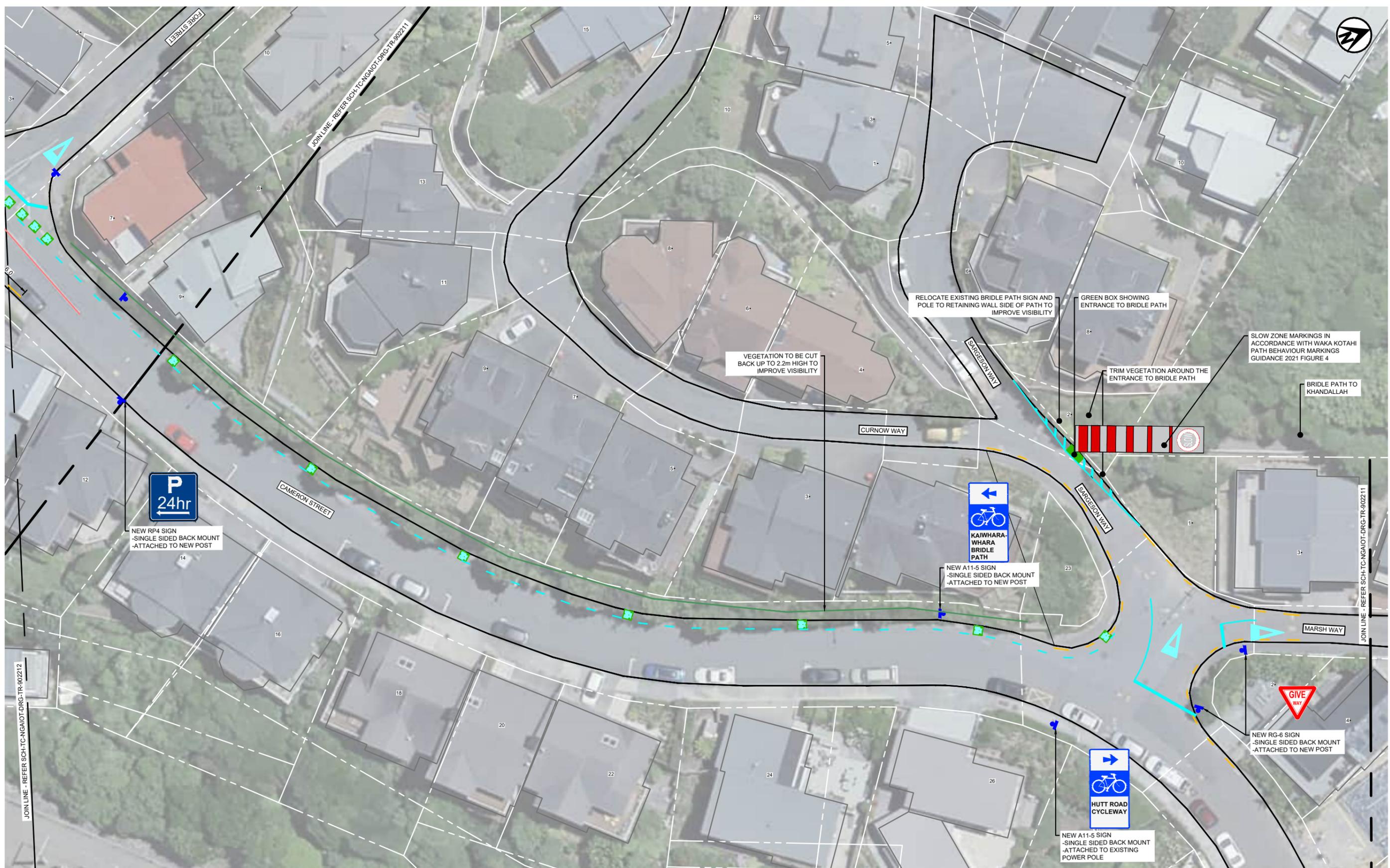
THIS DRAWING MAY CONTAIN COLOUR CONTENT. CORRECTLY PRINTED COPIES OF THIS DOCUMENT WILL DISPLAY THE LOGO IN COLOUR.

ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT	ORIGINATOR

LAST SAVED BY: CHLI(2022-10-20) LAST PLOTTED: 2022-10-20
 FILE LOCATION: PRINTED COPIES OF THIS DOCUMENT ARE UNCONTROLLED

REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022
C04	TRAFFIC RESOLUTION ISSUE UPDATE TO STAGING	C.LI	A.HEAD	21/ 10/ 2022

SCALE: 1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
A.HEAD	DES. CHECK: A.HEAD
APPROVED DATE:	DRAWN: C.LI
21/10/2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	CAMERON STREET	
	GENERAL ARRANGEMENT PLAN	
	SHEET 1	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902210	REVISION: C04

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT

ORIGINATOR

REV.	DESCRIPTION	DRAWN BY	APPRD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

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DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS				
	NGAIO TRANSITIONAL				
TITLE:	NGAIO GORGE ROAD SECTION COVERSHEET				
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902300	C03			

LAST SAVED BY: CHLI(2022-10-07) LAST PLOTTED: 2022-10-07

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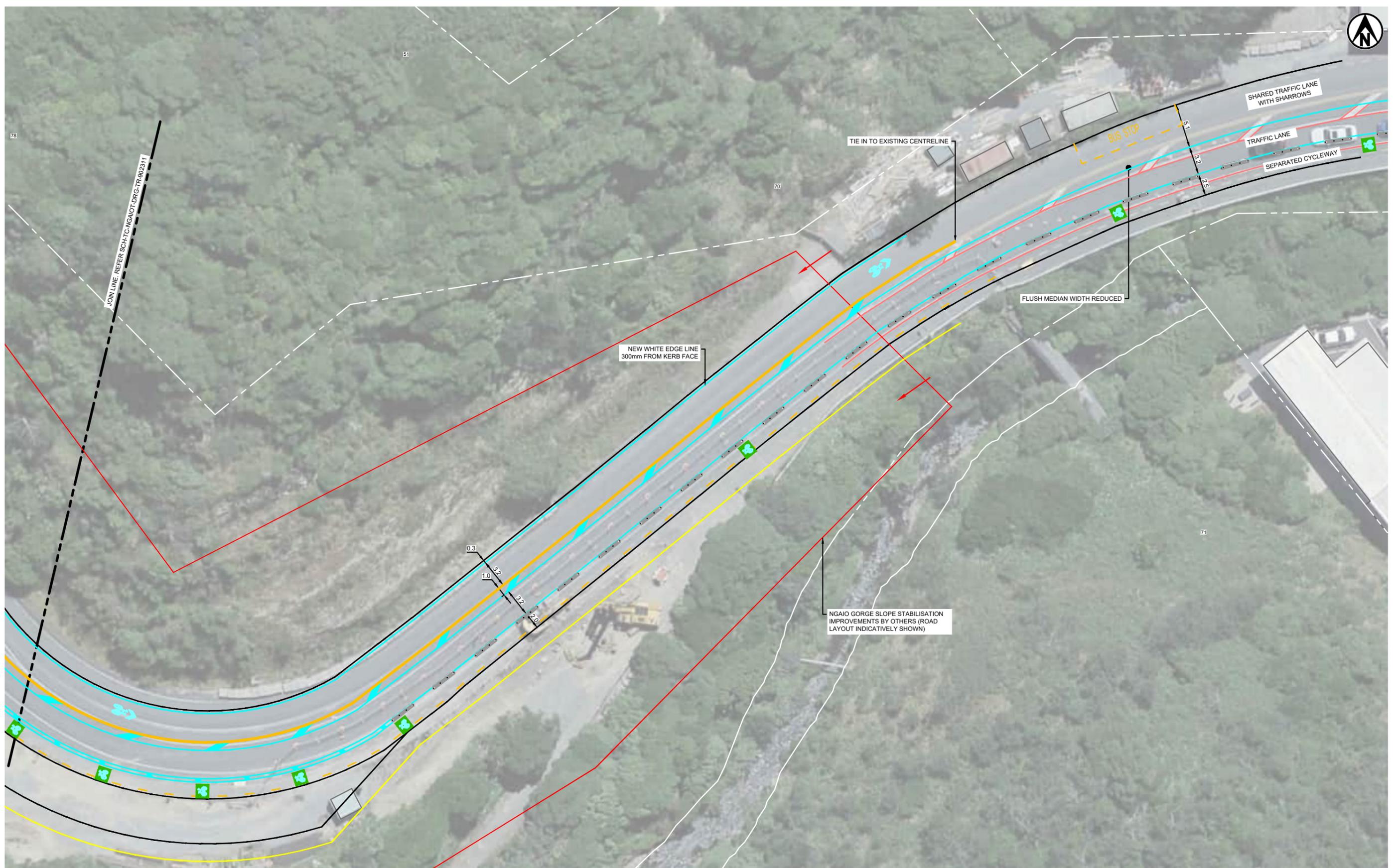
THIS DRAWING MAY CONTAIN COLOUR CONTENT. CORRECTLY PRINTED COPIES OF THIS DOCUMENT WILL DISPLAY THE LOGO IN COLOUR.

ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT	ORIGINATOR
	 Isthmus.

REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE	1:200
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DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 1			
DOCUMENT:	SCH - TC - NGAIOT - DRG - TR - 902310	SHEET	C03	REVISION

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07

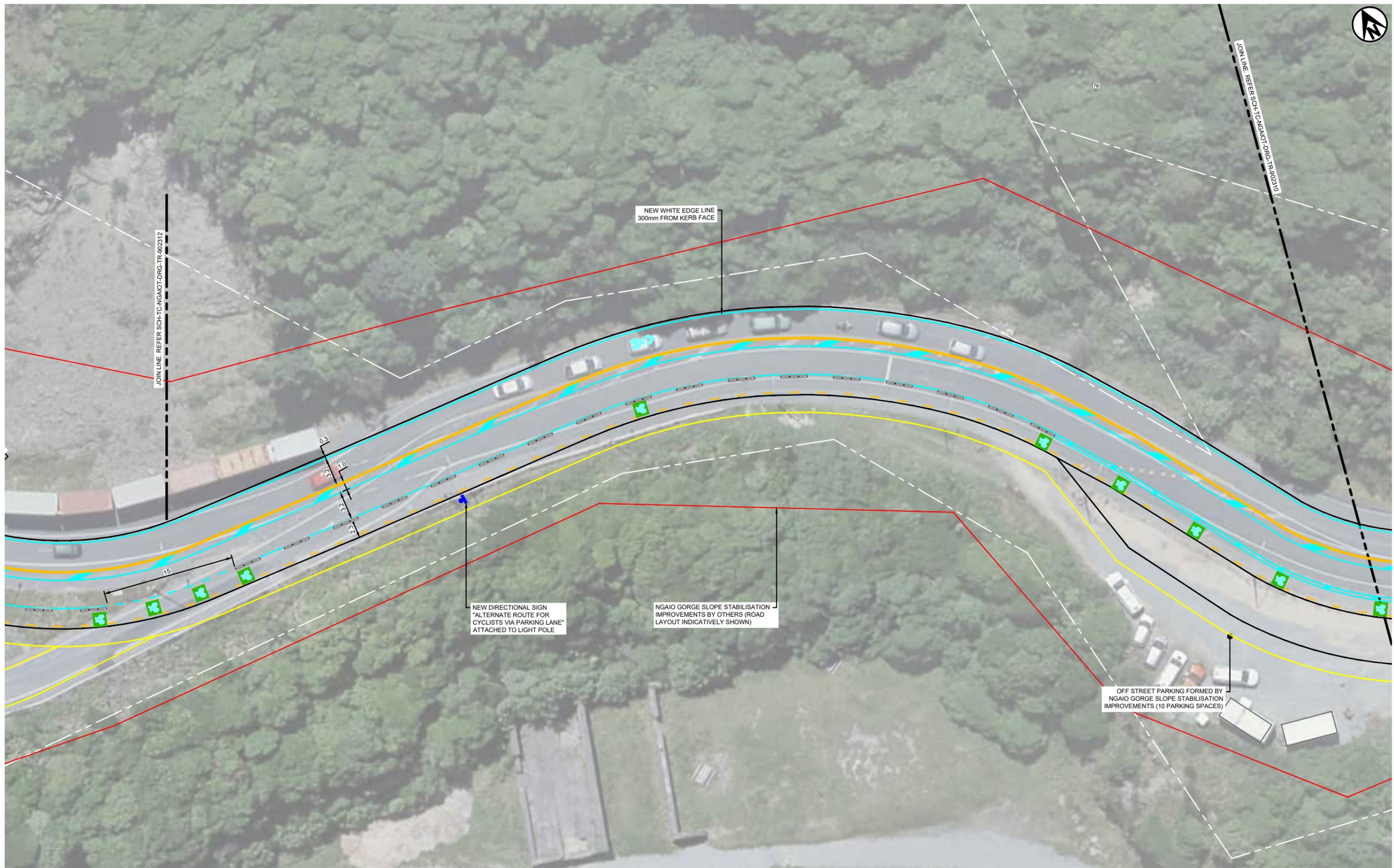
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FILE LOCATION:

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ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT	ORIGINATOR
	

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07
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REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
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C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

SCALE	0 5 10 m
DRAWING SCALE:	1:200

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE

PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 2			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902311	C03		

THIS DRAWING MAY CONTAIN COLOUR CONTENT. CORRECTLY PRINTED COPIES OF THIS DOCUMENT WILL DISPLAY THE LOGO IN COLOUR.

ORIGINAL SIZE: ISO A1 594mm x 841mm

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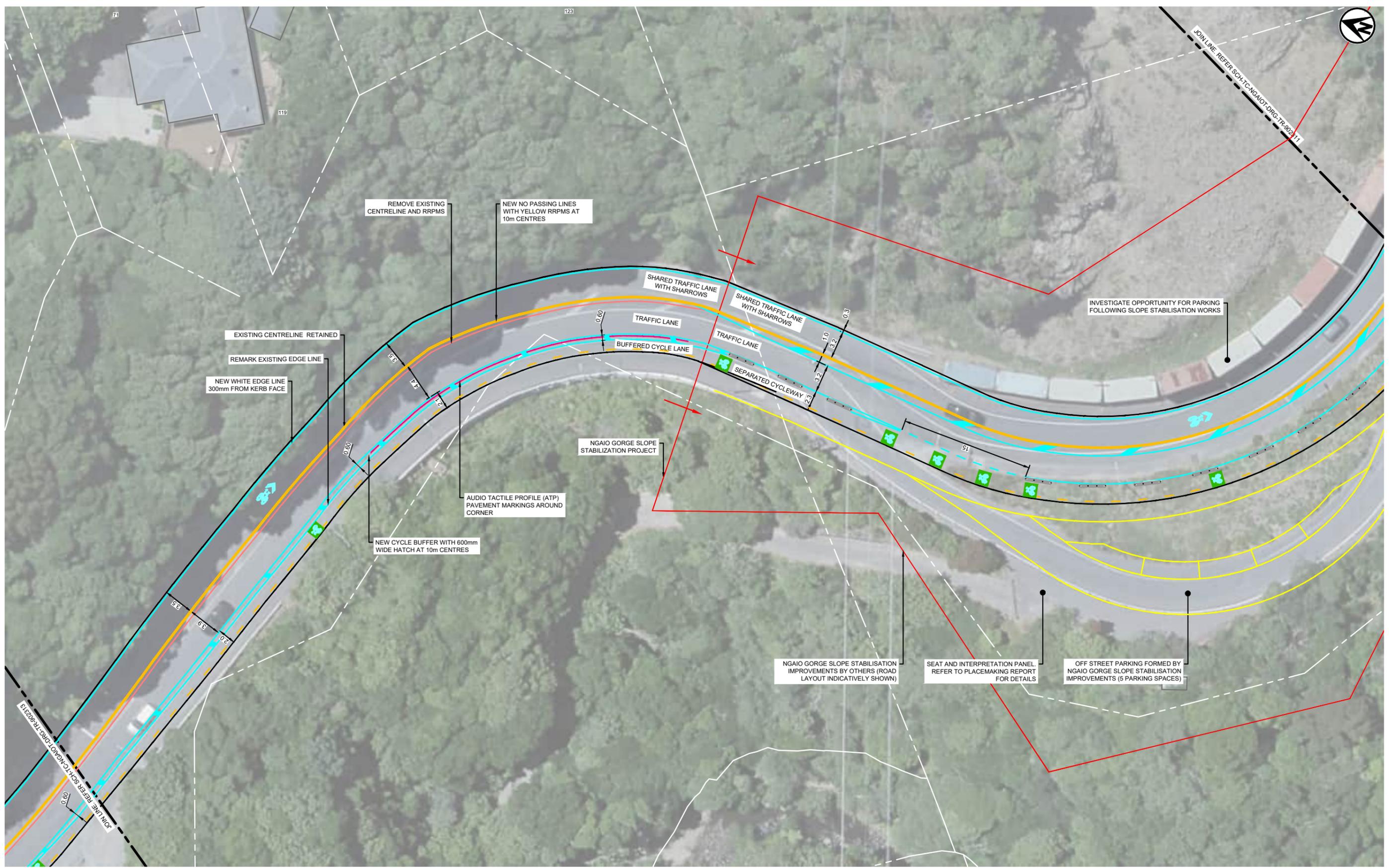
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CLIENT ORIGINATOR



REV.	DESCRIPTION	DRAWN BY	APRVD. BY	DATE
C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022

DESIGN STAGE	GATE
DETAILED DESIGN	TR
DRAWING STATUS	
NOT FOR CONSTRUCTION	
APPROVED BY:	DESIGNED: B.RODENBURG
J.WARD	DES. CHECK: J.WARD
APPROVED DATE:	DRAWN: C.LI
07/ 10/ 2022	DRG. CHECK: H.BENDLE



PROJECT:	TRANSITIONAL CYCLEWAYS	
	NGAIO TRANSITIONAL	
TITLE:	NGAIO GORGE ROAD	
	GENERAL ARRANGEMENT PLAN	
	SHEET 3	
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902312	C03

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07

FILE LOCATION:

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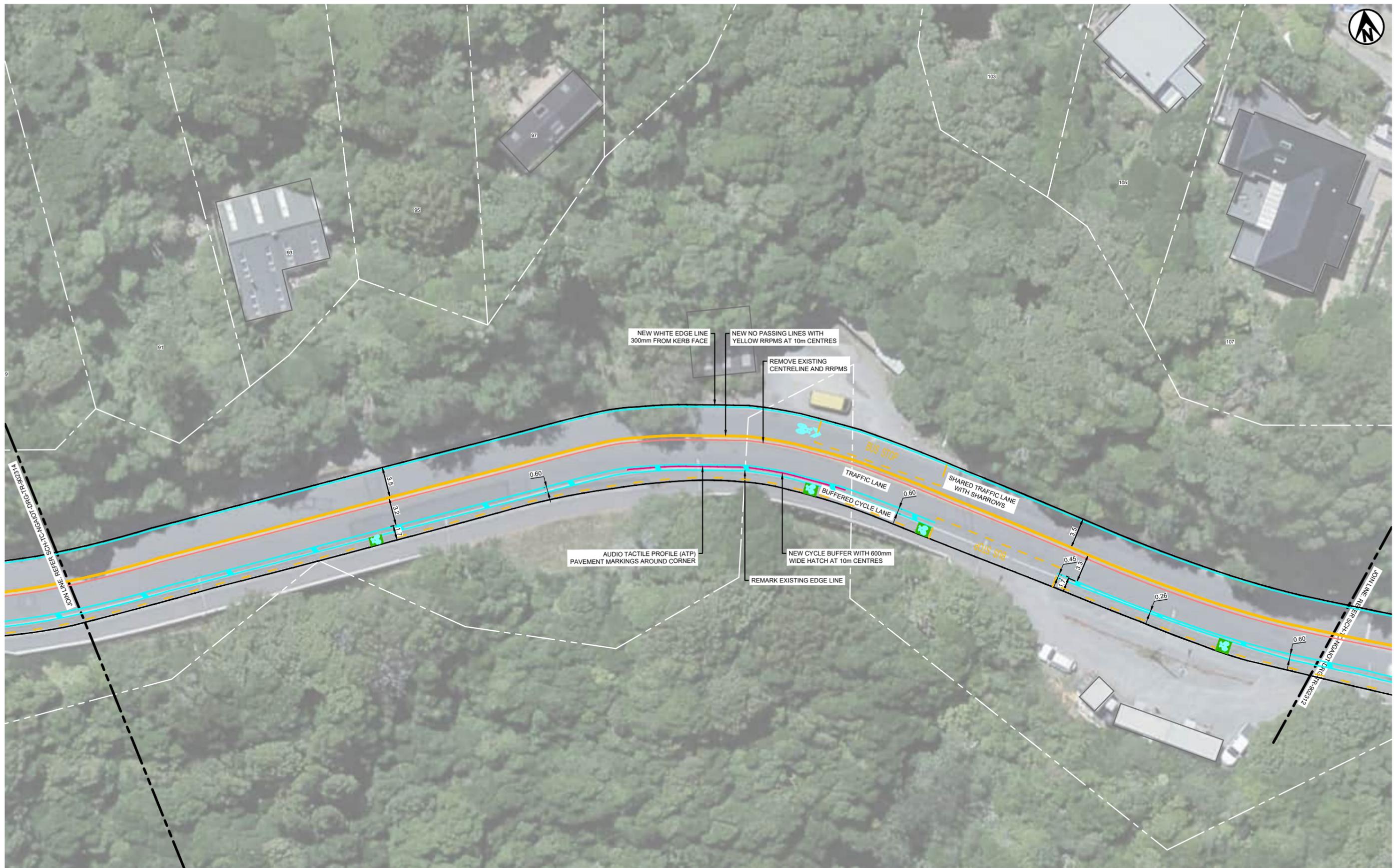
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ORIGINAL SIZE: ISO A1 594mm x 841mm

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CLIENT		ORIGINATOR		DESIGN STAGE		GATE		PROJECT:																															
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C01	30% DETAILED DESIGN	C.LI	A.HEAD	25/ 07/ 2022																																			
C02	90% DETAILED DESIGN	C.LI	A.HEAD	09/ 09/ 2022																																			
C03	TRAFFIC RESOLUTION ISSUE	C.LI	J.WARD	07/ 10/ 2022																																			
APPROVED BY:	DESIGNED:	B.RODENBURG																																					
J.WARD	DES. CHECK:	J.WARD																																					
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07/ 10/ 2022	DRG. CHECK:	H.BENDLE																																					
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ORIGINAL SIZE: ISO A1 594mm x 841mm

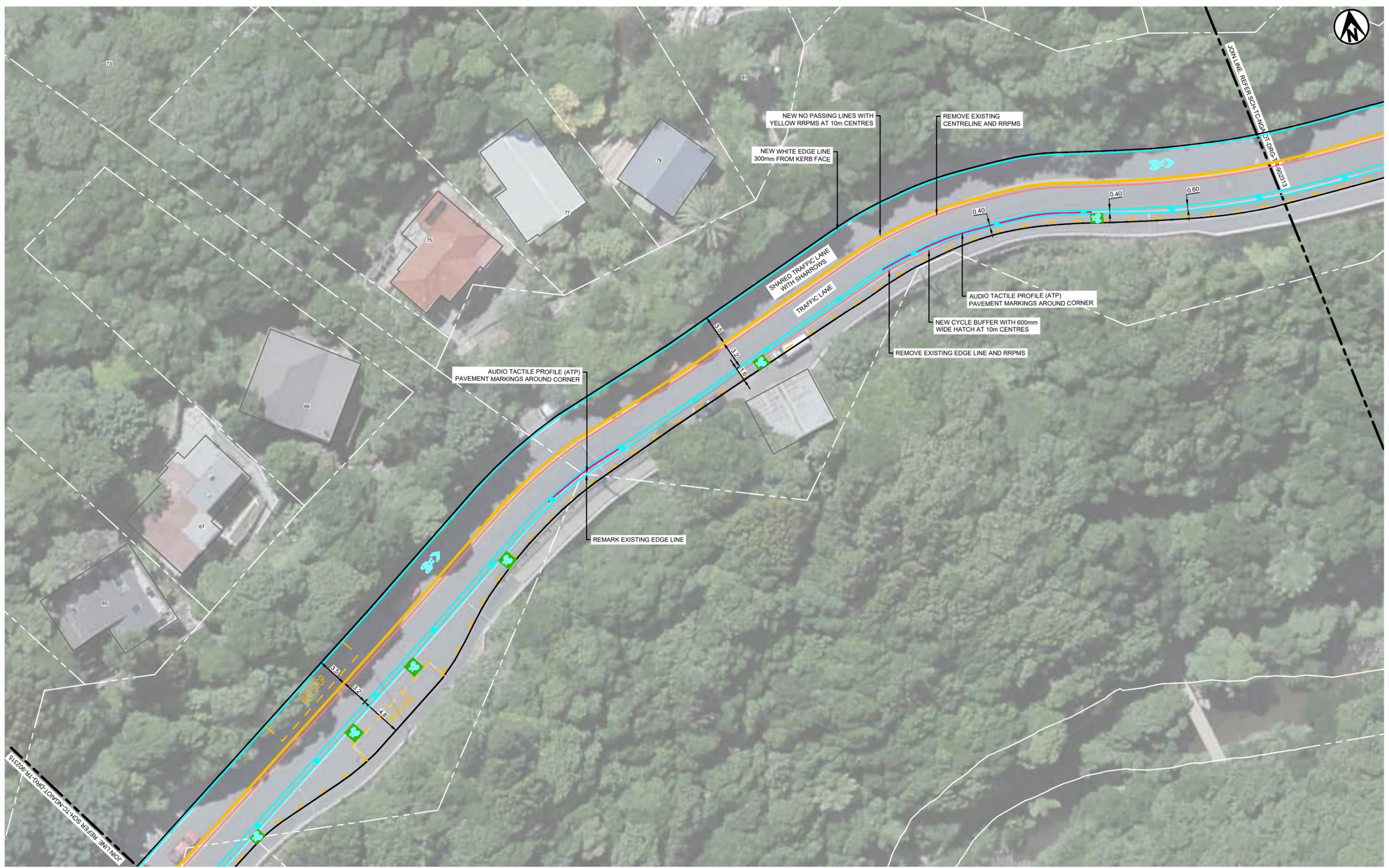
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CLIENT	ORIGINATOR

LAST SAVED BY: CHLI(2022-10-06) LAST PLOTTED: 2022-10-07
 FILE LOCATION:
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PROJECT:	TRANSITIONAL CYCLEWAYS			
	NGAIO TRANSITIONAL			
TITLE:	NGAIO GORGE ROAD			
	GENERAL ARRANGMENT PLAN			
	SHEET 5			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902314	SHEET	C03	REVISION

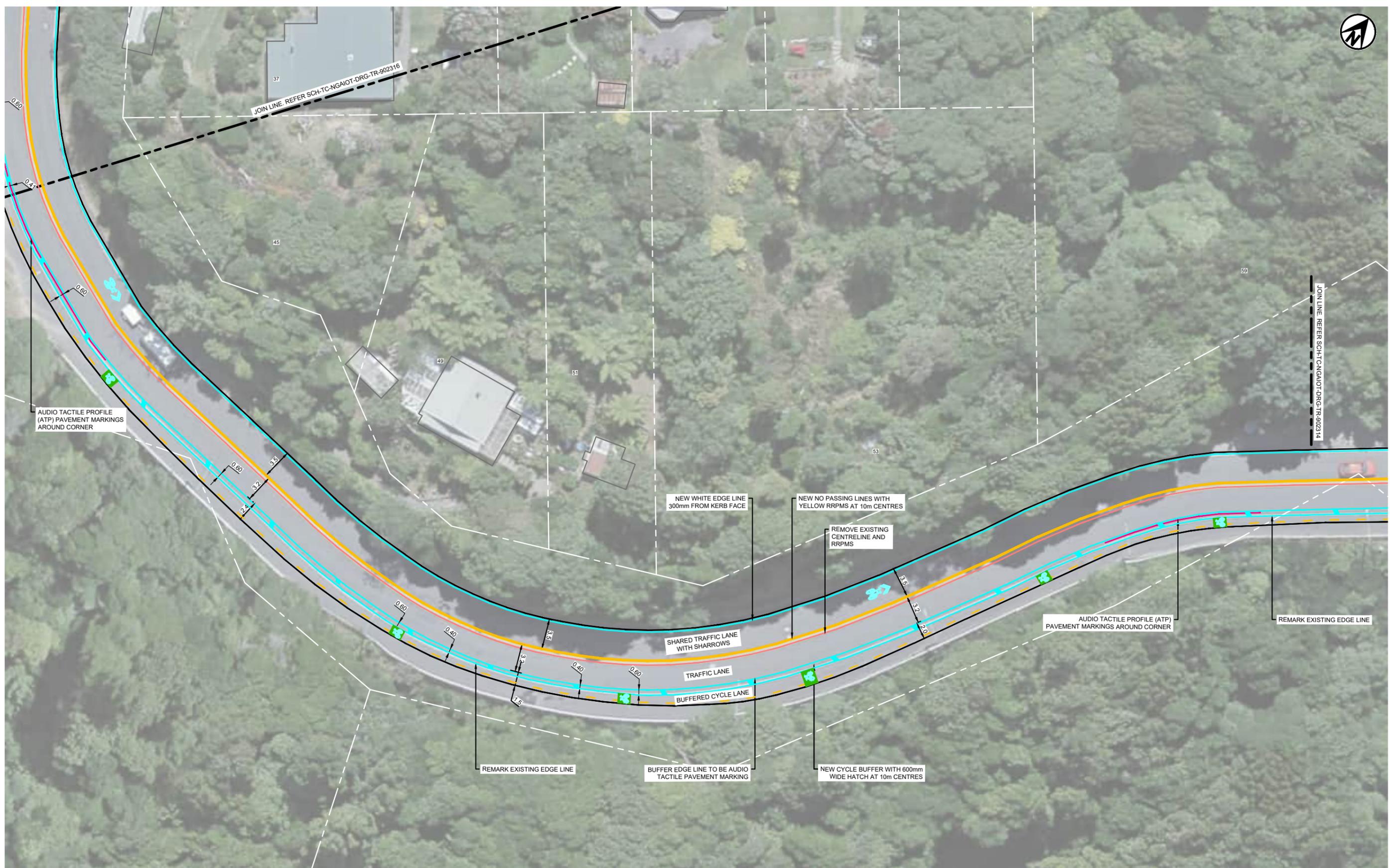
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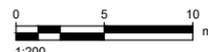
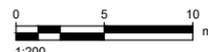
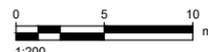
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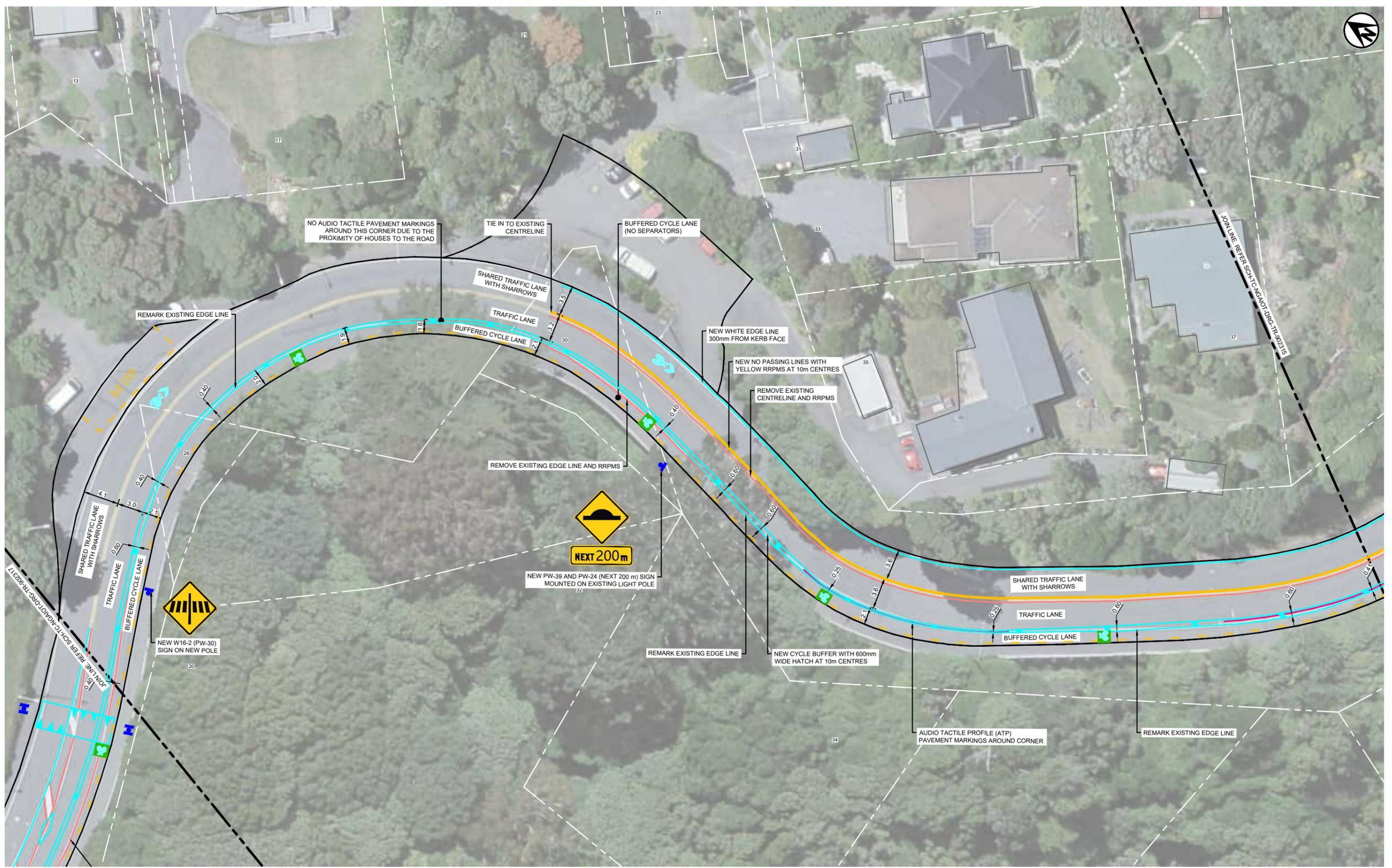
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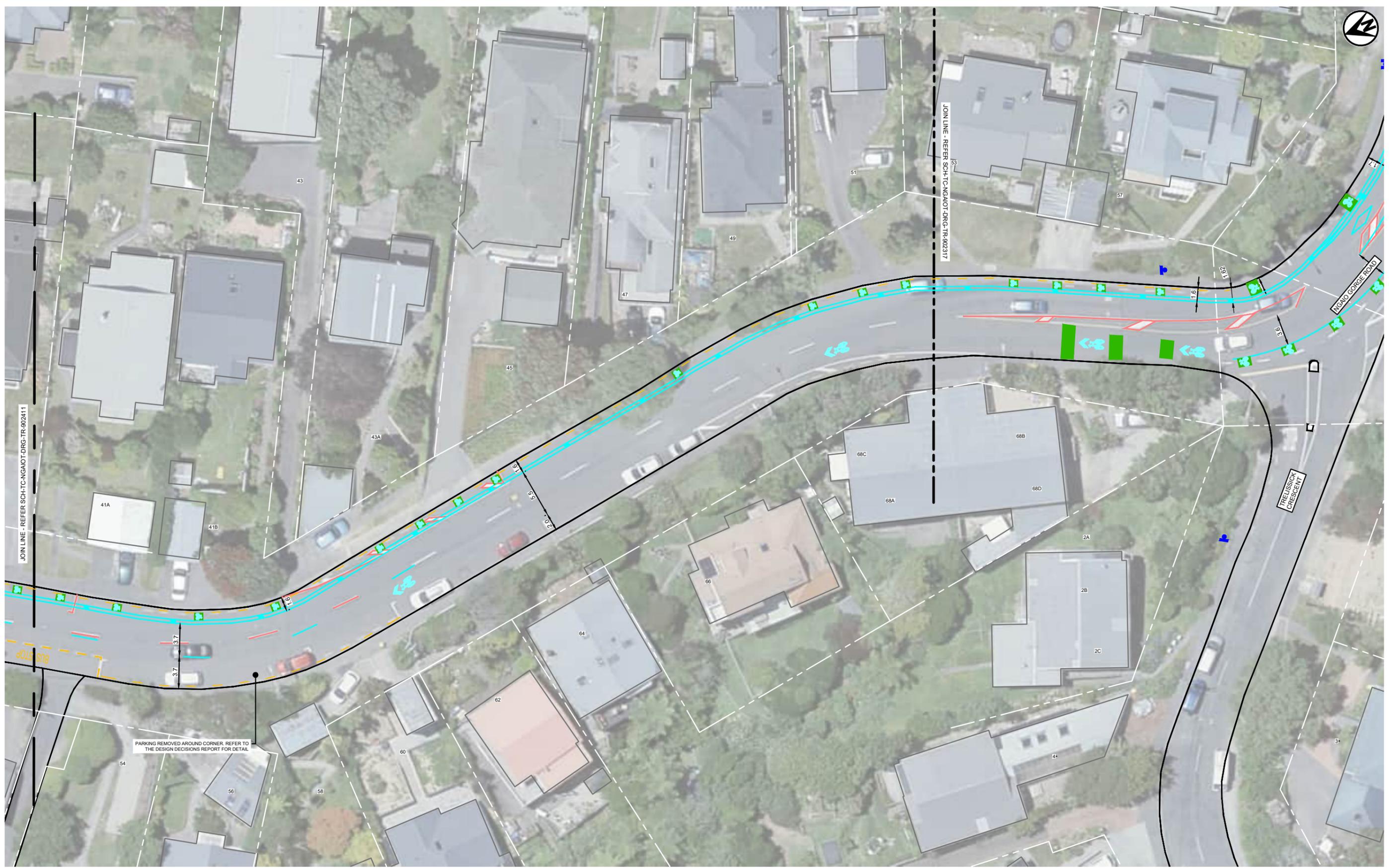
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PROJECT:	TITLE:	DOCUMENT:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	KENYA STREET GENERAL ARRANGEMENT SHEET 1	SCH - TC - NGAIoT - DRG - TR - 902410
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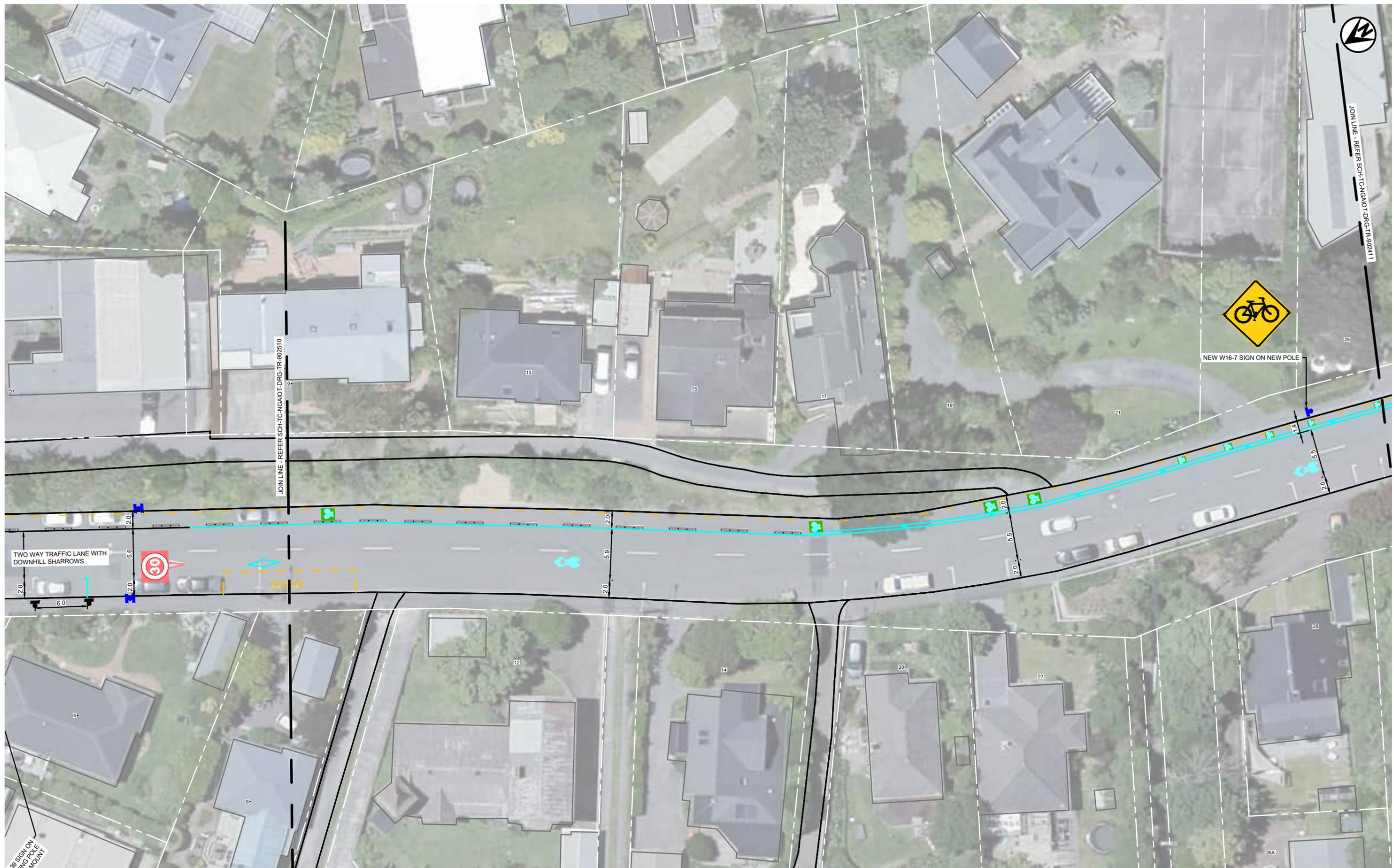
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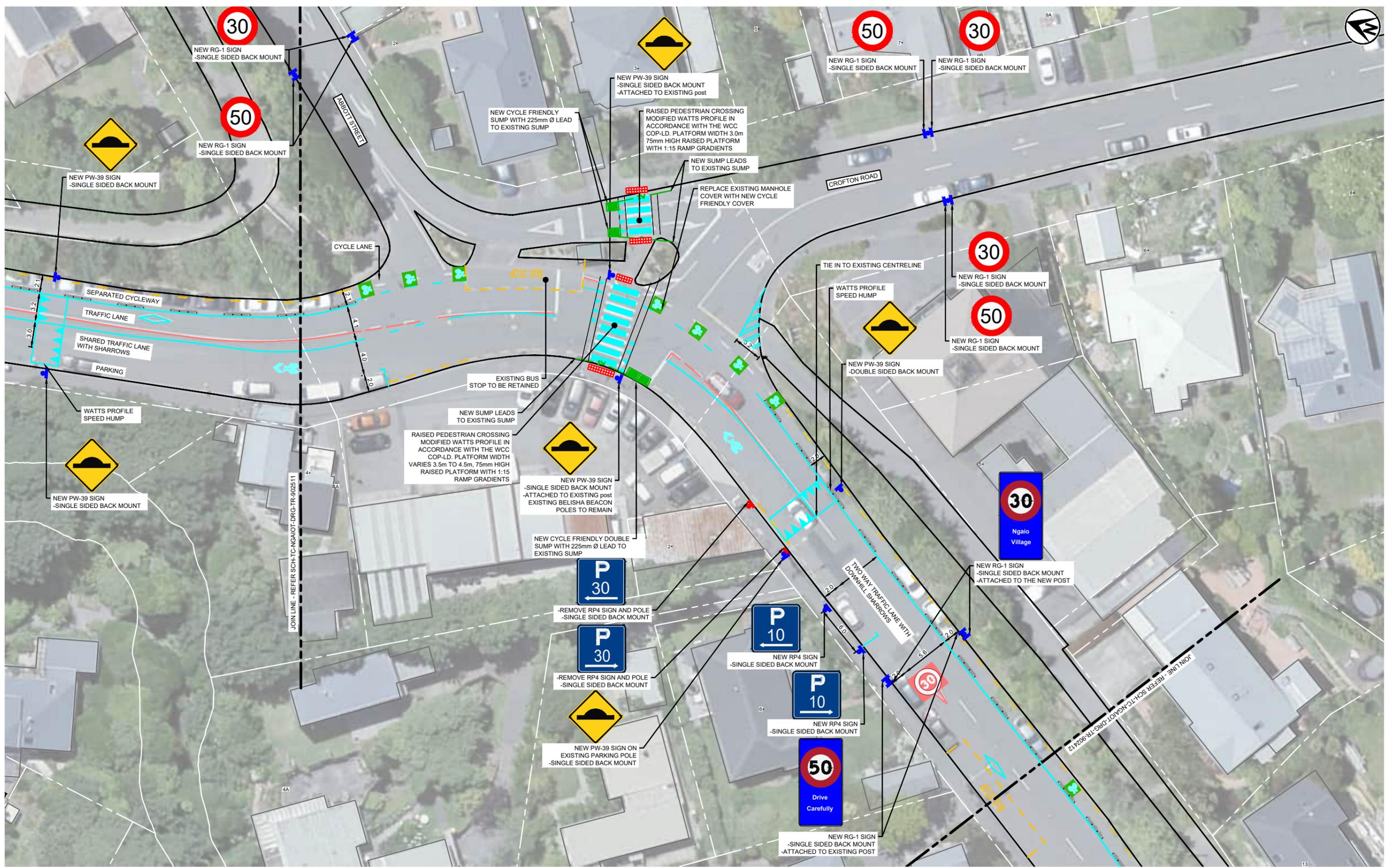
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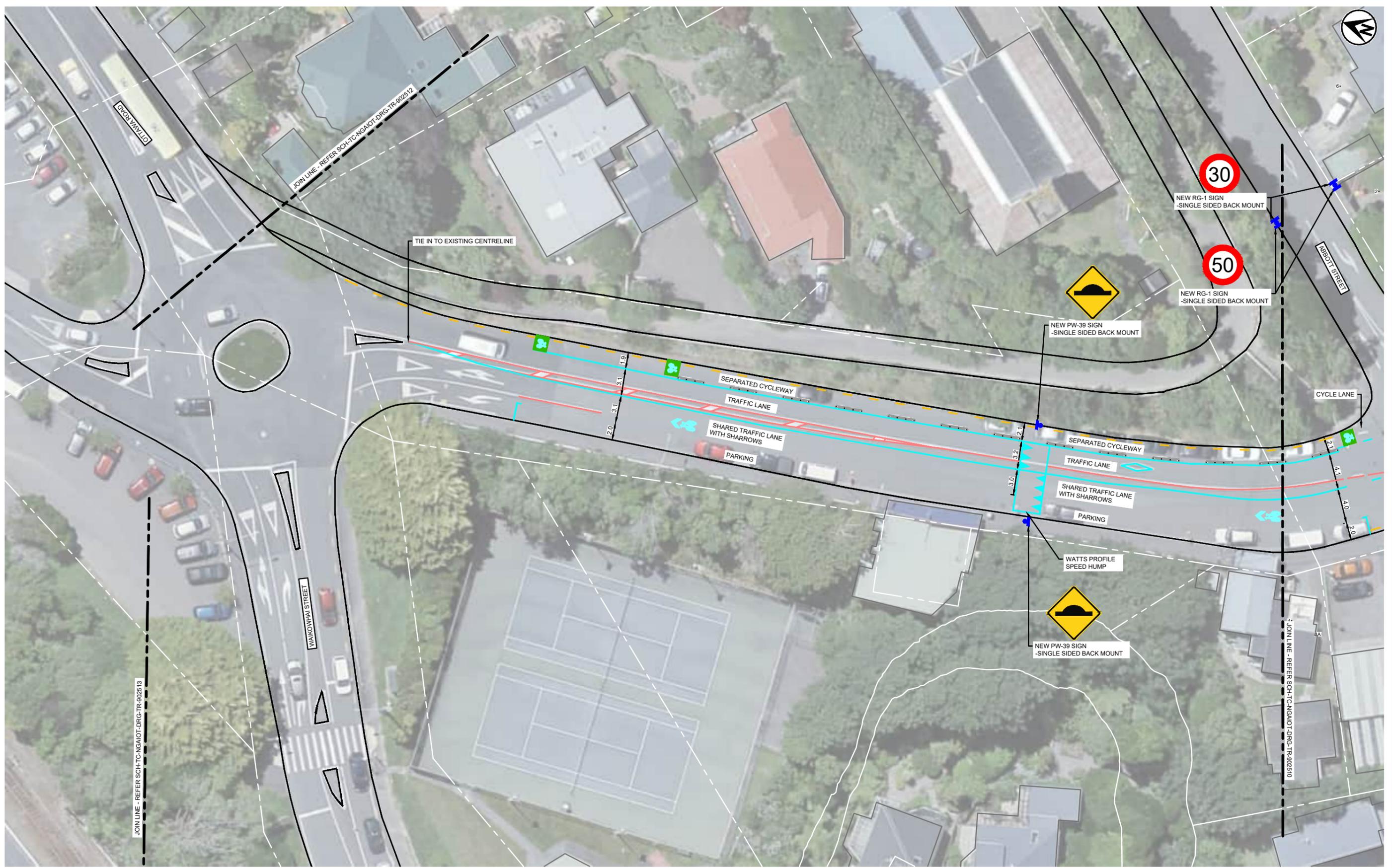
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	NGAIO TRANSITIONAL			
TITLE:	CROFTON ROAD			
	GENERAL ARRANGEMENT			
	SHEET 2			
DOCUMENT:	SCH - TC - NGAIoT - DRG - TR - 902511	C03		

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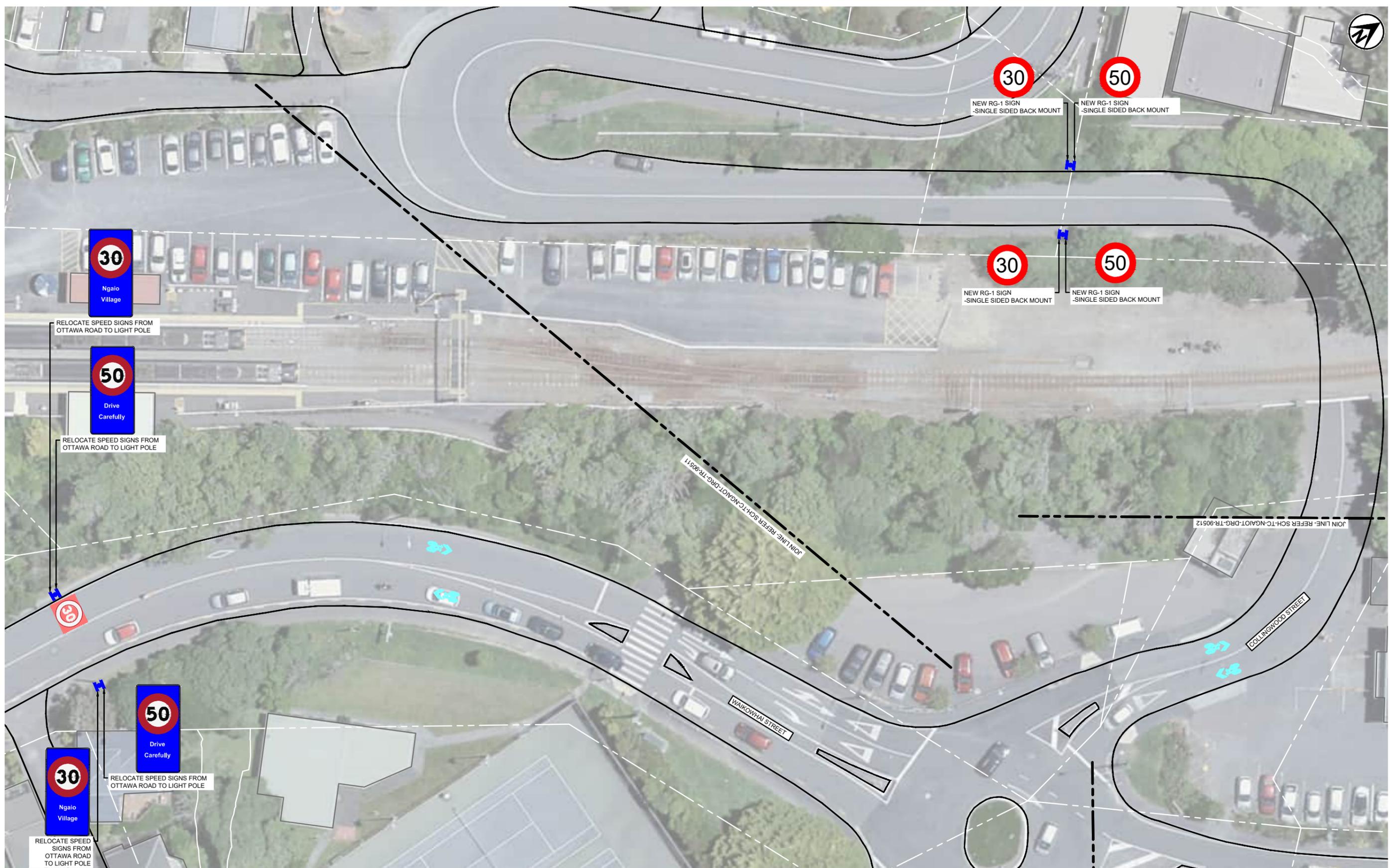
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PROJECT:	TITLE:	DOCUMENT:
TRANSITIONAL CYCLEWAYS NGAIO TRANSITIONAL	CROFTON ROAD GENERAL ARRANGEMENT SHEET 4	SCH - TC - NGAIOT - DRG - TR - 902513

ORIGINATOR	PROJECT	WORK STREAM	TYPE	DISCIPLINE	SHEET	REVISION
					42	C03

From: [Billy Rodenburg](#)
To: [Claire Pascoe](#)
Cc: [Jonathan Kennett](#); [Bri Peters](#)
Subject: RE: Ngaio Connection - Kenya St questions, and draft observations
Date: Thursday, 10 November 2022 10:53:39 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.jpg](#)
[image007.jpg](#)

Hi Claire,

I think there may be two questions here.

1. Why are the two numbers presented different (162 against 205)? **This is because the 162 is the number surveyed accounting for the construction. However, it seems we have done our maths wrong, so that number should be 160.**
2. What is the parking inventory per street in this area? **Here is the breakdown per street before construction is considered. The inventory is based on parking along one side only. It should be noted there is likely more capacity in Trelissick Crescent as the low volumes mean that parking on both sides with a single traffic lane where space permits (such as the south end opposite Perth Street)**

Street	Inventory
Trelissick Crescent	100
Ngaio Gorge Road	7
Kenya Street	98

Our parking survey included Trelissick Crescent and Crofton Road which where the alternate parking available during the road works. On street parking demand displaced by the works to park on these side streets was counted by this.

Is this the right level of information?

Thanks,

Billu

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Claire Pascoe <claire.pascoe@wcc.govt.nz>
Sent: Wednesday, 9 November 2022 5:06 pm
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Cc: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>; Bri Peters <Bri.Peters@wcc.govt.nz>
Subject: FW: Ngaio Connection - Kenya St questions, and draft observations

Hi Billy,

█ from the library yesterday has sent through the email below and I'm just making a bit of a plan to respond to him.

I wondered if you might be able to help me uncover the answer to his question about the Kenya St inventory – what was considered the 'overall section' and whether there are any issues with the numbers, particularly given the works on the days it was surveyed?

I'm going to talk to Pam Brown about the speed data, where exactly it's taken and how far back we can get it from. I think maybe getting another speed count organised as part of the baseline survey could have some merit, so we can understand speed changes in a few places on Kenya St (doesn't impact this consultation though).

In terms of CAS, I'm going to respond that CAS is the standard way of tracking crash data for transport projects, but we definitely acknowledge this is likely to be an underestimation, particularly for active users. For that reason, our baseline survey asked people who cycle the route whether they've had a near miss and we'll be tracking this afterwards as well.

If you could just help draft me a response for the parking survey bit, that'd be grand,

Claire

From: █
Sent: Wednesday, 9 November 2022 3:03 pm
To: Claire Pascoe <claire.pascoe@wcc.govt.nz>
Subject: Ngaio Connection - Kenya St questions, and draft observations

Great thanks Claire

I understand the huge pressure that you and your team are under. I found the meeting extremely useful. But to make an informed submission I need more information/data.

Some of my draft notes and questions

Parking



This leaves 107 parks that are not on Kenya Street, and could be available for use. Over 60% of our parking are planned to be removed, so where exactly are the Kenya Street residents and visitors to find nearby unrestricted parking spaces.

Ideally it would be great to have a map of the whole “Kenya Street Section” that shows all the unrestricted carparks, or a table with the locations of all parks,

- The parking surveys were eight counts carried out over two days, while a large section of the road/parks were closed.
- There were road works at the top of Kenya Street and Trelissick Crescent on the days of the surveys, so not typical at all. All residents on Kenya Street and Trelissick Crescent within 130 metres of the Kenya Street/Trelissick Cres corner were told not to park in the area or they would be towed away. The Parking survey state that they “works were affecting the parking supply in this area (on both Kenya St and Trelissick Crescent). The parking occupancy data has been adjusted”. 25 spaces were closed, which is 26% of the survey area, this is significant. Extrapolating data from the rest of Kenya Street is not valid as the residential characteristics are quite different for the top of Kenya Street and the bottom.
- Section 5.1 states “There are no shops or commercial activities in this area”. There is a food takeaways at 2 Kenya Street, and Motor Doctors at the corner of Kenya Street and Crofton Road. Motor Doctor’s customers occupy 15 -20 car parks for most of the day, on business days
- Section 5.1 states the area assessed was Kenya Street and the top of Ngaio Gorge Road. No assessment was carried out for Trelissick Crescent, but this is the proposed replacement car parking. This is where many residents parked during the construction
- The report uses Litman’s occupancy of 85%, which I understand was for urban not suburban parking, and much about the use of pricing to change habits.

The parking report concluded that for Kenya Street “entire section has an occupancy less than 40%”, of the 98 current spaces. Removing 63 of the 98 spaces leaves 36 spaces, which would mean an occupancy of 114%, significantly above the 85% maximum. **This is not stated in the report**

I believe that this report should have appendices with all the data

Speed.

- The MCA states the “Recorded 85th percentile speeds (46 km/hr) are lower than the posted speed limit (50 km/hr)”.
- However, the WCC has rated the three corners at the top of Kenya Street to be no more than 35km/hr with the south bound sign outside 37 Kenya Street. Note there are 5 WCC 35km/hr speed guideline signs plus one electronic speed warning sign. The proposal is not doesn’t mention the 35km/hr section, and as this is a safety traffic calming measure that must remain in place. The recorded speeds (46 km/hr) is 11km/hr or **131% of the speed guidance for this area.**
- Kenya Street should not be considered as being all the same for traffic management and of the street has very different characteristics.
 1. From the Ngaio end, up to 41/52 Kenya street, it is
 - i. About 360m at approximately 4% gradient
 - ii. Relatively straight
 - iii. Speed limit 50km/hr
 - iv. About 60 houses, any very few that do not have some off-street parking
 - v. Estimated on street car parks 76
 2. From the Ngaio from about to 43/54 Kenya Street to Ngaio Gorge Road, it is
 - i. About 140m relatively flat
 - ii. Three tight corners

- iii. The WWC have a speed advisory speed limit of 35km/hr
- iv. About 21 houses/apartments
- v. Four houses and one apartment **have no off-street parking**
- vi. The lack of parking in Trelissick Crescent means the Trelissick Crescent frequently park in Kenya Street.

The speeds should have been taken for both sections of Kenya Street, the 50km/hr zone and the section under the WCC 35km/hr speed guidance signs.

- A new survey should be conducted in the 35km/hr section.
- The report must be updated or notes that no speeds were recorded for the 35km/hr section of Kenya Street
- Reporting only one statistic is not that useful.
 - It would be very interesting to see the speed of the top 15%.
 - How big was the survey sample
 - Is it split between north bound and south bound traffic?
 - The data and graphs should be available as a graph
- Over the past two years the Ngaio Gorge Road has been under construction and for most of the time traffic held up by traffic lights. This has generally reduced the speed of traffic as the slower vehicles in a queue behind them, so all vehicles are travelling at this lower speed. The results must be used with caution

The proposed plan appears to be that, along the 500m of Kenya Street from Ngaio direction, ie south, the speed goes from 30km/hr into 250m of 50km/hr, down to 35km/hr and then down to 30km/hr. This is mad, and will burn extra fuel in short gains of speed and braking. **All of Kenya Street should be 30km/hr**, now, not left for some time in the future.

Accidents

The report uses the Crash Analysis System data collected by Police.

About 85% of crashes recorded by NZ Police are attended by Police officers, who interview those involved. The reports generated have a lot of detail. The remainder of crashes recorded by NZ Police are reported by one or more people involved in a crash to the front counter of a Police station. These reports have less detail. They are for non-injury crashes and sometimes the crash has been one where a vehicle has struck a parked vehicle and then moves on, so the time of the crash is unknown.

Under-reporting of crashes is a known issue. It is estimated that there is 40% under-reporting of serious crashes. There has been some thought given to obtaining crash data from other sources for reporting purposes but there has been little or no progress made. Integrating data from various sources to create consistent information is complicated.

Source Warwick Taylor, Senior Crash Data Quality and Access Advisor, Crash Data Processing Team

I believe that a summary of this statement from the CAS team should be in the report, at least in a footnote.

Best regards

[Redacted signature block]

[REDACTED]

From: [Claire Pascoe](#)

Sent: 09 November 2022 13:52

To: [REDACTED]

Cc: [Bri Peters](#); [Trish Given](#)

Subject: RE: WCC consultation policy

Hi [REDACTED]

Lovely to meet you at the library yesterday, and apologies again we were late to the session. Our whole team had a bit of a rough day with a few members off sick.

Here's the list of people who were there yesterday:

Me (Claire Pascoe) – Transitional Programme manager
Jonathan Kennett (briefly) – Project Lead
Bri Peters – Engagement Lead
Trish Given – Activation Lead
Billy Rodenberg – consultant design lead.

Let me know what information requests you have outstanding, and we'll aim to get them back to you as soon as possible.

Kind regards,

Claire

From: Trish Given <Patricia.Given@wcc.govt.nz>

Sent: Wednesday, 9 November 2022 1:05 pm

To: Claire Pascoe <claire.pascoe@wcc.govt.nz>

Subject: FW: WCC consultation policy

From: [REDACTED]

Sent: Wednesday, 9 November 2022 12:46 pm

To: Trish Given <Patricia.Given@wcc.govt.nz>

Subject: Re: WCC consultation policy

Hi Trish

How at you, I am sure you are flat out.

Can you please send me the names and roles of the WCC team at the Cummings Park Library meeting yesterday?

Also the email address of the team leader as I want to send her the information that I am waiting on to make my submission.

Thanks in anticipation

Best regards



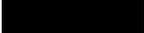
On 8 November 2022 5:21:01 pm Trish Given <Patricia.Given@wcc.govt.nz> wrote:

[Significance and Engagement Policy – June 2018 \(wellington.govt.nz\)](#)

Patricia Given (She/Her)

Activation Lead | Transport Planning

City Design | Wellington City Council

M 

E patricia.given@wcc.govt.nz | W Wellington.govt.nz | W TransportProjects.org.nz | W

BikeThere.org.nz |

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From: Jonathan Kennett
To: John Lieswyn; Megan Gregory
Subject: RE: Nga o crossing stats
Date: Thursday, 27 October 2022 5:48:00 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.jpg](#)

Morning John and Megan,

The WCC safety engineer has just requested the updated drawing. Could you let me know what time this morning you will be able to send that

It is clear that he starts work early and finishes early too

Cheers
Jonathan

From: John Lieswyn <john@viastrada.nz>
Sent: Wednesday, October 26, 2022 3:20 PM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Cc: Megan Gregory <megan@viastrada.nz>
Subject: Re: Ngaio crossing stats

Hi Jonathan,

No problem, we'll update the graphic sent earlier to include the other approach
We also discovered a small calculation error, so please wait for the updated graphic

The proposed fee for this is slightly more than 8 hours - \$1,800, mainly Megan today and a bit of time from David and I yesterday. We've also been checking in with Glen for advice as the situations here are not straightforward compared to Austroads! Is that fee proposal okay?

John Lieswyn [REDACTED]

From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Date: Wednesday, 26 October 2022 at 2:51 PM
To: Megan Gregory <megan@viastrada.nz>, John Lieswyn <john@viastrada.nz>
Subject: RE: Ngaio crossing stats

Hi there,

I just missed the safety engineer. He must have left earlier than planned for some reason

This morning he left a message for me:

Hi Jonathan, I think it would be worth checking in the uphill direction. Design driver eye height (1.1m) and I think a child height pedestrian (6yo at about 1m).'

I think to be sure we get this over the line tomorrow you should verify in the other directions.

Would you be able to do that today. And also, could you please send a price for this work, and I'll arrange the budget for you.

Thanks
Jonathan

From: Megan Gregory <megan@viastrada.nz>
Sent: Wednesday, October 26, 2022 2:00 PM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>; John Lieswyn <john@viastrada.nz>
Subject: RE: Ngaio crossing stats

Hi Jonathan,

We think these two sight lines and calcs cover the critical cases discussed – we can verify in other directions if needed?

In short, visibility to the pedestrian crossing appears to just scrape in on the sight distance requirements. We're comfortable that the 53km/h approach speed in the calc for the speed hump is overly conservative



Kind regards,
Megan

Megan Gregory 
Senior Transportation Engineer

 Sustainable active transport is worth celebrating during Biketober

From: Jonathan Kennett <Jonathan.Kennett@wrc.govt.nz>
Sent: Wednesday, 26 October 2022 12:30 pm
To: John Lieswyn <john@viastrada.nz>; Megan Gregory <megan@viastrada.nz>
Subject: RE: Ngaio crossing stats

Here you are John and Megan The designs for Ngaio Gorge Rd

From: Jonathan Kennett
Sent: Tuesday, October 25, 2022 6 00 PM
To: john@viastrada.nz
Cc: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Subject: FW: Ngaio crossing stats

Hi John,

Please find attached the crossing measurements I used a tape measure for height, walking wheel on the footpath for distance (so not a straight line), and clinometer for gradient

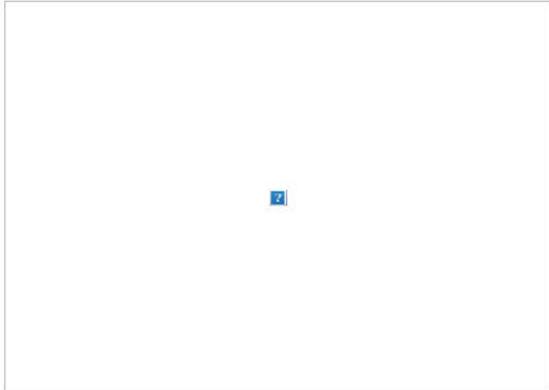
Please let me know if this is all you need I live a few minutes walk from this crossing and can go out tonight and remeasure things

I also noticed locals crossing at the site chosen for the crossing and asked one woman why she chose that location and she said it was dangerous road and that was the best place to cross

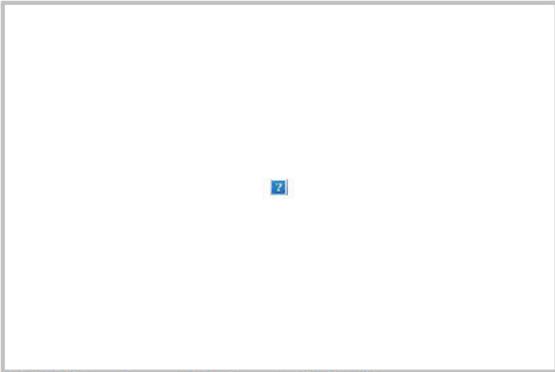
Visibility of the hump warning sign on the right side of the road, approaching from the west (Ngaio Gorge Road) is 50m

Visibility of hump warning sign on right side of road approaching from the east (Kenya St) is 36m

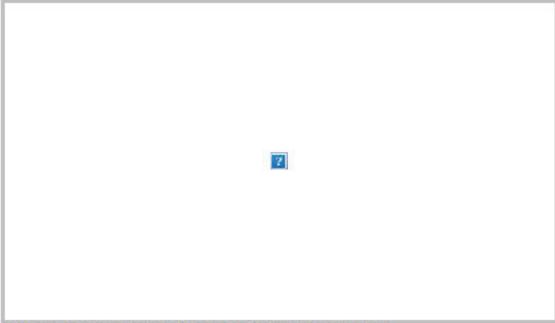
Vertical crest from RSP is 42-50m depending on side of road measured to Centre is approximately 46m



Visibility looking down from south side of crossing at 1.1m is 98m On the other side of the road it is 65m



Visibility looking up from south side of crossing at 1.1m is 52m



Visibility looking up from north side of crossing at 1.1m is 49m

Gradient from the crossing heading downhill is 7% on both sides of the road

Gradient heading uphill from the crossing on the southern side of the road is 7% for 20m, then 6% for 10m, then 3% for 10m, then -1% for 10m, then -3% for 20m, then -4% heading downhill

Gradient heading uphill from the crossing on the northern side of the road is 6% for 40 metres, then -2% for 10 metres, then -4% for 10 metres then minus 5% for 10 metres, then -4%

Cheers
Jonathan

--
Kennett Brothers
3rd Floor, 4 Bond Street, P O Box 11310, Wellington 6142 Phone 04 499 6376 www.kennett.co.nz



From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#); [Ann-Marie Head](#)
Subject: RE: Ngaio Cycleway approved by council today
Date: Wednesday, 14 December 2022 5:01:59 pm
Attachments: [image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)
[image009.jpg](#)

Thanks Jonathan,

I'd kind of figured the change to the transition markings already following the discussion in the review session last week and we showed the Waka Kotahi Design in the Kilbirnie 90% Drawings as a result. We can copy them across from Kilbirnie

Following the review session we have also summarised the additional details required for 100% drawings;

- Split each general arrangement into separate signage and markings plans (48 sheets)
- Project notes page incorporating the Specification (1 sheet)
- Layout plans for Cameron St kerb buildout, Loading zone cycle platform, and 2x raised crossings (4 sheets)
- Service plans for 2x raised crossings (including lighting) (2 sheets)
- Typical details (cycleway markings, poles and foundations, humps & tables, ramps & tactiles, pavement and stormwater details) (6 sheets)

What do you think of this breakdown?

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Wednesday, 14 December 2022 4:41 pm

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>; Ann-Marie Head <ann-marie.head@abley.com>

Subject: RE: Ngaio Cycleway approved by council today

Hi Billy,

This is all we have – I think you are aware of them all.

The only other thing we need to do is be consistent with the transition from cycle lane to sharrows. Here is the guidance; [Coloured surfacing principles: design guidance note \(nzta.govt.nz\)](https://www.nzta.govt.nz/resources/coloured-surfacing-principles-design-guidance-note/) I'll be in touch about this tomorrow, but it looks like we may have to follow the national guidance, rather than the WCC design. Sorry about that.

90% to 100% Design change for Ngaio Transitional Project

6 December 2022

Change requested	Comments
<p>Remove Stage 2 (extension of clearway times on Kaiwharawhara Road from 4-7pm to 2-9pm).</p> <p>Pull the proposed Stage 3 changes that relate to 220 metres on Kaiwharawhara Road back to early 2024.</p>	<p>We will have only two Stages.</p> <p>Stage 1 will be installed in March-June 2023 and includes 220 metres of a 4-7pm clearway on Kaiwharawhara Road.</p> <p>Stage 2 will simply replace the 220 metres of clearway sections with the cycleway (except for the loading zone, which will remain in place).</p>
<p>Change the P30 car parks on Kaiwharawhara Road to P60s</p>	<p>This is to allow people to stay for a lunch meeting, gym session, etc.</p>
<p>Remove the raised pedestrian refuge crossing from Kaiwharawhara Road and delete from the Traffic Resolution 18 metres of no stopping restrictions at 28 Kaiwharawhara Road (to retain 4 carparks).</p>	<p>This is to retain 4 carparks outside the gym, and will require sacrificing the median refuge crossing.</p> <p>Designer to consider how to make it safer for people turning right into Cameron St. This will not require future changes to the traffic resolution.</p>
<p>Replace two car parks at the bottom of Cameron Street on the eastern side with no stopping restrictions.</p>	<p>Traffic safety – residents complained about having to cross the centre line.</p>
<p>Delete from the Traffic Resolution the conversion of 4 P60 angle car parks to 2 P60 parallel parks at the bottom of Cameron Street.</p>	<p>This is to offset the loss of the two car parks on the other side of Cameron St.</p> <p>Will remove flax so cars can park further off the street.</p>

	Add a painted buffer and move Cameron St centre line over.
Retain the loading zone at 53 – 55 Kaiwharawhara Road (this will no longer be removed at Stage 2).	This is to enable tow trucks to drop off cars at Auto Electrician. Would be good to remove this in future if the business changes.
Move bus stop 5447 outside 52 Kenya Street south by 14 metres and install one unrestricted car park.	This is to add one car park outside 52 Kenya St.
Remove 40 metres of cycleway along Crofton Road, from the roundabout heading towards the city, and install three P10 car parks.	This short stay car parking for dairy customers heading towards the city.
Add an additional wayfinding sign to direct people to the Kaiwharawhara Bridle Track.	Wayfinding to help those that do walk/cycle up Fore St.
Mark out the parking limits at driveway entrances on Kenya Street.	Requested by residents.
Investigate a kerb build out on Crofton Road pedestrian crossing to improved sightlines.	I doubt this is possible due to road geometry.
Remove flaxes in front of angle parks at the bottom of Cameron Street so that long vehicles can park without sticking out on the road.	A simple fix. A submitter noted that these angle parks are on private land. Investigation has revealed that WCC has an agreement to use this land for car parks.
Updated the Parking Management Plan – removing private roads and Fore Street.	Fore Street is too narrow for parking. The plan has since been updated.

Separate but associated to these designs:

1. There will be a separate minor improvements project on the Kaiwharawhara Bridle Track in the first half of 2023. This project is to slow cyclists down and encourage safe sharing of this shared path. Also, in 2020, council built a separate walking only track from the end of Cameron Street to the top of the Bridle Track, so there is now an alternative track for

- walkers who do not want to share a track with cyclists.
2. Purchase of private road at 1 Curnow Way.

Cheers
Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>
Sent: Wednesday, 14 December 2022 2:44 pm
To: Ann-Marie Head <ann-marie.head@abley.com>; Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Subject: RE: Ngaio Cycleway approved by council today

Great news, nice work Jonathan.

Thinking of next steps, are there any other changes to the plans required beyond what you have already sent through? I'm keen to collate them all in to one set of changes to update.

Thanks,
Billy

From: Ann-Marie Head <ann-marie.head@abley.com>
Sent: Wednesday, 14 December 2022 1:23 pm
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>; Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>
Subject: RE: Ngaio Cycleway approved by council today

Hi Jonathan,

That is great news! Well done to you too, I know its been a difficult balancing act at times. An excellent finish to the year!

Ngā mihi,
Ann-Marie

Ann-Marie Head BE(Hons) CEngNZ CPEng IntPE(NZ)
Associate Director, Abley

+64 3 367 9005 | 
abley.com

From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Wednesday, 14 December 2022 1:15 PM
To: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>; Ann-Marie Head <ann-marie.head@abley.com>
Subject: Ngaio Cycleway approved by council today

[EXTERNAL]

Hi Billy and Ann-Marie,

Just a quick note to let you know that council voted to approved the Ngaio cycleway traffic

resolution today. The vote was 7 in favour and 1 against.

Thank you for all the work you have done to help us get this result today.

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |  

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: Ngaio Gorge Road ped crossing issue - URGENT
Date: Tuesday, 25 October 2022 1:44:19 pm
Attachments: [image003.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image007.png](#)
[image008.png](#)
[image009.jpg](#)

Hi Jonathan

We haven't specifically measured the vertical beyond standing on site and confirming you can see to the crossing from the approaches. We note that the crest of the road is near Kenya Street about where you achieve the horizontal visibility.

It might be worth getting Via Strada to provide comment also particularly in light of the feedback which is pushing response from the safety auditors.

Thanks

Billy

[Billy Rodenburg | Civil & Transport Engineer](#)

BE (Hons) CPEng CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Tuesday, 25 October 2022 1:38 pm

To: Billy Rodenburg <brodensburg@tonkintaylor.co.nz>

Subject: RE: Ngaio Gorge Road ped crossing issue - URGENT

Hi Billy

Are you able to confirm that there are adequate sight lines through the vertical?

If not, I could get Via Strada to do the calculations this afternoon.

Cheers

Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>

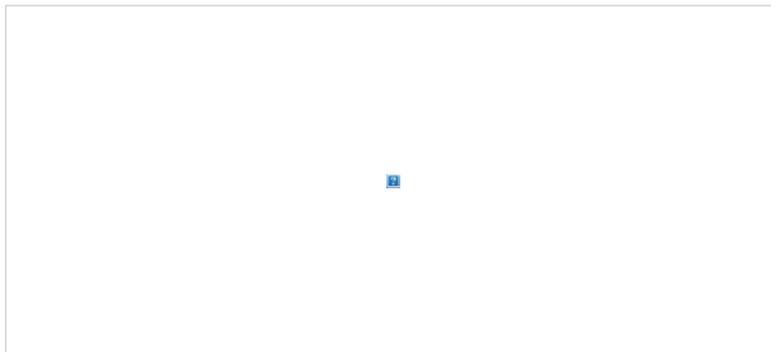
Sent: Tuesday, October 25, 2022 11:55 AM

To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Subject: RE: Ngaio Gorge Road ped crossing issue

Hi Jonathan

The forward visibility is discussed in the design decisions report which was also reviewed by the safety auditors as part of the 90% design audit. I would suggest the reason it is not raised in the safety audit is because this was already completed prior to their audit. In summary, the speed humps reduce vehicle approach speeds so that the existing sight distance is compliant.



Thanks

Billy

[Billy Rodenburg | Civil & Transport Engineer](#)

BE (Hons) CPEng CMEngNZ

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[\[redacted\]](#)

From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Tuesday, 25 October 2022 11:28 am

To: Billy Rodenburg <brodensburg@tonkintaylor.co.nz>

Subject: Ngaio Gorge Road ped crossing issue

Hi Billy

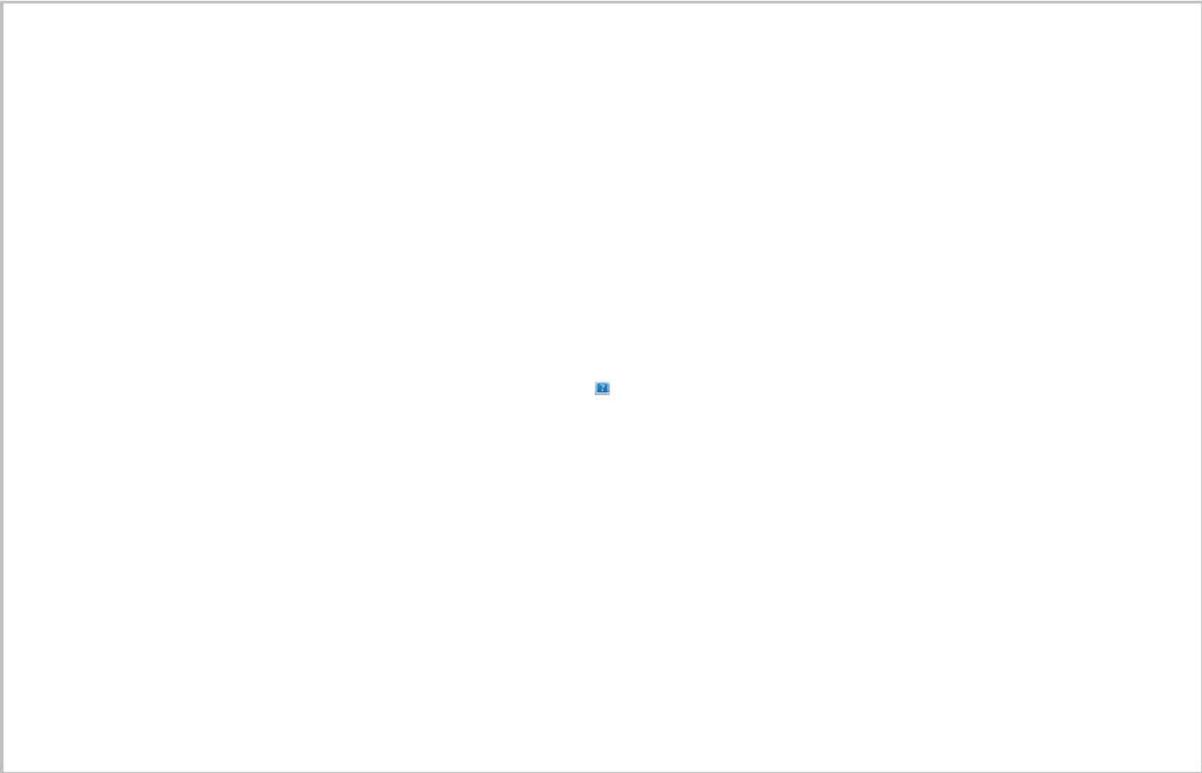
I am hoping you can help with some advice on a review comment received today about the crossing at the top of Ngaio Gorge Road

I reviewed the attached documents with one serious safety concern as follows:

- The proposed zebra pedestrian crossing on the Ngaio Gorge Road west for Trellissick Cres is unsafe due to the gradient and curve layout of the carriageway which limit the forward visibility required between drivers and pedestrians to safely use this proposed zebra crossing. I suggest that a safety audit is conducted for this proposed zebra crossing before the public consultation proceeds.

This has been through RSA and was not picked up in the past. Here are the issues that were raised.

How would you respond to the highlighted concern?



Cheers

Jonathan

Jonathan Kennett

Project Lead - Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz  

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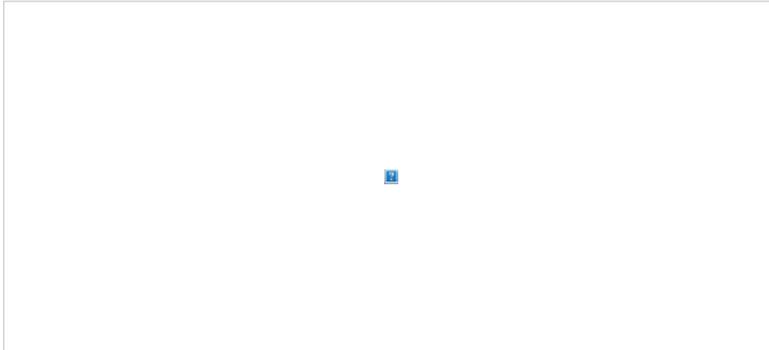
Image removed by sender.



From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: Ngaio Gorge Road ped crossing issue
Date: Tuesday, 25 October 2022 11:55:27 am
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image007.png](#)
[image001.png](#)

Hi Jonathan,

The forward visibility is discussed in the design decisions report which was also reviewed by the safety auditors as part of the 90% design audit. I would suggest the reason it is not raised in the safety audit is because this was already completed prior to their audit. In summary the speed humps reduce vehicle approach speeds so that the existing sight distance is compliant.



Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Tuesday, 25 October 2022 11:28 am

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: Ngaio Gorge Road ped crossing issue

Hi Billy,

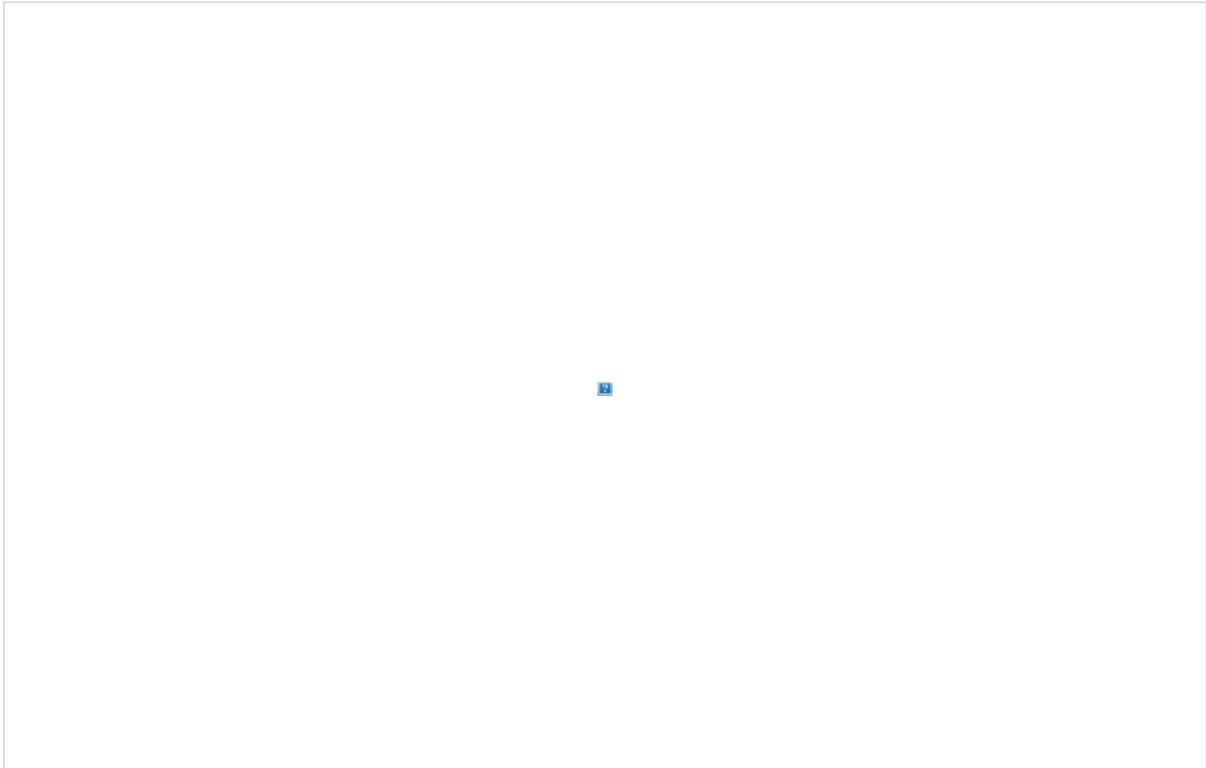
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This has been through RSA, and was not picked up in the past. Here are the issues that were raised.

How would you respond to the highlighted concern?



Cheers
Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council
E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: PMP and minor changes for Ngaio
Date: Friday, 14 October 2022 3:58:07 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.jpg](#)

Hi Jonathan,

Just updating you that the change to Kaiwharawhara parking is taking longer to update than expected. The changes to the graphs and figures are underway but won't be tech reviewed until Monday morning. We will prioritise this on Monday and send it through once complete.

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Billy Rodenburg
Sent: Friday, 14 October 2022 11:22 am
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Subject: RE: PMP and minor changes for Ngaio

Hi Jonathan,

Sounds good, we will update accordingly. This will also change several of the legal descriptions in the traffic resolution, but if suitable we'll wait to update those until your full list of changes on Monday if that is okay?

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Friday, 14 October 2022 11:06 am

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: RE: PMP and minor changes for Ngaio

Hi Billy,

We are going to go ahead with the crossing, and will remove 3 parks at some stage (we are likely to push it out to stage 3, or transformation, as businesses need time to recover from covid and the other changes in the area).

It would be good to think about a raised median – not a crossing – for stage 1. Possibly not worth it, but I'm thinking about how we can improve safety for right turning cyclists heading into Cameron St.

Just finalise the PMP as if we are going to remove 3 parks at corner of Kaiwharawhara/Cameron.

Cheers

Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>

Sent: Friday, October 14, 2022 11:01 AM

To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Subject: RE: PMP and minor changes for Ngaio

Hi Jonathan,

We've drafted the PMP and it's being removed today, but were waiting on confirmation of any changes to the parking at the Kaiwharawhara Road pedestrian refuge as this will impact the percentages reported. How is this going?

Otherwise those changes seem pretty minor.

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Friday, 14 October 2022 10:57 am
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Subject: PMP and minor changes for Ngaio

Hi Billy,

Just a friendly reminder that the parking management plan is due today.

Also, we will have a small list of changes. Nothing major, but two that relate to parking are:

Cameron Street

Change **P24hr Except Residents** to just **P24hr** so that we don't have to install a Residents Parking scheme in Cameron Street, which the residents would pay for.

Kaiwharawhara Road

Change the **Loading Zone** unlimited time to **Loading Zone P10**

I'll get all the changes to you on Monday.

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz | [REDACTED] [REDACTED]

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90% to 100% Design change for Ngaio Transitional Project

6 December 2022

Change requested	Comments	Designers' response
remove Stage 2 (extension of clearway times on Kaiwharawhara Road from 4-7pm to 2-9pm)	We will have only two Stages. Stage 1 will be installed in March-June 2023 and included the 4-7pm clearway on Kaiwharawhara Road. Stage 2 will simply replace the two clearway sections with the cycleway (except for the loading zone, which will remain).	No response required.
Change the P30 car parks on Kaiwharawhara Road to P60s	This is to allow people to stay for a lunch/meeting.	Noted.
Remove the raised pedestrian refuge crossing from Kaiwharawhara Road and delete from the Traffic Resolution 18 metres of no stopping restrictions at 28 Kaiwharawhara Road (to retain 4 car parks).	<p>This is to retain 4 car parks outside the gym, and will require sacrificing the median refuge crossing.</p> <p>Need to consider how to make it safer for people turning right into Cameron St.</p>	<p>Consider making these P60, so that all car parks from Cameron St to Hutt Road on Kaiwharawhara are P60s (you can't go to the gym for only 30min)</p>
Replace two car parks at the bottom of Cameron Street on the eastern side with no stopping restrictions.	Traffic safety as residents complained about having to cross the centre line.	Noted.
Delete from the Traffic Resolution the conversion of 4 P60 angle car parks to 2 P60 parallel parks at the bottom of Cameron Street.	<p>This is to offset the loss of the two car parks on the other side of Cameron St.</p> <p>Will remove flax so cars can park further off the street.</p>	<p>Signs "Look for cyclists when reversing".</p> <p>Specific monitoring and evaluation.</p>

	Consider a painted buffer/moving Cameron St centre line over.	
Retain the loading zone at 53 – 55 Kaiwharawhara Road (this will no longer be removed at Stage 2).	This is to enable tow trucks to drop off cars to Auto Electrician. Would be good to remove this if the business changes.	Stage 2: Widen footpath by 500-800mm and have a segregated shared zone, and also enough space on loading zone for cyclists to ride through loading zone when no vehicles were in it.
Move bus stop 5447 outside 52 Kenya Street south by 14 metres and install one unrestricted car park.	This is to add one car park outside 52 Kenya St.	Noted
Remove 40 metres of cycleway along Crofton Road, from the roundabout heading towards the city, and install three P10 car parks.	This short stay car parking for dairy customers heading towards the city.	Noted.
Remove two wayfinding signs that direct people down Fore Street.	Because we are not making this a shared path.	Noted
Add an additional wayfinding sign to direct people to the Kaiwharawhara Bridle Track.	Wayfinding to help those that do walk/cycle up Fore St.	Noted
Mark out the parking limits at driveway entrances on Kenya Street.	Requested by residents.	Noted.

Investigate a kerb build out on Crofton Road pedestrian crossing to improved sightlines.	I doubt this is possible due to road geometry.	Won't work due to vehicle tracking requirements.
Remove flaxes in front of angle parks at the bottom of Cameron Street so that long vehicles can park without sticking out on the road.	A simple fix.	Noted.
Updated the Parking Management Plan – removing private roads and Fore Street.	Fore Street is too narrow for parking.	Impact on commuters is high. Impact on short stay visitors is low. Impact on residents expected to be low as they have off-street parking, but if the impact is
Remove the speed hump to the west of the Crofton Road raised pedestrian crossing (close to the Crofton Road Dairy).	The raised pedestrian crossing is sufficient to improve safety given that drivers are approaching from a 30km/h speed zone.	

Note that there will be a separate minor improvements project on the Kaiwharawhara Bridle Track in the first half of 2023. This project is to slow cyclists down and encourage safe sharing of this shared path. Also, in 2020, council built a separate walking only track from the end of Cameron Street to the top of the Bridle Track, so there is now an alternative track for walkers who do not want to share a track with cyclists.

Separate to these designs:

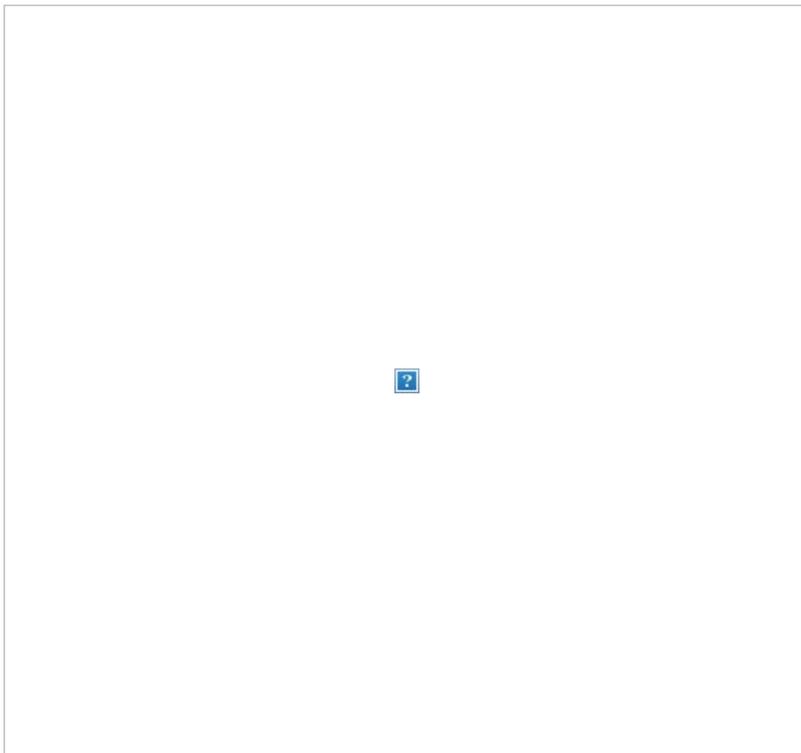
1. Safety treatments on the Kaiwharawhara Bridle Track as part of a Minor Improvements project.
2. Purchase/easement of private road at 1 Curnow Way.

From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Cc: [Claire Pascoe](#)
Subject: RE: Specific sightline issue for Kaiwharawhara Road pedestrian crossing.
Date: Wednesday, 12 October 2022 8:29:50 am
Attachments: [image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)
[image009.png](#)
[image010.png](#)
[image001.png](#)
[image002.png](#)

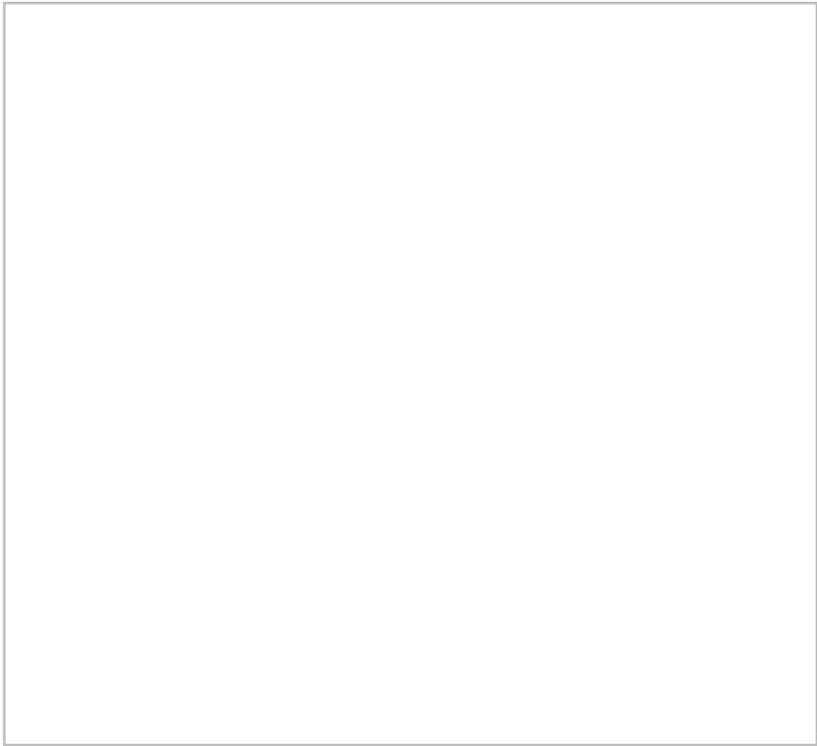
Hi Jonathan,

It is possible but would require the following compromises in the design;

- Moving the pedestrian crossing closer by a carpark would remove the functionality of the turning bay as drivers would no longer be able to wait in it clear of through traffic (they would be at an angle to get around the island and sticking out in to the northbound lane).
- It would also require increasing the radius of the kerb buildout at the end of Cameron Street to allow for tracking of large vehicles, increasing the speed light vehicles can travel around this turn.
- An additional parking space would also be removed on the west side during Stages 1 and 2 (this is removed completely in Stage 3)
- Vehicle tracking below for the current location (this is the body only without the 0.5m offset usually applied to physical obstructions).



12.5m vehicle



8m vehicle

We also considered locating the refuge on the north side of Cameron Street but this would still remove upstream carparks and also not align with the footpath on the south side of Cameron Street.

Thanks,
Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Sent: Wednesday, 12 October 2022 7:21 am
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Cc: Claire Pascoe <claire.pascoe@wcc.govt.nz>
Subject: FW: Specific sightline issue for Kaiwharawhara Road pedestrian crossing.

Hi Billy,

Are you able to answer the question highlighted below.

Thanks
Jonathan

From: Dennis Davis <Dennis.Davis@wcc.govt.nz>
Sent: Wednesday, October 12, 2022 6:52 AM
To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>
Cc: Claire Pascoe <claire.pascoe@wcc.govt.nz>; Billy Rodenburg <brodenburg@tonkintaylor.co.nz>
Subject: RE: Specific sightline issue for Kaiwharawhara Road pedestrian crossing.

Hi Jonathan,

Just back from a few days away.

I think the safety benefits of achieving the sightlines for the pedestrian crossing outweigh the loss of parking.

However, is there any way that the pedestrian crossing can be shifted closer to the Cameron Street corner, so that another carpark can be provided downstream of the crossing?

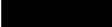
Dennis

Dennis Davis

Principal Transport Engineer

| Transport & Infrastructure | Wellington City Council

E dennis.davis@wcc.govt.nz | W Wellington.govt.nz |

M 

From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Monday, 10 October 2022 3:04 pm

To: Dennis Davis <Dennis.Davis@wcc.govt.nz>

Cc: Claire Pascoe <claire.pascoe@wcc.govt.nz>; Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: Specific sightline issue for Kaiwharawhara Road pedestrian crossing.

Hi Dennis,

We are grappling with a difficult balancing act between safety and carparks and hoped you might be able to offer some advice.

When we went out for engagement we told businesses on Kaiwharawhara Road that we would not be removing carparks on the downhill direction of the road (which is a clearway from 7-9am).

Then we decided it would be good to install a raised pedestrian crossing on Kaiwharawhara Road.

This has meant we have had to remove 8.6m of carparking space, as show in the final 90% plans – see yellow highlighted area below.



Ideally, for safety, we would remove another two carparks to improve sightlines for pedestrians crossing the road. But if we do that, we will lose support from the two businesses affected. It would be great to get your advice. How critical is it to remove another two carparks between the highlighted area and Cameron St? Is there anything else we could/should do to mitigate the risks of crossing the road, or are we already making enough safety gains?

Regards,
Jonathan

Jonathan Kennett
Project Lead – Transitional Cycleways | Wellington City Council
E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |

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From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: Street mix images
Date: Monday, 17 October 2022 1:57:56 pm
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.jpg](#)
[Cameron Street cycle shoulder.pdf](#)

Hi Jonathan

How does this look?

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Monday, 17 October 2022 1:27 pm

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: RE: Street mix images

Hi again Billy,

Could you create a new streetmix for Cameron St, showing a 1m wide 'cycle shoulder'?

What we have at the moment doesn't reflect the design.

Cheers

Jonathan

From: Billy Rodenburg <BRodenburg@tonkintaylor.co.nz>

Sent: Monday, October 17, 2022 1:12 PM

To: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Cc: Oli du Bern <Oli.duBern@wcc.govt.nz>

Subject: RE: Street mix images

Hi Jonathan,

Attached is the non-optimised versions of the drawings which have a higher resolution. Is this suitable?

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Monday, 17 October 2022 10:31 am

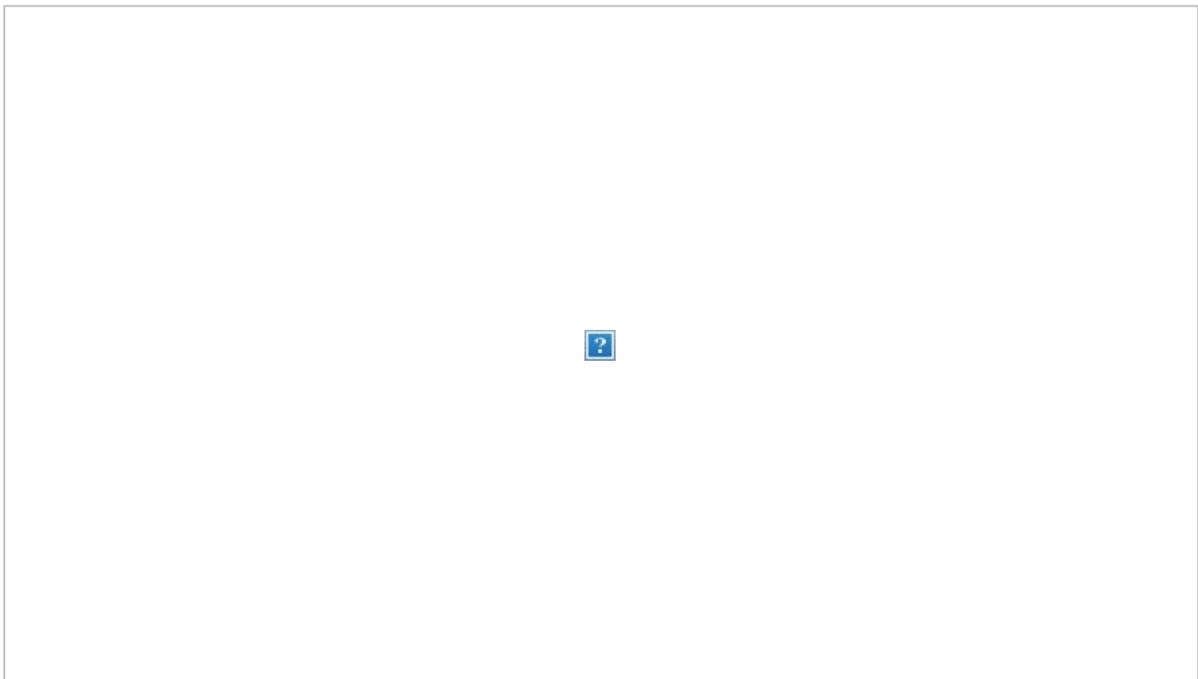
To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Cc: Oli du Bern <Oli.duBern@wcc.govt.nz>

Subject: Street mix images

Hi Billy,

Do you have higher resolution images for each section along Ngaio:



This is from your optimised concept design, but we need something sharper to go on the website for consultation.

Just one streetmix for each section please.

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |  

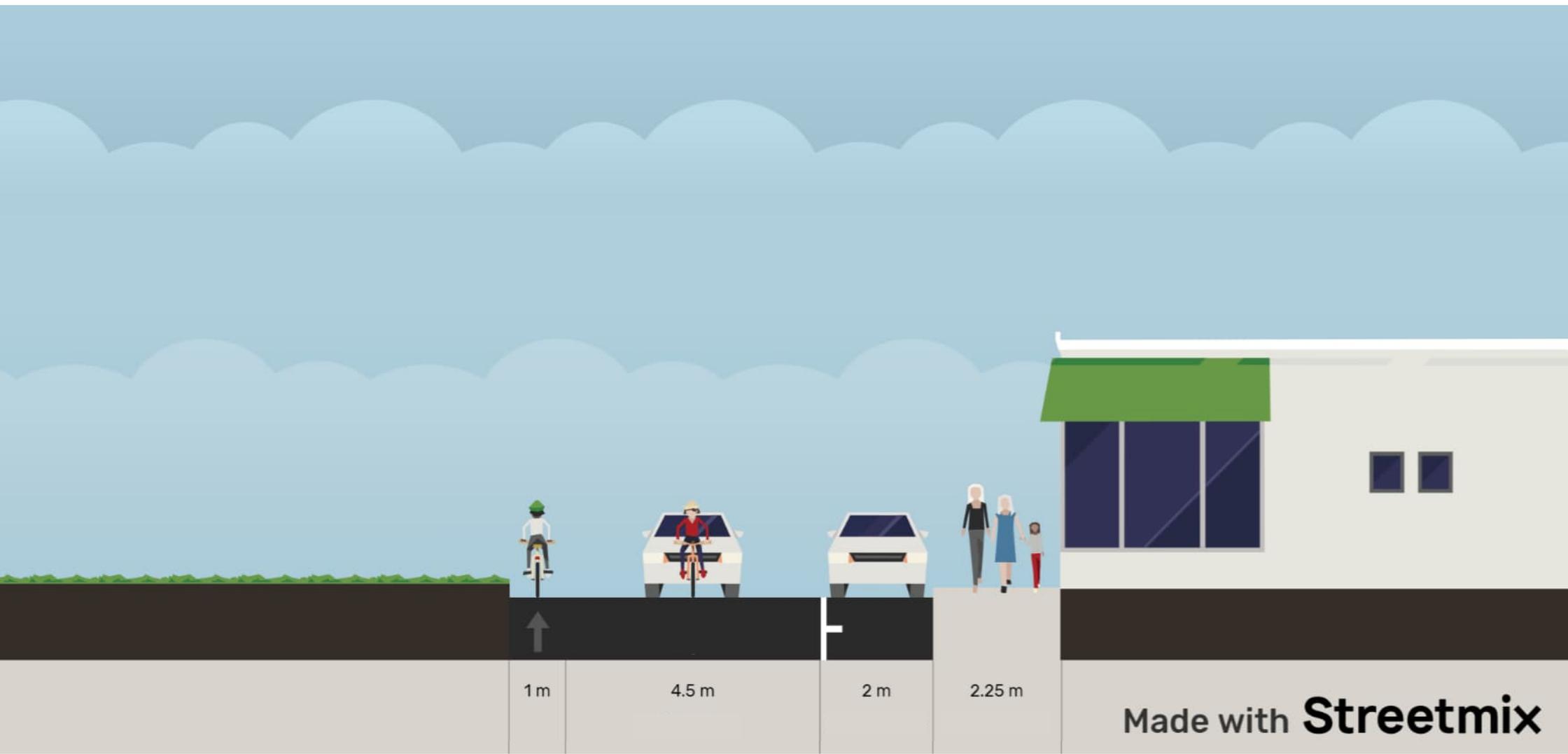
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Image removed by sender.





From: [Billy Rodenburg](#)
To: [Jonathan Kennett](#)
Subject: RE: Submission requiring your response
Date: Tuesday, 29 November 2022 1:44:44 pm
Attachments: [image001.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)
[image007.png](#)
[image008.png](#)

Hi Jonathan,

The constrained road width is a big one, there isn't space to provide a roundabout with suitable deflection without significant changes and possible property purchase outside the scope of the transitional cycleway. I also agree with your comment about roundabouts being less desirable for cyclists.

The suggestion about a hook turn for cyclists could be marked but would not be enforceable as cyclists are legally permitted to ride in a the traffic lane and we encourage them to do this down Kenya Street. This would likely lead to most cyclists taking the most convenient and direct route. It also introduces additional conflict between vehicles turning left into Trellisick Crescent being unsure if cyclists are crossing or not.

Thanks,

Billy

Billy Rodenburg | Civil & Transport Engineer

BE (Hons), CPEng, CMEngNZ

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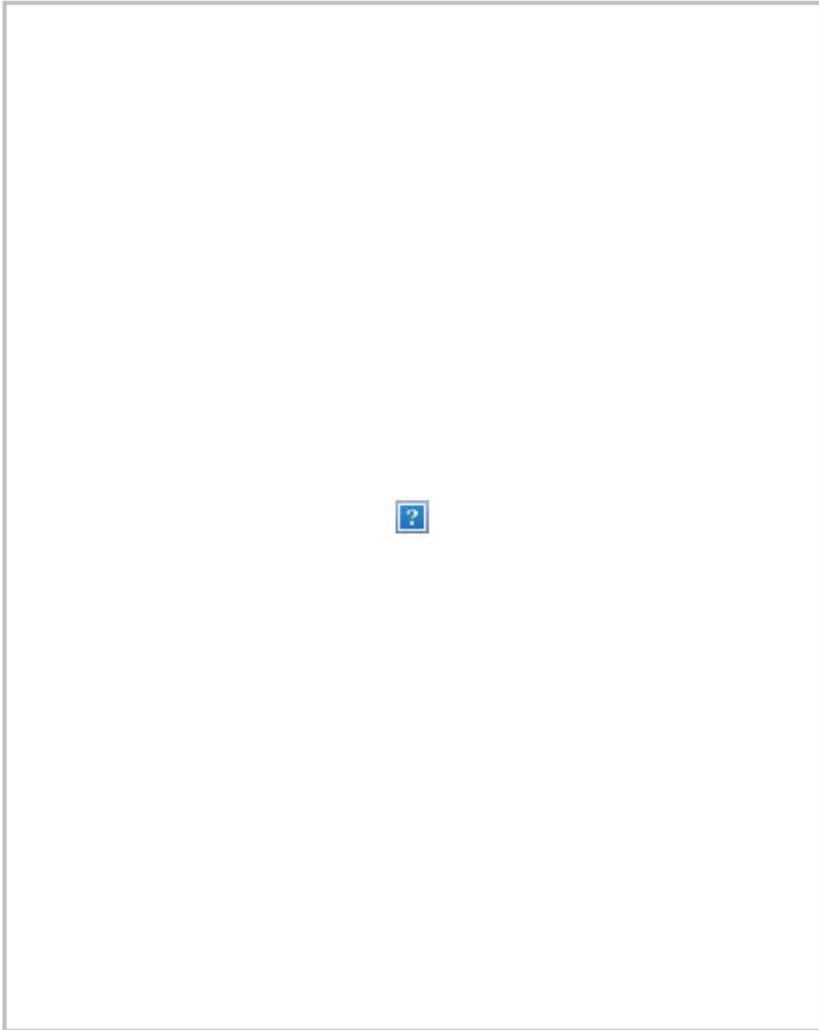
From: Jonathan Kennett <Jonathan.Kennett@wcc.govt.nz>

Sent: Tuesday, 29 November 2022 10:14 am

To: Billy Rodenburg <brodensburg@tonkintaylor.co.nz>

Subject: RE: Submission requiring your response

Here is a snip of the more detailed submission:



From: Jonathan Kennett

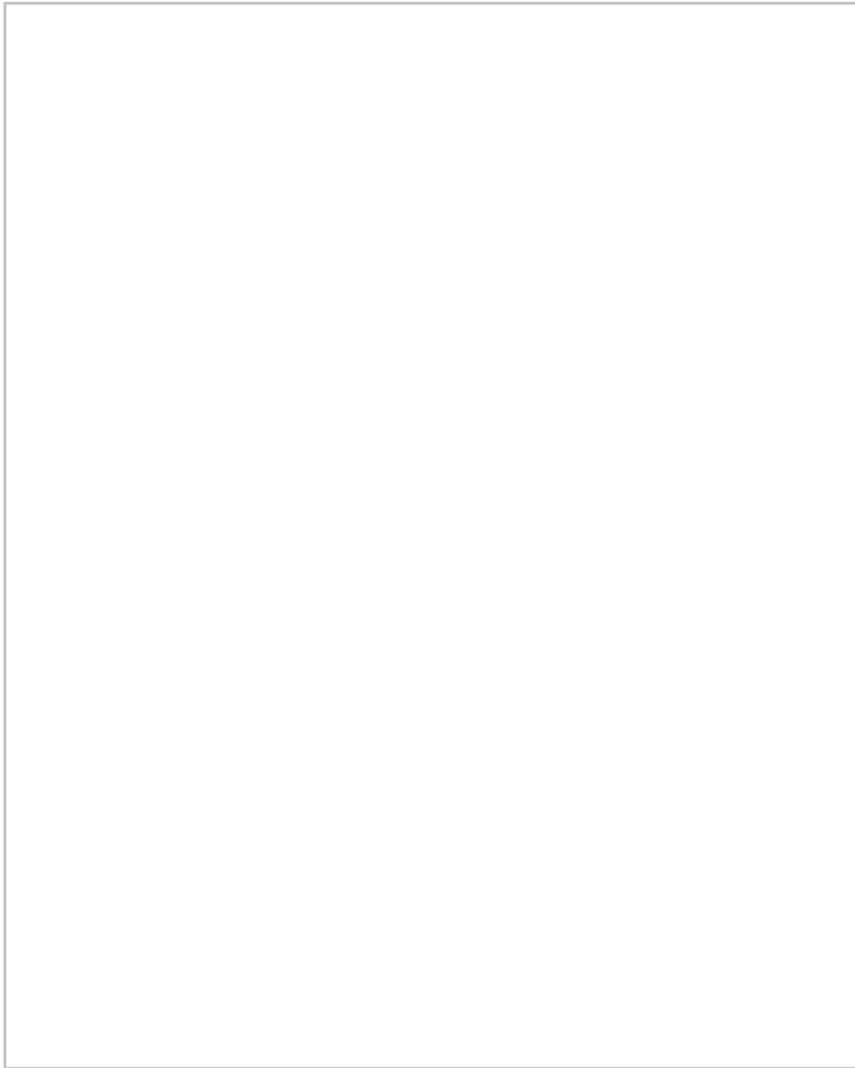
Sent: Tuesday, 29 November 2022 9:55 am

To: Billy Rodenburg <brodenburg@tonkintaylor.co.nz>

Subject: Submission requiring your response

Hi Billy,

In the 30% audit the idea of a roundabout at Kenya/Trelissick was raised. See below.



In submissions this idea has been raised again.

My response points out that roundabouts improve traffic efficiency, but are more dangerous for cyclists. Also that in this constrained road width there is not room for a roundabout and cycle lanes.

Do you agree, and is there anything else that should be added?

Cheers

Jonathan

Jonathan Kennett

Project Lead – Transitional Cycleways | Wellington City Council

E Jonathan.Kennett@wcc.govt.nz | W Wellington.govt.nz |

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